

RULE 10-1

Oral authorization and acknowledgments between foremen and engineers for trains to pass red **CONDITIONAL STOP** signs must be worded in the following forms:

"SP FOREMAN AT MP CALLING SP (Train No.)"

(After engineer answers giving his train identification):

Foreman's Response

"THIS IS SP FOREMAN ... IN CHARGE OF THE WORK BETWEEN MP ... AND MP ... SP TRAIN ORDER NO. WE ARE IN THE CLEAR AND YOU MAY PROCEED PAST THE RED **CONDITIONAL STOP** SIGN AND THROUGH THE LIMITS OF ORDER AT MPH, REPEAT MPH"*

Engineer's Response

"THIS IS ENGINEER SP TRAIN I MAY PROCEED PAST THE RED **CONDITIONAL STOP** SIGN AND THROUGH THE LIMITS OF ORDER NO. BETWEEN MP ... AND MP ... AT (Speed). REPEAT (Speed) MILES PER HOUR."

Foreman must acknowledge engineer's response as follows: "SP TRAIN ORDER NO., BETWEEN MP AND MP ... MPH* OK."

*When no speed restriction account above Form Y Train Order, tell train engineer "At Maximum Authorized Speed."

Oral authorization and acknowledgments between foremen and engineers for trains to pass red **CONDITIONAL STOP** signs in multiple main track territory must be worded in following forms:

Foreman's Response

"THIS IS SP FOREMAN IN CHARGE OF THE WORK BETWEEN MP AND MP SP TRAIN ORDER NO. ... WE ARE IN THE CLEAR OF TRACK ... AND YOU MAY PROCEED PAST THE RED **CONDITIONAL STOP** SIGN ON TRACK ... AND THROUGH THE LIMITS OF ORDER AT MPH, REPEAT MPH."

Engineer's Response

"THIS IS ENGINEER SP TRAIN I MAY PROCEED PAST THE RED **CONDITIONAL STOP** SIGN AND THROUGH THE LIMITS OF ORDER NO. ... ON TRACK BETWEEN MP AND MP AT (Speed), REPEAT (Speed) MILES PER HOUR."

Foreman must acknowledge engineer's response as follows: "SP TRAIN ORDER NO. ON TRACK, BETWEEN MP AND MP MPH OK."

Southern Pacific Transportation Company



LAFAYETTE DIVISION TIMETABLE

132

EFFECTIVE SUNDAY, OCTOBER 25, 1981

AT 12:01 A.M.

CENTRAL STANDARD TIME

R. D. KREBS,
Vice President-Operations.

W. J. LACY,
General Manager.

R. D. BREDENBERG,
Assistant General Manager.

L. G. SIMPSON,
*Assistant Vice President-
Operations Planning and Control.*

J. J. WILLIS,
Assistant Vice President-Transportation.

H. D. FISHER,
Superintendent.

W. L. MARTIN,
J. W. BOUDREAUX,
D. J. DOYLE,
Assistant Superintendents.

TERMINAL SUPERINTENDENTS

F. L. CLIFTON Beaumont
 C. F. HALEY Baytown
 J. E. GROTHER New Orleans

TRAINMASTERS

W. F. LANFORD Baytown
 C. W. MARLEY Beaumont
 R. D. STRANG Beaumont
 J. W. REED Beaumont
 T. J. EARLE Lake Charles
 I. M. COMMER Lake Charles
 R. DUPLECHAIN Lafayette
 N. J. LeGLUE New Orleans
 R. L. PODAWILTZ New Orleans

ROAD FOREMEN OF ENGINES

G. W. STOKES, Jr. Beaumont
 R. P. GUIDRY Lafayette

ASSISTANT TERMINAL SUPERINTENDENT

H. L. McDOWELL New Orleans

ASSISTANT TRAINMASTERS

R. M. WARFIELD Lake Charles
 G. R. GRACIN Baytown
 D. F. DAWSON Beaumont
 T. J. PRICE Lafayette
 W. P. LAMAR, SR. Morgan City
 W. M. McSPARRAN New Orleans
 P. A. LOBELLO New Orleans
 C. R. TROXELL New Orleans

GENERAL YARDMASTERS

C. J. CORDILLA New Orleans

AMTRAK OFFICERS

L. J. HARTZOG, Trainmaster Houston
 R. B. LUTTON, Assistant Trainmaster San Antonio
 T. R. MALISH, Assistant Trainmaster San Antonio

CHIEF TRAIN DISPATCHERS

R. W. REGAN Lafayette
 J. J. MORRISEY Lafayette

MANAGER OF DISPATCHING OPERATIONS

E. L. HORD Houston

ASSISTANT MANAGER OF DISPATCHING OPERATIONS

R. W. TABB Lafayette
 B. L. BALDWIN Houston
 F. J. SIEMS Houston

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SPEED TABLE

Time Per Mile		Miles Per Hour	Time Per Mile		Miles Per Hour	Time Per Mile		Miles Per Hour
Mins.	Sec.	Hour	Mins.	Sec.	Hour	Mins.	Sec.	Hour
—	45	80.0	1	08	52.9	1	46	34.0
—	46	78.3	1	10	51.4	1	48	33.3
—	47	76.6	1	12	50.0	1	50	32.7
—	48	75.0	1	14	48.6	1	52	32.1
—	49	73.5	1	16	47.4	1	54	31.6
—	50	72.0	1	18	46.1	1	56	31.0
—	51	70.6	1	20	45.0	1	58	30.5
—	52	69.2	1	22	43.9	1	—	30.0
—	53	67.9	1	24	42.9	2	—	30.0
—	54	66.6	1	26	41.9	2	10	27.7
—	55	65.5	1	28	40.9	2	15	26.7
—	56	64.2	1	30	40.0	2	24	25.0
—	57	63.2	1	32	39.1	2	30	24.0
—	58	62.6	1	34	38.3	2	45	21.8
—	59	61.0	1	36	37.5	3	—	20.0
1	—	60.0	1	38	36.8	3	30	17.1
1	02	58.0	1	40	36.0	4	—	15.0
1	04	56.2	1	42	35.3	5	—	12.0
1	06	54.2	1	44	34.6	6	—	10.0

LAFAYETTE SUBDIVISION

EAST- WARD FIRST CLASS	STATIONS		STATION NUMBER	WEST- WARD
				FIRST CLASS
2 Psg	Lafayette Line			1 Psg
Lv. Sun. Tue., Thur.	Mile Post			Ar. Mon. Wed., Fri.
AM 10.40	1.2	A-PB HOUSTON BKPO	76100	PM s10.40
	357.4	ENGLEWOOD BKIYPQ	76103	
	10.55	TO-R TOWER 87 IPO	79007	10.05
	11.04	10735 FAUNA I	79014	9.55
	11.09	2504 Yd Lmts SHELDON PQ	79018	9.47
	11.15	2488 CROSBY	79024	9.40
	11.28	13130 TO-R DAYTON BKIPO	79039	9.26
	11.34	3481 LIBERTY	79207	9.18
	11.38	2545 AMES	79211	9.12
	11.42	3123 RAYWOOD	79216	9.06
	11.46	11643 TO DEVERS PQ	79221	9.00
AM 11.56	297.9	2248 NOME	79232	8.48
PM 12.01	293.0	11500 TO CHINA KP CTC	79239	8.40
s12.35	280.2	11800 TO-R BEAUMONT BKIYPQ	79250	s8.25
	277.0	TOWER 31 IP	79505	
	12.54	10800 TO CONNELL KIPQ	79507	7.51
	1.03	10175 FRANCIS	79521	7.36
	1.07	TO ORANGE SIDING KBP	79523	7.33
	1.09	ORANGE	79530	7.30
	1.19	7589 Yd Lmts TO-R ECHO BKPO	90000	7.23
	1.29	2207 VINTON	90021	7.12
	1.34	2869 EDGERLY	90027	7.06
	1.41	10500 BRIMSTONE	90034	7.01
	1.51	LOCKMOOR I	90045	6.50
s2 10	218.8	LAKE CHARLES IP	90200	s6.45
	2 13	11400 TO-R LAKE CHARLES YARD BKIYPQ	90250	6.33
	215.3	MALLARD JCT.	90410	
	2.25	3501 IOWA	90611	6.22
	2.39	1237 WELSH P	90624	6.10
	2.44	9947 ROANOKE	90631	6.06
	2.50	3055 Yd Lmts TO JENNINGS PQ	90637	6.00
	2.54	2284 MERMENEAU P	90642	5.55
	3.02	4420 Yd Lmts MIDLAND Y	91000	5.50
	3.11	3227 TO CROWLEY IP	91320	5.41
	3.15	10690 CROWLEY SIDING	91340	5.39
	3.22	2277 RAYNE	91345	5.34
	3.26	2567 DUSON	91351	5.29
	3.32	2656 SCOTT	91358	5.24
3.37 PM	147.1	Yd Lmts TO-R LAFAYETTE YARD BKIYPQ	91362	5.21 PM
Ar. Sun. Tue., Thur.		(215.9)		Lv. Mon. Wed., Fri.
2				1

LAFAYETTE SUBDIVISION

MAXIMUM AUTHORIZED SPEED FOR TRAINS

BETWEEN	PSGR	FRT	
HOUSTON and LAFAYETTE	70	55	
Exceptions:	PSGR FRT	Exceptions: PSGR FRT	
1.10 and 360.70	15 15	220.62 and 219.70	40 40
360.70 and 352.70	30 20	219.70* and 217.80*	20 20
345.00 and 342.20	50 40	217.80* and 217.75*	10 10
327.60* and 326.20*	30 30	217.75* and 217.50*	20 20
326.20 and 322.30	60 40	207.70* and 205.25*	45 45
322.30* and 319.90*	30 30	205.25 and 205.20	50 50
285.10* and 276.90	20 20	196.00* and 193.50*	25 25
276.90 and 275.60	50 40	185.50 and 183.90	25 25
259.43 and 253.30	30 25	183.90 and 180.36	— 40
253.30 and 249.95	35 35	180.36 and 180.26	35 35
249.95 and 247.50	50 40	180.26 and 167.60	— 40
242.05* and 240.20*	25 25	167.60 and 165.80	30 30
229.44* and 226.70*	25 25	160.80 and 159.30	30 30
222.85 and 220.73	40 40	148.60 and 143.00	25 25
220.73 and 220.62	25 25		

"RULE 10-J. Speed may be increased as soon as lead locomotive has passed increase speed sign at these locations. "K" trains must not exceed 30 MPH between MP 205.07 and 207.7.

The following establishes the maximum allowable speeds for freight trains provided speed is not otherwise restricted: (e.g., Restricted cars or engines, AB Rule 33, etc.)

a. AVLAT and LAAVT are authorized to operate at Freight Train Speed.

b. Light engine with operative dynamic brake is authorized to operate at passenger train speed.

Exception: without dynamic brake in operation, must operate at freight train speed.

c. Other freight trains may be authorized by train dispatcher to operate at Freight Train Speed.

d. Trains not covered in items a, b, or c will operate at freight train speed not to exceed 45 MPH, except as provided in A.B. Rule 65.

MAXIMUM HORSEPOWER PER TON RATIOS

AVLAT, LAAVT	2.0
All trains — Rockland Branch	2.0
All other trains	1.3

(Refer to A.B. Rule 65)

SPEED ON OTHER THAN MAIN TRACK:

No. 1 Track, Lafayette Yard, from east switch to Sunbeam Lane	25
Harbor, Lake Charles Harbor Spur	25
Sidings Devers, China, Francis	25
U. S. Steel Lead (Baytown Branch)	20
Iowa Jct. MP Interchange	7
Locomotive Maintenance Facility Tracks	
Beaumont, Inside Track No. 338 and Outside Track No. 339; Lake Charles Yard, Track No. 7 (No. 60) and No. 8 (No. 602); Lafayette Yard, Engine Holding Track (No. 601); Middle Track (No. 602) and Sandhouse Track (No. 603)	5
Orange siding, new lead in field track	5
Lake Charles yard No. 1 track over M.P. Crossing	5
Jennings, house track	5
Lafayette Yard rip tracks, cabooses track and stock pen	5
East and west leg of wye track Prosser	5
Track scale Gulf Spencer Plant	5
All other tracks, Lafayette Subdivision	10

25 MPH through turnout between No. 2 Main Track and Single Track, Tower 87.

Lake Charles: Trains and engines must approach Kirkman Street (third crossing east of station) prepared to stop short of vehicles that may be stopped on track account traffic light.

10 MPH entering Shattuck Street crossing until engine or cars have covered crossing; 20 MPH until cabooses of train covers crossing.

LAFAYETTE SUBDIVISION

EAST-WARD		STATIONS			WEST-WARD	
THIRD CLASS					THIRD CLASS	
102 A. & N.R. Freight					101 A. & N.R. Freight	
Leave Daily	Mile Post	Rockland Branch			Station Number	Arrive Daily
AM 8.45	118.2	Yd. Limits ABS	TO-R	LUFKIN ¹⁹ BKYPQ	78200	AM 11.20
8.55	120.1			PROSSER ²⁵ IP	78404	11.10
9.20	5.0	Yd Lmts		HERTY ³⁵	79490	10.55
9.35 AM	132.1	Yd Lmts R		DUNAGAN ^{47.5}	79487	9.45 AM
	84.6				WOODVILLE ^{27.26}	79461
	57.34	Yd. Lmts R	8350	PINE ^{2.74}		
	54.6					KOUNTZE ^{14.5}	79430
	40.1	Yard Limits		LOEB JCT.	P 79412	

SEE SPECIAL INSTRUCTIONS, RULE 812, FOR TRAIN MOVEMENTS BETWEEN SANTA FE JCT. AND LOEB JCT.

	30.5	Yd. Limits	TO-R	SANTA FE JCT. ¹³	79405	
	280.2				BEAUMONT ¹¹⁸⁰⁰ BKIYPO	79250
Arrive Daily				(110.7)		Leave Daily
102						101

RULE 5. Time at Loeb Jct. applies at junction with A.T.&S.F. Ry. Co.

EAST-WARD		STATIONS			WEST-WARD	
Mile Post		Sabine Branch			Station Number	
280.2	11800 TO-R Yard Limits			BEAUMONT ¹⁸⁵ BKIYPO	79250	
12.7	Yard Limits			WEST PORT ARTHUR	Y 79360	
				(18.5)		

Baytown Branch

22.2	Yard Limits	TO-R	BAYTOWN ^{6.2} BKPO	79130
16.0		R	ELDON ^{3.0}	79118
13.0			MONT BELVIEU ^{13.0}	79115
0.0		13130 TO-R	DAYTON	BKIPO 79039
			(22.2)	

Lake Arthur Branch

217.2	11400 TO-R Yard Limits	LAKE CHARLES YARD ^{35.3} BKIPO	90250
33.4		LAKE ARTHUR	90545
		(35.3)	

LAFAYETTE SUBDIVISION

MAXIMUM AUTHORIZED SPEED FOR TRAINS

BETWEEN	ROCKLAND BRANCH	ALL TRAINS
PROSSER AND LOEB JCT.		30
Exceptions:	ALL TRAINS	Exceptions: ALL TRAINS
Prosser and 132.10	20	92.0 and 89.6
104.25 and 103.90	10	53.60 and 53.30

SABINE BRANCH

BEAUMONT and WEST PORT ARTHUR	25
Exceptions:	ALL TRAINS
30.17 and 25.70	15
Exceptions:	ALL TRAINS
	21.50 and 12.70

BAYTOWN BRANCH

BAYTOWN and DAYTON	20
Exceptions:	ALL TRAINS
16.20 and 12.75	10
Exceptions:	ALL TRAINS
7.50	10
Exceptions:	ALL TRAINS
	6.00 and 5.74
	0.30 and 0.00

LAKE ARTHUR BRANCH

MALLARD JCT. and LAKE ARTHUR	10
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ADDITIONAL STATIONS

Mile Post	Station	Station Number	Mile Post	Station	Station Number
Lafayette Line					
353.2	Dawes	70910	228.4	Sulphur	90037
331.9	Audrey	79031	220.9	West Lake	90050
303.3	Felicia	79226	213.0	Chloe	90605
284.0	Amelia	79245	205.5	Iowa Jct.	(M.P. Conn.)
276.4	Korf	79506	201.4	Lacassine	90617
265.5	Bobsher	79514	172.6	Estherwood	91305
263.0	Tulane	79517	171.9	Tortue	91310
231.5	Stegall	90032	Baytown Branch		
			5.23	East Baytown	79119

NOTE: East Baytown is station on Cedar Point Industrial Spur.

Sabine Branch					
25.5	Guffey	79305	16.0	Port Acres	79351
25.5	Chaison	79320	14.0	Williams	79354
21.3	Viterbo	79347	3.1	Port Arthur	79380

NOTE: Chaison is on spur 3.3 miles from Guffey.
Port Arthur is on spur 3.1 miles from West Port Arthur.

Rockland Branch					
129.3	Buck Creek	79484	64.8	Village Mills	79450
126.9	Huntington	79482	Lake Arthur Branch		
114.3	Zavalla	79478	215.3	Mallard Jct.	90410
109.2	Dolan	79475	4.1	Harbor	90510
94.1	Colmesneil	79466	9.5	Holmwood	90515
87.6	Doucette	79463	16.1	Bell City	90525
76.9	Hillister	79457	18.7	Hayes	90528
72.7	Warren	79454	22.7	Niblett	90533
			26.4	Thornwell	90538

SPECIAL INSTRUCTIONS

For movements within terminal limits Houston, also see Special Instructions, Houston Terminals, Houston Division.

Trains of the Rockland Branch will be governed by special instructions of Houston Division, Shreveport Subdivision as to movements between Lufkin and Prosser.

RULE P. Impaired Side Clearance:

MP	Description	MP	Description
343.77	Bridge	180.2	Drawbridge
321.98	Bridge	163.1	Overpass
KCS 766.4	Hotbox detector	146.0	Bridge
KCS 765.1	Hotbox detector	Rockland Branch	
250.00	Bridge	103.92	Bridge
220.6	Drawbridge	Lake Arthur Branch	
205.5	Overpass	3.1	Gate Mast
186.2	Overpass	19.8	Drawbridge

RULE 5. Siding Beaumont is first track south of main track No. 2.

Siding Lake Charles Yard is first track south of main track.

Siding Crowley is first track south of main track.

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RULE 10-H. Exceptions: On the Lake Arthur, Baytown and Sabine Branches, when a yellow flag is required it will be displayed one-half mile from point of restriction.

RULE 15. Exceptions: On the Lake Arthur, Baytown and Sabine Branches, the explosion of a torpedo requires movement at restricted speed for one mile from point where torpedo was exploded.

RULE 20 and 21. Rockland Branch trains will display signals between Santa Fe Jct. and Loeb Jct. according to designation on Rockland Branch.

S.P. trains between Beaumont and Tower 31 will display classification lights authorized on S.P.

RULE 26-B. William, MP 14.0 Sabine Branch: Special light type signal installed on loading shed and new platform at Arco Polymers Inc., Tracks 1064, 1065 and 1066. Display of red aspect indicates loading platforms are in lowered position and cars must not be coupled into nor moved while light illuminated. When loading platforms are in raised position, light is extinguished; however, before coupling into cars inspection must be made to insure loading equipment is clear.

Special light type signal installed at tank loading racks, Tracks 1061, 1063 and 1067. Display of red aspect indicates tank cars connected and cars must not be coupled into or moved while light is illuminated.

Lockmoor: Signal lights located above track at entrance to City Services Coke loading spur displays either flashing red or green. When signal displays flashing red track must not be entered. Track may be entered when signal displays green.

RULE 31. Lake Charles: City ordinance prohibits sounding of engine whistle except where there is imminent danger of an accident. In observing this ordinance, engineer should sound whistle if in his judgment an accident may be prevented.

RULES-71. Between Prosser and Lufkin there is no superiority of trains on main track and between these points trains must move at restricted speed.

RULE 82-A. Eastward first-class trains originating at Houston may assume the schedule, as instructed by train dispatcher or yardmaster, without clearance, but must obtain clearance bearing OK, Time and Initials of Chief Train Dispatcher before leaving Tower 87.

Eastward first-class trains and extra passenger trains operating through Lafayette Yard must obtain clearance and train orders at Lafayette Yard; such clearance and train orders to be delivered to the relieving conductor and engineer at Lafayette, as prescribed by Rule 220.

Rockland Branch extra trains originating at Prosser must obtain clearance and train orders from train-order office at Lufkin.

Crew arriving Dunagan on No. 102 may assume the schedule of No. 101 and leave without a clearance.

RULE 83. Eastward trains may identify westward trains between Houston and Tower 87 to be applied at end of two main tracks.

Inferior trains may identify superior trains on two main tracks between Langham Road and K.C.S. MP C-766 Beaumont, to be applied at end of two main tracks and at Connell.

RULE 83-A. At following stations, only trains indicated will register:

Hardy Street Trains originating or terminating.
 Dayton Trains originating or terminating.
 Eldon Trains directed by train order.
 Beaumont Trains originating or terminating.
 Woodville Trains directed by train order.
 Kountze Trains directed by train order.

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Lake Charles Yard ... Trains originating or terminating.
 Iowa Jct. Trains directed by train order.
 Lufkin Trains originating or terminating.
 Conductors of trains originating or terminating at Prosser will register at Lufkin and indicate arrival or departure time which applies at Prosser, by showing "Prosser" in parenthesis above time.

Dunagan: Train register will be checked by engineer when Conductor is not present at head end of train.

RULE 83-B. At open train-order offices, trains may register by ticket as follows:

Tower 87 All trains.
 Dayton Trains originating or terminating.
 Echo First-class trains and extra passenger trains.
 Lafayette Yard First-class trains and extra passenger trains.

Trains originating or terminating Beaumont will register by ticket leaving with yard clerk, who will deliver to train-order operator via pneumatic tube; except eastward trains from Rockland Branch and westward extra trains terminating will throw off register ticket at train-order office.

Unless directed by train order to do so, extra trains originating or terminating at Dunagan need not register. Conductor will fill out register ticket and deliver to train-order operator at Lufkin.

RULE 93. Location of yard limits:

351.00	Houston (Lafayette Line)	352.70
	Sheldon	342.50
330.10	Dayton-Liberty-Ames (Lafayette Line) ...	317.00
22.20	Dayton-Baytown (Baytown Branch)	
286.75	Beaumont (Lafayette Line)	275.62
	Beaumont (Sabine Branch)	23.44
45.50	Beaumont (Rockland Branch)	
59.50	Pine-Kountze	53.00
266.00	Orange-Francis	254.64
253.36	Echo	249.84
	Baytown-Eldon	12.00
117.16	Lufkin (Houston Division)	120.84
	Lufkin (Rockland Branch)	2.73
4.05	Herty	6.80
7.87	Dunagan	130.00
85.50	Woodville	84.00
13.29	Port Arthur-West Port Arthur	
225.60	Lake Charles Yard (Lafayette Line)	214.54
	Lake Charles Yard (Lake Arthur Branch)	4.75
187.04	Jennings	183.41
176.41	Midland (Lafayette Line)	171.19
168.67	Crowley	163.57
148.69	Lafayette Yard	142.99

Beaumont: Between Santa Fe Jct. and Beaumont, and between South Street and Crockett Street, Beaumont, there is no main track. Between these points all tracks are yard tracks.

RULE 98. Railroad crossings at grade not interlocked

NORMAL POSITION OF GATES AT CROSSING

Location	Normal Position
Rockland Branch	
Kountze MP 54.6	A.T.&S.F.

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Location	Normal Position
Lake Arthur Branch	
MP 3.1, east of Mallard Jct.	S.P.
MP 3.7, east of Mallard Jct.	M.P.

Drawbridges Not Interlocked

Bayou Lacassine, MP 19.89, Lake Arthur Branch.

RULE 99. Exceptions: On the Baytown and Sabine Branches when protection by flagman is required by this rule, distances specified for placement of torpedoes and flag protection will be one-half and one mile from train being protected.

RULE 99-C. Will apply on Sabine and Rockland Branches.

RULE 103. For train, engine and switching movements over following crossings, a member of crew must take position at crossing to afford warning:

Dawes	Houston Lighting & Power Spur	U.S. Highway 90
Liberty	Sand Pit Spur	U.S. Highway 90
Baytown	Main	Pine
Baytown	Main	Carnegie
Baytown	Main	Martin King
Baytown	Main and Pruitt Team	Pruitt
Baytown	Main	Gulf
Baytown	Main	Pearce
Baytown	Main	Texas
Baytown	Main and House	Defee
Baytown	Main	Sterling
Baytown	Main	James
Baytown	Main	Fayle
Baytown	Main	Dyer
Baytown	Main	Hafer Street
Orange	Chemical Row Lead	Western Ave.
Port Arthur	Old Main Track	Thomas Boulevard
Port Arthur	Old Main Track	Sixteenth Street
Port Arthur	K.C.S. Transfer	Tx. Highway 87
Sulphur	Old Siding over Huntington Street crossing	
West Lake	Spur leading to Mathieson Chemical Plant over old U. S. Highway 90	
Jennings	Foster Spur, U. S. Highway 90	
Crowley	Parkerson Ave. (Ice House Track)	
Port Arthur	Texaco Refinery	West Gate Crossing

Orange: In making switching moves to the DuPont Plant engine or cars should be stopped clear of road crossing near entrance of the plant before proceeding.

Listed below are locations and tracks where movements do not actuate automatic crossing warning signals. When movements are made over these crossings on tracks listed, member of crew must take position at crossing to afford warning to vehicular traffic:

Lafayette Line

Dayton	Main Street	House Track
Liberty	San Jacinto Street	North Industrial Track
Liberty	Travis Street	House Track
Liberty	Main Street	House Track
Liberty	Bowie Street	Siding
Devers	Road Crossing	House Track
Duson	La. 95	Siding and House Track

Rockland Branch

Colmesneil	Road Crossing	Spur
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Eldon: Dwarf-type signal installed south side of Rice Farm Road within U. S. Steel Plant. Signal is inter-connected with crossing protection device, and will display red aspect. When

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flasher light signals protecting vehicular traffic begin functioning, signal will display green aspect.

When signal displays red aspect, member of crew must take position at crossing to afford warning to traffic while movement is being made.

Lake Arthur: Main track ends at MP 33.34.

RULE 208. Fourth paragraph does not apply at Echo, or to westward trains at Dayton, China and Beaumont, or to eastward trains at Lake Charles Yard. When train-order signal remains in stop position and has not been operated as prescribed by Rule 211, train may proceed without stopping, but must not pass fouling point of switch at which an opposing train may enter siding until it is known train orders received do not restrict train at that station.

RULE 211. Roanoke-Jennings: When Form "N" train order is held by operator, eastward trains restricted at Roanoke upon receiving authority from operator Jennings under Rule 211 (c) may proceed on main track to train-order office to receive orders.

Crowley Siding-Crowley: When Form "N" train order is held by operator, westward trains restricted at Crowley Siding upon receiving authority from operator Crowley under Rule 211, (b) or (c), may proceed on main track to train-order office to receive orders.

RULE 221. Following are train-order offices only as indicated:

Tower 87	Eastward trains
Orange Siding	Trains originating
Joint S.P. and M.P. Light type train-order signal located adjacent to Passenger Station, Beaumont.	
Top unit governs S.P. trains.	
Lower unit governs M.P. trains only.	

Unit for display of flashing white light installed at the following locations:

Station	Location	Direction
Sheldon	Signal 3451	Eastward
Devers	Signal 3060	Westward
Lake Charles Yard	On Train Order Signal	Westward
Jennings	Signal 1844	Westward

RULE S-240. Applies at following locations:

Territory	Register location
MP 4.75 and Lake Arthur (Lake Arthur Branch)	Lake Charles Yard

RULE 306. Block signals with "P" plates:

Signal	Protection	Signal
P-3510	Spring switches, Fauna	P-3487
P-3292	Spring switch, west end siding, Dayton	
P-3084	Spring switches, Devers	P-3065
	Spring switch, east end yard, Lufkin	P-1203
P-2948	Spring switches, China (West switch, China)	P-SA
P-2614	Spring switches, Francis	P-1
P-2524	Spring switches, Echo	P-2507
P-2320	Spring switches, Brimstone	P-2299
	Spring switch, east end siding, Lake Charles Yard	P-2155
P-1924	Spring switches, Roanoke	P-1905
P-1756	Spring switch, west end siding, Midland	
P-1660	Spring switches, Crowley Siding	P-1639
P-1482	Spring switch, west end yard, Lafayette Yard	

RULE 508. When SA signals governing M.P. crossing, MP 259.3, display red aspect, after complying with Rule 663, movement may proceed beyond interlocking limits in accordance with Rule 507 instead of Rule 508.

LAFAYETTE SUBDIVISION

When any of the following listed automatic block signals display red aspect, Rule 507 instead of Rule 508 will apply:

Crowley Yard Limits	Midland Yard Limits
1639, 1660	1713, 1722
1670	1735, 1734
Jennings Yard Limits	1745, 1756
1845, 1856	Lake Charles Yard Limits
1869, 1870	2155, 2180
Orange Yard Limits	2165, 2190
2559, 2572	2199
2567, 2584	
2573, 2614	
2583, 2636	
2635	

RULE 516. Overlap posts

Brimstone	MP 230.70	governing eastward trains
West Lake	MP 220.90	governing westward trains
Lake Charles Yard	MP 216.01	governing eastward trains
Chloe	MP 212.27	governing westward trains

RULE 538. Spring switches equipped with facing point locks

Location		Normal Position
Fauna	West and east end siding	Main Track
Dayton	West end siding	Main Track
Devers	West and east end siding	Main Track
China	West and east end siding	Main Track
Francis	West and east end siding	Main Track
Brimstone	West and east end siding	Main Track
Lake Charles Yard	East end siding	Main Track
Roanoke	West and east end siding	Main Track
Midland	West end siding	Main Track
Crowley Siding	West and east end siding	Main Track
Lafayette Yard	West end yard	Main Track

Spring switches not equipped with facing point locks:

Location		Normal Position
Echo	West and east end siding	Main Track
Lufkin	East end yard	Main Track

RULE 606. Tower 87—Fauna: Interlocking limits on main track extend from eastward interlocking signals at fouling point, end of two main tracks west of Tower 87 MP 355.2 to westward interlocking signals at fouling point west end of siding Fauna MP 350.1. Interlocking limits on other than main track are indicated by interlocking signals.

Dayton: East switch of siding is dual control switch; switch and signals controlled by operator located in train-order office.

Langham Road MP 282.4 (Beaumont) and Connell MP 270.76: Two main tracks between Langham Road MP 282.4 and end of two main tracks K.C.S. MP C-766 designated as follows:

Main Track No. 1	North Track
Main Track No. 2	South Track

Main Tracks Nos. 1 and 2 are signalled for movements in either direction.

K.C.S. Crossing: On spur between Guffey and Chaison, 1.84 miles east of Guffey, no operator on duty. Normally lined for K.C.S.

(K.C.S. Crossing) Port Arthur MP 1.1: No operator on duty. Normally lined for K.C.S. Push-button controller and instructions for operation located at crossing.

Sabine River MP 250.2: Governs movement over Sabine River Drawbridge.

LAFAYETTE SUBDIVISION

(K.C.S. Crossing) Lockmoor MP 222.81: No operator on duty. Normally lined for S.P.

(K.C.S. Crossing) West Lake MP 221.24: No operator on duty. Normally lined for S.P.

When signal displays stop indication and no train or engine approaching on conflicting route, member of crew may operate Key-Release by inserting switch key and turning SLOWLY one complete turn to right. If signal does not clear after time release has functioned, Rule 663(c) will govern.

Calcasieu River MP 220.9: Governs movement over Calcasieu River Drawbridge.

(K.C.S. Crossing) Lake Charles MP 219.10: No operator on duty; normally lined for S.P. main track movement. Hand-operated switch with pipe connected derail to Ball Park Track opens within interlocking limits. Rule 81-A will apply to movement entering main track. If, after lining switch S-A signal continues to display red aspect Rule 663(c) will apply.

(M.P. Crossing) Lake Charles Yard MP 217.97: No operator on duty. Normally lined for S.P.

Mermentau River MP 180.3: Governs movement over Mermentau River Drawbridge.

(M.P. Crossing) MP 167.57: No operator on duty. Normally lined for S.P.

Lafayette Yard: East switch of yard track No. 1 is dual control; switch and signals controlled by operator in train order office.

RULE 680. M.P. Crossing MP 259.3 Lafayette Line.
M.P. Crossing MP 205.28 Lafayette Line.

RULE 705. Indicators located as follows:

Illum. Letter	On Signal	Authorizes and requires movement as follows
M	2524	Proceed on main track to east end siding.
S	2524	Enter siding.
M	2507	Proceed on main track to west end siding.
S	2507	Enter siding.
M	Westward "SA" Signal East End China	Proceed on main track to west end of siding.
S	Westward "SA" Signal East End China	Enter siding.
M	2948	Proceed on main track to east end of siding.
S	2948	Enter siding.
M	3059	Proceed on main track to West end of siding.
S	3059	Enter siding.
M	3084	Proceed on main track to East end of siding.
S	3084	Enter siding.
M	3292	Proceed on main track to East end of siding.
S	3292	Enter siding.
M	3487	Proceed on main track to West end of siding.
S	3487	Enter siding.

Fauna: Eastward trains required to take siding at Fauna, may be directed by operator, Tower 87, to proceed on Main Track to opposite end of siding. Operator, Tower 87, will not authorize such movement until:

- (1) Train dispatcher has authorized movement.
- (2) Letter-type indicator governing westward train movements at opposite end of siding, Fauna, has been illuminated to require opposing trains to enter siding.

Eastward trains authorized to proceed on Main Track, Fauna, to fouling point at east end of siding will be governed by indication of automatic block signal governing eastward trains at west end siding, and Rules 306, 508 or 509 will apply.

LAFAYETTE SUBDIVISION

RULE 760. Beaumont and China: CTC in effect on Main Track between Eastward absolute signals MP 292.7 (China) and Eastward interlocking signals MP 282.4 (Beaumont).

Signals controlled by S.P. operator Beaumont (Tower 74), acting upon authority of train dispatcher.

Interlocking signals out of siding, South Main and North Main tracks at Beaumont govern westward movements into CTC at MP 282.4. Applicable interlocking rules will apply within Interlocking and CTC rules will apply within CTC portion of block beyond the interlocking limits. When signals display stop indication, trains or engines must receive the following authority before proceeding:

- (a) M.P. Interlocking operator under provisions of Rule 663.
- (b) S.P. Operator, Beaumont, under provisions of Rule 776.

RULE 812. A. & N. R. RR.: Trains and engines using tracks of Angelina and Neches River Railroad between Prosser and Dunagan will be governed by Rules and Regulations of the Transportation Department of the Southern Pacific Transportation Company, its Lafayette Division timetable, timetable bulletins and by train orders issued over initials of Chief Train Dispatcher of that division.

RULE 827. Location of Dragging and/or derailed equipment detectors. Lafayette Line: MP 347.1, 337.3, 315.5, 309.5, 253.7, 246.6, 225.8, 183.3 and 175.5. **Rockland Branch:** MP 45.40, 51.25, 61.00, 70.00, 80.25, 92.00, 103.03, 111.85 and 120.25. **Baytown Branch:** MP 8.62 and 13.66.

HOT BOX DETECTORS

MP	Type	Location of Type D Recorder At Mechanical Facility	Directions
351.3	D	Englewood	Westward
312.3	C	.....	Both
273.9	D	Beaumont	Westward
288.3	C	.....	Both
26.8	D	Beaumont	Westward
256.1	C	.....	Both
245.7	C	.....	Both
224.4	D	Lake Charles Yard	Eastward
210.0	C	.....	Both
177.4	C	.....	Both
153.1	D	Lafayette Yard	Eastward

RULE 827-A. "K" trains must have train inspection made before leaving Pine, for Westward trains, or Dolan, for Eastward trains. Roll-by inspection by crew or other train crews will satisfy inspection requirements.

RULE 872. Will not apply at Prosser, Lafayette Yard and Lafayette when engine consist remains intact.

MISCELLANEOUS

1. MOVEMENT OF PASSENGER TRAINS

Passenger trains are to be given preference over freight trains and are restricted to main track and following sidings:

Fauna	Brimstone
Dayton	Lake Charles Yard
Devers	Roanoke
China	Jennings
Connell	Midland
Francis	Crowley Siding
Echo	

EXCEPTIONS:

Passenger trains may operate on No. 1 track Lafayette Yard and Lake Charles Yard when directed to do so by Yardmaster.

AVONDALE SUBDIVISION

EAST-WARD FIRST CLASS	STATIONS		STATION NUMBER	WEST-WARD FIRST CLASS
	2 Psgr	1 Psgr		
Lv. Sun. Tues., Thur.	Mile Post	Avondale Line		Ar. Mon. Wed., Fri.
PM 3 37	147.1	TO-R	LAFAYETTE YARD BKIYPQ	91362 PM 5 21
s4.00	144.5	Yd. Lmts 2885 R	LAFAYETTE BKP	91700 s5.15
4.07	140.8	6411	ELKS P	91804 4.56
4.12	138.4	1666 Yd Lmts	BROUSSARD P	91806 4.54
4.19	132.1	8127	CADE P	91815 4.49
4.26	126.1	Yd. Lmts 6633 TO-R	WEST TOWER IPO	91821 4.44
s4.30	125.6	TO-R	NEW IBERIA BKYP	91840 s4.42
4.50	112.5	6084	JEANERETTE SIDING P	92043 4.18
5.00	105.1	Yd. Lmts 10611 TO	BALDWIN BKPO	92050 4.10
5.05	101.0	Yd. Lmts	FRANKLIN P	92240 4.05
5.13	95.6	9917	BAYOU SALE P	92270 3.59
5.28	81.8	Yd. Lmts 8749	BERWICK	92416 3.44
5.32	80.1	TO-R	MORGAN CITY BKPO	92419 3.41
5.42	71.2	3495	URSA P	92437 3.29
s6.05	55.0	Yd. Lmts 7760 TO	SCHRIEVER BKPO	92455 s3.12
6.23	40.2	Yd. Lmts 10828 TO	RACELAND JCT. BKYPQ	92712 2.53
6.35	31.6	2477	DES ALLEMANS P	92810 2.43
6.45	24.2	5068 Yd Lmts	BOUTTE P	92826 2.36
6.50	18.9	9762	SALIX IP	92833 2.30
6.58	11.3	TO-R	AVONDALE BKIYPQ	92840 2.23
7 00 PM	10.5	Yd. Lmts	WEST BRIDGE JCT. IPO	92872 2 20 PM
TIME AT NEW ORLEANS UNION PASSENGER STATION AND EAST BRIDGE JCT. FOR INFORMATION ONLY. SEE CURRENT TIMETABLES AND RULES OF OPERATING DEPARTMENTS FOR MOVEMENTS AS FOLLOWS: NOUPT RAILROAD BETWEEN SOUTHPORT AND NOUPT STATION; I.C.G.R.R. BETWEEN EAST BRIDGE JCT. AND SOUTHPORT; AND NEW ORLEANS PUBLIC BELT RAILROAD BETWEEN WEST BRIDGE JCT. AND EAST BRIDGE JCT.				
PM 7 17		5.3	EAST BRIDGE JCT. PQ	92874 PM 2 03
s8 10 PM		5.8	R NEW ORLEANS UPT STATION BKYP	92900 1 50 PM
Ar. Sun. Tues., Thur.		(136.9)		Lv. Mon. Wed., Fri.
2				1

RULE 5: Morgan City time applies at Signals 796 and 797.

AVONDALE SUBDIVISION

BETWEEN		PSGR	FRT
LAFAYETTE YARD and WEST BRIDGE JCT.		70	55
Exceptions:	PSGR	FRT	Exceptions:
148.60 and 143.00 ...	25	25	80.80 and 79.10 ...
138.70* and 137.90* ...	40	25	79.10 and 77.80* ...
129.25 and 126.70 ...	60	40	77.80* and 75.90* ...
126.70* and 123.00* ...	15	15	73.43 and 73.31 ...
115.20* and 113.30* ...	25	25	52.10 and 51.70 ...
113.30 and 104.14 ...	60	40	51.70 and 51.63 ...
104.14 and 104.00 ...	35	35	51.63 and 43.00 ...
104.00 and 101.60 ...	60	40	32.13 and 32.05 ...
101.60* and 100.90* ...	25	25	14.60 and 12.25 ...
100.90 and 96.80 ...	60	40	12.25 and 10.50 ...
96.80 and 91.60 ...	60	50	

* **RULE 10-J.** Speed may be increased as soon as lead locomotive has passed increase speed sign at these locations.

The following establishes the maximum allowable speeds for freight trains provided speed is not otherwise restricted: (e.g., Restricted cars or engines, AB Rule 33, etc.)

a. AVLAT and LAAVT are authorized to operate at Freight Train Speed.

b. Light engine with operative dynamic brake is authorized to operate at passenger train speed.

Exception: without dynamic brake in operation, must operate at freight train speed.

c. Other freight trains may be authorized by train dispatcher to operate at Freight Train Speed.

d. Trains not covered in items a, b, or c must operate at freight train speed not to exceed 45 MPH, except as provided in A.B. Rule 65.

***MAXIMUM HORSEPOWER PER TON RATIOS:**

AVLAT, LAAVT	2.0
All other trains	1.3

(Refer to A.B. Rule 65)

* Will not apply on Mississippi River Bridge

SPEED ON OTHER THAN MAIN TRACK:

Sidings Cade, Bayou Sale, Raceland Jct., and Salix ...	25
No. 1 Track Lafayette Yard, from east switch to Sunbeam Lane	25
Rip Tracks, Caboose Track and Stock Pen, Lafayette Yard	5
Locomotive maintenance facility tracks Lafayette Yard:	
Engine holding track (No. 601), Middle track (No. 602) Sandhouse track (No. 603)	5
Patoutville Spur, MP 116.20	5
Garden City Spur, MP 97.90	5
Cabot Spur, MP 95.70	5
Ramos Spur, MP 74.40	5
New track, House, north and storage tracks, Schriever Wye track, Raceland Jct.	5
Texaco Spur, Vallier MP 28.4	5
Locomotive maintenance facility tracks, Avondale: East Turntable lead (No. 600), Wrecker Track (No. 602) Tie-up tracks off west turntable lead (Nos. 603, 604 and 605) Sandhouse track	5
Levert Spur (St. Martinville Branch)	5
Runaround Track Weeks (Cypremort Branch)	5
Colley Spur, MP 17.00 (Houma Branch)	5
Supreme Sugar Refinery tracks (Napoleonville Branch) ..	5
All other Tracks, Avondale Subdivision	10

AVONDALE SUBDIVISION

EAST-WARD Mile Post	STATIONS		WEST-WARD Station Number
	Cypremort Branch		
18.8	Yd Lmts	WEEKS	91235
0.0	10611 Yd Lmts TO-R	BALDWIN	92050
(18.8)			

Midland Branch			
30.0	2050 Yd Lmts	KAPLAN	P 91127
21.5	1600 Yd Lmts TO	ABBEVILLE	P 91138
14.9	1000 Yd Lmts	ERATH	91152
5.4	Yd Limits { R	I. & V. JUNCTION	91906
4.8		DAVIDS	Y 91904
0.0	Yd Limits { 6633 TO-R	NEW IBERIA	BKYP 91840
126.1		WEST TOWER	IPO 91821
(30.5)			

Youngville Branch			
33.1	Yard Limits {	YOUNGVILLE	91935
20.5		DAVIDS	Y 91904
18.4		PESSON	91917
(14.7)			

Salt Mine Branch			
5.4	Yd Limits {	I. & V. JUNCTION	91906
9.8		SALT MINE	91914
(4.4)			

MAXIMUM AUTHORIZED SPEED FOR TRAINS

BETWEEN	CYPREMORT BRANCH	ALL TRAINS
WEEKS and BALDWIN		10
Exceptions:	ALL TRAINS	Exceptions: ALL TRAINS
7.00 and 6.40	5	3.00 and 2.00
		5
MIDLAND BRANCH		
KAPLAN and NEW IBERIA		25
Exceptions:	ALL TRAINS	Exceptions: ALL TRAINS
31.0 and 16.25	10	5.40 and 00.0
16.25 and 5.40	20	
YOUNGVILLE BRANCH		
YOUNGVILLE and PESSON		10
SALT MINE BRANCH		
I & V JUNCTION and SALT MINE		10

AVONDALE SUBDIVISION

EAST-WARD	STATIONS		WEST-WARD
Mile Post	Alexandria Branch		Station Number
85.1	Yd. Limits TO-R	ALEXANDRIA 0.8	BP 91680
84.3		S. P. JUNCTION	
SEE M.P.R.R. COMPANY'S CURRENT TIMETABLE SPECIAL INSTRUCTIONS AND RULES FOR TRAIN MOVEMENTS BETWEEN CHENEYVILLE AND S.P. JUNCTION.			
60.1	Yd. Limits 2656	CHENEYVILLE 38.2	P 91660
21.9		OPELOUSAS 1.1	IP 91630
20.8		OPELOUSAS SIDING 20.3	91627
0.5		ALEX JCT. 1.8	91370
147.1	ABS TO-R	LAFAYETTE YARD	BKYPQ 91362
(86.9)			

St. Martinville Branch

5.7	Yd. Limits ABS TO-R	ST. MARTINVILLE 20.9	P 91730
0.3		B-R JCT. 2.0	91368
147.1		LAFAYETTE YARD	BKYPQ 91362
(22.9)			

Houma Branch

14.5	Yard Limits	HOUMA 14.5	P 92519
0.0	7760 Yd Lmts	SCHRIEVER	BKPY 92455
(14.5)			

Napoleonville Branch

55.0	Yard Limits TO-R	SCHRIEVER 4.1	BKPY 92455
3.2		NAPOLEONVILLE JCT. 11.6	92605
14.8		SUPREME 0.4	92627
15.2		END OF TRACK	
(16.1)			

Lockport Branch

9.9	Yard Limits	LOCKPORT 9.9	P 92729
0.0	10828 Yd Lmts TO-R	RACELAND JCT.	BKPY 92712
(9.9)			

AVONDALE SUBDIVISION

MAXIMUM AUTHORIZED SPEED FOR TRAINS			
BETWEEN	ALEXANDRIA BRANCH		ALL TRAINS
ALEX JCT. and CHENEYVILLE			25
Exceptions:	ALL TRAINS	Exceptions:	ALL TRAINS
60.10 and 48.00	10	10.00 and 0.50	10
24.00 and 20.20	10		

ST. MARTINVILLE BRANCH

St. Martinville to B-R Jct.			20
Exceptions:	ALL TRAINS	Exceptions:	ALL TRAINS
BR Jct. and MP 7.68, Breaux	10	16.50 and 13.00	10
Bridge	10	13.00 and 7.00	5
19.38 and 16.50	5	7.00 and 6.00	10

HOUMA BRANCH

Schriever and Houma			10
Exceptions:			ALL TRAINS
14.50 and 7.00			5

NAPOLEONVILLE BRANCH

Schriever and MP 15.28			25
Exceptions:			ALL TRAINS
4.50 and 2.00			10

LOCKPORT BRANCH

Lockport to Raceland Jct.			10
Exceptions:			ALL TRAINS
5.00 and 0.00			5

Opelousas: 20 MPH between interlocking signals M.P. Crossings.

ADDITIONAL STATIONS

Mile Post	Station	Station Number	Mile Post	Station	Station Number
Avondale Line					
145.3	Alex Jct.	91370	95.7	Cabot	92320
145.1	B-R Jct.	91700	95.7	North Bend	92330
137.0	Billeaud	91808	86.8	Patterson	92409
130.0	Ara	91817	74.4	Ramos	92429
120.9	Olivier	92007	73.3	Boeuf	92434
116.2	Patoutville	92012	69.2	Zacarter	92440
114.2	Jeanerette	92020	64.5	Donner	92445
101.7	Sterling Junction	92210	28.4	Vallier	92815
97.9	Garden City	92265	27.6	Paradis	92821
			1.3	Algiers	92898

NOTE: North Bend is on spur 4.3 miles from Bayou Sale.
Cabot is on spur 4.4 miles from Bayou Sale
Algiers is on spur 9.2 miles from West Bridge Jct.

Cypremort Branch

18.2	Gajan	92134	8.2	Glencoe	92110
14.9	Cypremort	92125	5.8	Achee	92067
13.1	United	92119	3.2	Caffery	92220
12.3	Ivanhoe	92117	1.6	Sterling	92215
11.1	Florence	92115			

NOTE: Sterling and Caffery are stations on spur 2.0 and 3.2 miles, respectively, from Sterling Junction within Yard Limits Baldwin-Bayou Sale.

Midland

26.5	Nunez	91131	15.9	West Erath	91149
20.0	Youngs	91141	12.2	Delcambre	91158
18.3	Grosse Isle	91145			

Salt Mine Branch

9.4	Avery	91912	6.2	Emma	91908
9.1	McIlhenny	91910	2.1	Brannon	91902

Youngsville Branch

28.5	Lozes	91928
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AVONDALE SUBDIVISION

ADDITIONAL STATIONS

Mile Post	Station	Station Number	Mile Post	Station	Station Number
Alexandria Branch					
53.2	Carboco	91648	22.7	Lansom	91634
51.9	Eola	91646	19.2	Veltin	91623
32.4	Beggs	91639	13.3	Sunset	91617
27.6	Washington	91637	7.1	Carencro	91611
St. Martinville Branch					
5.3	Anse LaButte	91708	8.1	Levert	91725
18.7	Breaux Bridge	91711			
Houma Branch					
17.0	Colley	92525	12.7	Southdown Siding	92515
13.4	Southdown	92517			
NOTE: Colley is on spur 2.5 Miles from Houma.					
Napoleonville Branch					
3.8	Thibodaux	92610	5.5	Leighton	92617
4.4	Naquin	92613	12.0	Labadieville	92624
NOTE: Thibodaux is on spur 0.6 Mile from Napoleonville Jct.					
Lockport Branch					
13.7	Jay	92740	1.9	Raceland	92718
5.5	Mathews	92725	1.4	Godchaux	92715
NOTE: Jay is on spur 3.8 Miles from Lockport.					

SPECIAL INSTRUCTIONS

RULE P. Impaired Side Clearance

MP	Description	MP	Description
80.4	Drawbridge	73.3	Drawbridge
77.4	Overpass	32.0	Drawbridge
		9.6	Building

RULE 10-H. Exceptions: On the St. Martinville, Midland, Youngsville, Salt Mine, Cypremort, Napoleonville, Houma, Lockport and Alexandria Branches, when a yellow flag is required it will be displayed one-half mile from point of restriction.

RULE 15. Exceptions: On the St. Martinville, Midland, Youngsville, Salt Mine, Cypremort, Napoleonville, Houma, Lockport and Alexandria Branches, the explosion of a torpedo requires movement at restricted speed for one mile from point where torpedo was exploded.

RULE 20, 20-A, 21, 82-A and 83-B. Westward trains leaving New Orleans UPT Station will display identification signals for train for which crew is ordered and need not obtain clearance at West Bridge Jct. but must obtain clearance bearing OK, time and initials of Chief Train Dispatcher before leaving Avondale.

Westward trains with crews operating through Avondale will display identification signals for which crew is ordered, unless otherwise instructed by train-order operator, Avondale, and conductor will prepare register ticket accordingly.

Eastward trains will display signals from West Bridge Jct. to New Orleans UPT Station according to designation on Avondale Subdivision.

RULE 26-B. North Bend — Columbian Carbon Company Plant: Special light type signal installed at switch to tracks Nos. 1 and 2, protects movable platform across these tracks. Switch located on light support must be turned on to illuminate light. If platform is raised, light will display green aspect, and track may be entered. If light does not burn when switch is turned on, loading foreman must be contacted for permission to enter tracks.

Cabot — Cabot Corporation Plant: Special light type signal protecting hopper track installed on shed at bin No. 1. When light displays red aspect, track must not be entered or cars disturbed, without first obtaining permission from loading foreman. When light displays green aspect, track may be entered without permission of loading foreman.

AVONDALE SUBDIVISION

Boutte — Monsanto Chemical Company Plant: Tracks 2A, 2B, 3, 5A, 5B, 9A, 9B, 10A and 10B protected by special light type signals (in addition to derails).

When signal displays red aspect track must not be entered. When signal displays yellow aspect track may be entered only with permission of loading foreman. In addition it must be known that derails are in position for movement.

RULE 82-A. Eastward first-class trains and extra passenger trains operating through Lafayette Yard must obtain clearance and train orders at Lafayette Yard; such clearance and train orders to be delivered to the relieving conductor and engineer at Lafayette, as prescribed by Rule 220.

Conductor and engineer of westward first-class trains and extra passenger trains operating through Lafayette Yard will deliver all train orders and instructions held to the relieving conductor and engineer at Lafayette, as prescribed by Rule 220.

Westward trains originating at Lafayette Yard operating to Alexandria will obtain M.P. train orders and M.P. clearance from Operator, Lafayette Yard.

RULE 83-A. At following stations, only trains indicated will register:

Lafayette	First class and extra passenger trains.
West Tower	Trains originating and terminating.
I.&V. Junction	Trains directed by train order.
Morgan City	Trains originating or terminating.

Trains operating to or from New Orleans UPT Station must register on NOUPT Train Register at that station.

RULE 83-B. At open train-order offices, trains may register by ticket as follows:

Lafayette Yard	First-class trains.
West Tower	Trains originating and terminating.
Avondale	First-class trains and trains with crews operated through Avondale.

Midland Branch eastward trains arriving West Tower may register by ticket, leaving same with waybills at New Iberia Freight Station.

RULE 93. Location of yard limits:

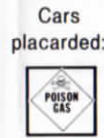
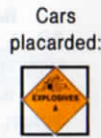
West MP		East MP
148.69	Lafayette Yard	142.99
4.02	Lafayette Yard (Alexandria Branch)	
3.17	Lafayette Yard (St. Martinville Branch)	
139.00	Broussard	136.69
128.77	New Iberia (Avondale Line)	121.07
	Kaplan	29.71
22.37	Abbeville	20.39
17.00	Erath	14.50
5.77	I.&V. Junction-Davids (Youngsville Branch)	4.35
	(Salt Mine Branch)	End of Br.
2.14	New Iberia (Midland Branch)	End of Br.
108.00	Baldwin-Bayou Sale	94.80
1.10	Baldwin (Cypremort Branch)	
	Weeks	17.00
82.76	Morgan City	74.00
56.54	Schriever (Avondale Line)	53.02
3.00	Schriever (Houma Branch)	
	Schriever (Napoleonville Branch)	1.39
42.70	Raceland Jct. (Avondale Line)	39.52
1.98	Raceland Jct. (Lockport Branch)	
29.56	Vallier MP 28.40	27.90
24.97	Boutte	22.66
17.77	Avondale	
53.47	Eola	51.38
24.97	Opelousas	18.00
	Houma-Southdown Siding	12.18
	Lockport	8.50

Trains moving on main track between Lafayette Yard and B-R Jct., in either direction, will be governed by block signals, indications of which will supersede the superiority of trains.

Position in train of placarded cars containing hazardous materials

NOTE: Cars with same placards may be placed next to each other.

Shippers may use either words or numbers on placards. Numbers shown are samples. Other numbers may appear on placards.



RESTRICTIONS

Must not be nearer than the sixth car from the engine occupied caboose or passenger car	X	X		X			
When train length does not permit, must be placed as near the middle of train as possible but not nearer than the second car from the engine, occupied caboose or passenger car	X	X		X			
Engine, occupied caboose or passenger car	X	X	X	X	X		
Car occupied by guard or escort	X(1)	X(1)		X(1)			
Loaded plain flat car	X	X		X			
Loaded bulkhead flat car	X(2)	X(2)		X(2)			
Loaded TOFC/COFC flat car	X(3)	X		X(4)			
Car loaded with vehicles	X	X		X(5)			
Open top car with shiftable load	X(2)	X(2)		X(2)			
Car with internal combustion engine in operation. Car with any heating apparatus or any lighted stove, heater or lantern	X	X		X			
Car placarded EXPLOSIVES A		X	X	X		X	
Car placarded POISON GAS	X		X	X		X	
Car placarded RADIOACTIVE	X	X		X		X	
Any loaded placarded car (other than COMBUSTIBLE or same placard)	X	X	X				

MUST NOT BE NEXT TO:

NO RESTRICTIONS

(1) A placarded rail car must be next to and ahead of any car occupied by the guards or technical escorts accompanying this car. However, if a car occupied by guards or technical escorts is equipped with a lighted heater or stove, it must be the fourth car behind any car placarded EXPLOSIVES A.

(2) Restriction applies only when any of the lading protrudes beyond the car ends or when any of the lading extending above the car ends is liable to shift so as to protrude beyond the car ends.

(3) Cars placarded EXPLOSIVES A may be placed next to each other.

(4) Restriction applies only to loaded flatbed or opentop trucks and trailers and to loaded trucks and trailers without securely closed doors.

(5) Restriction does NOT apply to a car loaded with vehicles secured by a device designed for that purpose and permanently installed on the car and of a type generally accepted for handling in interchange between railroads.

AVONDALE SUBDIVISION

RULE 98. Railroad crossings at grade not interlocked

Location	
Avondale Line	
M.P. Crossing	0.5 mile west of Sterling (Sterling Junction-Sterling Spur)... See Note.
Alexandria Branch	
MP 52.2 (M.P.)	See Note.
St. Martinville Branch	
Breaux Bridge	MP 19.2 See Note.

NOTE: Crossings protected by "Stop" signs.

Normal position of gates at crossings

Location	Normal Position
Midland Branch	
Dauids MP 4.8#	Midland Branch
#Trains and engines must approach prepared to stop and when gate is set against Youngsville Branch movements, Midland Branch movements may be made without stopping, not exceeding 10 MPH until crossing is covered.	
Youngsville Branch	
MP 24.1, west of Davids	See Note
Davids MP 4.8 (M.P. Crossing)	Midland Branch
NOTE: Crossing protected by "Stop" signs.	
Algiers Spur	
MP 2.3 (M.P. Crossing)	S.P.

DRAWBRIDGES NOT INTERLOCKED

Houma Branch

Intracoastal Canal, MP 14.82

St. Martinville Branch

Bayou Teche, MP 8.0 Levert Sugar Co. Spur.

Midland Branch

Bayou Vermilion, MP 21.47, 0.1 mile west of Abbeville.#
Bayou Carlin, MP 11.89, 0.4 mile east of Delcambre.#

#Gates protecting drawbridges 21.47 and 11.89. Normal position is for water traffic. "STOP" signs are displayed in advance of gates. Movement must not be made until it has been determined running rails on each end of bridge are in place or proceed signal received from bridgetender.

RULE 99. Exceptions: On the Alexandria, Midland and Cypremort Branches, when protection by flagman is required by this rule, distances specified for placement of torpedoes and flag protection will be one-half and one mile from train being protected.

RULE 99-C. Will apply on Cypremort Branch, between Opelousas and Cheneyville (Alexandria Branch) and between I.&V. Junction and Kaplan (Midland Branch).

RULE 103. For train, engine and switching movements over following crossings, a member of crew must take position at crossing to afford warning:

AVONDALE SUBDIVISION

Avondale Line

New Iberia	La. State Highway 182
Power House Spur (New Iberia)	La. State Highway 182
Olivier (spur)	La. State Highway 182
Jeanerette (spur)	La. State Highway 182
Garden City (spur)	La. State Highway 182
Pelican State Lime Company Spur MP 74.43	U.S. Highway 90

Alexandria Branch

Opelousas	Grolee and Guidry Streets west of Freight Station; Bellevue and Cherry Streets east of Freight Station.
Alexandria	Third Street and Main Street crossings.

Napoleonville Branch

Thibodaux	Saint Mary Street Crossing
Napoleonville Jct.	Street and Highway Crossings

Baldwin-Cypremort Road: Movements from Cypremort Branch must not enter this crossing unless gates are down or a member of crew has taken position at crossing to afford warning to vehicular traffic.

RULE 104. Houma: Normal position of switch to Ashland extension is for Ashland extension.

RULE 104-D. Boutte (Monsanto Chemical Plant), drops must not be made inside plant.

RULE 105. Weeks: Main track ends at MP 18.43.

Salt Mine: Main track ends at MP 9.57.

RULE 208. Fourth paragraph will not apply to westward trains at West Tower. Train order restricting movement of a westward train at that station may be delivered while train is moving but operator must not clear interlocking signal at leaving end of siding until train has stopped or restriction has expired.

RULE 211. Berwick-Morgan City: When Form "N" train order is held by operator, eastward trains restricted at Berwick upon receiving verbal authority from operator Morgan City, may proceed on main track to train-order office to receive orders.

RULE 221. Unit for display of flashing white light installed at the following locations:

Station	Location	Direction
Morgan City	Signal 792	Westward
Baldwin	Signal 1075	Eastward
Raceland Jct.	Signal 421	Eastward

Light will not be displayed in train-order signal at Abbeville. Avondale is train-order office only for westward trains.

RULE S-240. Applies at following locations:

Territory	Register location
MP 1.39 and end of track MP 15.2 (Napoleonville Branch)	Schriever
MP 1.98 and Lockport (Lockport Branch)	Raceland Jct.
MP 3.17 and St. Martinville (St. Martinville Branch)	Lafayette Yard

RULE 306. Block Signals with "P" plates.

Eastward	Protection	Westward
P-1326	Spring switches, Cade	P-1309
P-1074	Spring switches, Baldwin	P-1053
	Spring switch, east end siding, Bayou Sale	P- 957
P- 826	Spring switches, Berwick	P- 809
P- 422	Spring switches, Raceland Jct.	P- 401
	Spring switch, east end siding, Salix	P- 183

Special Signals — Morgan City: Unit for display of flashing white light is installed on south side of Signal Bridge 797. This signal may display a flashing white light in each direction when a

AVONDALE SUBDIVISION

train is occupying west approach between ABS 835 west of siding Berwick, and Fourth Street, Morgan City. Normal indication dark.

The only purpose of display of flashing white light is to give information to yard crews that a train is approaching from the west.

Display or non-display of flashing white light does not relieve trainmen and enginemen from compliance with Rule 80 or Rule 81-A before entering or fouling main track and will confer no authority for movement of train or engine.

RULE 508. When Automatic Block Signals Nos. 782, 957, 1435, 1443, 1444, 1481 or 1482 displays Red Aspect Rule 507 instead of 508 will apply.

RULE 538. Spring switches equipped with facing point locks are located as follows:

Location	Normal Position
Cade	West and east end siding
Baldwin	West and east end siding
Bayou Sale	East end siding
Berwick	West end siding
Raceland Jct.	West and east end siding
Salix	East end siding

Spring switches not equipped with facing point locks are located as follows:

Location	Normal Position
Berwick	East end siding

RULE 606. Lafayette Yard: East switch of Yard Track No. 1 is dual control; switch and signals controlled by operator in train order office.

West Tower: Interlocking limits on main track extend from signal located 12 feet west of west switch of siding (MP 127.6) to signals on both tracks located 275 feet east of M.P. Crossing (MP 126.1); and on other than main track, from dwarf signal near fouling point west end of siding to connection with main track and from dwarf signal near fouling point east end of siding to signals located on both tracks 275 feet east of M.P. Crossing.

Charenton Canal MP 104.1: Governs movement over Charenton Canal Drawbridge.

Atchafalaya River MP 80.5: Governs movement over Atchafalaya River Drawbridge.

Trains stopping at Berwick or Morgan City must stop so that no part of drawbridge interlocking limits will be fouled. The circuit must at all times be left clear so that draw span can be opened. Westward trains stopping to do work must clear Signal 810.

Boeuf MP 73.3: Governs movement over Bayou Boeuf Drawbridge.

Lafourche MP 51.6: Governs movement over Bayou Lafourche Drawbridge.

Des Allemands MP 32.5: Governs movement over Bayou Des Allemands Drawbridge.

Salix—Avondale: Interlocking limits on main track extend from eastward interlocking signals at fouling point east end of siding Salix MP 18.4 to westward interlocking signals at west end of Avondale yard MP 12.0.

Interlocking signals, switches and derails governing entrance to and movements on connection track with M.P. RR. to serve American Cyanamid Company and which opens off east end of Cyanamid Spur run-around track at MP 16.8 are controlled by operator of the M.P. at Avondale.

Harvey Canal MP 4.47 (Algiers Spur): governs movement over Harvey Canal Drawbridge.

RULE 680. Opelousas: MP 22.1 M.P. Crossings.

AVONDALE SUBDIVISION

RULE 705. Indicators located as follows:

Illum. Letter	On Signal	Approaching	Authorizes and requires movements as follows
M	1074	Baldwin	Proceed on main track to East end siding.
S	1074	Baldwin	Enter siding.
M	1053	Baldwin	Proceed on main track to West end siding.
S	1053	Baldwin	Enter siding.
M	826	Berwick	Proceed on main track to East end of siding.
S	826	Berwick	Enter siding.
M	809	Berwick	Proceed on main track to West end of siding.
S	809	Berwick	Enter siding.
M	556	Schriever	Proceed on main track to east end of siding.
S	556	Schriever	Enter siding.
M	541	Schriever	Proceed on main track to west end of siding.
S	541	Schriever	Enter siding.
M	422	Raceland Jct.	Proceed on main track to east end siding.
S	422	Raceland Jct.	Enter siding.
M	401	Raceland Jct.	Proceed on main track to west end siding.
S	401	Raceland Jct.	Enter siding.
M	196	Salix	Proceed on main track to beginning of interlocking.
S	196	Salix	Enter siding.
M	183	Salix	Proceed on main track to west end of siding.
S	183	Salix	Enter siding.

Rule 827. Location of Dragging and/or Derailed Equipment Detectors. MP 110.00, 99.20, 84.95, 76.10, 67.0, 59.76, 48.50, 34.10 and 26.50.

High and/or Wide Load Detectors located at MP 83.85 and 77.25.

HOT BOX DETECTORS

MP	Type	Locating of Type D Recorder at Mechanical Facility	Direction
138.9	D	Lafayette Yard	Westward
110.0	C	.....	Both
76.1	C	.....	Both
36.1	C	.....	Both
3.8	D	Alexandria Branch Lafayette Yard	Eastward

RULE 740. Between MP 12.4 and MP 10.5, Avondale:

Absolute signals at MP 12.4, MP 11.3 and MP 10.6 govern westward movement.

When absolute signal indicates stop, movement will be made in accordance with Rule 507.

Where no absolute signal governs entrance to main track, movement may be made as provided in Rules 81-A and 512.

Electric Switch Locks located as follows:

Avondale Switch to freight station tracks.

Automatic Crossing Gate: Avondale. Eastward movements after stopping for A-PB Signal MP 2.4 must know that automatic crossing gates are down or member of crew has taken position at crossing to afford warning to vehicular traffic before entering Avondale Garden Rd. Crossing.

RULE 812. M.P. Company's rules require conductors to call the M.P. dispatcher from S.P. Junction (Alexandria Branch) to secure permission to enter M.P. main track.

RULE 872. Will not apply at Lafayette Yard and Lafayette when engine consist remains intact.

AIR BRAKE RULES

RULE 14. Trains and transfer cuts exceeding 7,500 tons going over Mississippi River Bridge must have helper engine.

AVONDALE SUBDIVISION

RULE 58.L. On yard engines handling transfer trains using dynamic brakes, before entering or leaving turnout or crossover on descending grade, Mississippi River Bridge, dynamic braking force must be reduced to one-half of the maximum amperes, 500 feet before engine reaches and 1500 feet after passing through turnout or crossover, and if necessary, automatic brakes applied sufficiently so that speed of 15 MPH or allowable speed will not be exceeded until entire movement is clear of turnout or crossover.

Transfer trains using pusher engine must be stopped on descending grade clear of signal governing movements through turnout or crossover where pusher engine will be detached.

MISCELLANEOUS

Movement of Passenger Trains

Passenger trains are to be given preference over freight trains and are restricted to main track and following sidings:

Elks	Schriever
Cade	Raceland Jct.
West Tower	Boutte
Jeanerette Siding	Salix
Baldwin	Bayou Sale
Berwick	

EXCEPTIONS:

Passenger trains may operate on No. 1 track Lafayette Yard when directed to do so by Yardmaster.

ALL SUBDIVISIONS

SPECIAL INSTRUCTIONS

RULE 1 and 3. Standard Time may be obtained from Houston telephone extensions 6098, 6069 or 6083.

RULE S-72. Eastward trains are superior to trains of the same class in the opposite direction.

AUTOMATIC BLOCK SYSTEM

RULE 505. Where automatic protection is provided for movements from an adjacent track to main track, "Key-Releases", with time-release feature, may be installed on signal case near fouling point to clear signal on one track when control circuit of other track is occupied.

If governing signal displays stop indication and no train approaching, member of crew may insert switch key in slot below governing signal number on signal case and turn SLOWLY one complete turn to right, remove key and wait until time-release of 3 minutes* has functioned, after which signal should display proceed indication if block is clear.

* Exception: West and east end siding Francis and west end Lafayette Yard -7 minutes

RULE 825. At terminals where instructions require application of hand brakes on freight trains, outgoing crews must not release hand brakes until road engine is coupled and brake system charged.

RULE 827. Where high and/or wide load, dragging and/or derailed equipment detectors are installed as listed under subdivisions, revolving red beacon will be mounted on hot box detector house on post or relay case adjacent to detector and will be normally dark. When detector is activated, the revolving red light will be displayed. Train must be stopped and a walking inspection made of entire train.

When a revolving red beacon light is observed prior to engine passing detector location, train may proceed without stopping for inspection. Report must be made to train dispatcher promptly.

ALL SUBDIVISIONS

HOT BOX DETECTORS

Each hot box detector scanner site has a white light continuously illuminated on track side of detector instrument house. When a hot bearing is detected, the white light will start flashing. When flashing light is observed, train must be stopped promptly and inspection made to locate hot bearing(s).

The absence of a white light continuously illuminated on the track side of detector instrument house is an indication detector may be inoperative. Under such circumstances, train must be stopped and all bearings inspected except under the following conditions:

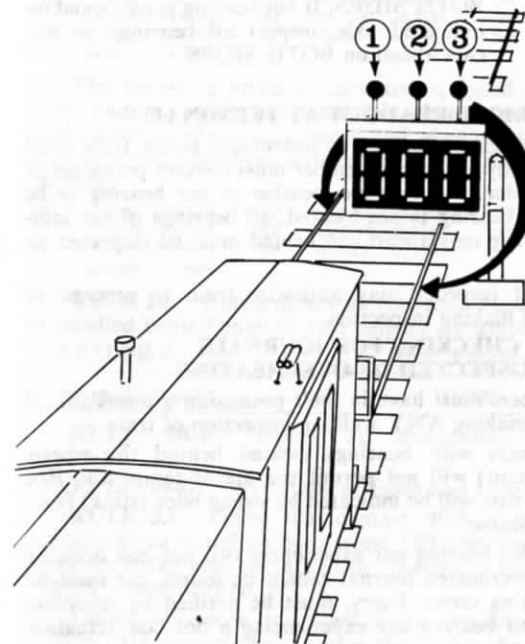
- If employees other than members of crew make a rolling inspection (train speed not to exceed 20 MPH) on both sides.
- If the monitor display board on a Type C detector displays "000" after train has passed scanner location.
- If personnel at location of recorder of a Type D detector advises it is safe to proceed to terminal.

The absence of a white light must be promptly reported to train dispatcher. To avoid unnecessary delay to trains passing an inoperative hot box detector, train dispatcher may authorize such trains to make the required walking inspection or rolling inspection under condition (a) at another location provided it is no more than 10 miles in advance of or beyond detector site.

TYPE A: LETTER "H" INDICATOR (RULE 705) WITH DIGITAL READOUT.

When Letter "H" is illuminated or a flashing white light on instrument house is observed, train must be brought to immediate stop. Inspection must be made of entire train to determine that it is safe to proceed to location of readout locator. Member of crew must then observe readout and be governed by instructions posted inside case. If hot bearing is not located, all bearings of car indicated by readout as well as five cars ahead and behind must be inspected on both sides. If readout fails to indicate location of hot bearing, then all bearings of train must be inspected on both sides. When the letter "W" is displayed, train must stop and not proceed until "W" is extinguished or permission is obtained from train dispatcher.

TYPE C: NUMERICAL DISPLAY BOARD WITH INDICATOR LIGHTS.



ALL SUBDIVISIONS

The diagram depicts a Type C hot box detector's monitor display board and indicator lights as it would be viewed looking back after rear of train has passed detector site. The indicator lights identified ①②③ are normally dark, but when a hot bearing is detected, lights ① (right side of train) or ② (left side of train) will immediately display a flashing white light to identify the side of train on which the hot bearing was detected.

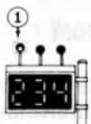
When an additional hot bearing is detected, the center indicator light ③ will also commence flashing. To assist in locating hot bearing, the detector will count the number of axles from the first hot bearing detected to the rear of train. Two seconds after train has passed the detector, the numerical board will illuminate and display the accumulated axle count for 90 seconds.

The following are the various displays possible as would be viewed looking back from rear of train and the corresponding required train inspection:

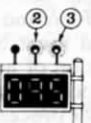
DISPLAY REQUIRED INSPECTION



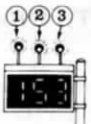
No inspection required



Inspect for one hot bearing on axle 234 from rear on side of train indicated. If hot bearing is not located, all bearings of car indicated as well as five cars ahead and behind must be inspected on BOTH SIDES.



Inspect for two or more hot bearings from rear of train to and including axle 095 on indicated side. If two or more hot bearings are not located, inspect all bearings from rear of train to and including five cars ahead of indicated axle on BOTH SIDES.



Inspect for two or more hot bearings from rear of train to and including axle 153 on BOTH SIDES. If hot bearing is not found on indicated axle, inspect all bearings on five cars ahead on BOTH SIDES.

TYPE D: REMOTE READOUT AT TERMINAL

When white light is flashing on instrument house, train must be stopped promptly and crew member must contact personnel at location of recorder to determine location of hot bearing to be inspected. If hot bearing is not located, all bearings of car indicated as well as five cars ahead and behind must be inspected on both sides.

Personnel at recorder may authorize train to proceed to terminal without making inspection.

CHECKING FOR JOURNALS SUSPECTED OF OVERHEATING

Crew members must have in their possession a tempilstik, if available, when making ANY walking inspection of train.

Passenger cars with bearings located behind the wheels (Amfleet equipment) will not permit the use of tempilstik. Hot bearing on these cars will be indicated by strong odor (stink) from built-in heat indicator.

When a roller bearing car experiences two hot box detector actuations and overheated journal cannot be found, car must be set out. Connecting crew, if any, must be notified by incoming crew of any roller bearing car experiencing a hot box actuation and car was not set out.

ALL SUBDIVISIONS

CONTINUOUS WELDED RAIL (CWR) TRAINS

A box car, or high-side gondola car must be positioned on each end of CWR train as a buffer car during all movement except preparatory to and during unloading.

When making walking inspection of a CWR train carrying a full or partial load, the following items must be inspected:

- Check for undesired movement of rail. The tops of rails are painted adjacent to the tie-down rack on the tie-down car which is located near center of train. Paint marks on each tier of rail must be in line; otherwise, this is an indication of an undesired movement of rail.
- Check each rail end to make certain it overhangs the last supporting roller by at least 12 feet and is no closer than 12 feet from the next empty roller. Rails are marked 12 feet from each end.

When any of these conditions are not as required, train must not be moved until train dispatcher has been contacted and further instructions are received.

HAZARDOUS MATERIALS

RULE 827-A.

Unless specifically authorized by Superintendent, "K" trains excluding locomotives must not exceed 8,000 feet, except:

Between Englewood and West Bridge Jct. must not exceed 10,000 feet, and;

On Rockland Branch must not exceed 8,000 tons or 6,000 feet in length.

RULE 874. Enginemen must specifically look for defects in Shock Absorber on locomotives equipped with HTC Trucks:

What To Do in Case Defect is Noted:

- Reduce train speed to not over 50 miles per hour.
- Notify Dispatcher of defective condition.
- Enter defect on Form CS 2326 for correction.

AIR BRAKE RULES

RULE 9. The following series of cars are equipped with empty-load brake system which has semi-automatic change-over feature:

SSW 75700 - 75799	SP 354000 - 354749	SP 491000 - 491059
SSW 78500 - 78599	SP 463500 - 464899	SP 492000 - 492039
SP 333500 - 334399	SP 467500 - 467549	SP 500604
SP 337500 - 337599	SP 480000 - 480193	SP 590000 - 590099
SP 345000 - 345699		

The following series of cars are equipped with empty-load brake system, which has automatic change-over feature proper brake function when car is loaded and when empty. This feature is fully automatic on these series and requires no action on part of engineer:

SP 323000 - 323239	SP 354750 - 355299	SP 481000 - 481149
SP 329310 - 329359	SP 463337 - 463486	SP 590100 - 590131
SP 329620 - 329629	SP 464900 - 467049	SP 595500 - 595624
SP 337600 - 337699		

RULE 14. Unless otherwise restricted maximum tonnage to be handled behind engines with helpers entrained:

TERRITORY	Road Engine	Helper Engine
All main tracks	10,000	8,500

RULE 24-E. Applies at Beaumont, Lafayette and Avondale.

RULE 24-G. Will apply at Echo and Lafayette yard.

RULE 33. Trains that contain 90% or more mechanical refrigerator cars and do not exceed 120 cars and/or 90 tons per operative brake may operate at the maximum speed permitted by "TOPS" identification; or

Unless otherwise restricted (e.g., restricted cars, engines, "TOPS" ID symbol, etc.), trains that meet the requirements of

ALL SUBDIVISIONS

the following table may operate at speeds specified above 45 MPH provided tons per axle of operative extended and/or standard range dynamic brake does not exceed 500 tons:

(This table is only to be used to compute allowed speeds above 45 MPH.)

TONS PER OPERATIVE BRAKE		
Number of Cars	80+ to 85	85+ to 90
1 to 40	Speed sign speed	Speed sign speed
41 to 45	Speed sign speed	Speed sign speed
46 to 50	Speed sign speed	Speed sign speed
51 to 55	Speed sign speed	Speed sign speed
56 to 60	Speed sign (minus) 5 MPH	Speed sign (minus) 5 MPH
61 to 65	Speed sign (minus) 10 MPH	Speed sign (minus) 10 MPH
66 to 70	Speed sign (minus) 15 MPH	Speed sign (minus) 15 MPH
71 to 75	Speed sign (minus) 20 MPH	Speed sign (minus) 20 MPH
		Maximum 45 MPH

TONS PER OPERATIVE BRAKE		
Number of Cars	90+ to 95	95+ to 100
1 to 40	Speed sign speed	Speed sign speed
41 to 45	Speed sign speed	Speed sign (minus) 5 MPH
46 to 50	Speed sign (minus) 5 MPH	Speed sign (minus) 10 MPH
51 to 55	Speed sign (minus) 10 MPH	Speed sign (minus) 15 MPH
56 to 60	Speed sign (minus) 15 MPH	Speed sign (minus) 20 MPH
61 to 65	Speed sign (minus) 20 MPH	Maximum 45 MPH
66 to 70	Maximum 45 MPH	Maximum 45 MPH
71 to 75	Maximum 45 MPH	Maximum 45 MPH

RULE 49. Not more than 10 units in multiple operative or inoperative may be entrained on head end of any train.

MISCELLANEOUS

1. SPEED RESTRICTIONS FOR TRAINS

a. Trains identified with multiple TOPS train identification symbols (example BSMFF/BSMFY 24) are authorized to operate at the highest maximum authorized speed permitted for any symbol within the train identity. Speed restrictions on empties, cars containing hazardous materials, and restricted cars are still applicable in determining maximum authorized speed.

b. When moving against current of traffic, or when movement is not protected by block signals, speed of passenger trains and light engines must not exceed 59 MPH and speed of freight trains must not exceed 49 MPH, nor may speed exceed that applying to normal operation.

2. SPEED RESTRICTIONS FOR LOCOMOTIVE:

LOCOMOTIVE NUMBER	MAX-IMUM SPEED	CLASS	DYN BRK	STARTING TRACTIVE EFFORT	WGT 000
SP-SSW					
1000-1002	70	AS600	SF	102,000	408
@1010-1013	65	ES400		65,250	261
@1100	65	ES408		51,750	207
@1105-1127	65	ES408	ST	58,250	233
@1191-1199	65	ES409		59,250	237
@1213-1277	60	AS409		58,750	235
@1300-1337	65	ES410		61,750	247
1500-1542	70	ES615	ST	82,500	330
@1600-1609	70	GS400	EF	70,000	280
@2250-2316	65	ES412		62,250	249
@2450-2759	65	ES415		65,250	261
2868-2899	70	ES418	ST	63,250	253
2964-2970	70	ES620	ET	97,500	390
2971-2976	50	ES620	EF	104,000	416
3100-3101	70	GS425	SF	67,000	268
3102-3109	70	ES625		95,500	390
3118-3135	25	AS628		97,750	391
3148-3153	25	AS630		101,000	404
3186-3196	70	EP418	ST	65,000	260
3197-3199	70	EP430	EF	70,000	280

ALL SUBDIVISIONS

LOCOMOTIVE NUMBER	MAX-IMUM SPEED	CLASS	DYN BRK	STARTING TRACTIVE EFFORT	WGT 000
3200-3209	70	EP636	ET	102,500	410
3301-3886	70	EF418	ST	63,250	253
4050-4152	70	EF420	ST	65,250	261
4160	70	EF420	ET	65,750	263
4200-4249	70	EF420	ET	66,500	266
4300-4451	70	EF618	ST	90,000	360
4700-4709	70	ES620	ET	97,500	390
4800-4844	70	EF420	EF	69,250	277
5002-5017	70	EF423	ST	66,000	264
5100-5114	70	GF423	EF	66,500	266
5300-5325	70	EF623	ET	104,250	417
6300-6681	70	EF425	ET	66,500	266
6801	70	GF425	SF	67,000	268
6901-6953	70	EF625	ET	97,500	390
7030-7033	70	SF428	SF	70,000	280
⊙ 7200-7201	70	EF435	EF	69,500	278
⊙ 7230-7231	70	EF435	EF	69,500	278
7300-7399	70	EF630	EF	102,750	411
7400-7599	70	EF632	EF	98,500	394
7600-7607	70	EF430	ET	67,560	278
7608-7677	70	EF430	EF	69,500	278
7770-7883	70	GF430	EF	70,000	280
7900-7929	70	GF630	EF	104,750	419
7930-7936	70	GF630	ET	104,750	419
⊙ 7940-7959	70	EF430	EF	69,500	278
#8230-8299	70	EF630	EF	97,750	391
#⊙ 8300-8341	70	EF630	EF	102,500	410
#⊙ 8350-8391	70	EF630	EF	102,500	410
8400-8488	70	EF630	ET	102,750	411
#8489-8573	70	EF630	EF	102,500	410
8585-8599	70	GF633	EF	104,750	419
8600-8687	70	GF633	ET	104,750	419
8688-8796	70	GF633	EF	104,750	419
8800-9156	70	EF636	ET	103,500	414
#9157-9404	70	EF636	EF	102,750	411
#9500-9504	70	EF642	ET	103,250	413
AMTRAK:					
200-360	70	EP430A		63,500	254
361-390	70	EP430A		64,750	259
500-649	70	EP630A		98,250	393
700-724	70	GP630A		96,500	386
L & N:					
@501-545	70	EF418		61,750	247
@900-904	70	EF418		61,750	247
910-914	70	AF418		62,750	251
@950-959	70	AF418		62,750	251
1000-1079	70	EF423		63,000	252
1100-1128	70	EF425		65,250	261
1200-1224	70	EF625		93,250	373
1225-1258	70	EF630		96,250	385
1259-1278	50	EF630		96,750	387
1279-1294	70	EF625		95,550	390
1330-1335	70	AF420		63,750	255
1350-1378	70	AF420		63,750	255
1425-1435	70	AF630		97,700	391
1470-1498	70	GF630		104,850	419
1500-1527	70	GF625		92,250	369
1529-1530	70	GF625		92,250	369
1532	70	GF625		92,250	369
1528, 1531, 1533	70	GF628		103,000	412
1534-1582	70	GF630		92,250	369
1600-1626	70	GF425		67,250	269
2500-2502	70	GF428		67,250	269
2503-2509	70	GF630		82,250	369
2700-2824	70	GF423		67,250	269
3000-3029	70	EF430		66,250	265

ALL SUBDIVISIONS

LOCOMOTIVE NUMBER	MAXIMUM SPEED	CLASS	DYN BRK	STARTING TRACTIVE EFFORT	WGT 000
3554-3613	50	EF630		66,250	265
4000-4144	70	EF420		67,000	268
5115-5129	70	GF423		67,250	269
6011-6050	70	EF420		67,000	268
7000-7035	70	GF630		96,250	385
7513-7523	70	AF628		97,700	391
#8000-8039	70	EF630		96,250	385 la.2
MoPAC:					
2009-2334	70	EF420		65,750	263
2600-2616	70	EF420		65,750	263
#3090-3321	70	EF630		98,000	392
3500-3529	70	EF435		83,400	278
4500-4684	70	GF423		67,500	266
#6000-6073	70	EF630		98,000	392
SOU:					
210-214	70	EF425		63,250	253
215-223	70	EF625		94,000	376
2525-2643	70	EF423		62,750	251
2645-2715	70	EF425		64,500	258
2716-2822	70	EF430		63,250	253
2823-2886	70	EF420		62,250	249
3000-3099	70	EF625		95,500	382
3100-3169	70	EF636		98,750	395
3170-3200	70	EF630		94,750	379
3201-3287	50	EF630		93,750	375
3288-3328	70	EF630		93,750	375
3800-3804	70	GF630		98,500	394
3805-3814	70	GF633		99,250	397
3815-3818	70	GF436		70,000	280
3900-3969	70	GF423		64,500	259
3970-4023	70	GF423		65,250	261
4600-4605	70	EF426		63,750	255
5000-5256	70	EF420		69,250	277
7000-7092	70	EF423		64,250	257
SCL:					
250-392	70	GF418		61,750	247
550-559	70	EF420		67,000	268
700-1002	56	EF415		62,250	249
1063-1065	56	EF418		61,250	245
1309-1343	56	EF423		62,500	250
1400-1415	56	EF425		65,250	261
1500-1635	59	EF430		66,250	265
1640-1656	59	EF430		66,500	266
1700-1718	60	GF430		66,250	265
1720-1747	67	GF633		66,750	267
1748-1855	60	GF436		67,750	271
1900-1970	61	EF625		93,250	373
2000-2044	66	EF636		98,000	392
2045-2059	71	EF636		96,750	387
2121-2124	67	GF630		92,250	369
5100-5114	62	GF423		67,250	269
5140-5154	70	GF423		66,750	267
6000-6050	70	EF420		66,000	264
7016-7031	68	GF630		96,250	385
#8040-8066	71	EF630		96,250	385

Equipped with HTC trucks and truck snubbers.

- ⊙ RCE Master.
- ⊙ RCE Remote.
- ⊙ Mother. ⊙ Mate.

@ Locomotives not equipped with alignment control couplers.

A locomotive that is NOT listed in these tables must NEVER be operated in a train unless it is specifically authorized by a train order. The train order must include the speed and weight of the locomotive as well as its starting tractive effort if it is to be operative in the train.

ALL SUBDIVISIONS

Unless otherwise notified in writing or verified by a Mechanical Department employee, a locomotive that does not appear in these tables must be considered as a locomotive that is NOT equipped with alignment control couplers.

Engines 9157-9404 may only be operated between Englewood and West Bridge Jct.

Engines 8800-9156 may only be operated between Englewood and West Bridge Jct. when authorized by Train Order Reading "You are authorized to operate with units in your consist."

Trains with AMTRAK EP630A locomotives in consist, must not exceed 50 MPH from point where engine enters curve until engine and first car behind engine are again on tangent track between the following mile post locations:

Lafayette Subdivision: MP 244.0 and 247.5

Avondale Subdivision: MP 51.8 and 55.8.

3. SPEED RESTRICTIONS WITH CERTAIN EQUIPMENT	MAIN TRACKS OTHER THAN BRANCHES	MAIN TRACKS ON BRANCHES
Scale test cars		
SPMW 5868, SSW 99203	30	30
NBS-1 (must be handled in rear 20 cars of train)	60	49
Relief outfits with steam derrick	45*	25*
Locomotive Crane-Piledrivers		
SPMW 4027, 4028, 4029, 4088, 4091, 5437, 5479, 5595, 5852, 5870, 5874, 5899, 6601, 6602, 6603, 6604, 8000, 8002, 8003, 8004, SSWMW 96404 and 96405:		
With boom in place, either end		
forward 1	25*	15*
With boom disconnected, heavy end forward	40	25
boom end forward	20*	15*
With boom disconnected and removable counterweight properly positioned, either end forward	40	25
Steam pile driver SPMW 4053	35	25*
Jordan Spreaders:		
Moving backward	25	20
Moving forward	35	35

*On curves where authorized speed is more than 15 MPH speed must be reduced to 5 MPH less than speed permitted.

⊙When moving in train with boom in place, operator must be on board.

Unless specifically authorized, all relief outfit cranes, locomotive cranes and pile drivers must not operate over routes having maximum load limits of less than 263,000 lbs.; and must observe all restrictions applying to cars weighing over 210,000 lbs.

MAXIMUM SPEED PERMITTED WITH RELIEF CRANES

Location	Main Track
SP MW 7140 El Paso	45
SP MW 5846 Sanderson	45
SP MW 5848 Lafayette	35
SSW MW 96006 Pine Bluff	45
SP MW 7113 Houston	35

On curves where speed is 45 MPH or less speed must be reduced to 5 MPH less than shown on speed signs.

Relief outfits, with boom forward, are restricted to 20 MPH.

4. OTHER SPEED RESTRICTIONS	MPH
Trains handling hazardous material listed in Rule 827-A	50
Engines operated from other than lead locomotive	20
Trains handling empty bulkhead flat cars	45
Trains handling empty, specially equipped gondola cars (TOPS car kind code "GP")	45
Trains handling pipe loaded on 89 ft. flat cars	55
PC 598500 to 598999 (Gondolas)	45
Loaded Continuous Welded Rail (CWR) Trains	45*
Trains handling empties, except cabooses	55

ALL SUBDIVISIONS

*Loaded CWR trains must be handled separately from other trains.

5. PLACEMENT OF RESTRICTED CARS IN TRAIN WITH OR WITHOUT HELPER

(a) On Train AVLAT and trains on the Rockland Branch any car measuring less than 42 feet in length must not be coupled to a car longer than 73 feet in length. This restriction will not apply to rear 20 cars of train.

Trains and engines with cars between Avondale and East Bridge Jct. in excess of 7500 tons must not have any cars measuring less than 42 feet in length coupled to a car longer than 73 feet in length within the first 40 cars behind engine.

(b) It is the responsibility of yardmasters and conductors to take into consideration the overall distribution of tonnage when making up or changing consist of train. The following are requirements governing train makeup:

1. Trains consisting of predominantly empty cars will have any block of loaded cars entrained near the head end.
2. Train makeup requirements will prevail when they conflict with outstanding blocking instructions unless authorized by Division Officer or Chief Dispatcher.
3. Train mass profile graph should be used to monitor train makeup when available.
4. When in doubt as to proper distribution of train tonnage, yardmaster or conductor will contact Division Officer or Chief Dispatcher for instruction.

(c) The following restriction will apply on the Rockland Branch and to trains HOSOY and HOLNY:

When the tonnage of any train including local or road switcher exceeds 4,000 tons, each of the first five cars behind engine must weigh 50 tons or more.

This restriction will not apply where there are less than 20 loaded cars in train.

(d) Caboose are not to be moved other than at rear of train, unless specifically authorized, except when handling a few cars in local or road switcher service.

6. LOAD LIMIT: (car and contents):

Other than Branches	315,000 pounds
Branches	315,000 pounds
Exceptions:	
Lake Arthur Branch between MP 4.1 and Lake Arthur	210,000 pounds
Alexandria Branch between Cheneyville and Opelousas	251,000 pounds
Kaplan and I. & V. Jct.	251,000 pounds
Youngsville Branch	251,000 pounds
Houma Branch between:	
Schriever and Houma	263,000 pounds
Houma and Colley	210,000 pounds
Napoleonville Branch	251,000 pounds
St. Martinville Branch between:	
Lafayette Yard and Breaux Bridge	251,000 pounds
#Breaux Bridge and St. Martinville	230,000 pounds

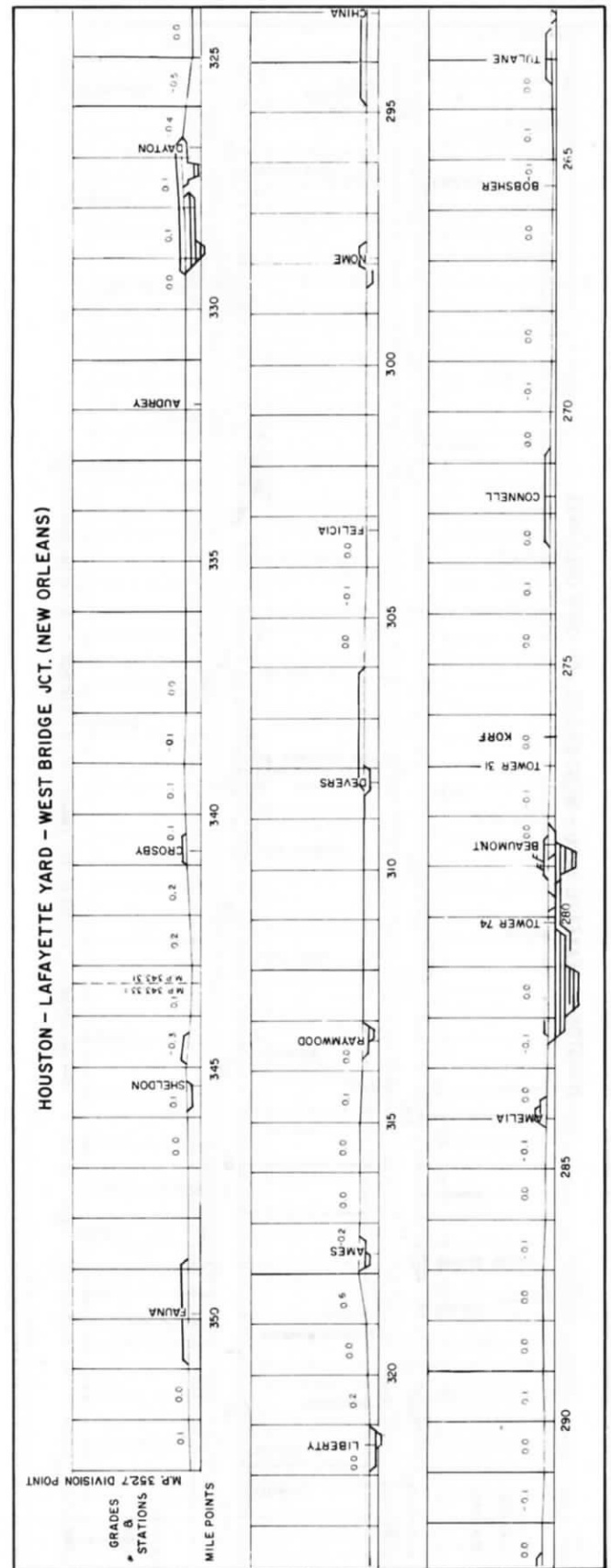
Unless authorized by Superintendent, heavier loads will not be handled.

Maximum load limit on spur between Patoutville and Patoutville Sugar Refinery must not exceed 251,000 pounds gross load.

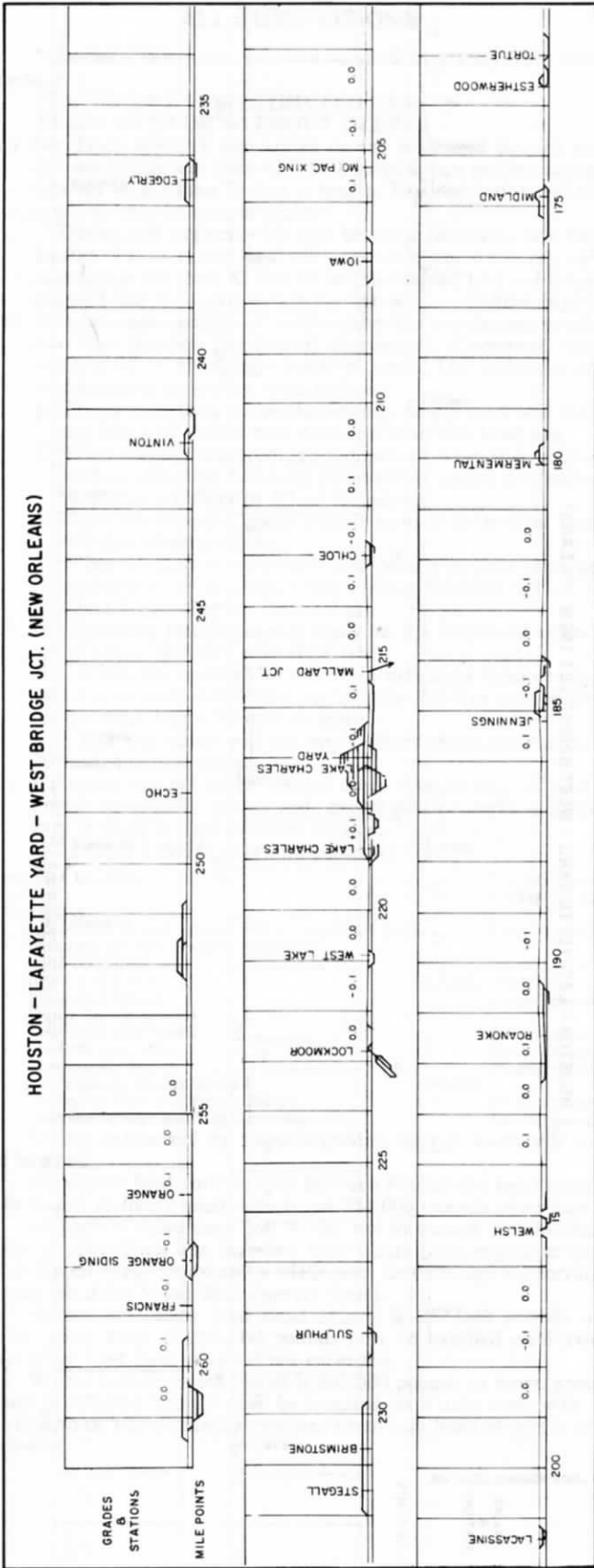
#Loads of more than 169,00 lbs. not to exceed 230,000 lbs. must be spaced not less than two empty cars from engine or any load. Speed must not exceed 6 MPH over Drawbridge 8.1 serving Levert St. John Sugar Mill (Levert Spur).

Where maximum load limit shown is 263,000 pounds or more, gross loads of 395,000 pounds may be handled on 6 axle cars when load limit of car is not exceeded.

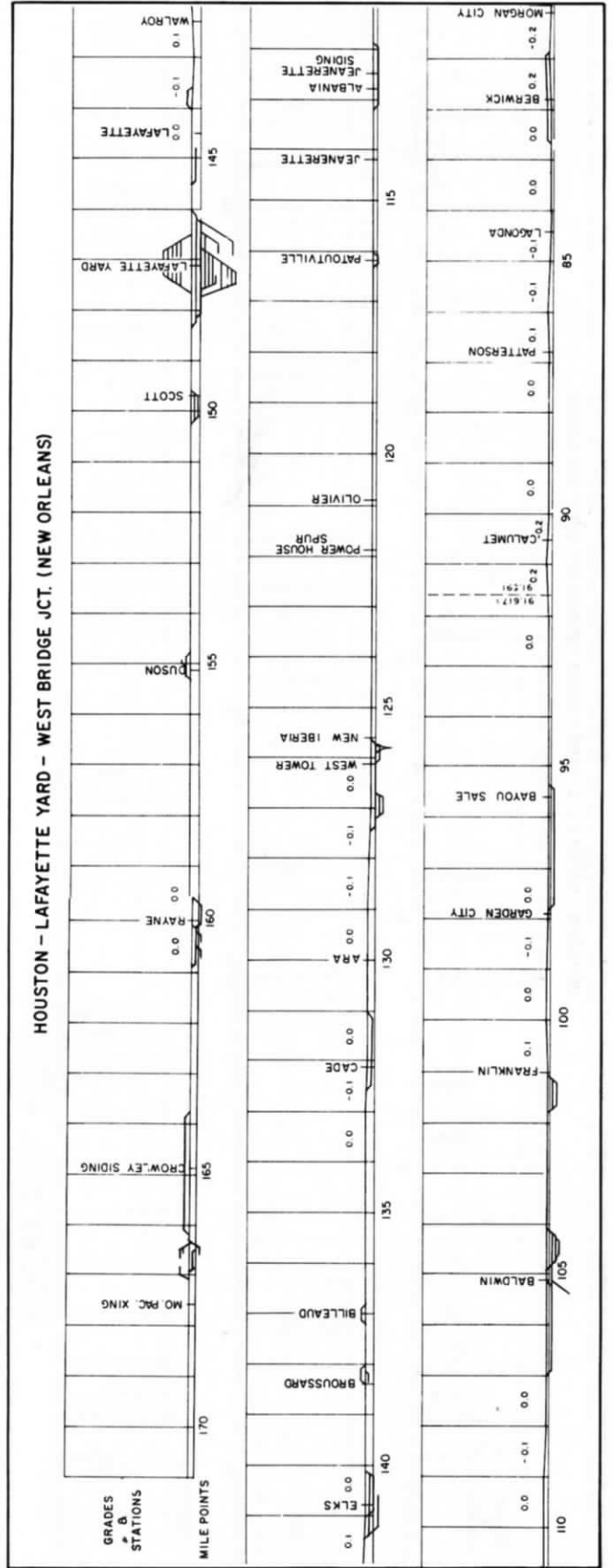
Where maximum load limit is 263,000 pounds or more, gross loads of 526,000 pounds may be handled on 8 axle cars, with a maximum of 3 cars coupled together, when load limit of cars is not exceeded.



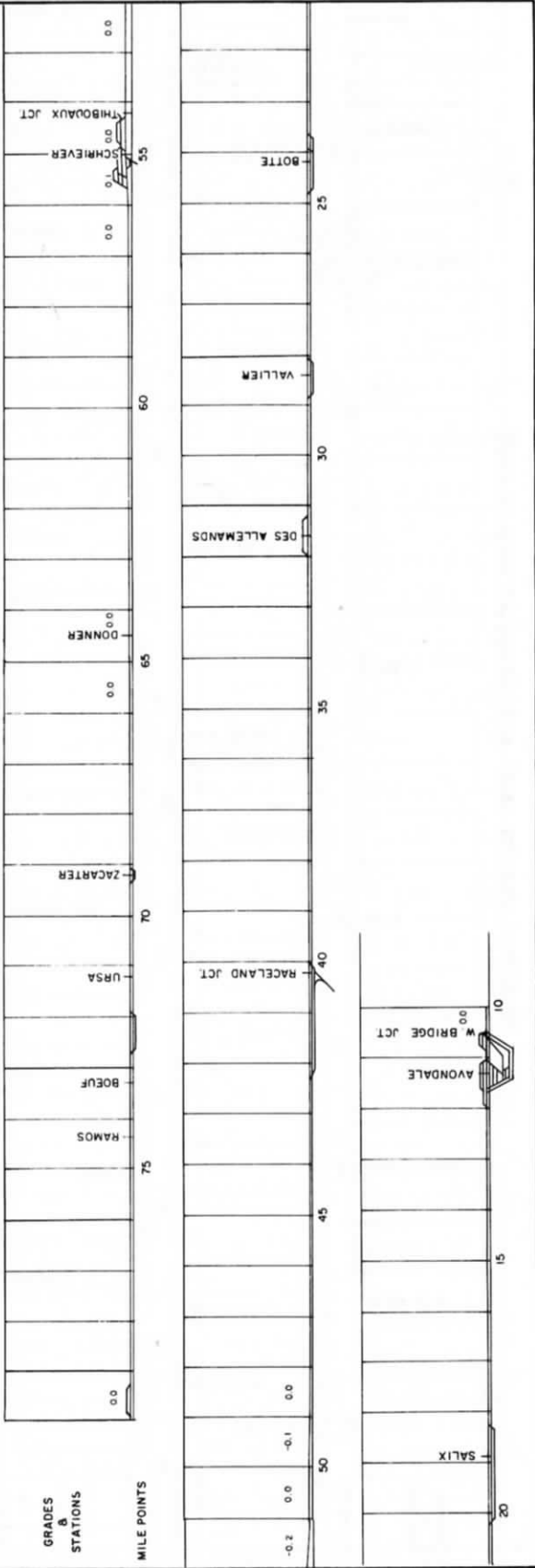
HOUSTON - LAFAYETTE YARD - WEST BRIDGE JCT. (NEW ORLEANS)



HOUSTON - LAFAYETTE YARD - WEST BRIDGE JCT. (NEW ORLEANS)



HOUSTON - LAFAYETTE YARD - WEST BRIDGE JCT. (NEW ORLEANS)



ROCKLAND BRANCH

