

SOUTHERN



REGION

TRACK CHART

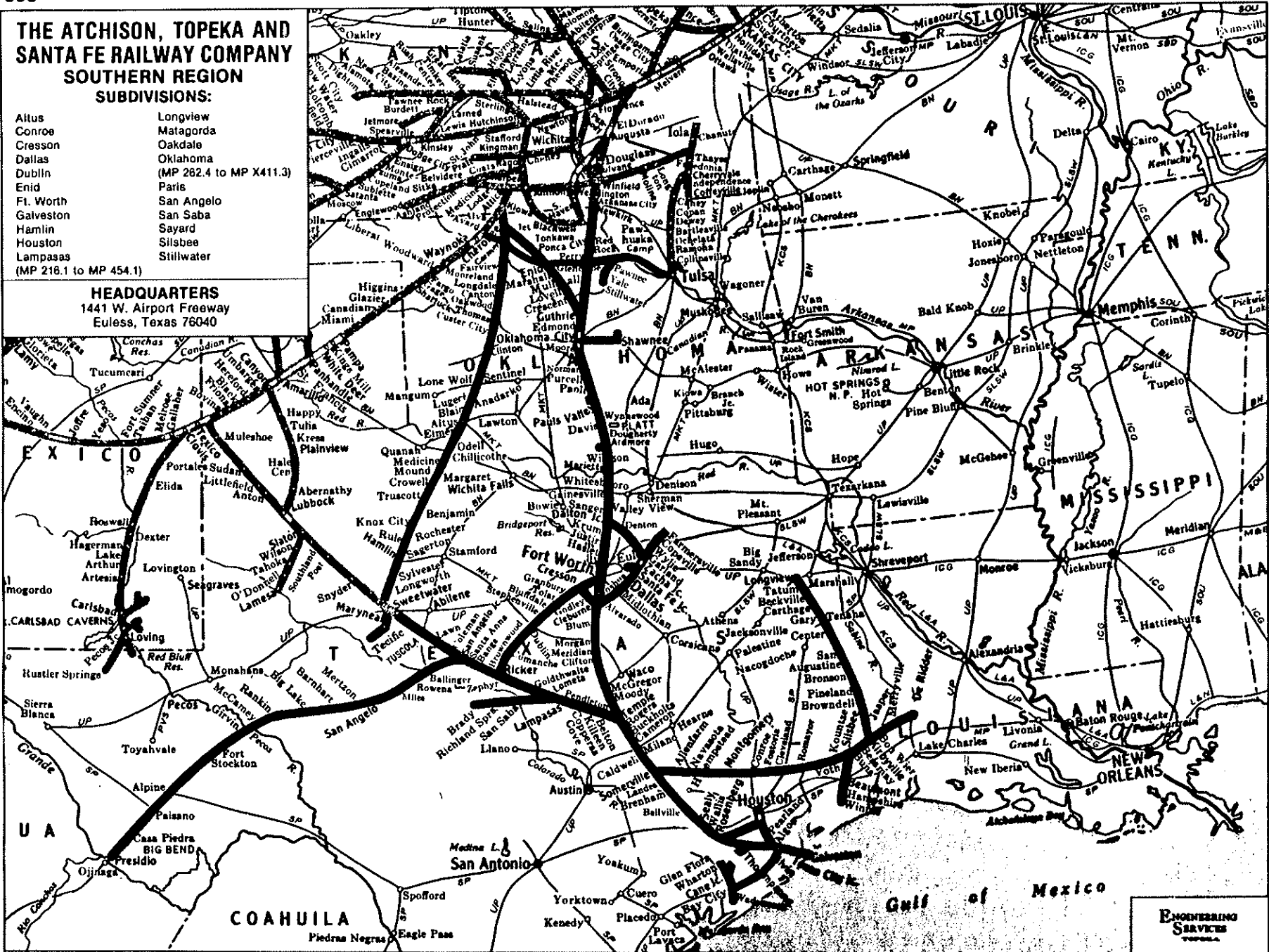
**THE ATCHISON, TOPEKA AND SANTA FE RAILWAY COMPANY  
SOUTHERN REGION**

**SUBDIVISIONS:**

- |           |                         |
|-----------|-------------------------|
| Altus     | Longview                |
| Conroe    | Matagorda               |
| Cresson   | Oakdale                 |
| Dallas    | Oklahoma                |
| Dublin    | (MP 282.4 to MP X411.3) |
| Enid      | Paris                   |
| Ft. Worth | San Angelo              |
| Galveston | San Saba                |
| Hamlin    | Sayard                  |
| Houston   | Silsbee                 |
| Lampasas  | Stillwater              |
- (MP 216.1 to MP 454.1)

**HEADQUARTERS**

1441 W. Airport Freeway  
Eules, Texas 76040



ENGINEERING  
SERVICES

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## SOUTHERN REGION

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SUBDIVISION	FROM		TO		PAGES
	MILEPOST	STATION	MILEPOST	STATION	
Enid	0.0	Kiowa	116.4	Guthrie	470-494
San Angelo	0.0	San Ang. Jct.	1029.1	Presidio	386-468
Silsbee	21.1	Silsbee	1.1	Beaumont	380-384
Longview	21.1	Silsbee	207.8	Longview	338-377
Conroe	0.0	Somerville	152.6	Silsbee	304-336
Paris	91.0	Farmersville	62.6	Zacha Jct.	295-301
Dallas	0.0	Cleburne	111.2	Dallas	265-294
Cresson	0.0	Cleburne	18.4	Cresson	260-264
Dublin	0.0	Birds	134.5	Ricker	231-258
Lampasas	454.1	Tecific	218.1	Temple	180-230
Houston	19.5	Houston	0.0	Alvin	173-177
Galveston	218.2	Temple	0.0	Galveston	121-171
Ft. Worth	409.1	Gainesville	218.2	Temple	76-120
Oklahoma	262.4	Arkansas City	409.1	Gainesville	1-73

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## SOUTHERN REGION

STATION	STATE	PAGE NUMBER	SUBDIVISION	APPROX MILEPOST	COUNTY
ALGOA	TX	164	GALVESTON	24.4	GALVESTON
ALGOA	TX	163	GALVESTON	24.4	BRAZORIA
ALLENFARM	TX	308	CONROE	18.3	BRAZOS
ALPINE	TX	451	SAN ANGELO	944.3	BREWSTER
ALPINE JCT.	TX	451	SAN ANGELO	945.6	BREWSTER
ALTA LOMA	TX	165	GALVESTON	18.2	GALVESTON
ALVARADO	TX	292	DALLAS	12.7	JOHNSON
ALVIN	TX	163	GALVESTON	28.6	BRAZORIA
ALVIN	TX	177	HOUSTON	0.0	BRAZORIA
ANTELOPE GAP	TX	213	LAMPASAS	300.3	MILLS
ARCADIA	TX	164	GALVESTON	20.7	GALVESTON
ARCOLA	TX	159	GALVESTON	42.6	FORT BEND
ARDMORE	OK	56	OKLAHOMA	450.4	CARTER
ARKANSAS CITY	KS	2-3	OKLAHOMA	263.4	COWLEY
ASP	OK	18	OKLAHOMA	328.4	NOBLE
BALDRIDGE	TX	433	SAN ANGELO	863.8	PECOS
BALL	TX	148	GALVESTON	104.6	AUSTIN
BALLINGER	TX	394	SAN ANGELO	36.9	RUNNELS
BANGS	TX	200	LAMPASAS	357.9	BROWN
BARNHART	TX	415	SAN ANGELO	771.6	IRION
BEACH	TX	319	CONROE	74.6	MONTGOMERY
BEAUMONT	TX	384	SILSBEE	1.7	JEFFERSON
BECKVILLE	TX	371	LONGVIEW	181.4	PANOLA
BELCO	TX	117	FORT WORTH	221.2	BELL
BELDING	TX	440	SAN ANGELO	892.9	PECOS
BELLVILLE	TX	146-147	GALVESTON	106.2	AUSTIN
BELT JUNCTION	TX	232	DUBLIN	0.9	TARRANT
BELTON	TX	228	LAMPASAS	226.4	BELL
BENEDUM	TX	423	SAN ANGELO	809.2	REAGAN
BESSMAY	TX	342	LONGVIEW	37.4	JASPER
BIG LAKE	TX	419	SAN ANGELO	790.6	REAGAN
BIRDS	TX	91	FORT WORTH	342.8	TARRANT
BIRDS	TX	232	DUBLIN	342.8	TARRANT
BLACK BEAR	OK	16	OKLAHOMA	316.3	NOBLE
BLANKET	TX	256	DUBLIN	121.7	BROWN
BLANTON	OK	482	ENID	58.2	GARFIELD
BLUFFDALE	TX	243	DUBLIN	55.1	ERATH
BLUM	TX	101	FORT WORTH	303.5	HILL
BOBVILLE	TX	314	CONROE	48.9	MONTGOMERY
BOOTH	TX	158	GALVESTON	55.0	FORT BEND
BOSS	TX	235	DUBLIN	15.0	TARRANT
BRAGG	TX	330	CONROE	128.1	HARDIN
BRAZLIME	TX	101	FORT WORTH	300.2	HILL
BRENHAM	TX	142	GALVESTON	126.0	WASHINGTON
BRITTON	OK	28	OKLAHOMA	376.8	OKLAHOMA
BRONSON	TX	355	LONGVIEW	104.7	SABINE
BROWDELL	TX	352	LONGVIEW	87.4	JASPER
BROWNWOOD	TX	202-204	LAMPASAS	348.4	BROWN
BUCKHOLTS	TX	126	GALVESTON	196.0	MILAM
BUNA	TX	342	LONGVIEW	36.1	JASPER
BURLINGTON	OK	472	ENID	8.8	ALFALFA
BURNETT	OK	31	OKLAHOMA	385.7	OKLAHOMA
CALDWELL	TX	134	GALVESTON	157.8	BURLESON
CALGARY	TX	360	LONGVIEW	127.0	SAN AUGUSTINE
CALL	TX	344	LONGVIEW	48.0	JASPER
CAMP	OK	70	OKLAHOMA	58.2	PAWNEE

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STATION	STATE	PAGE NUMBER	SUBDIVISION	APPROX MILEPOST	COUNTY
CAMERON	TX	128	GALVESTON	188.0	MILAM
CARTHAGE	TX	369	LONGVIEW	171.7	PANOLA
CASA PIEDRA	TX	462	SAN ANGELO	1002.9	PRESIDIO
CASTOR	TX	212	LAMPASAS	306.1	MILLS
CEDAR HILL	TX	288	DALLAS	34.6	DALLAS
CENTER	TX	362	LONGVIEW	139.8	SHELBY
CENTEX	TX	254	DUBLIN	110.8	COMANCHE
CHANCELLOR	TX	443	SAN ANGELO	904.3	PECOS
CHARTER OAK	TX	229	LAMPASAS	225.0	BELL
CHEROKEE	OK	474	ENID	19.7	ALFALFA
CHILOCCO	OK	4	OKLAHOMA	268.6	KAY
CHRISMAN	TX	132	GALVESTON	165.8	BURLESON
CLAY	TX	307	CONROE	11.9	BURLESON
CLEBURNE	TX	96-98	FORT WORTH	317.5	JOHNSON
CLEBURNE	TX	261	CRESSON	317.5	JOHNSON
CLEBURNE	TX	294	DALLAS	0.0	JOHNSON
CLEVELAND	TX	323	CONROE	94.9	LIBERTY
CLIFSTONE	TX	108	FORT WORTH	266.0	BOSQUE
CLIFTON	TX	107	FORT WORTH	270.4	BOSQUE
COLEMAN	TX	196	LAMPASAS	378.3	COLEMAN
COLLINS	TX	350	LONGVIEW	78.7	JASPER
COMANCHE	TX	253	DUBLIN	108.1	COMANCHE
CONROE	TX	319	CONROE	72.2	MONTGOMERY
COPEVILLE	TX	297	PARIS	84.3	COLLIN
COPPERAS COVE	TX	223	LAMPASAS	254.3	CORYELL
COWLEY	TX	273	DALLAS	75.3	COLLIN
COZART	TX	185	LAMPASAS	432.0	TAYLOR
CRABB	TX	157	GALVESTON	58.6	FORT BEND
CRAWFORD	TX	111	FORT WORTH	250.1	MC LENNAN
CRESCENT	OK	491	ENID	102.8	LOGAN
CRESSON	TX	264	CRESSON	18.4	JOHNSON
CRESSON	TX	236	DUBLIN	22.0	HOOD
CROWLEY	TX	93	FORT WORTH	333.7	TARRANT
CRUSHER	OK	53	OKLAHOMA	465.7	MURRAY
DALLAS - U. TERM.	TX	281	DALLAS		DALLAS
DALLAS CITY SPUR	TX	280, 282	DALLAS	53.2	DALLAS
DALTON JCT.	TX	81	FORT WORTH	386.8	DENTON
DALTON JCT.	TX	266	DALLAS	111.2	DENTON
DANCI	TX	94	FORT WORTH	328.3	JOHNSON
DANIELS	TX	368	LONGVIEW	165.6	PANOLA
DANT	TX	145	GALVESTON	110.3	AUSTIN
DAVIDSON	TX	135	GALVESTON	151.3	BURLESON
DAVIS	OK	51	OKLAHOMA	478.0	MURRAY
DELAWARE	TX	257	DUBLIN	128.0	BROWN
DENTON	TX	268	DALLAS	104.7	DENTON
DIES	TX	332	CONROE	138.3	HARDIN
DOBBIN	TX	314	CONROE	49.9	MONTGOMERY
DOLEN	TX	326	CONROE	107.3	LIBERTY
DOLESE	OK	53	OKLAHOMA	466.9	MURRAY
DOUGHERTY	OK	53	OKLAHOMA	469.6	MURRAY
DOUGLAS	OK	487	ENID	80.4	GARFIELD
DUBLIN	TX	249	DUBLIN	86.1	ERATH
DUKE	TX	159	GALVESTON	44.2	FORT BEND
DUNCANVILLE	TX	286	DALLAS	40.1	DALLAS
EASTON	TX	374	LONGVIEW	195.4	GREGG
EDMOND	OK	26-27	OKLAHOMA	370.1	OKLAHOMA

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STATION	STATE	PAGE NUMBER	SUBDIVISION	APPROX MILEPOST	COUNTY
EL PLEASANT	TX	151	GALVESTON	87.1	AUSTIN
ENID	OK	483	ENID	62.3	GARFIELD
ESTES	TX	375	LONGVIEW	202.7	HARRISON
EVADALE	TX	340	LONGVIEW	27.7	JASPER
FAIRMONT	OK	485	ENID	72.8	GARFIELD
FARMERSVILLE	TX	296	PARIS	91.0	COLLIN
FLYNN	OK	31-36	OKLAHOMA	390.5	OKLAHOMA
FLYNN	OK	36	OKLAHOMA	390.5	CLEVELAND
FORT HOOD	OK	224	LAMPASAS	246.1	BELL
FORT STOCKTON	TX	438	SAN ANGELO	881.7	PECOS
FORT WORTH	TX	89-90	FORT WORTH	346.0	TARRANT
FOSTORIA	TX	322	CONROE	89.6	MONTGOMERY
FUQUA	TX	328	CONROE	117.7	LIBERTY
GAINESVILLE	TX	64-66	OKLAHOMA	411.3	COOKE
GALVESTON	TX	168-171	GALVESTON	2.2	GALVESTON
GARLAND	TX	301	PARIS	66.4	DALLAS
GARY	TX	367	LONGVIEW	161.7	PANOLA
GENE AUTRY	OK	54	OKLAHOMA	460.3	CARTER
GENERAL MOTORS	OK	33	OKLAHOMA	5.0	OKLAHOMA
GIRVIN	TX	430	SAN ANGELO	849.6	PECOS
GOBER	TX	230	LAMPASAS	219.9	BELL
GODLEY	TX	263	CRESSON	10.3	JOHNSON
GOLDSBORO	TX	191	LAMPASAS	402.9	COLEMAN
GOLDTHWAITE	TX	211	LAMPASAS	313.3	MILLS
GRANBURY	TX	239	DUBLIN	36.5	HOOD
GRIMES	TX	182	LAMPASAS	445.8	NOLAN
GUTHRIE	OK	23	OKLAHOMA	352.6	LOGAN
GUTHRIE	OK	494	ENID	116.7	LOGAN
HALE	TX	285	DALLAS	45.7	DALLAS
HARRIET	TX	399	SAN ANGELO	63.1	TOM GREEN
HASLET	TX	86	FORT WORTH	362.0	TARRANT
HASTINGS	TX	177	HOUSTON	4.1	BRAZORIA
HEIDENHEIMER	TX	123	GALVESTON	212.3	BELL
HILLSDALE	OK	480	ENID	47.8	GARFIELD
HITCHCOCK	TX	166	GALVESTON	14.1	GALVESTON
HONEA	TX	317	CONROE	63.8	MONTGOMERY
HONEY ISLAND	TX	332	CONROE	135.5	HARDIN
HORTON	TX	351	LONGVIEW	84.2	JASPER
HOUSTON	TX	174	HOUSTON	20.3	HARRIS
HOVEY	TX	445	SAN ANGELO	917.2	BREWSTER
HOYTE	TX	129	GALVESTON	181.3	MILAM
IMMERMERE	TX	244	DUBLIN	62.5	ERATH
ISLAND	TX	168	GALVESTON	4.1	GALVESTON
ITUNA	OK	481	ENID	53.5	GARFIELD
J & E JCT.	TX	345	LONGVIEW	53.0	JASPER
JASPER	TX	349	LONGVIEW	73.6	JASPER
JET	OK	477	ENID	31.8	ALFALFA
JOSHUA	TX	94	FORT WORTH	325.3	JOHNSON
JUSTIN	TX	84	FORT WORTH	370.6	DENTON
KEENAN	TX	317	CONROE	60.6	MONTGOMERY
KEITHTON	TX	348	LONGVIEW	67.1	JASPER
KEMPNER	TX	221	LAMPASAS	263.7	LAMPASAS
KILDARE	OK	7	OKLAHOMA	281.2	KAY
KILLEEN	TX	225	LAMPASAS	243.5	BELL
KIOWA	KS	471	ENID	0.0	BARBER
KIRBYVILLE	TX	345	LONGVIEW	52.4	JASPER
KNOWD	TX	123	GALVESTON	215.7	BELL

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STATION	STATE	PAGE NUMBER	SUBDIVISION	APPROX MILEPOST	COUNTY
KOPPERL	TX	103	FORT WORTH	294.4	BOSQUE
KOUNTZE	TX	333	CONROE	143.8	HARDIN
KRUM	TX	82	FORT WORTH	383.5	DENTON
LAMPASAS	TX	219	LAMPASAS	273.7	LAMPASAS
LANDES	TX	141	GALVESTON	132.9	WASHINGTON
LAWN	TX	190	LAMPASAS	409.5	TAYLOR
LAWRIE	OK	22	OKLAHOMA	347.2	LOGAN
LE VERTE	TX	343	LONGVIEW	43.2	JASPER
LELAVAL	TX	331	CONROE	133.4	HARDIN
LEWISVILLE	TX	270	DALLAS	90.8	DENTON
LOEB JCT.	TX	383	SILSBEE	10.3	HARDIN
LOMETA	TX	215	LAMPASAS	291.7	LAMPASAS
LONGVIEW	TX	376-377	LONGVIEW	207.6	GREGG
LOVELL	OK	490	ENID	95.1	LOGAN
LUMBERTON	TX	382	SILSBEE	14.1	HARDIN
MAGLAB	TX	286	DALLAS	43.4	DALLAS
MANHATTAN	TX	111	FORT WORTH	255.0	MC LENNAN
MANVEL	TX	160	GALVESTON	36.0	BRAZORIA
MARIETTA	OK	60	OKLAHOMA	433.1	LOVE
MARLAND	OK	11	OKLAHOMA	300.3	NOBLE
MARSHALL	OK	488	ENID	88.4	LOGAN
MARTIN LAKE JCT.	TX	371	LONGVIEW	184.9	PANOLA
MC CAMEY	TX	428	SAN ANGELO	838.6	UPTON
MC GREGOR	TX	113	FORT WORTH	243.4	MC LENNAN
MERIDIAN	TX	105	FORT WORTH	280.7	BOSQUE
MERTZON	TX	410	SAN ANGELO	745.7	IRION
MIDLOTHIAN	TX	289	DALLAS	26.9	ELLIS
MILANO	TX	131	GALVESTON	174.4	MILAM
MILES	TX	397	SAN ANGELO	54.2	RUNNELS
MINCHIN	TX	268	DALLAS	102.4	DENTON
MONTGOMERY	TX	316	CONROE	55.6	MONTGOMERY
MOODY	TX	115	FORT WORTH	233.5	MC LENNAN
MOORE	OK	36	OKLAHOMA	393.2	CLEVELAND
MORGAN	TX	104	FORT WORTH	287.8	BOSQUE
MULHALL	OK	20	OKLAHOMA	338.8	LOGAN
MULLEN	TX	209	LAMPASAS	323.6	MILLS
MYKAWA	TX	175	HOUSTON	14.0	HARRIS
NASH	OK	478	ENID	40.0	GRANT
NAVASOTA	TX	310	CONROE	28.1	GRIMES
NEUVILLE	TX	361	LONGVIEW	131.4	SHELBY
NEWKIRK	OK	6	OKLAHOMA	275.8	KAY
NOBLE	OK	39	OKLAHOMA	408.1	CLEVELAND
NOELKE	TX	412	SAN ANGELO	756.1	IRION
NOLANVILLE	TX	226	LAMPASAS	235.7	BELL
NORMAN	OK	38	OKLAHOMA	401.8	CLEVELAND
NORTH FORT WORTH	TX	89	FORT WORTH	348.8	TARRANT
NOVICE	TX	192	LAMPASAS	396.5	COLEMAN
NOWERS	OK	29-30	OKLAHOMA	380.6	OKLAHOMA
OAK CLIFF	TX	285	DALLAS	49.6	DALLAS
OBREGON	TX	199	LAMPASAS	364.2	COLEMAN
OGLES	TX	217	LAMPASAS	283.3	LAMPASAS
OKLAHOMA CITY	OK	29-30	OKLAHOMA	384.0	OKLAHOMA
ORCHARD	TX	153	GALVESTON	76.2	FORT BEND
ORLANDO	OK	19	OKLAHOMA	332.7	LOGAN
OTOE	OK	15	OKLAHOMA	312.7	NOBLE
OVERBROOK	OK	58	OKLAHOMA	443.0	LOVE

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STATION	STATE	PAGE NUMBER	SUBDIVISION	APPROX MILEPOST	COUNTY
PAISANO	TX	453	SAN ANGELO	956.9	PRESIDIO
PAISANO JCT.	TX	453	SAN ANGELO	956.7	PRESIDIO
PAOLI	OK	46	OKLAHOMA	502.6	GARVIN
PAULS VALLEY	OK	47-48	OKLAHOMA	495.6	GARVIN
PEARLAND	TX	175	HOUSTON	10.0	BRAZORIA
PENDLETON	TX	116	FORT WORTH	225.4	BELL
PERDIZ	TX	459	SAN ANGELO	984.5	PRESIDIO
PERRY	OK	17	OKLAHOMA	321.6	NOBLE
PHILLIPSBURG	TX	143	GALVESTON	120.1	WASHINGTON
PINELAND	TX	354	LONGVIEW	97.5	SABINE
PLANTERSVILLE	TX	313	CONROE	43.4	GRIMES
PLATA	TX	460	SAN ANGELO	993.7	PRESIDIO
PONCA CITY	OK	8-9	OKLAHOMA	288.9	KAY
PONDER	TX	83	FORT WORTH	377.3	DENTON
PRESIDIO	TX	467	SAN ANGELO	1026.7	PRESIDIO
PRIMROSE	TX	233	DUBLIN	8.4	TARRANT
PROCTOR	TX	251	DUBLIN	95.3	COMANCHE
PURCELL	OK	43	OKLAHOMA	517.5	MC CLAIN
PURCELL	OK	41-42	OKLAHOMA	417.3	MC CLAIN
QUINN	TX	341	LONGVIEW	30.1	JASPER
RANKIN	TX	425	SAN ANGELO	819.9	UPTON
RAYBURN	TX	326	CONROE	105.5	LIBERTY
RAYFORD	OK	52	OKLAHOMA	473.3	MURRAY
REBECCA	TX	356	LONGVIEW	109.6	SAN AUGUSTINE
RED ROCK	OK	12	OKLAHOMA	306.8	NOBLE
REDBIRD	TX	286	DALLAS	42.2	DALLAS
REINHARDT	TX	276	DALLAS	60.3	DALLAS
RICHARDSON	TX	274	DALLAS	70.3	DALLAS
RICHMOND	TX	156	GALVESTON	63.3	FORT BEND
RICKER	TX	258	DUBLIN	344.4	BROWN
RICKER	TX	205	LAMPASAS	344.4	BROWN
RIO VISTA	TX	100	FORT WORTH	310.3	JOHNSON
ROGANVILLE	TX	347	LONGVIEW	62.4	JASPER
ROGERS	TX	125	GALVESTON	204.7	BELL
ROMAYOR	TX	327	CONROE	111.0	LIBERTY
ROSENBERG	TX	155	GALVESTON	65.8	FORT BEND
ROWENA	TX	396	SAN ANGELO	45.6	RUNNELS
SACHSE	TX	300	PARIS	71.6	DALLAS
SAGINAW	TX	88	FORT WORTH	353.9	TARRANT
SAN ANGELO	TX	401, 403	SAN ANGELO	714.5	TOM GREEN
SAN ANGELO	TX	401	SAN ANGELO	69.6	TOM GREEN
SAN ANGELO JUNCTION	TX	197	LAMPASAS	373.5	COLEMAN
SAN ANGELO JUNCTION	TX	387	SAN ANGELO	0.0	COLEMAN
SAN AUGUSTINE	TX	359	LONGVIEW	120.4	SAN AUGUSTINE
SANGER	TX	80	FORT WORTH	392.2	DENTON
SANTA ANNA	TX	198	LAMPASAS	369.7	COLEMAN
SCOFIELD	TX	306	CONROE	5.4	BURLESON
SEALY	TX	150	GALVESTON	94.6	AUSTIN
SEAMAN	TX	324	CONROE	98.1	LIBERTY
SECURITY	TX	321	CONROE	85.0	MONTGOMERY
SEWARD	OK	24-25	OKLAHOMA	360.1	LOGAN
SHAWNEE	OK	73	OKLAHOMA	132.0	POTTAWATOMIE
SILSBEE	TX	335-336	CONROE	152.2	HARDIN
SILSBEE	TX	339	LONGVIEW	21.0	HARDIN
SILSBEE	TX	381	SILSBEE	21.0	HARDIN
SILVER VALLEY	TX	193	LAMPASAS	391.0	COLEMAN



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STATION	STATE	PAGE NUMBER	SUBDIVISION	APPROX MILEPOST	COUNTY
SOMERVILLE	TX	137-139	GALVESTON	141.4	BURLESON
SOMERVILLE	TX	305	CONROE	0.0	BURLESON
SOUTH SHAWNEE	OK	73	OKLAHOMA	133.7	POTTAWATOMIE
STEPHENVILLE	TX	246	DUBLIN	72.3	ERATH
SULPHUR SPUR JCT.	TX	436	SAN ANGELO	869.4	PECOS
T&NO CROSSING	TX	174	HOUSTON	19.4	HARRIS
TALPA	TX	391	SAN ANGELO	20.9	COLEMAN
TANKERSLEY	TX	407	SAN ANGELO	732.4	TOM GREEN
TATUM	TX	372	LONGVIEW	187.8	RUSK
TECIFIC	TX	181	LAMPASAS	454.5	NOLAN
TEMPLE	TX	118-120	FORT WORTH	218.2	BELL
TEMPLE	TX	230	LAMPASAS	218.2	BELL
TEMPLE	TX	122	GALVESTON	218.2	BELL
TENAHA	TX	365	LONGVIEW	151.6	SHELBY
TESCO	TX	181	LAMPASAS	450.1	NOLAN
TEXAS CITY JCT.	TX	166	GALVESTON	11.0	GALVESTON
THACKERVILLE	OK	62	OKLAHOMA	423.1	LOVE
THOMPSONS	TX	158	GALVESTON	50.4	FORT BEND
TIMBER	TX	321	CONROE	83.1	MONTGOMERY
TINAJA	TX	456	SAN ANGELO	969.3	PRESIDIO
TITLEY	TX	449	SAN ANGELO	934.4	BREWSTER
TOLAND	TX	183	LAMPASAS	443.3	TAYLOR
TOLAND	TX	183	LAMPASAS	443.3	NOLAN
TOLAR	TX	241	DUBLIN	46.4	HOOD
TUSCOLA	TX	188	LAMPASAS	415.4	TAYLOR
VALERA	TX	389	SAN ANGELO	11.3	COLEMAN
VALLEY MILLS	TX	110	FORT WORTH	259.2	BOSQUE
VALLEY VIEW	TX	78	FORT WORTH	400.8	COOKE
VENABLE	TX	357	LONGVIEW	114.9	SAN AUGUSTINE
VENUS	TX	291	DALLAS	19.6	JOHNSON
VIEW	TX	186	LAMPASAS	426.6	TAYLOR
VILLA	TX	207	LAMPASAS	330.3	MILLS
VIRGINIA POINT	TX	167	GALVESTON	6.3	GALVESTON
VOTAW	TX	329	CONROE	121.5	HARDIN
VOTH	TX	383	SILSBEE	8.5	JEFFERSON
WALLIS	TX	152	GALVESTON	80.8	AUSTIN
WAPLES	TX	238	DUBLIN	30.7	HOOD
WARD SPUR	TX	290	DALLAS	23.7	ELLIS
WAUKEGAN	TX	320	CONROE	79.1	MONTGOMERY
WAYNE	OK	44-45	OKLAHOMA	510.2	MC CLAIN
WESTCLIFF	TX	232	DUBLIN	1.5	TARRANT
WHITE EAGLE	OK	9	OKLAHOMA	292.4	KAY
WHITE ROCK	TX	276	DALLAS	63.7	DALLAS
WITCO	TX	417	SAN ANGELO	782.8	REAGAN
WOOD	TX	311	CONROE	33.1	GRIMES
WYLIE	TX	299	PARIS	75.8	COLLIN
WYNNEWOOD	OK	49	OKLAHOMA	488.1	MURRAY
WYNNEWOOD	OK	49	OKLAHOMA	488.1	GARVIN
YARBORO	TX	312	CONROE	37.7	GRIMES
YOUENS	TX	320	CONROE	77.0	MONTGOMERY
ZACHA JCT.	TX	276-277	DALLAS	62.6	DALLAS
ZACHA JCT.	TX	301	PARIS	62.6	DALLAS
ZEPHYR	TX	206	LAMPASAS	336.2	BROWN

# — ABBREVIATIONS —

## BRIDGES

Ab. ——— Abutment	Msy. ——— Masonry
Ap. ——— Approach	O.D. ——— Open Deck
Ar. ——— Arch	Ped. ——— Pedestal
B.D. ——— Ballast Deck	P.T. ——— Pile Trestle
Bx. ——— Box	P. ——— Pipe
Brk. ——— Brick	P. Ar. ——— Pipe Arch
Br. ——— Bridge	P. C. ——— Pre-stressed Concrete
C.I.P. ——— Cast Iron Pipe	R. C. ——— Reinforced Concrete
C. ——— Concrete	S.S.C. ——— Steel Stringers & Caps
C.P. ——— Concrete Pipe	Sto. ——— Stone
Cor. P. ——— Corrugated Pipe	S.S.P. ——— Structural Steel Pipe
D. G. ——— Deck Girder	T.R. ——— T-Rail
D. T. ——— Deck Truss	Th. G. ——— Thru-Girder
Dbl. ——— Double	Th. T. ——— Thru-Truss
Dr. Sp. ——— Draw Span	T. ——— Timber
Ext. ——— Extension	Tr. ——— Treated
Ftg. ——— Footing	Trip. ——— Triple
Hdrl. ——— Handrail	Vit. P. ——— Vitrified Pipe
I-Bm. ——— I-Beam	Wr. I. P. ——— Wrought Iron Pipe
Lg. ——— Long	

## FACILITIES

B. ——— Booth Telephone	T. ——— Turntable
C. ——— Communication	W. ——— Water
F. ——— Fuel	W. Col. ——— Water Column
O. Col. ——— Oil Column	W. T. ——— Water Tank
O. T. ——— Oil Tank	Y. ——— Wye Track
R. ——— Radio Communication	

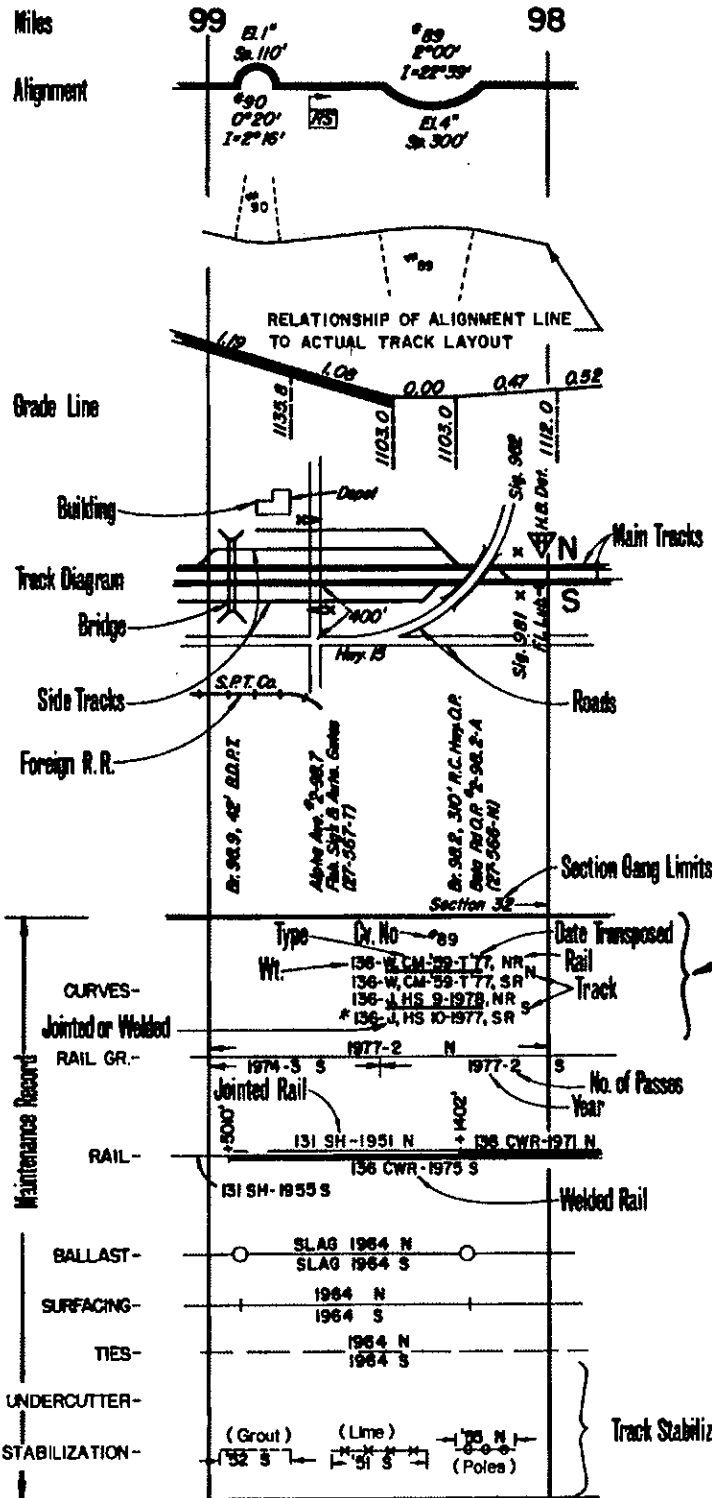
Add "s" to the above abbreviations, when the plural form is required.

## MISCELLANEOUS

A. B. S. ——— Automatic Block System	L. Sw. ——— Lock Switch
A. T. S. ——— Automatic Train Stop	M. ——— Middle Main Track
Auto. Gate. ——— Automatic Gate	N. ——— North Main Track
Ave. ——— Avenue	O. P. ——— Overpass
Blk. ——— Block	O.H. ——— Over Head
Blvd. ——— Boulevard	Pass. ——— Passenger
Bch. ——— Branch	Po. ——— Power
Br. Sig. ——— Bridge Signal	Pr. ——— Private
B. P. ——— Bumping Post	Pub. ——— Public
Cant. Sig. ——— Cantilever Signal	R. R. ——— Railroad
Co. ——— County	Ry. ——— Railway
Cr. ——— Creek	Reserv. ——— Reservoir
C. Sig. ——— Control Signal	R. S. ——— Resume Speed
C.T.C. ——— Centralized Traffic Control	Rt. ——— Right
D.C. Sw. ——— Dual Control Switch	R. W. ——— Right of Way
Div. ——— Division	Riv. ——— River
D.E. Det. ——— Dragging Equip. Detector	Rd. X-ing ——— Road Crossing
El. ——— Elevation of Curves	Sec. ——— Section
Fl. Lub. ——— Flange Lubricators	S. L. Det. ——— Shifted Load Detector
Fish. Sig. ——— Flashing Signal	Sig. ——— Signal
Fl. Lt. P. ——— Flood Light Pole	S. ——— South Main Track
Fl. Lt. T. ——— Flood Light Tower	Sp. ——— Sprial Length
Fr. ——— Freight	S. Sw. ——— Spring Switch
Gr. ——— Grade	Sto. ——— Station
Hwy. ——— Highway	St. ——— Street (also State)
H. B. Det. ——— Hot Box Detector	Trk. ——— Track
Ha. ——— House	U. P. ——— Underpass
I. ——— Total Curve Angle	U. T. P. ——— Under Track Plow
I. Sig. ——— Interlocking Signal	W. W. ——— Wig Wag
I. Sw. ——— Interlocking Switch	Yd. ——— Yard
Jct. ——— Junction	
Lav. ——— Lavatory	
Lt. ——— Left	

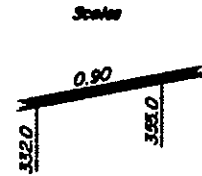
NOTE: O. H. Clearance noted in Track Chart is less than Minimum Required.  
(Minimum Required Clearance is 22'-6")

# EXPLANATIONS



1" = 3000' Horizontal scale for profile, Alignment and track (Exceptions noted)

1" = 200' Vertical scale for profile.



Ascending ruling grades for Subdivisions shown bold.

Figures on grade lines are % gradients.

Figures below grade lines are profile subgrade elevations in feet.

- + Flange Lubricator.
- x All types of signals, except bridge signals.
- o Columns, water and oil.
- ⊗ Crossing warning device, flashing signals with automatic gates.
- 75|55 Permanent slow speed sign with appropriate speeds shown.
- HS Permanent resume speed sign (green board).
- ⊕ Switch Heater.
- △ Shifted load detector.
- ⊠ Hot box detector.
- ⊞ Dragging equipment detector, arrow or arrows denote direction protected.
- YL Yard limit sign.
- SL Switch limit sign.
- #2 - 69.3 - A P.U.C. number, as shown on public road crossings.
- (26 - 007 - F) D.O.T. number as shown on public and private crossings.

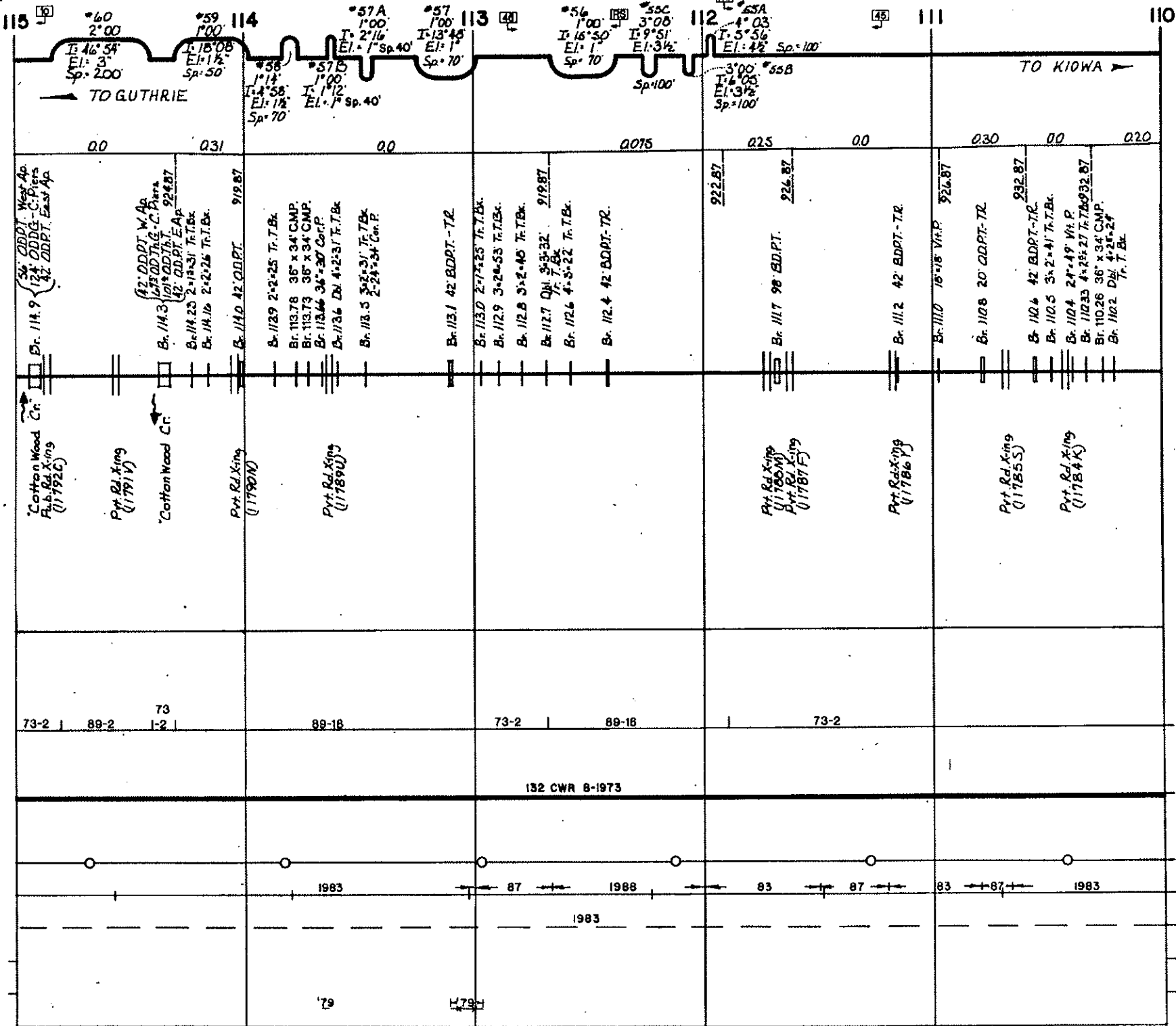
Car capacities shown on all station detail sheets are based on 50 ft. overall allowance per car.

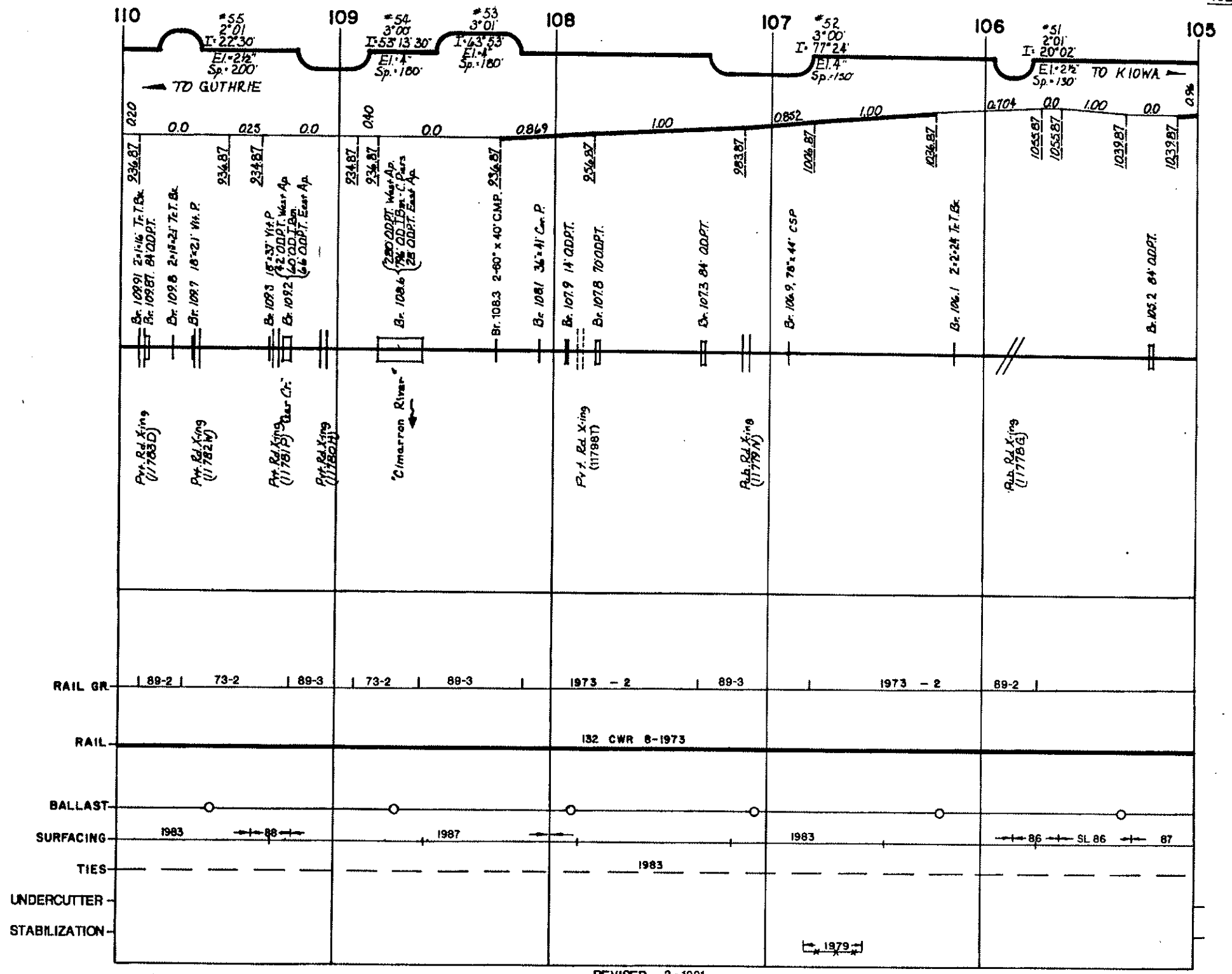
Type of Rail	Symbol
1. Flame Hardened	FH
2. Curvemaster (U.S. Steel Co.)	CM
3. Fully Heat Treated (Bethlehem Stl.)	FHT
4. Hi Si (C.F. & I. Steel Co.)	HS
5. Cromoly or Cromalloy (C.F. & I.)	CR (Cromo)
6. Japanese	JP (JAP)
7. Other	OR
(a) British Steel	followed by
(b) Chrome Vanadium	lower case
(c) Chrome 1%	letter.
(d) Open Hearth	Example:
(f) German	ORf

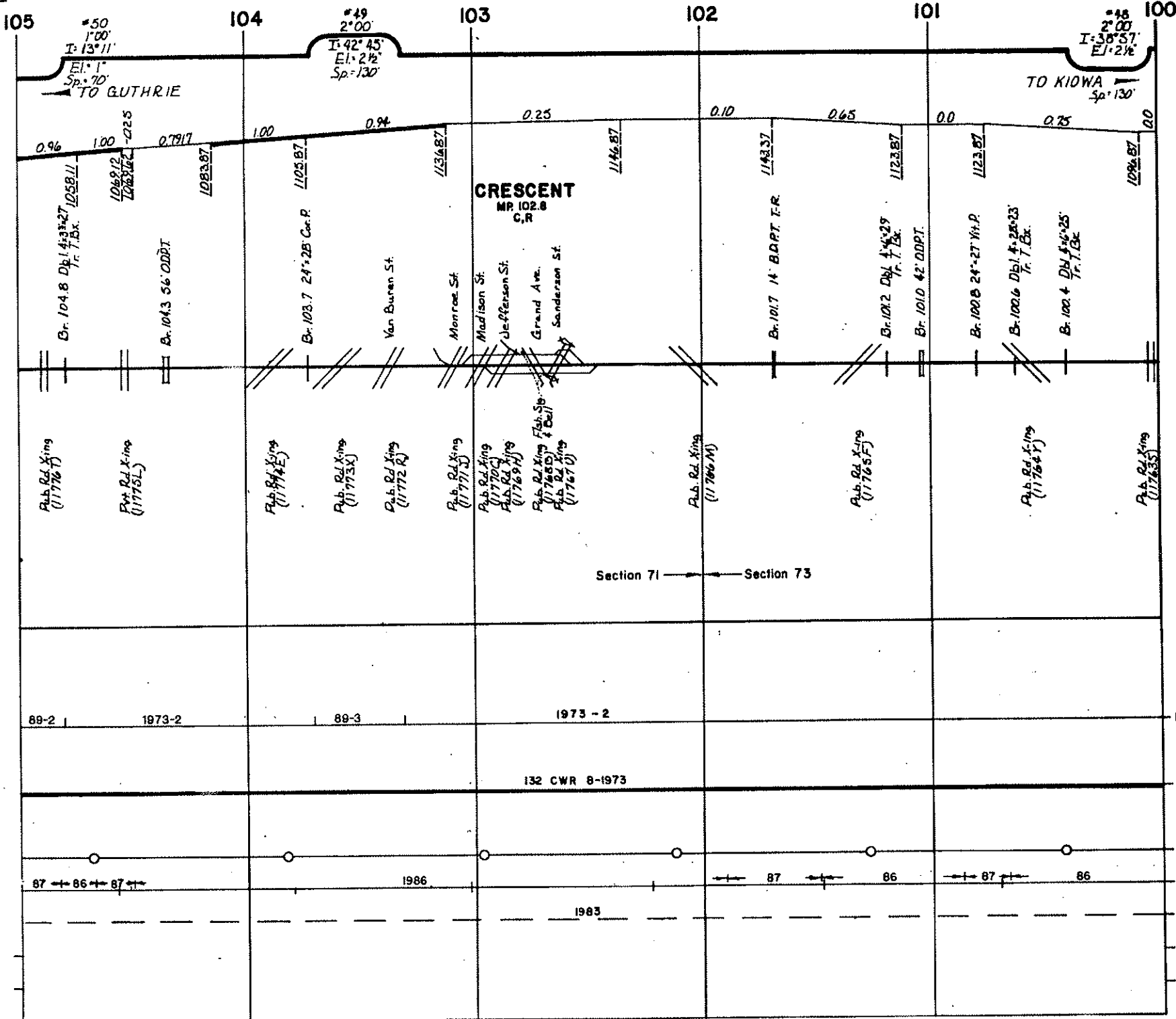
o Second Hand Rail

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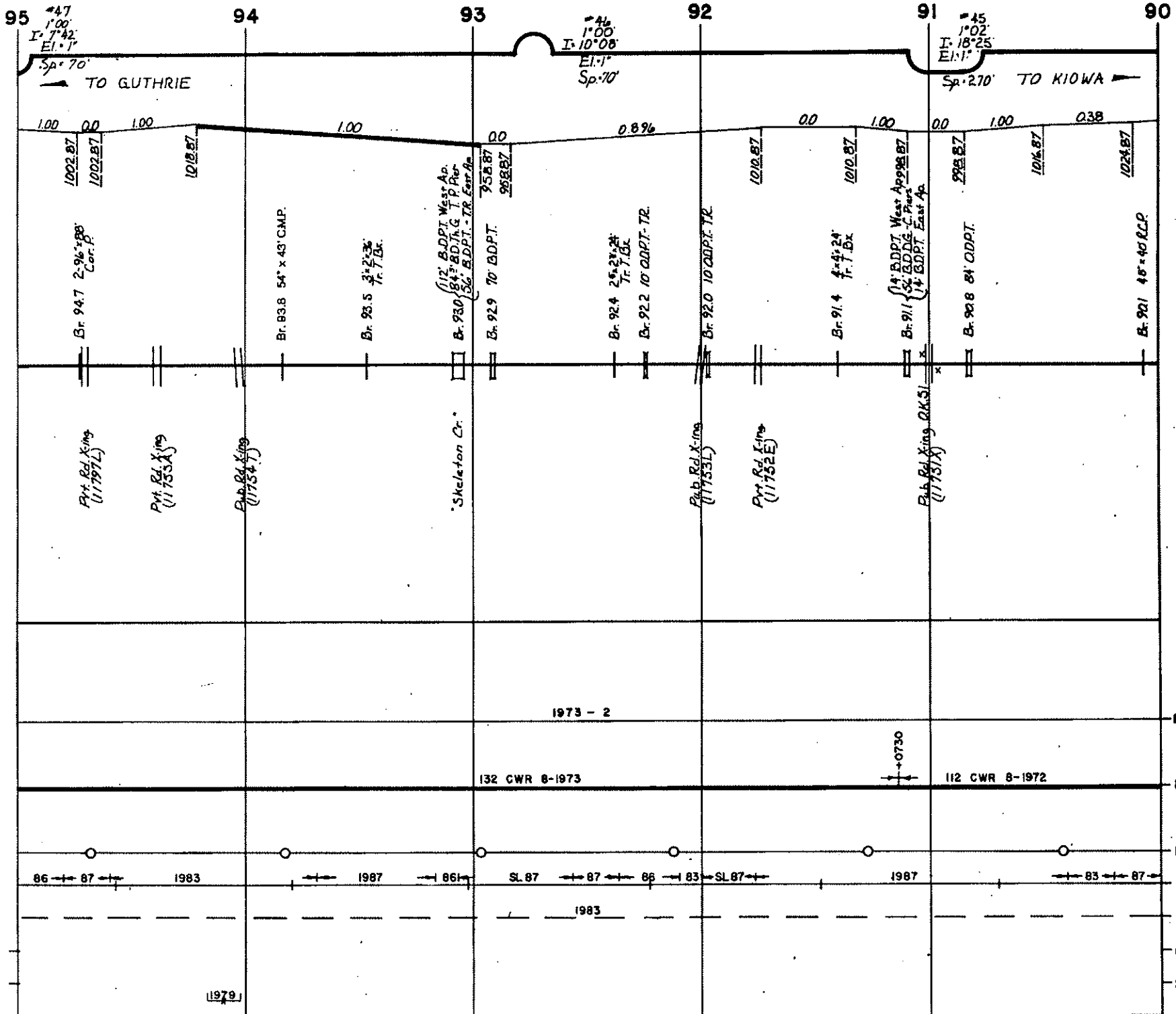


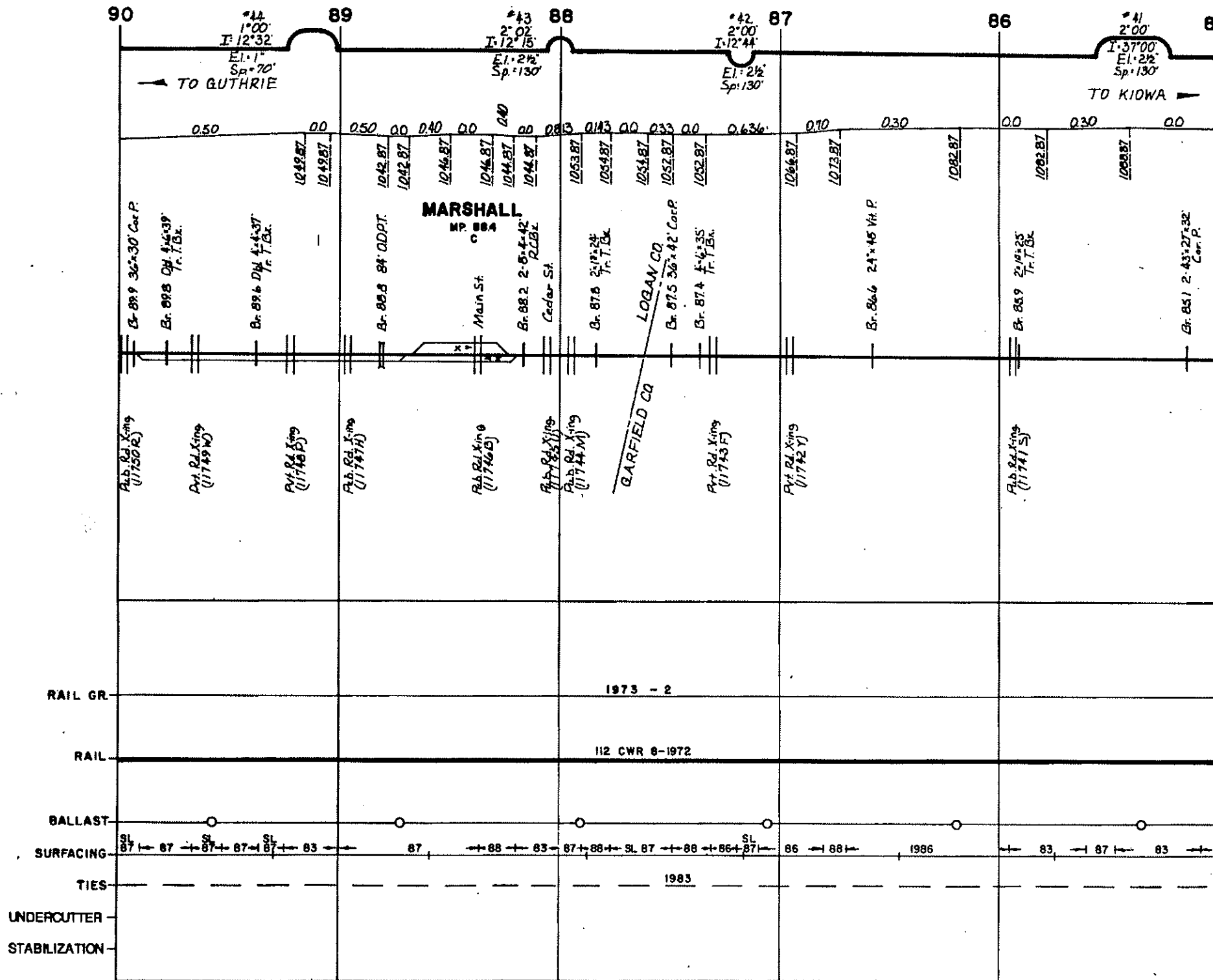


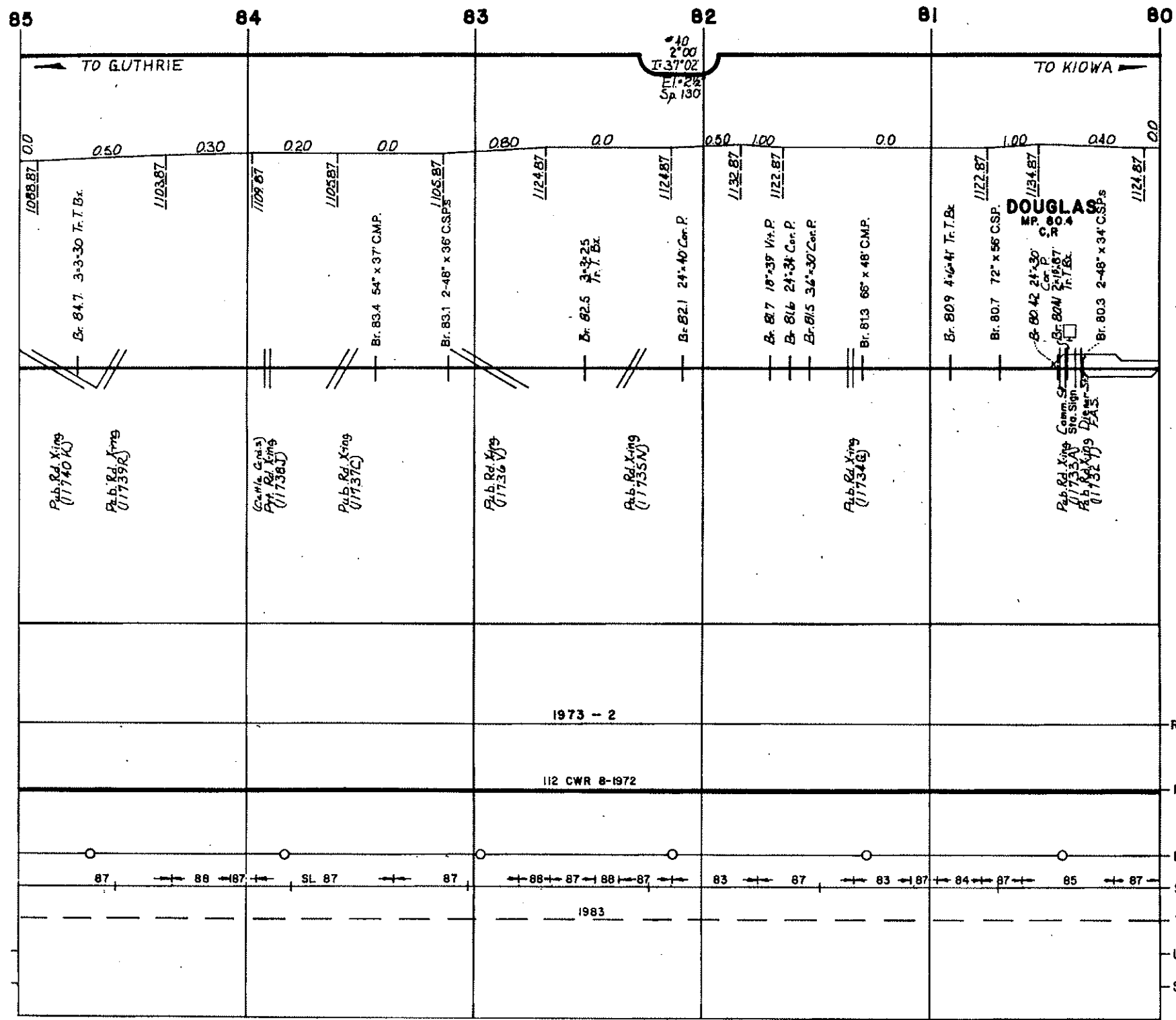


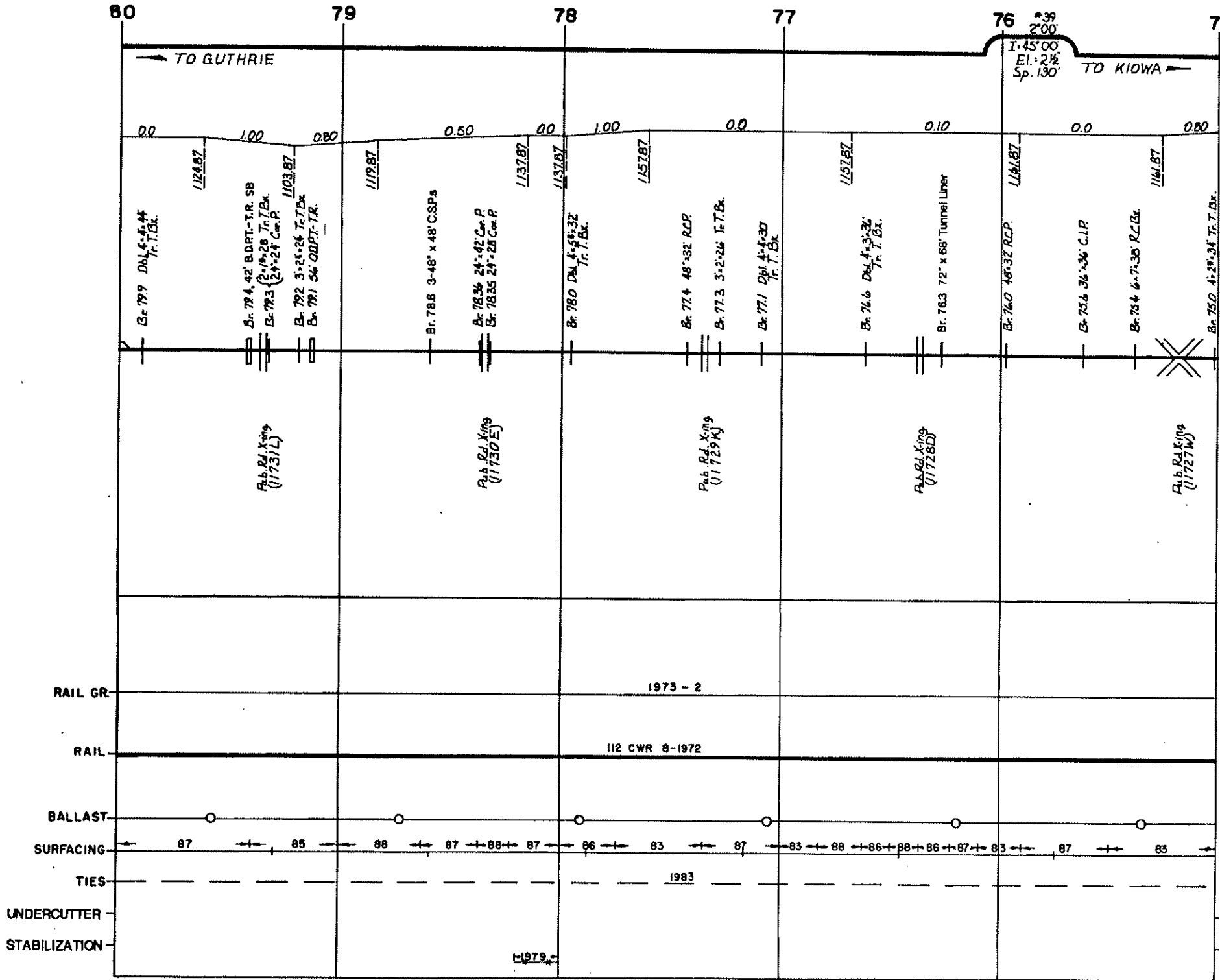




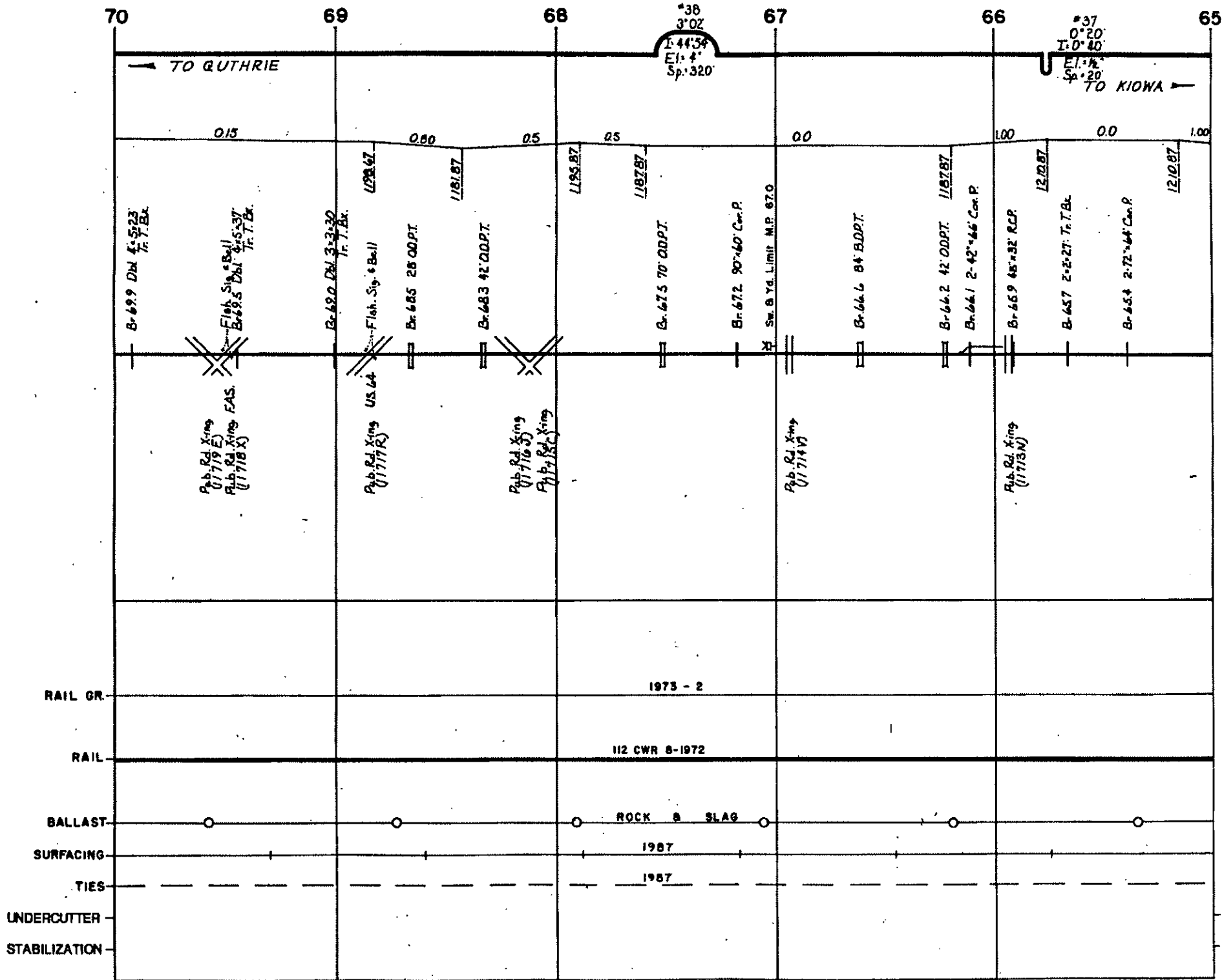






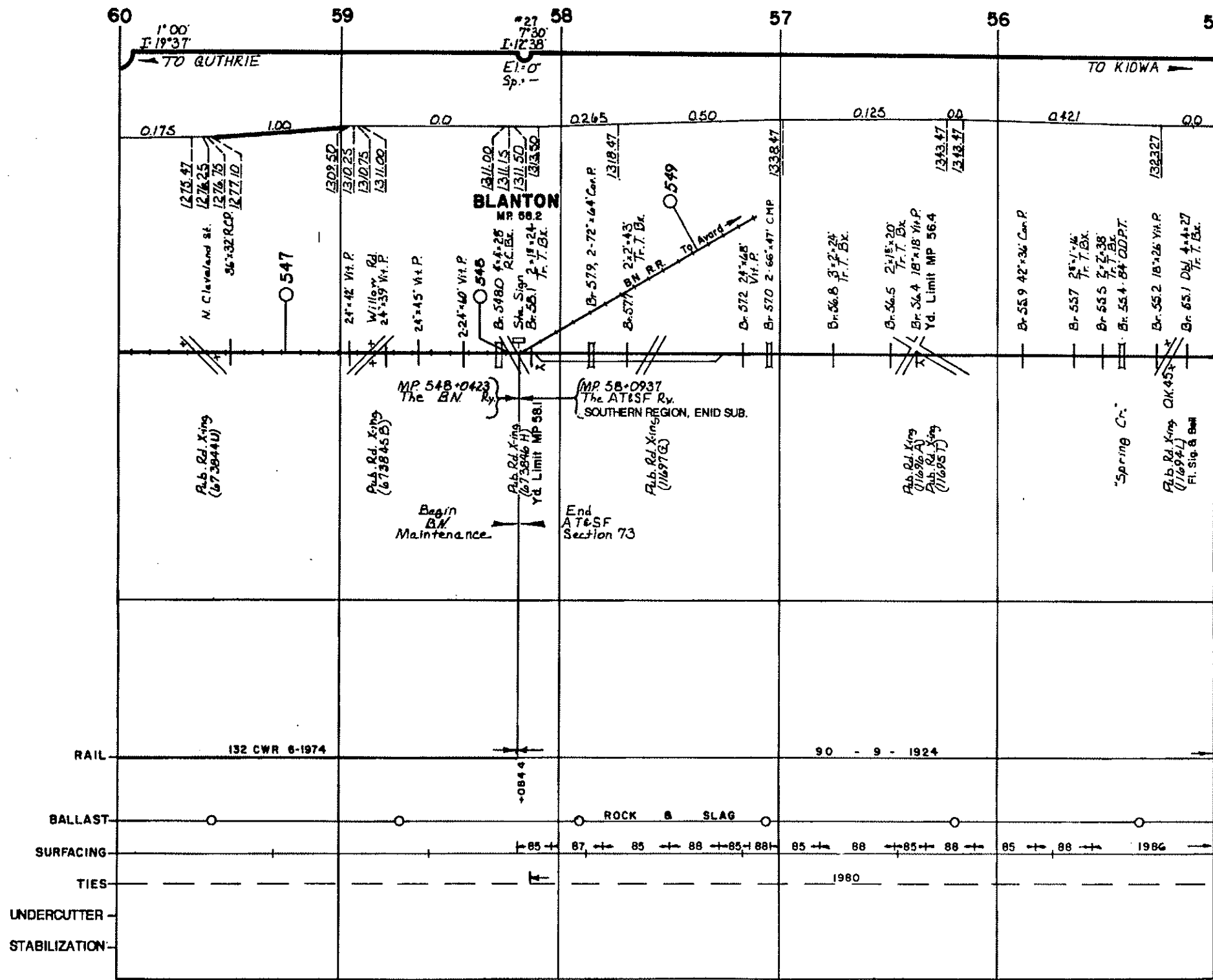


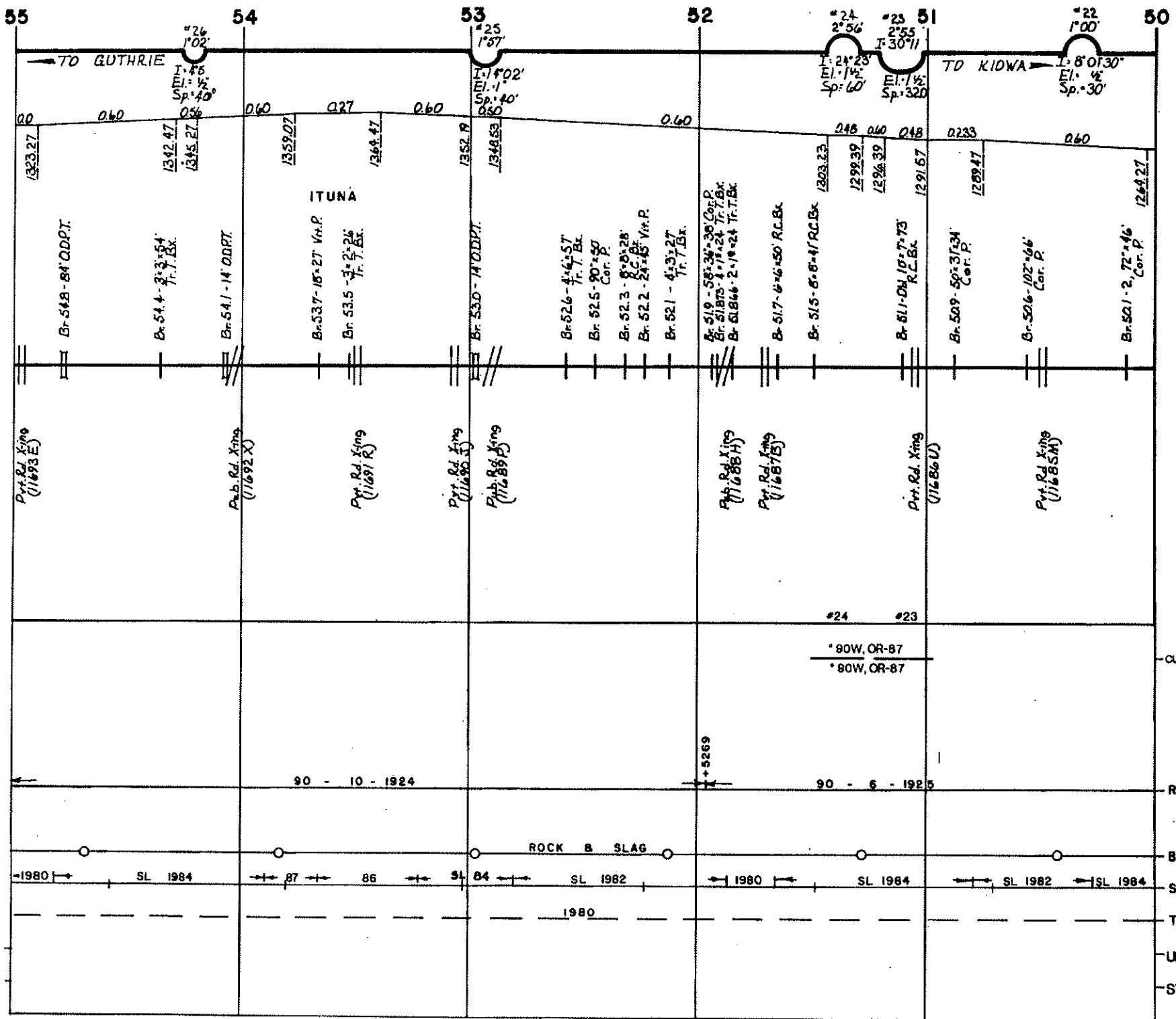


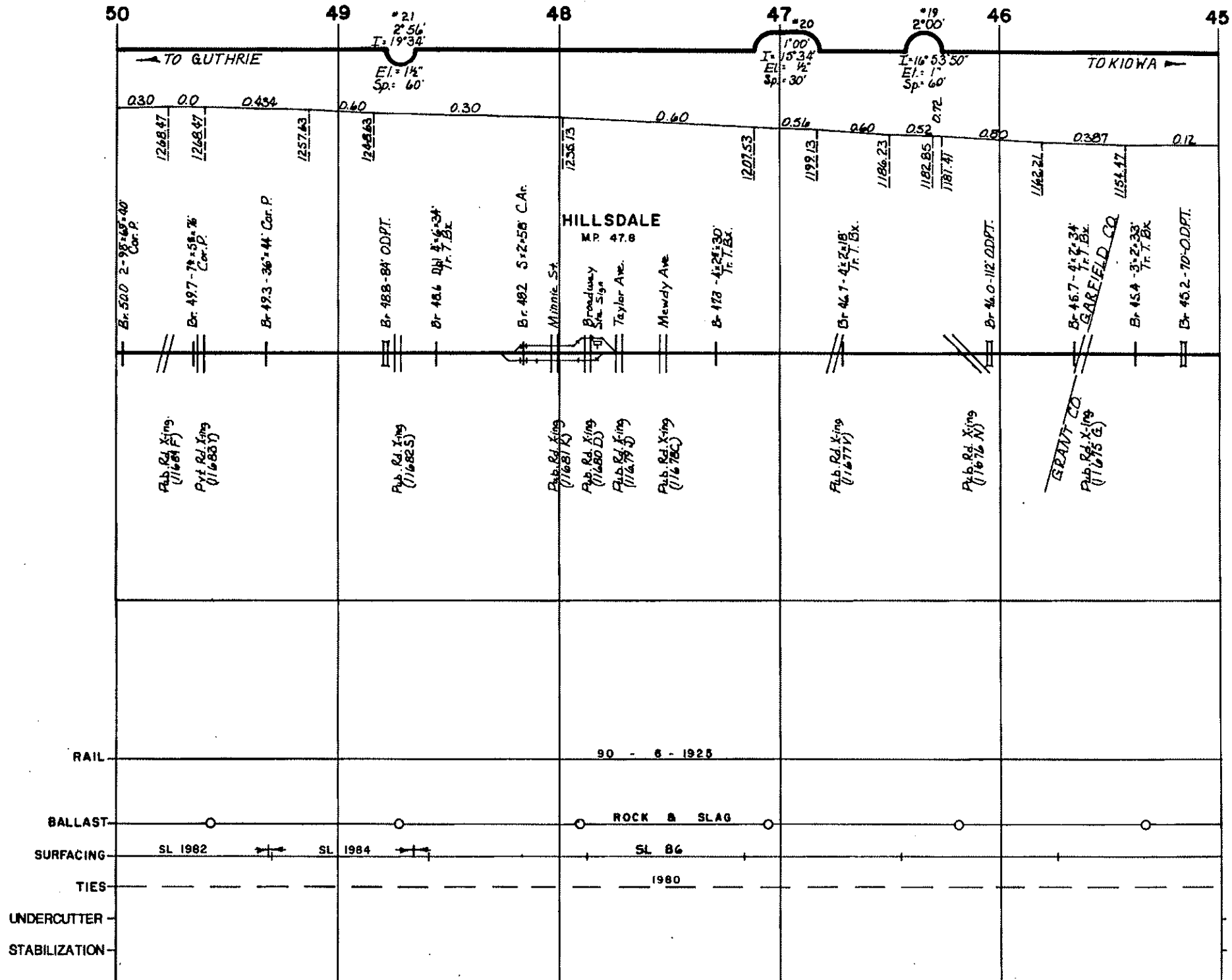


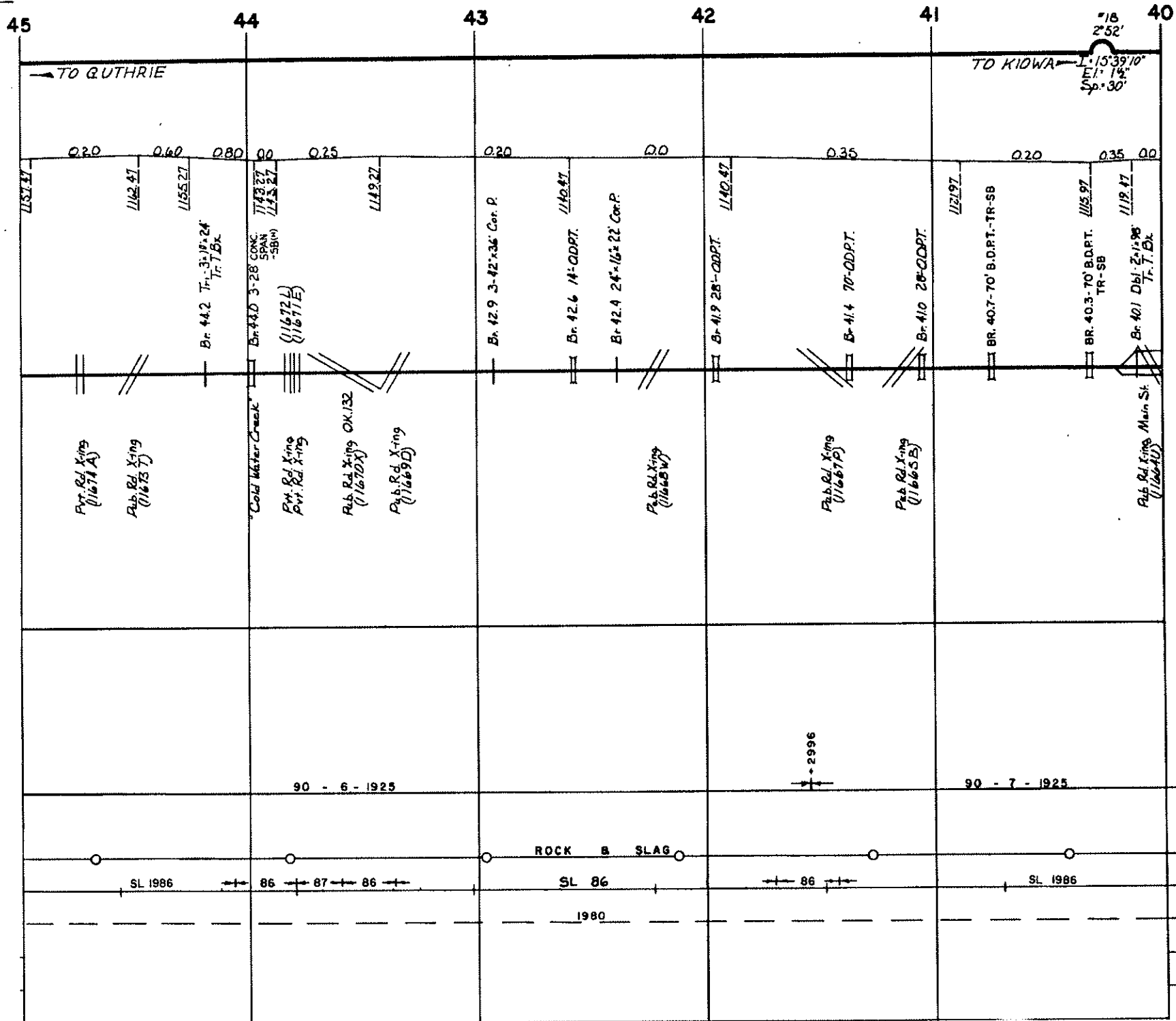


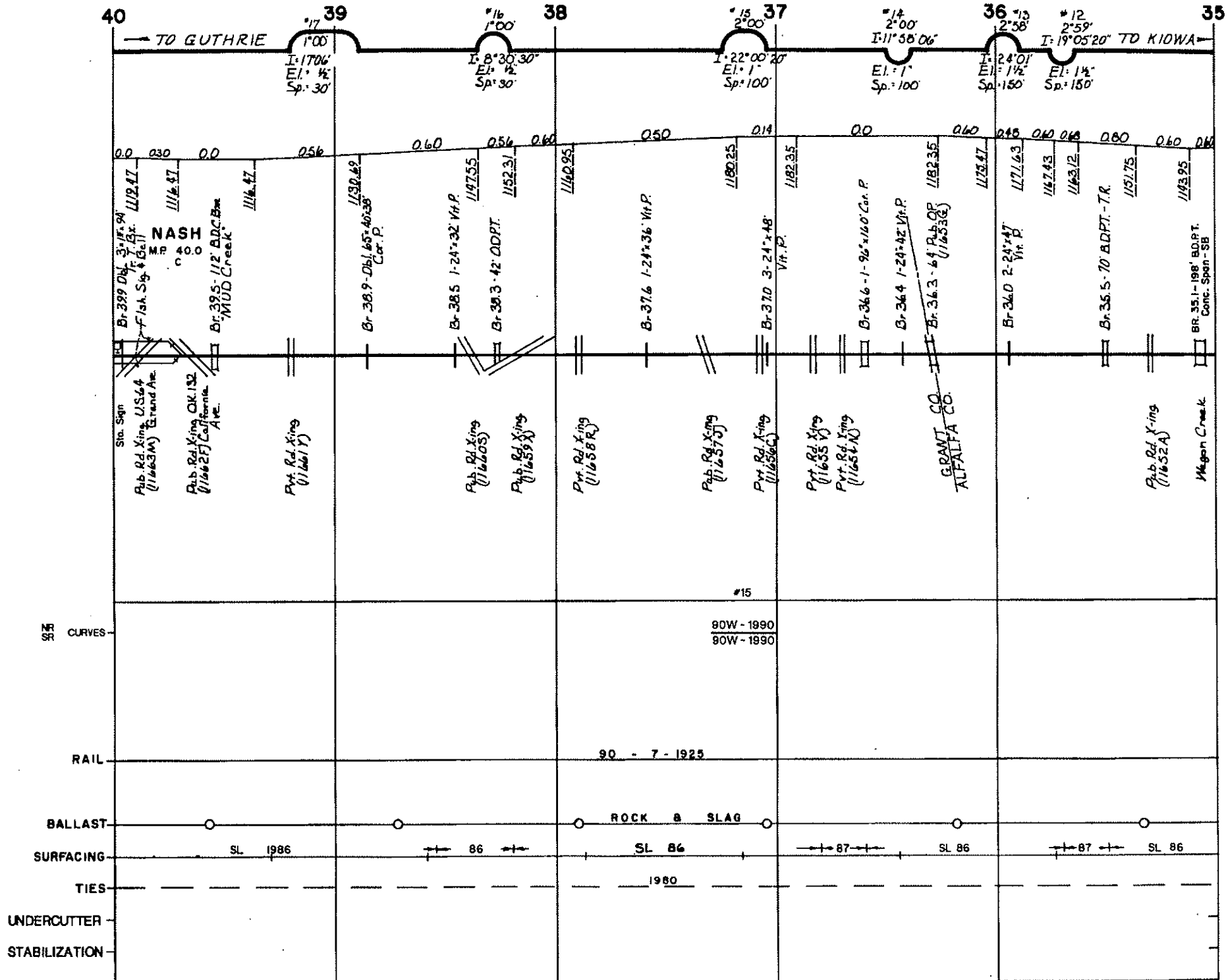




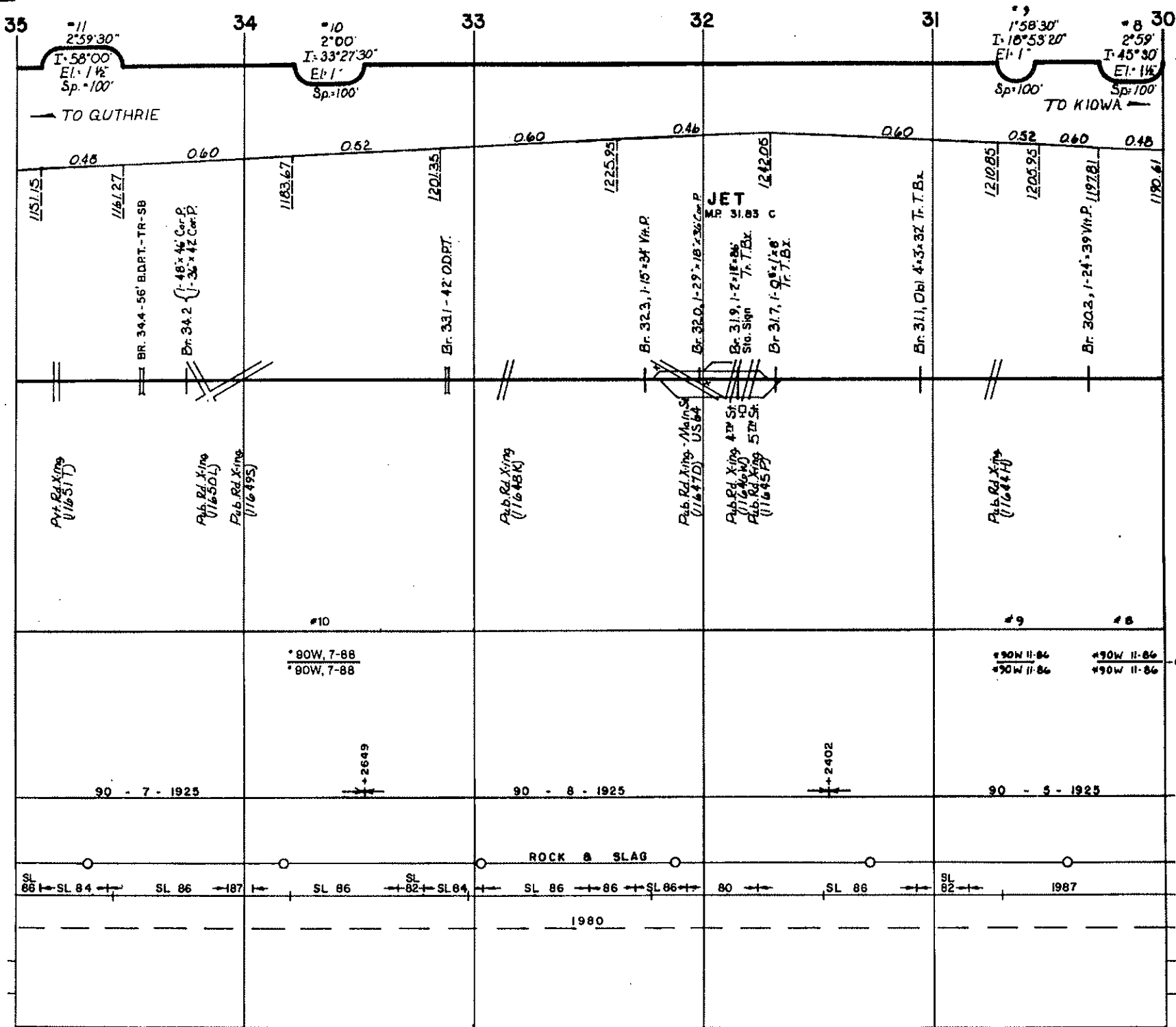




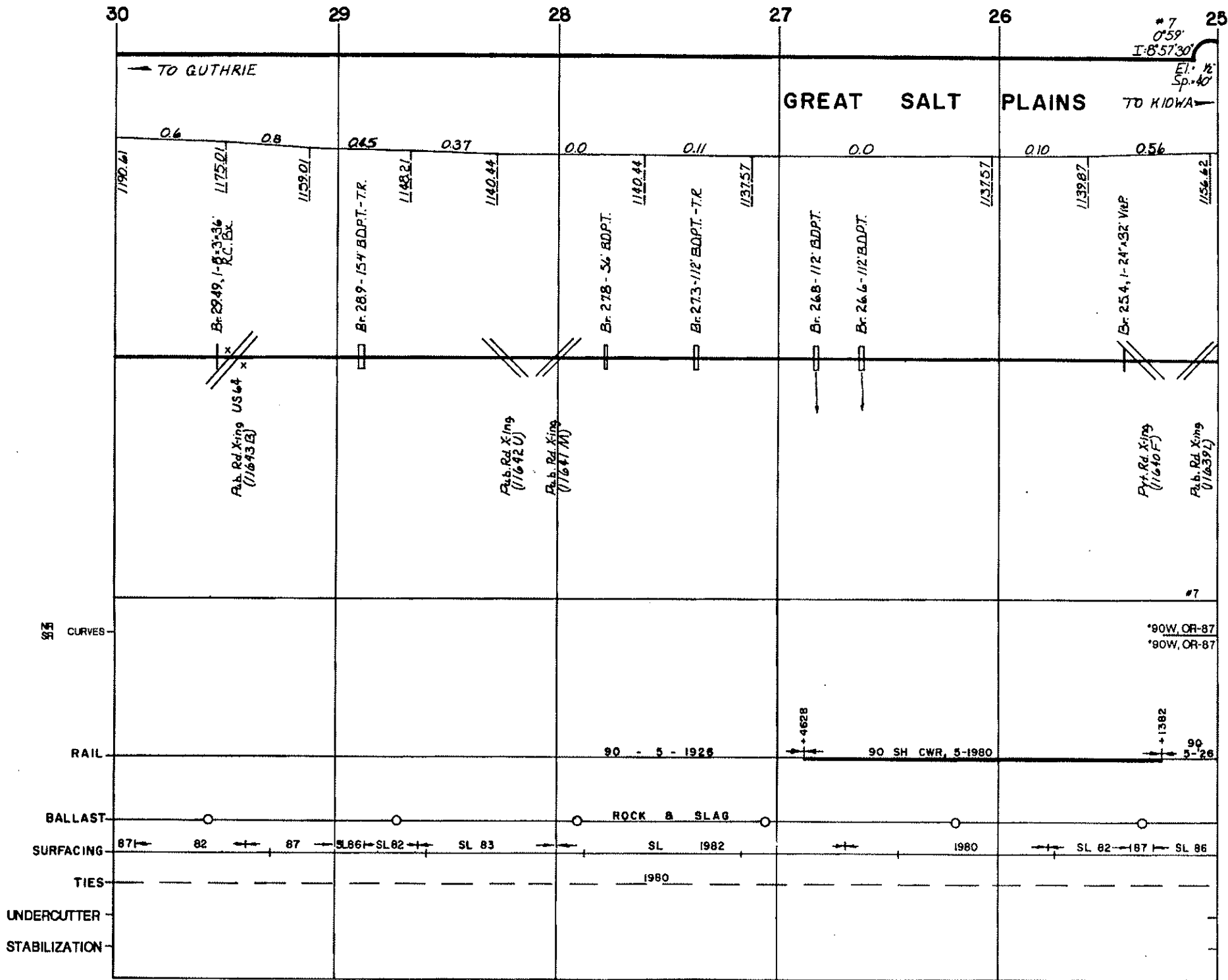


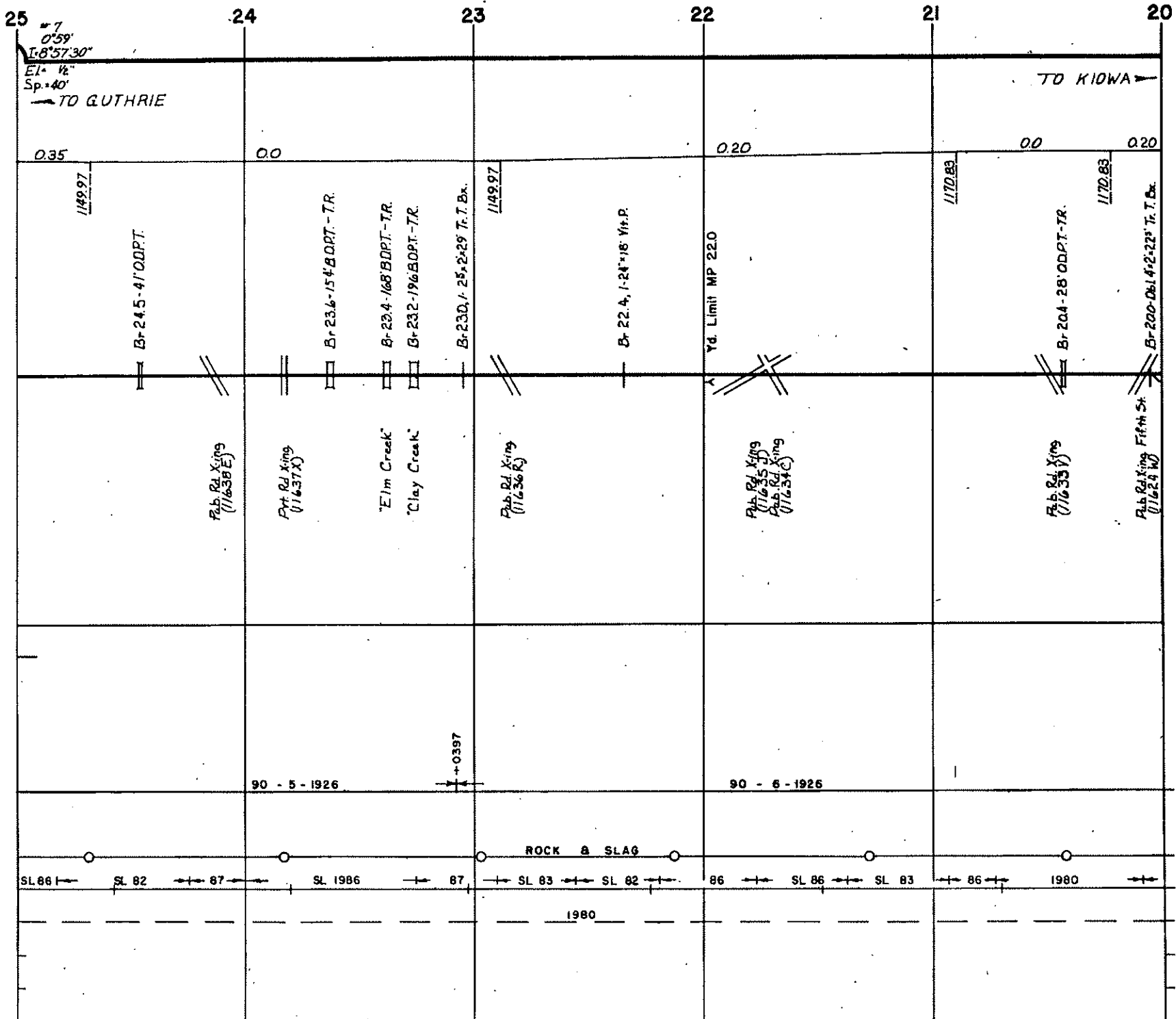


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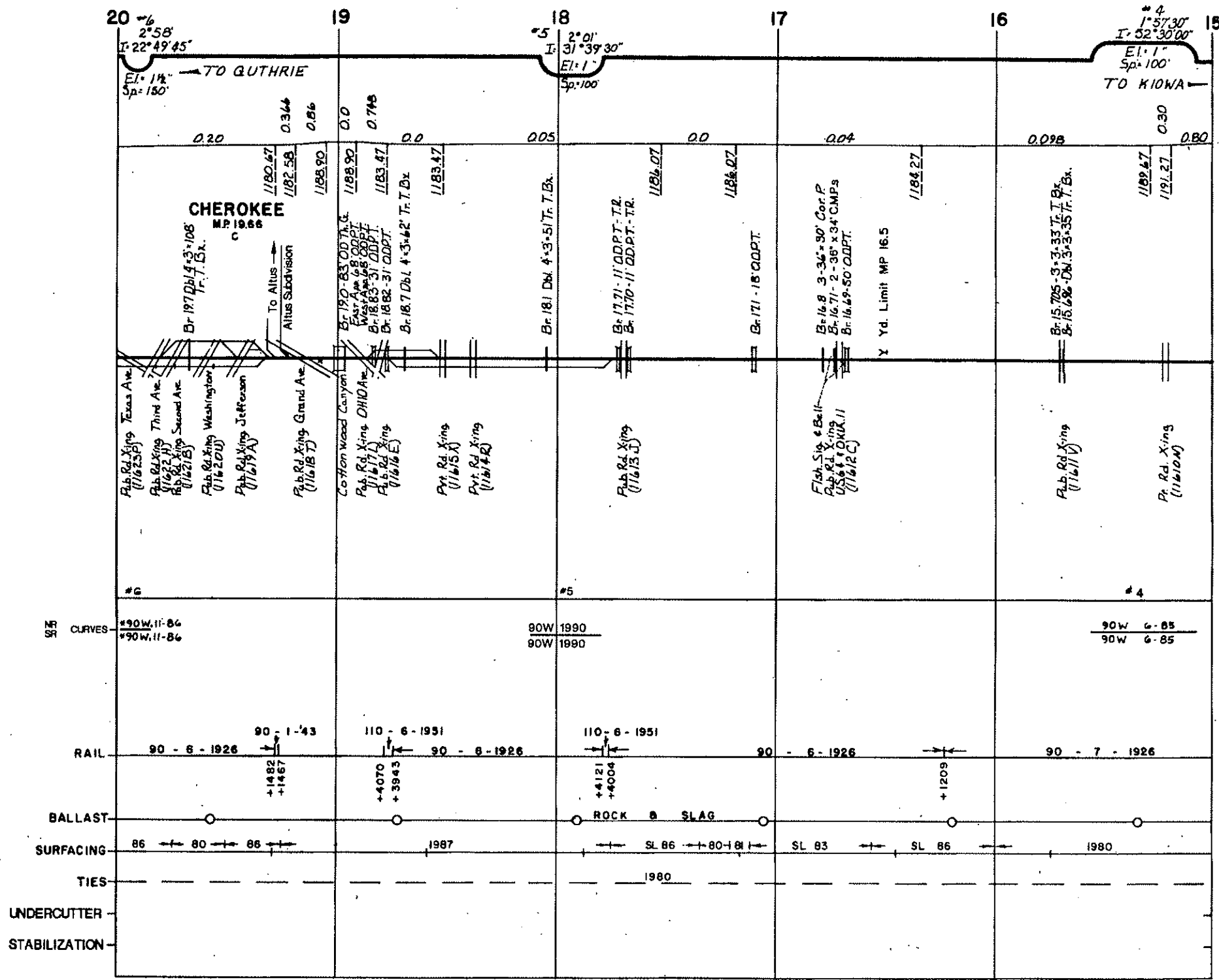


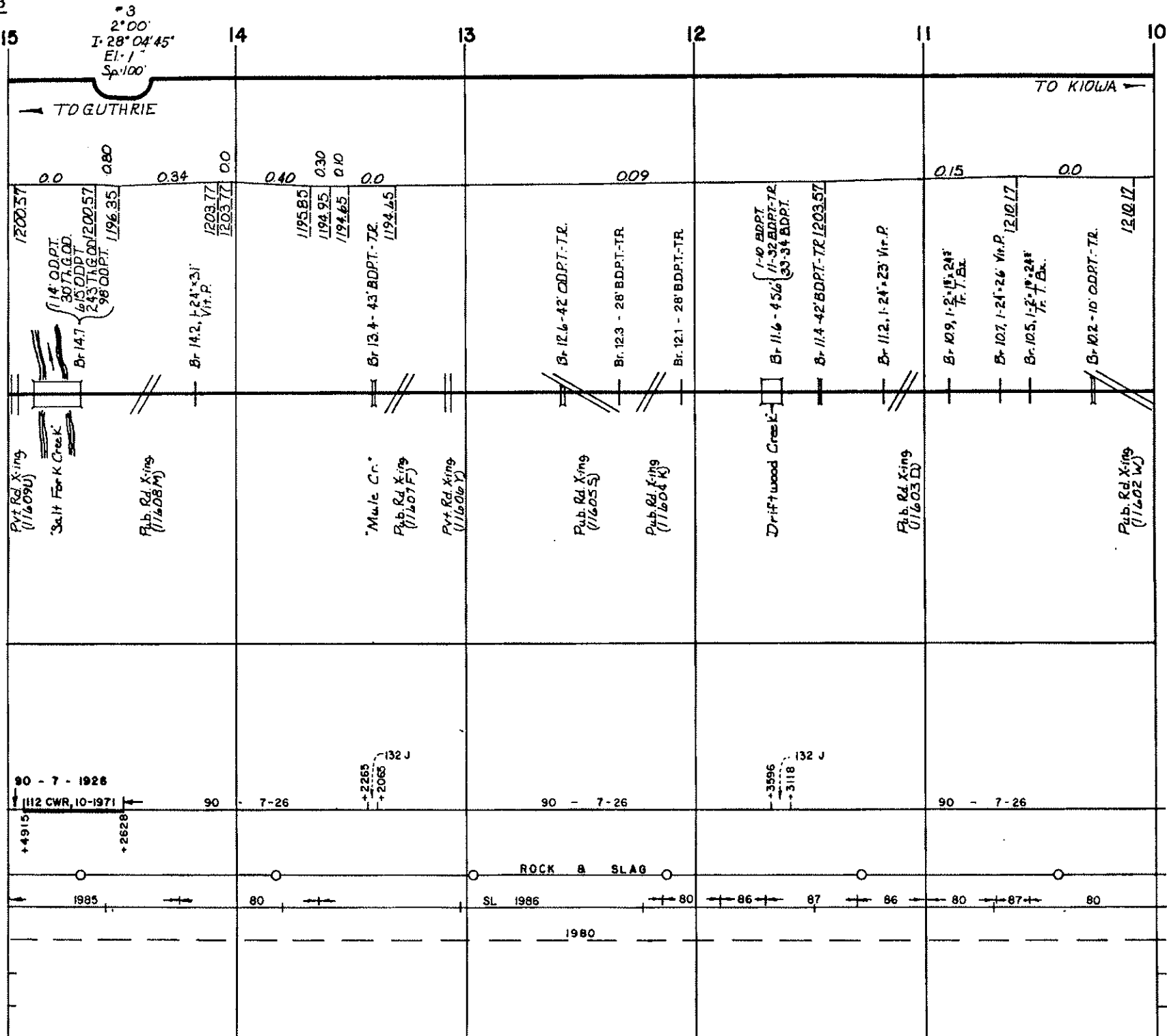
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SURFACING  
TIES  
UNDERCUTTER  
STABILIZATION



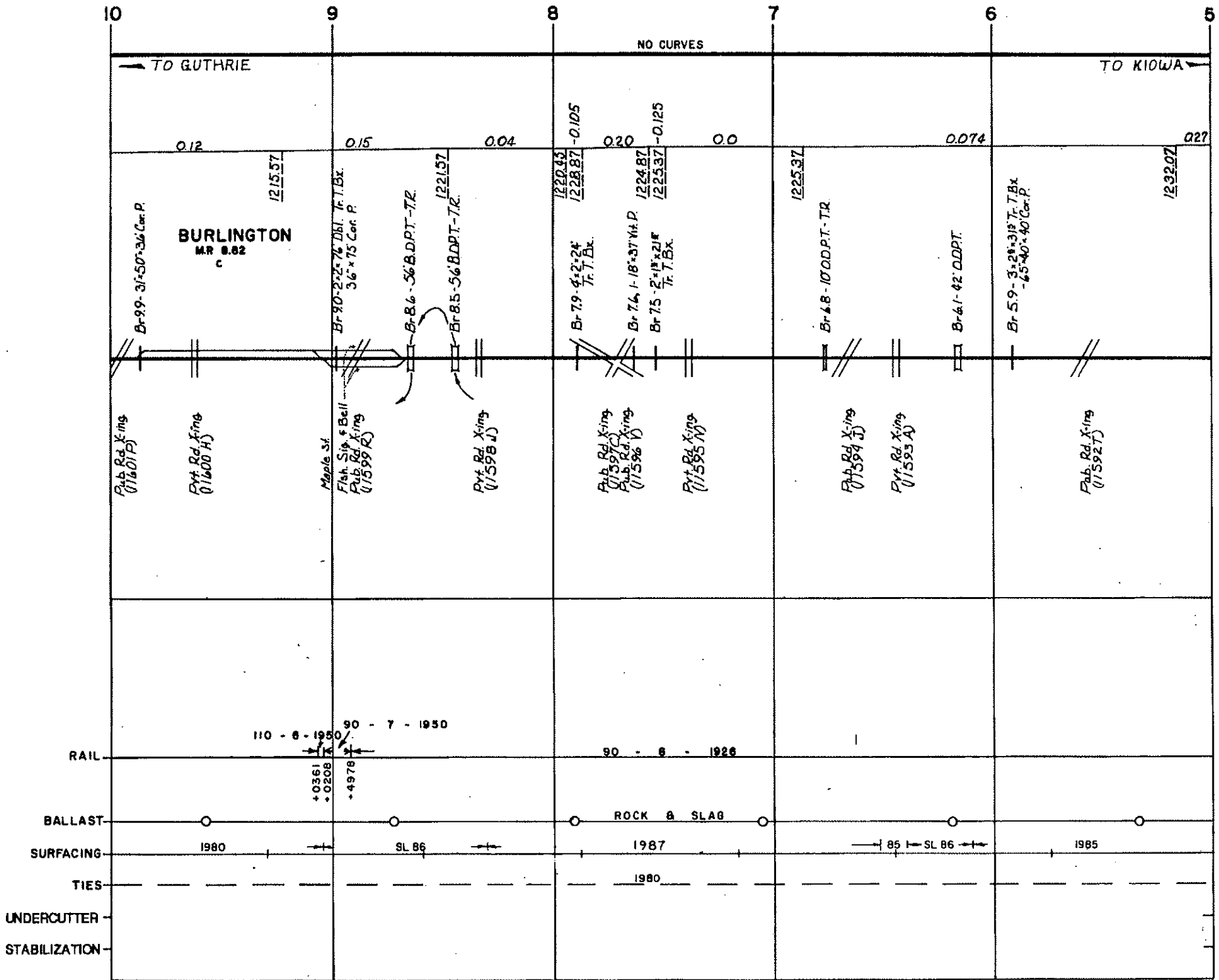


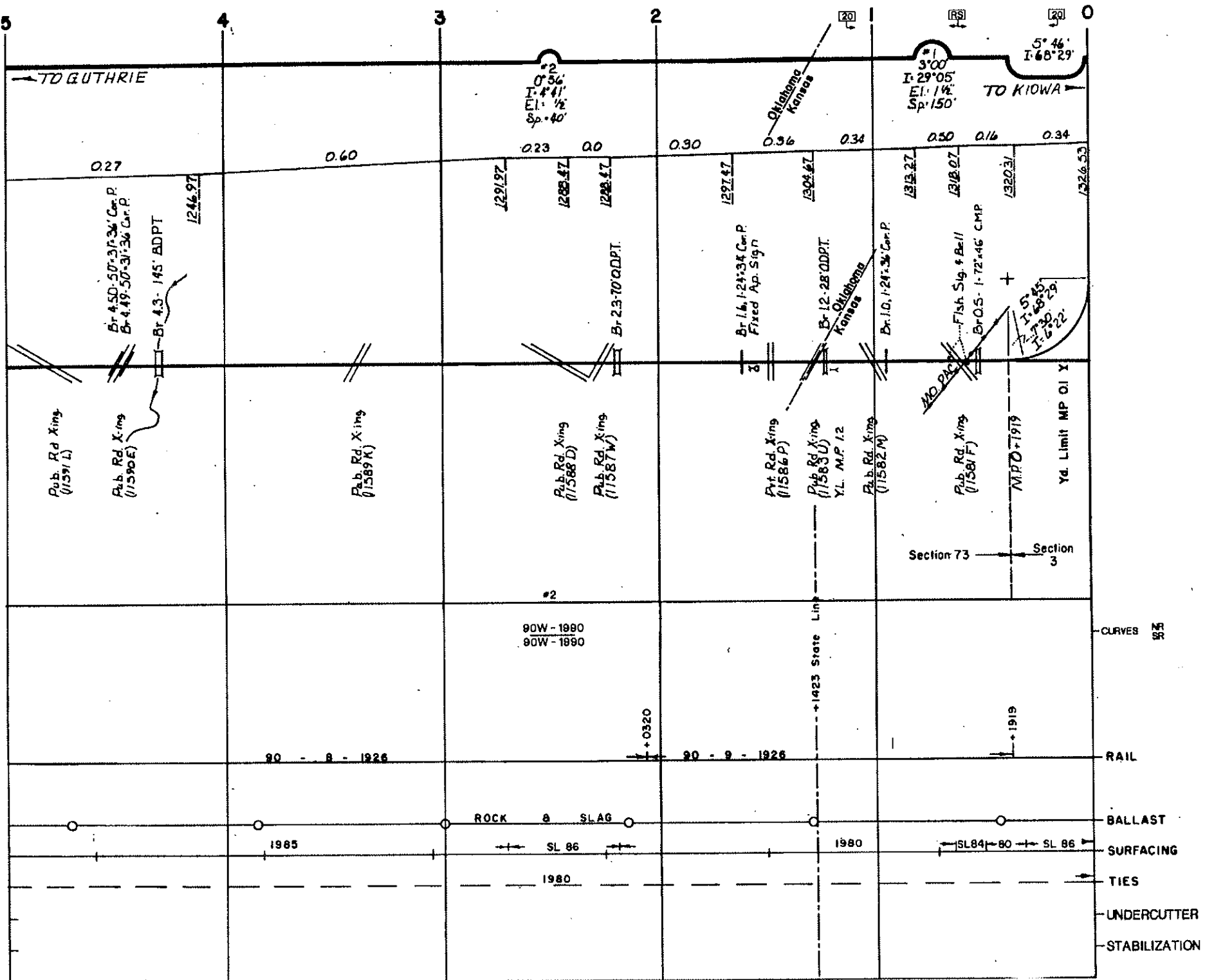






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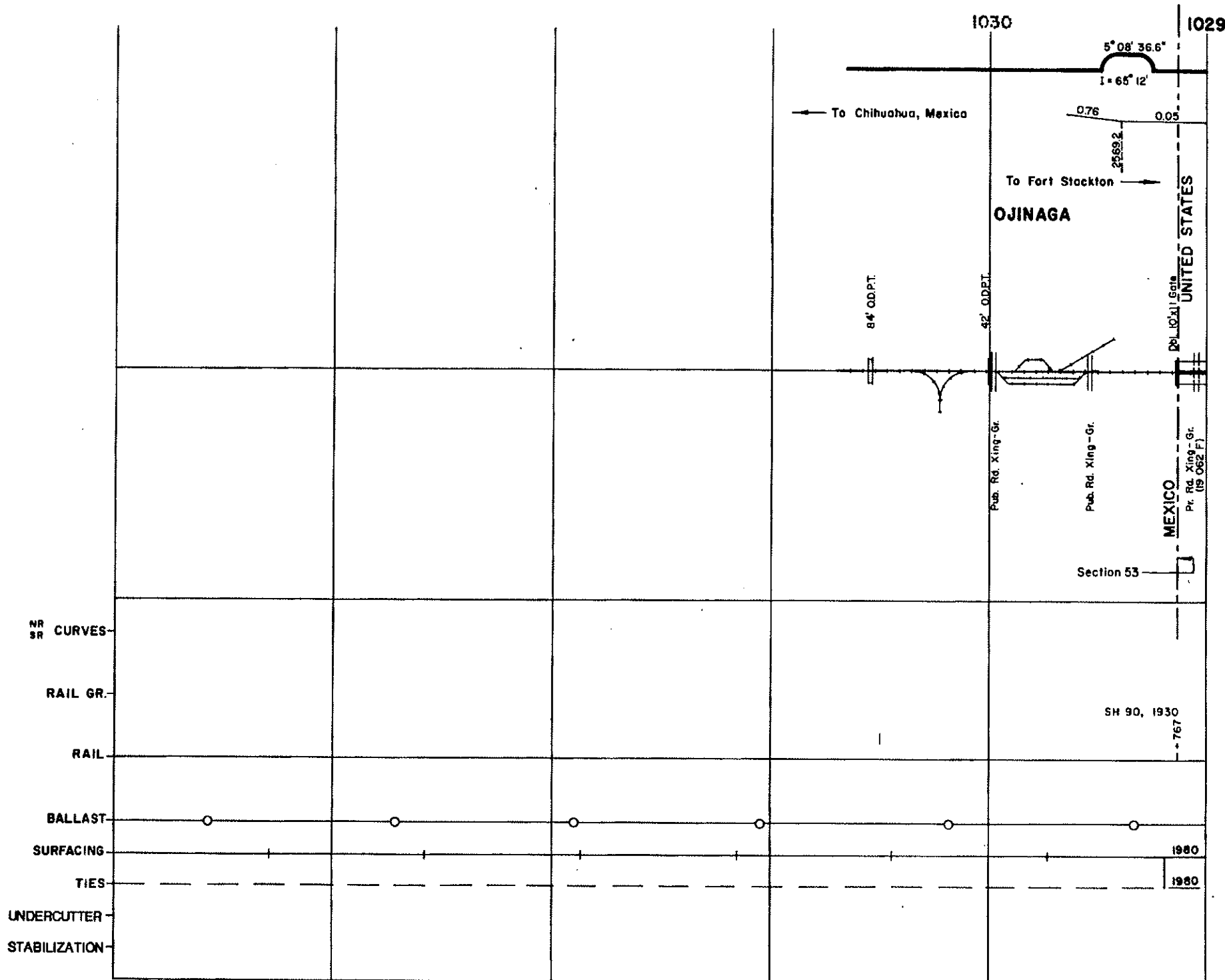




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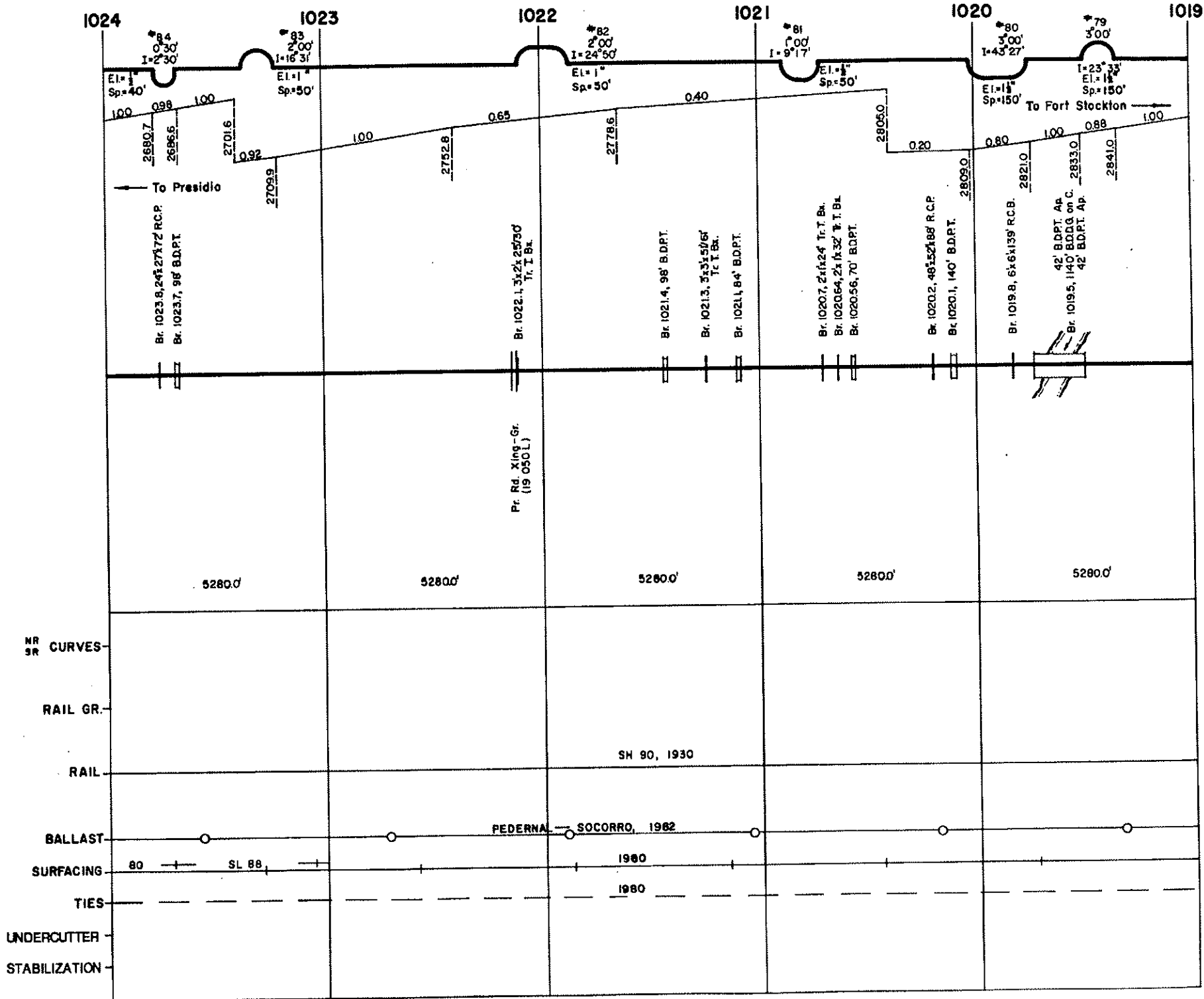
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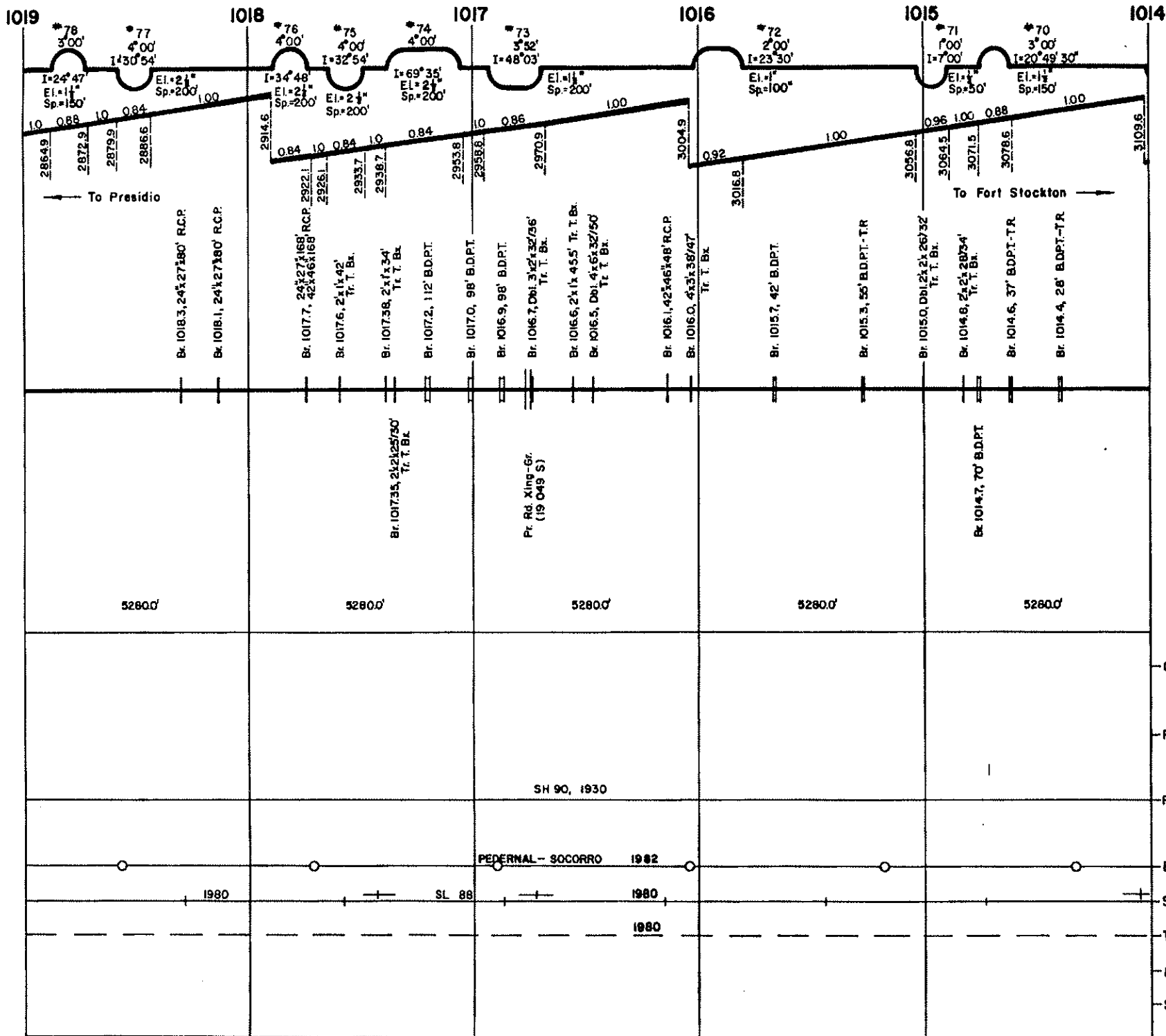
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CURVES <sup>NR</sup> <sub>SR</sub>

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RAIL

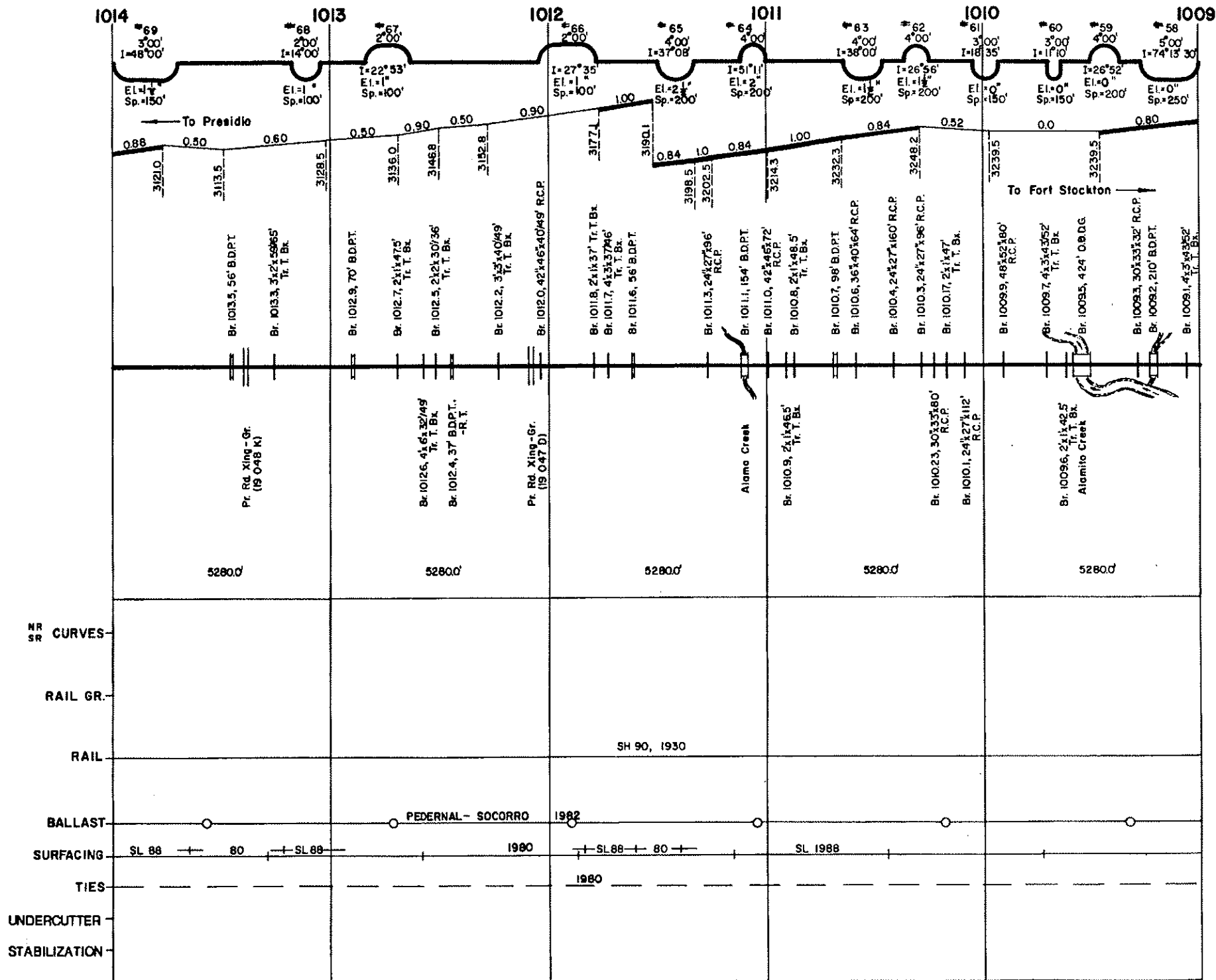
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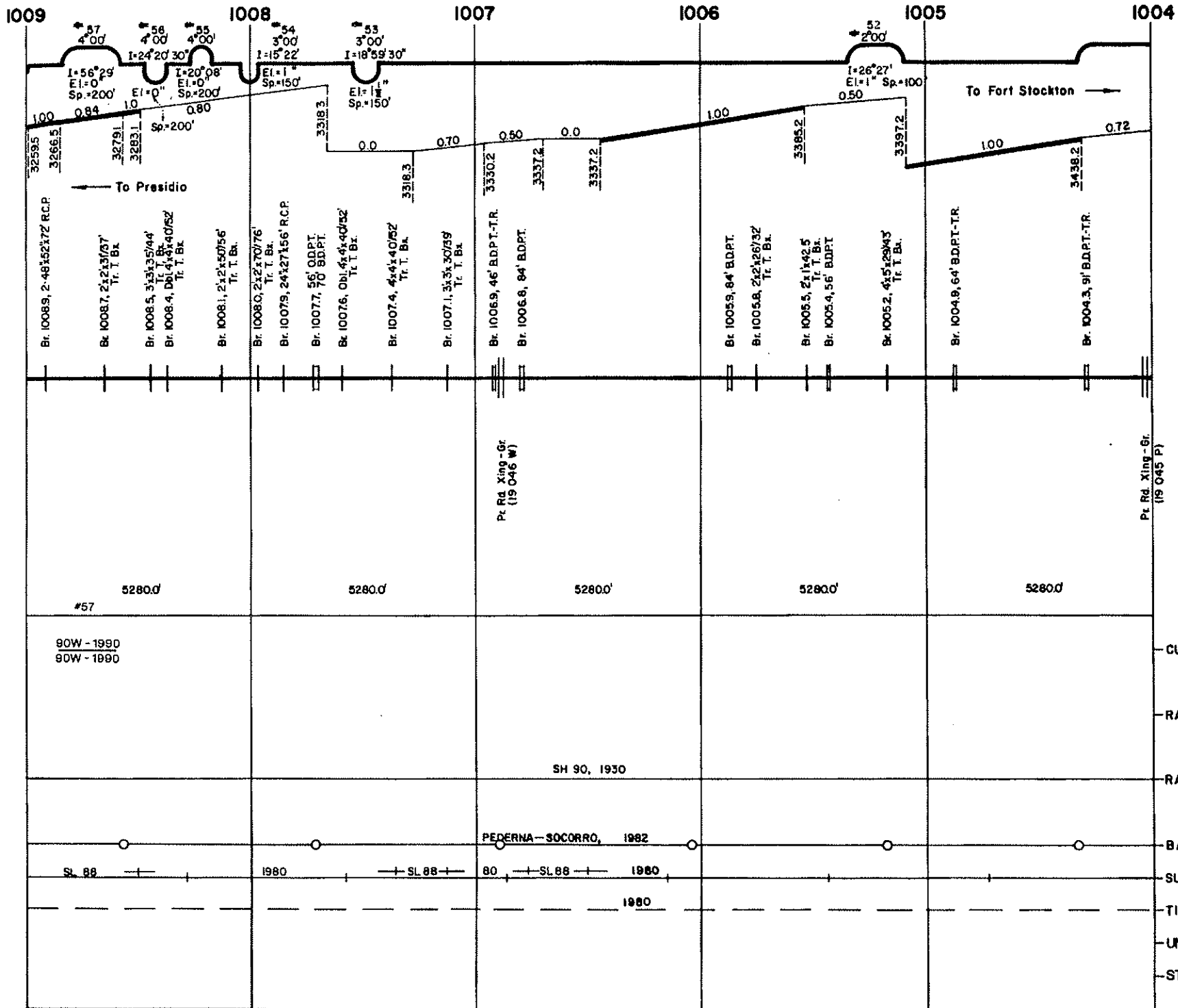
SURFACING

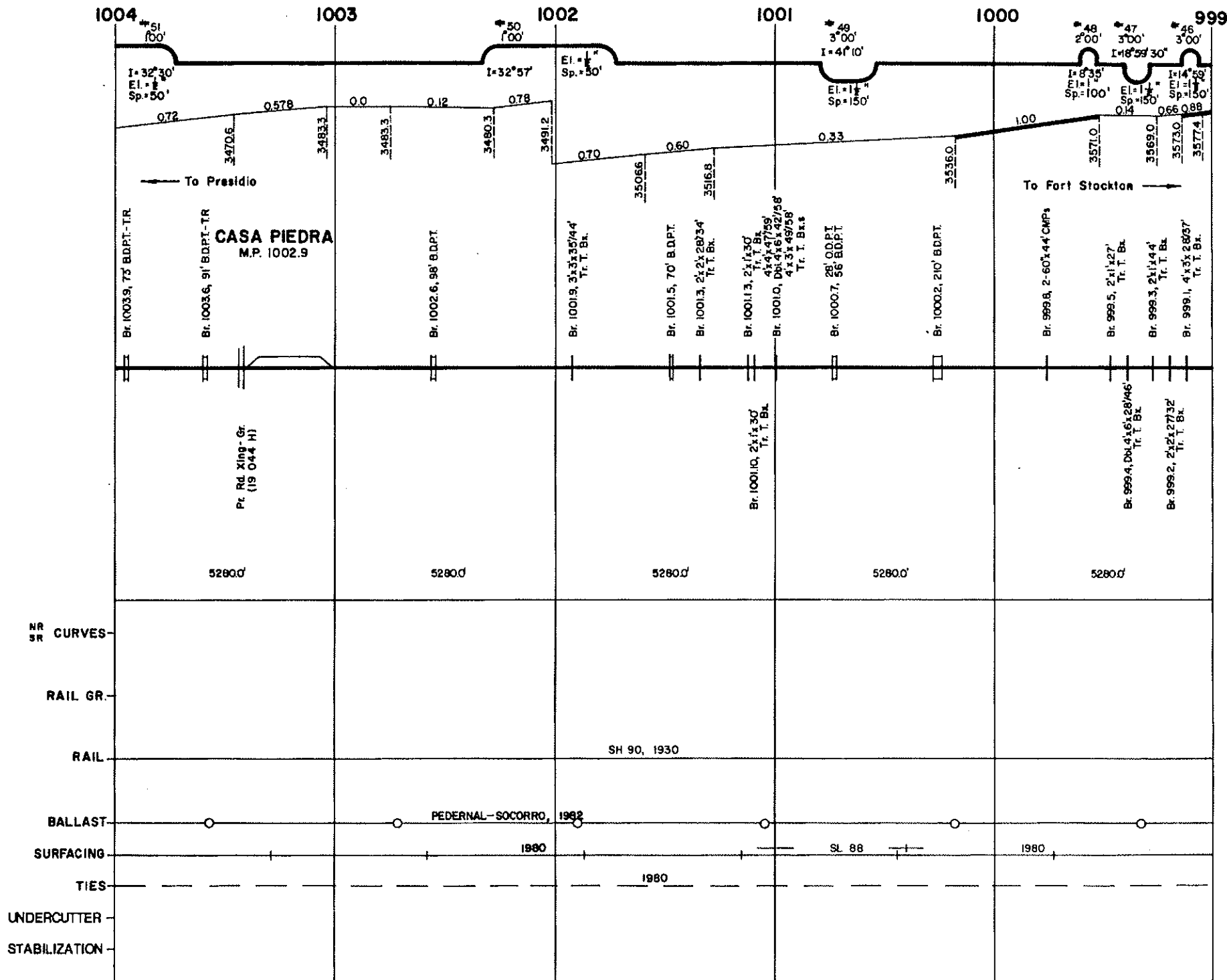
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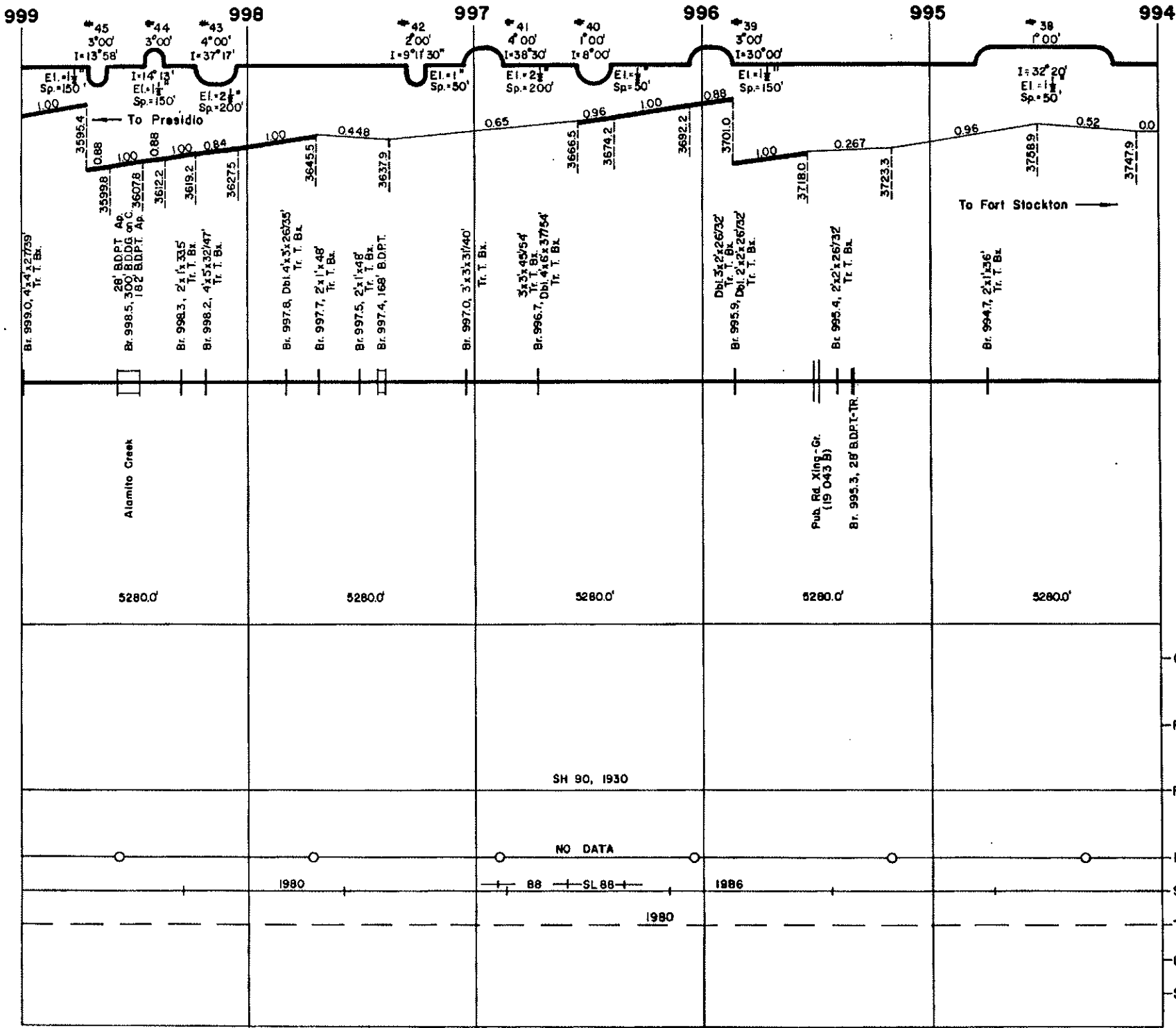
UNDERCUTTER

STABILIZATION

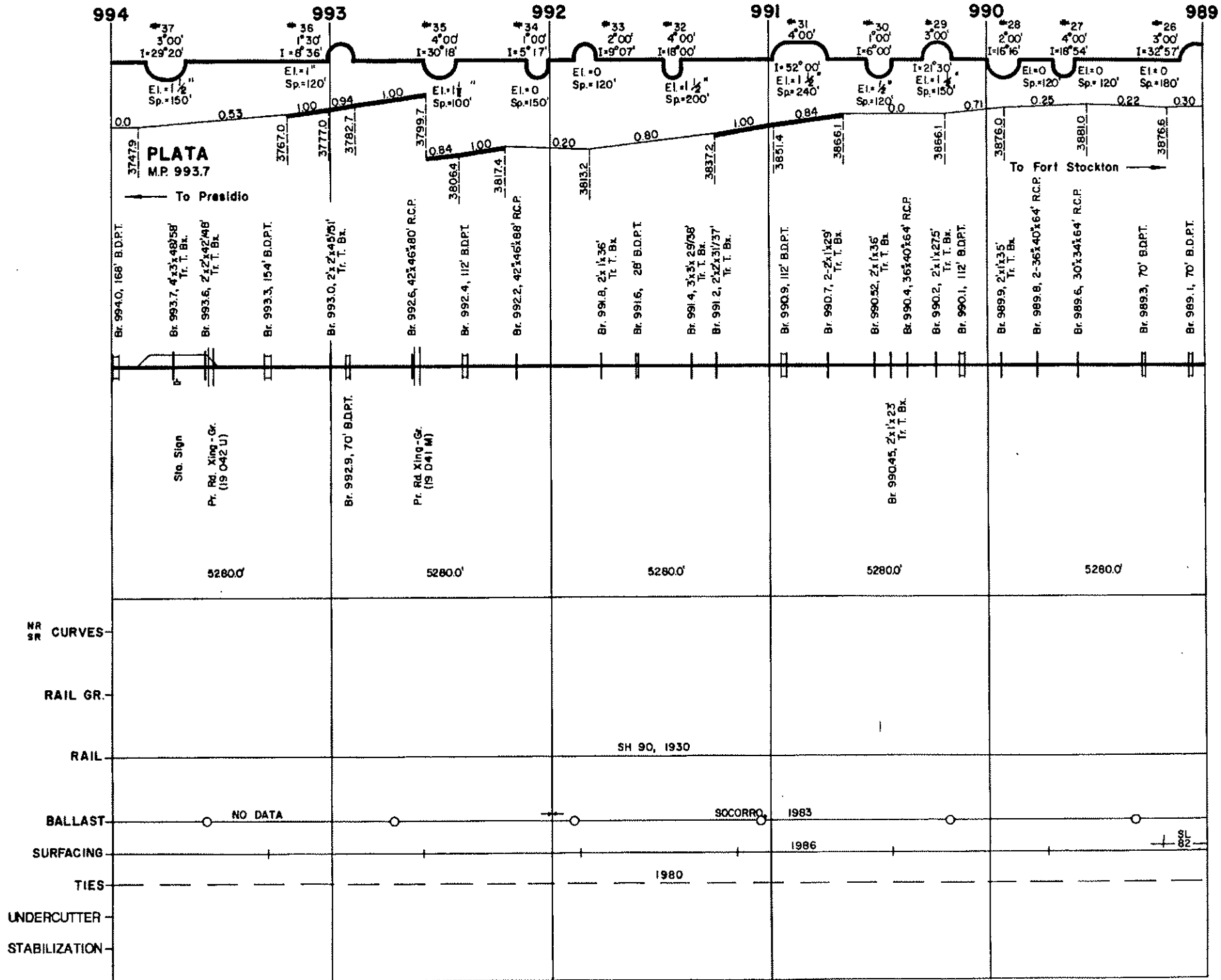


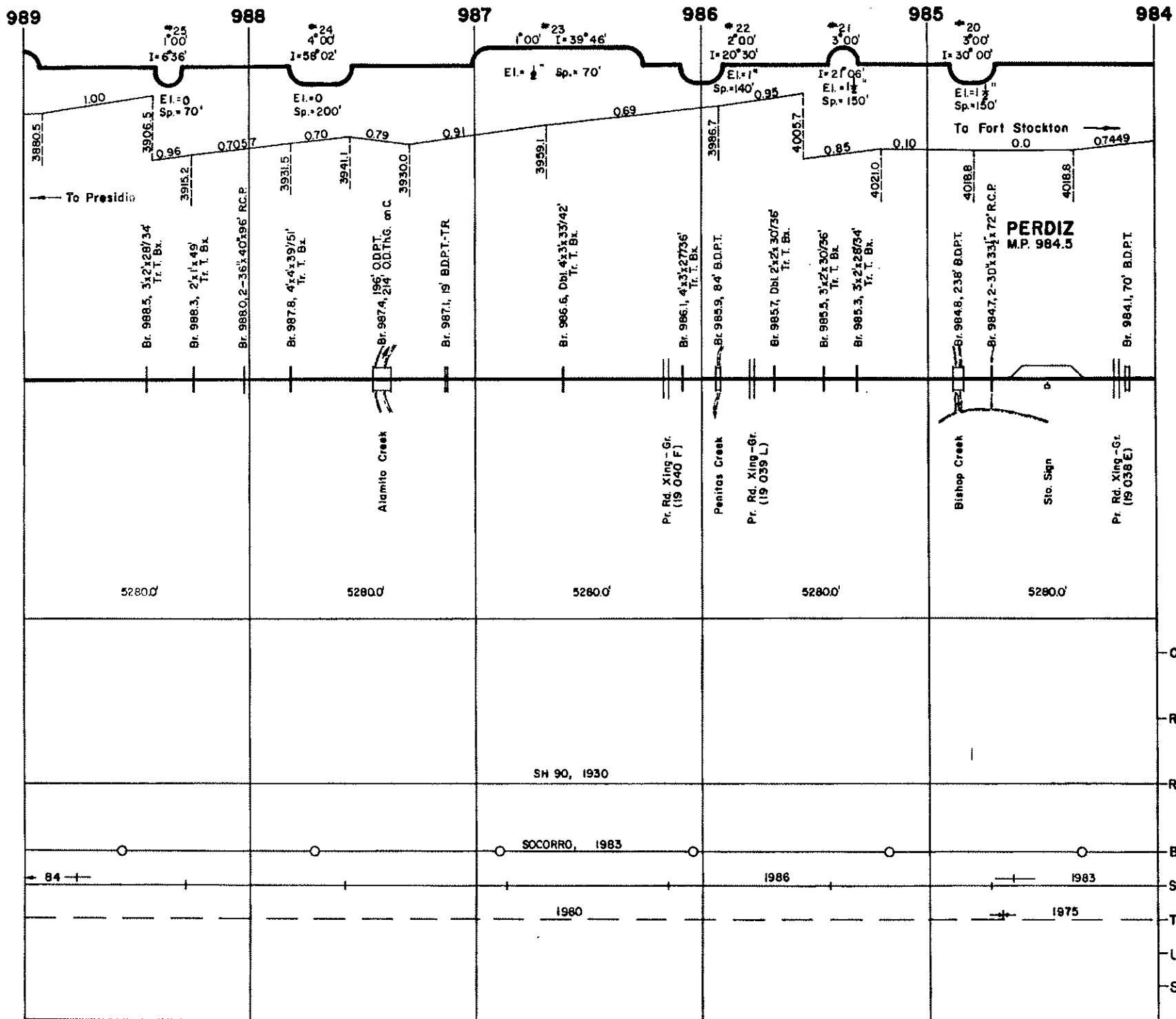




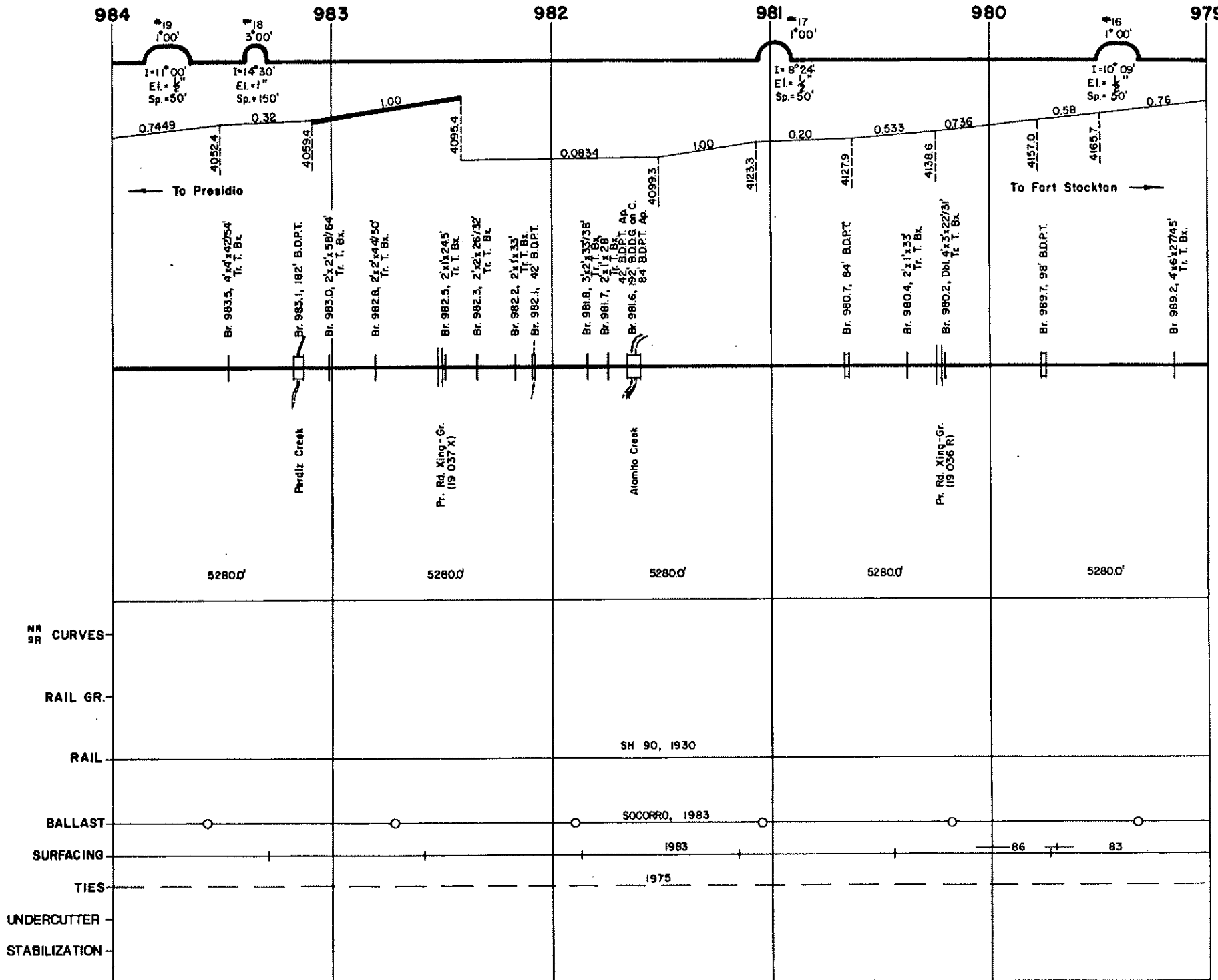


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 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION

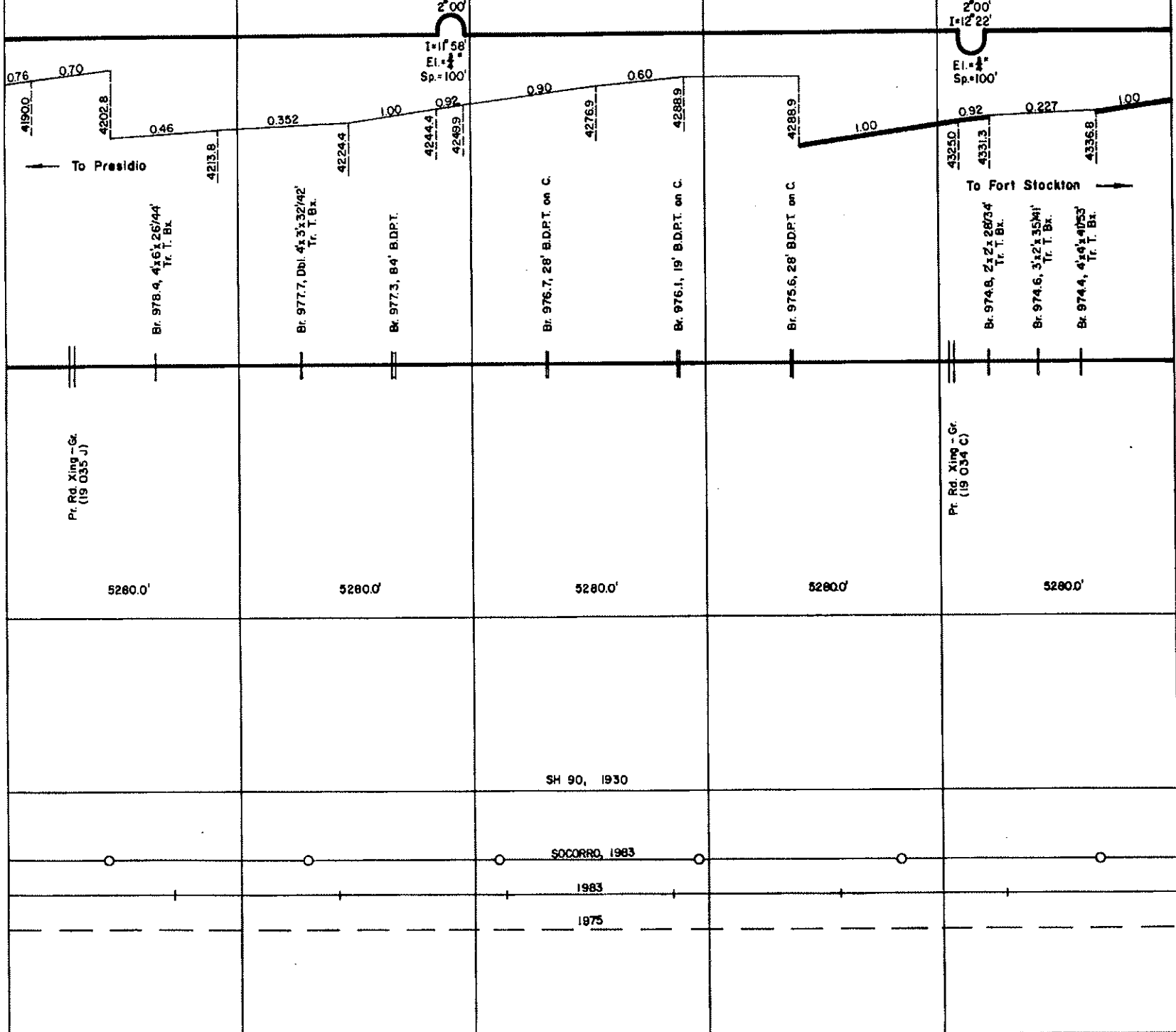








979 978 977 976 975 974



To Presidio

To Fort Stockton

Pr. Rd. Xing - Gr. (19 035 J)

Pr. Rd. Xing - Gr. (19 034 C)

Br. 978.9, 4'x6'x26'x44' Tr. T. Bx.

Br. 977.7, Dbl. 4'x3'x32'x42' Tr. T. Bx.

Br. 977.3, 8'4' B.D.P.T.

Br. 976.7, 28' B.D.P.T. on C.

Br. 976.1, 19' B.D.P.T. on C.

Br. 975.6, 28' B.D.P.T. on C.

Br. 974.8, 2'x2'x28'x34' Tr. T. Bx.

Br. 974.6, 3'x2'x35'x41' Tr. T. Bx.

Br. 974.4, 4'x4'x47'x53' Tr. T. Bx.

Cul. 15  
2'00'  
I=1f'58"  
El.=1/4"  
Sp.=100'

Cul. 14  
2'00'  
I=2'22"  
El.=1/4"  
Sp.=100'

CURVES NR SR

RAIL GR.

RAIL

BALLAST

SURFACING

TIES

UNDERCUTTER

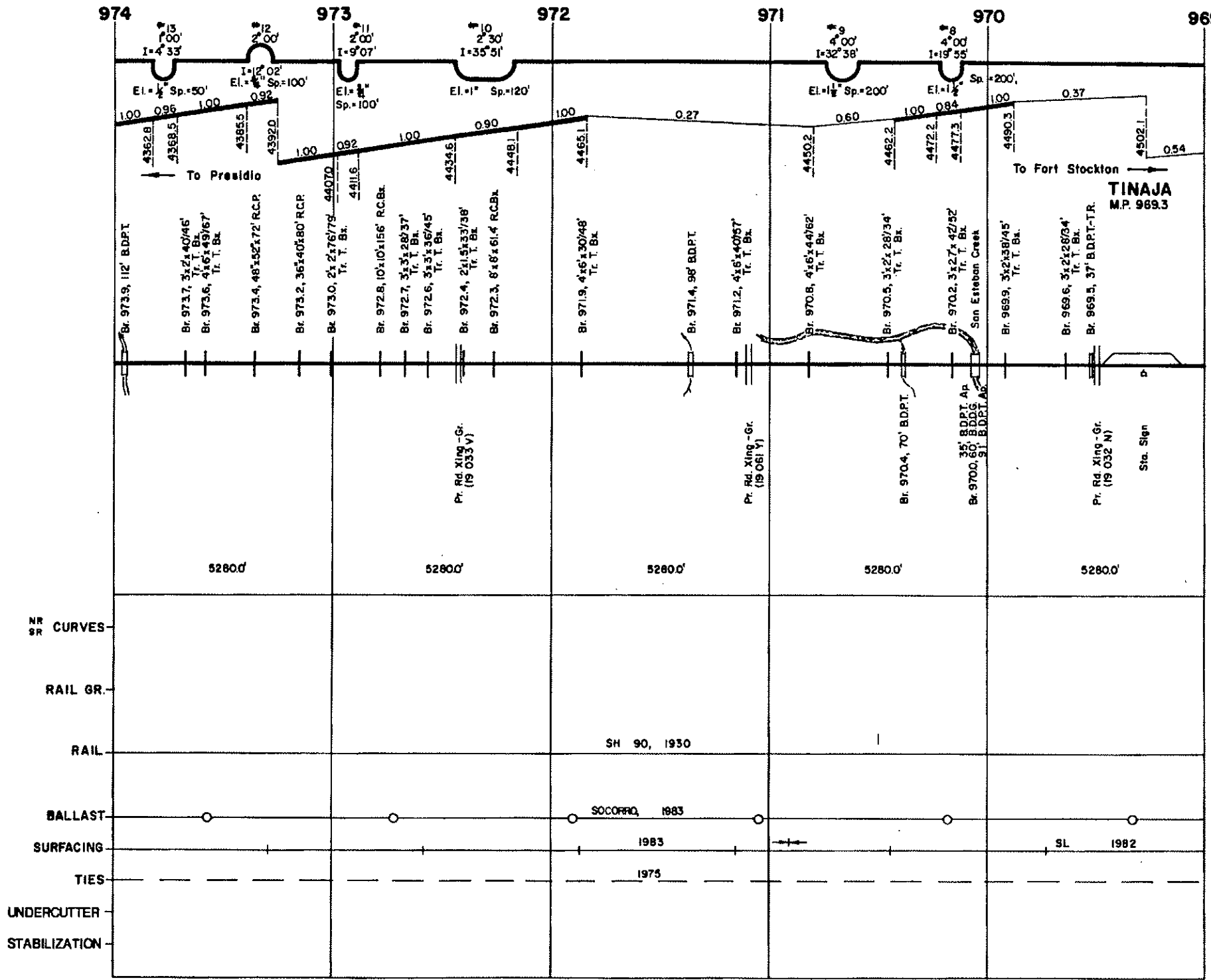
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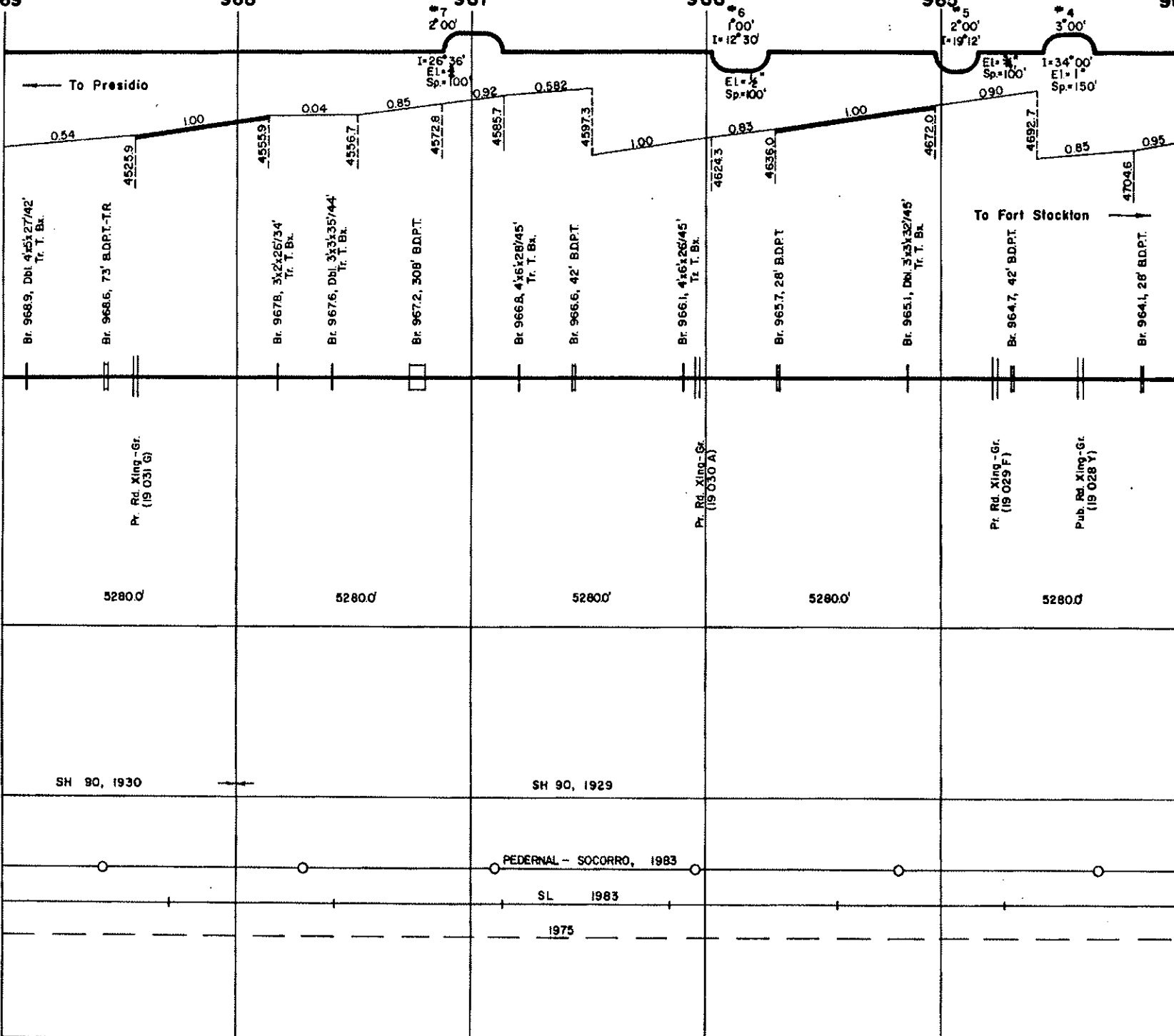
SOCORRO, 1983

1983

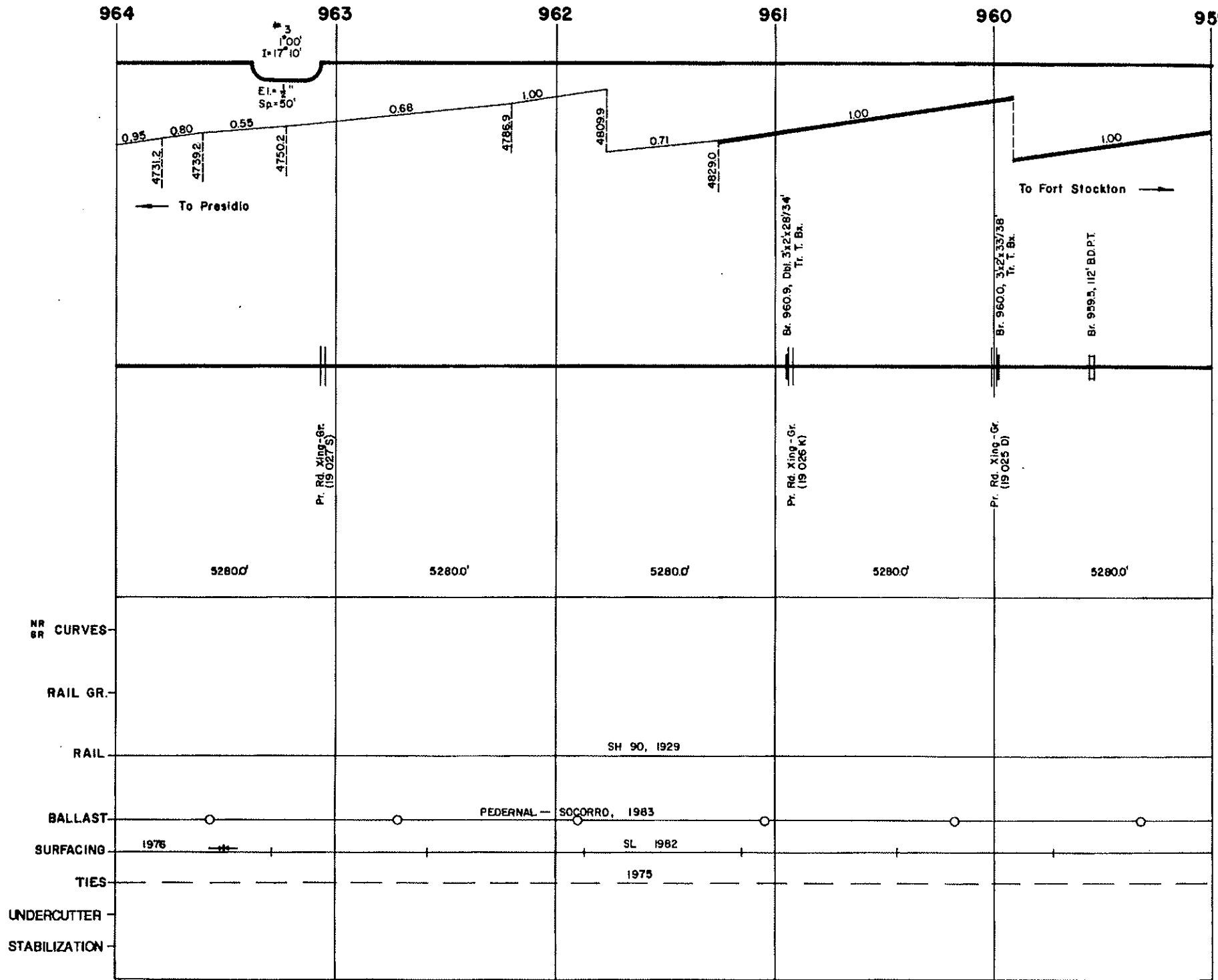
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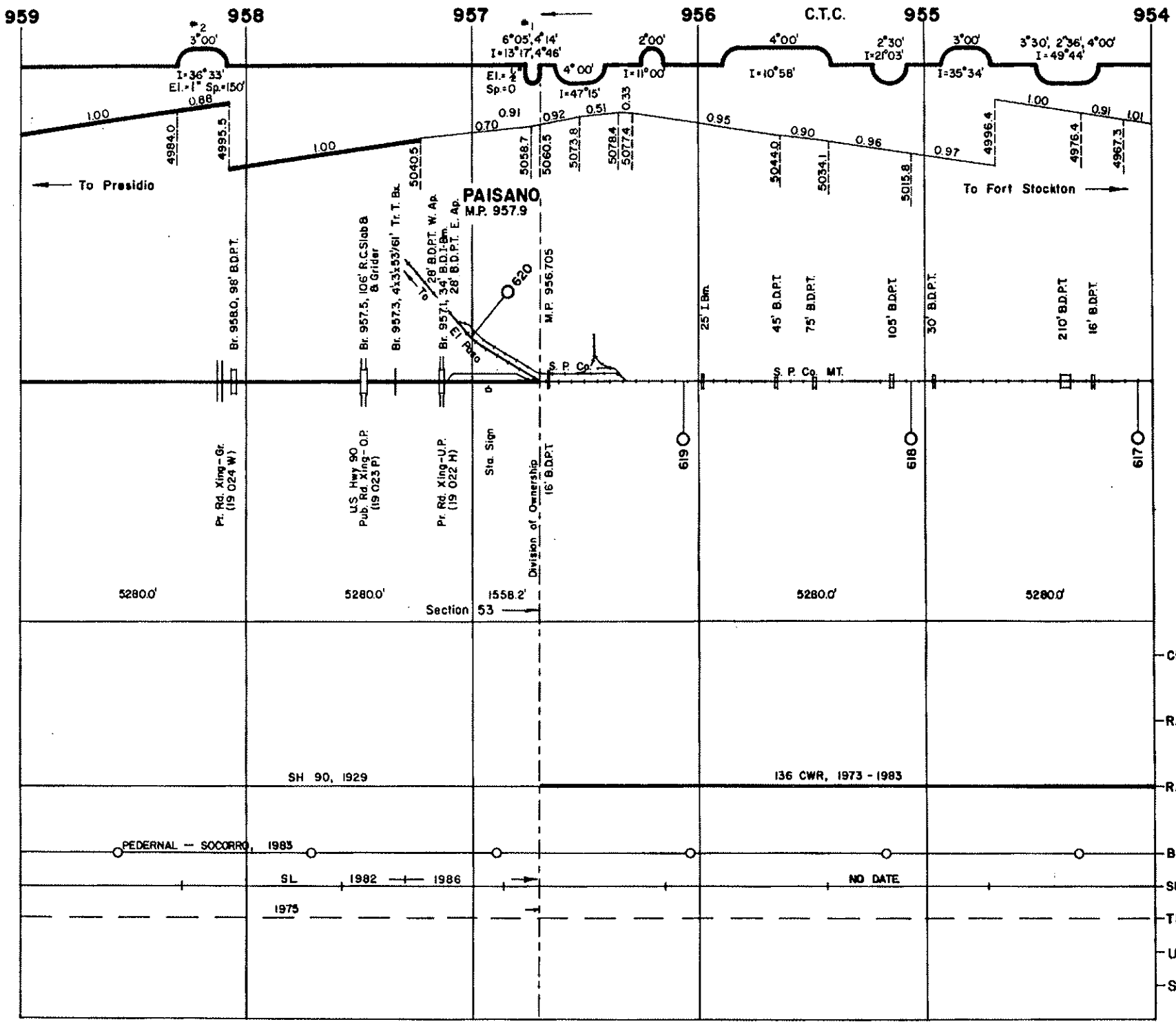


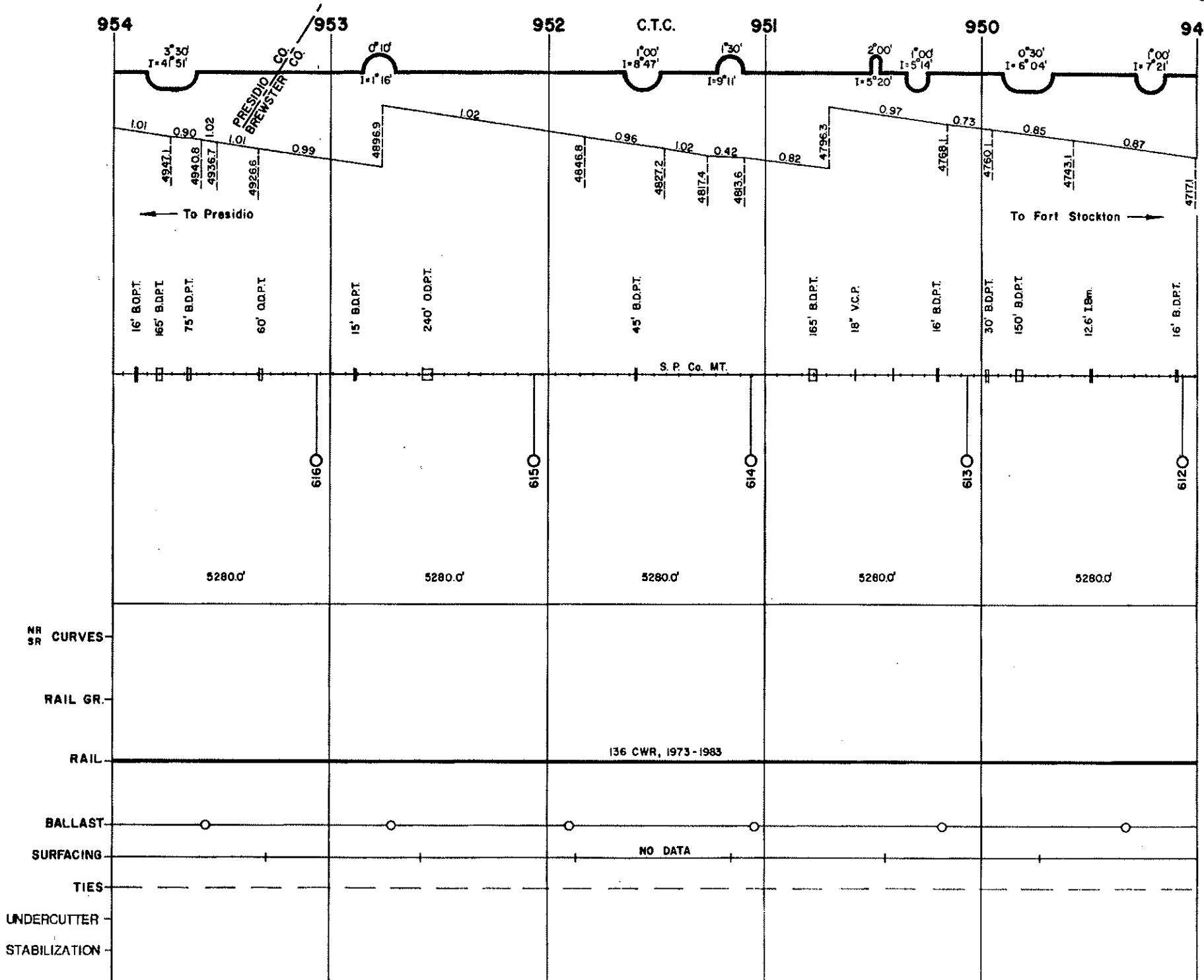
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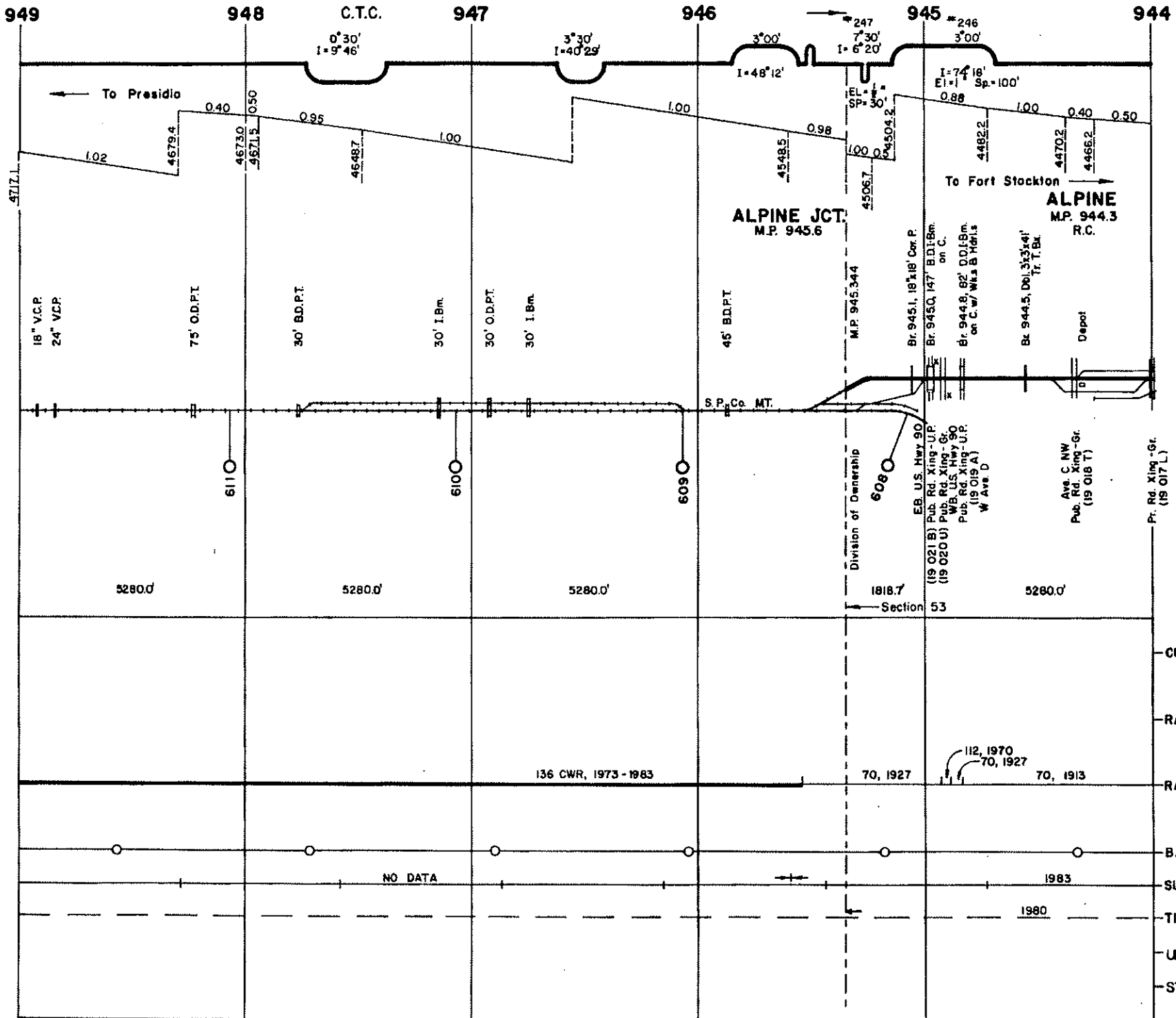


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 RAIL  
 BALLAST  
 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION

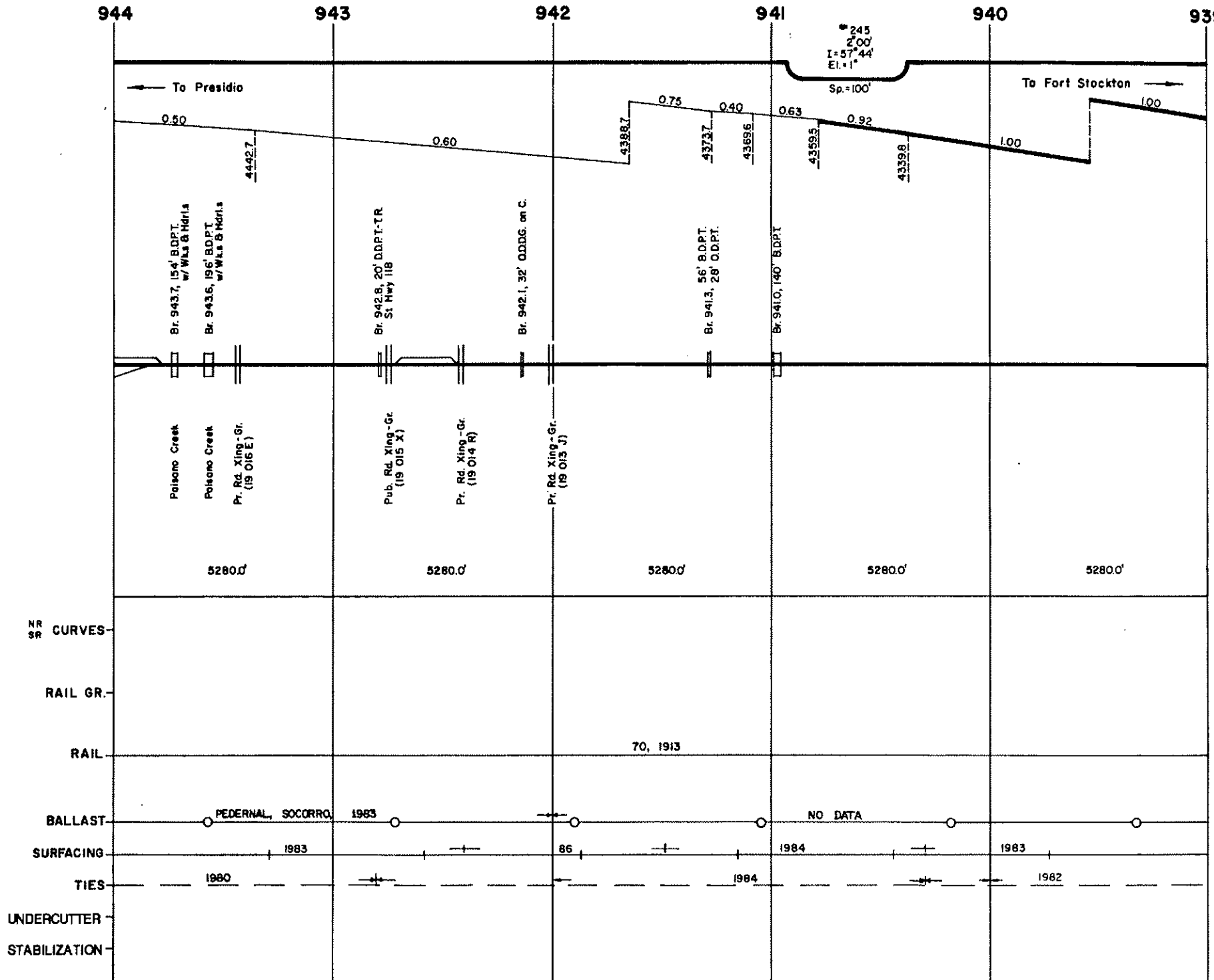


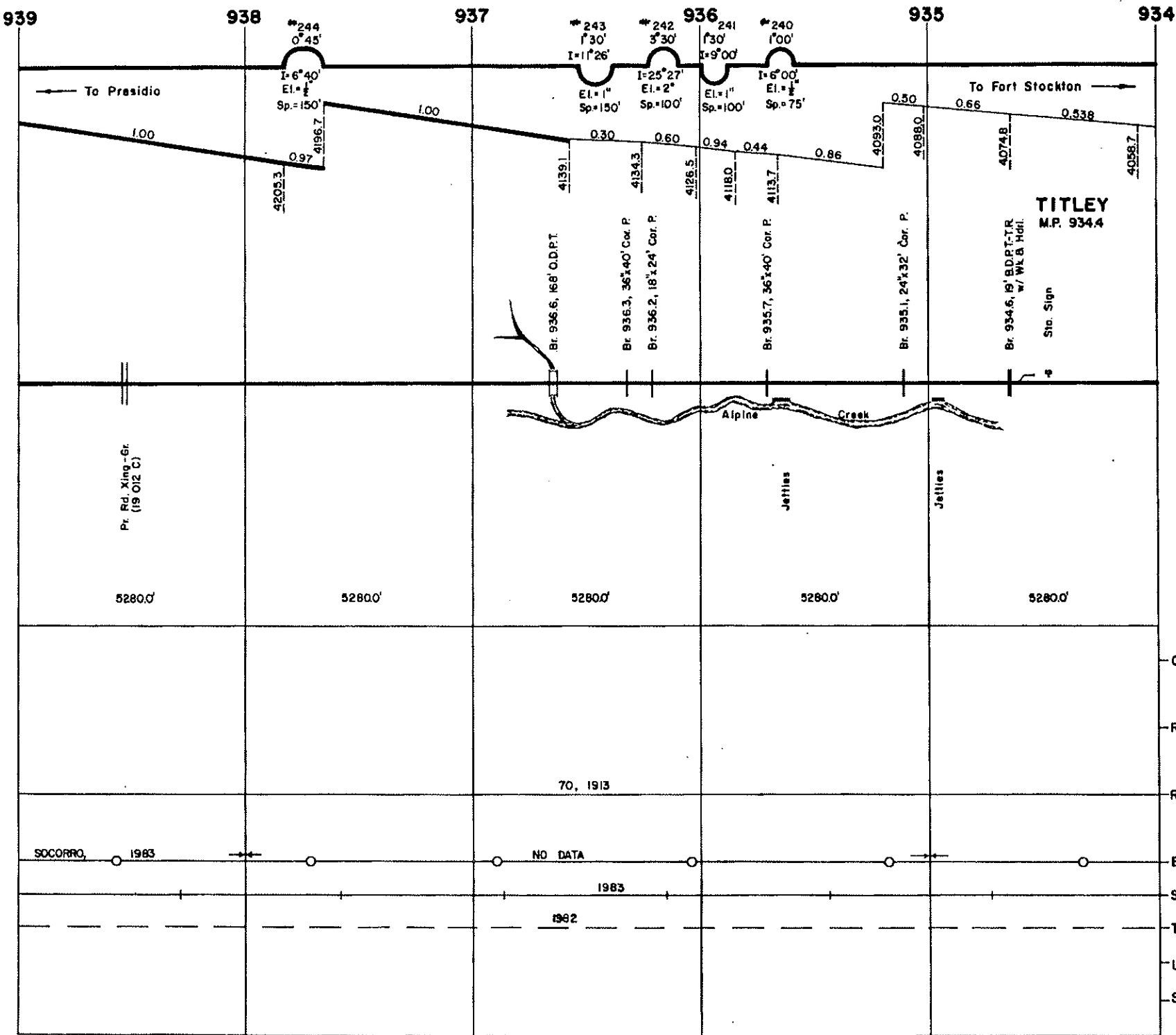












TITLEY  
M.P. 934.4

CURVES NR SR

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RAIL

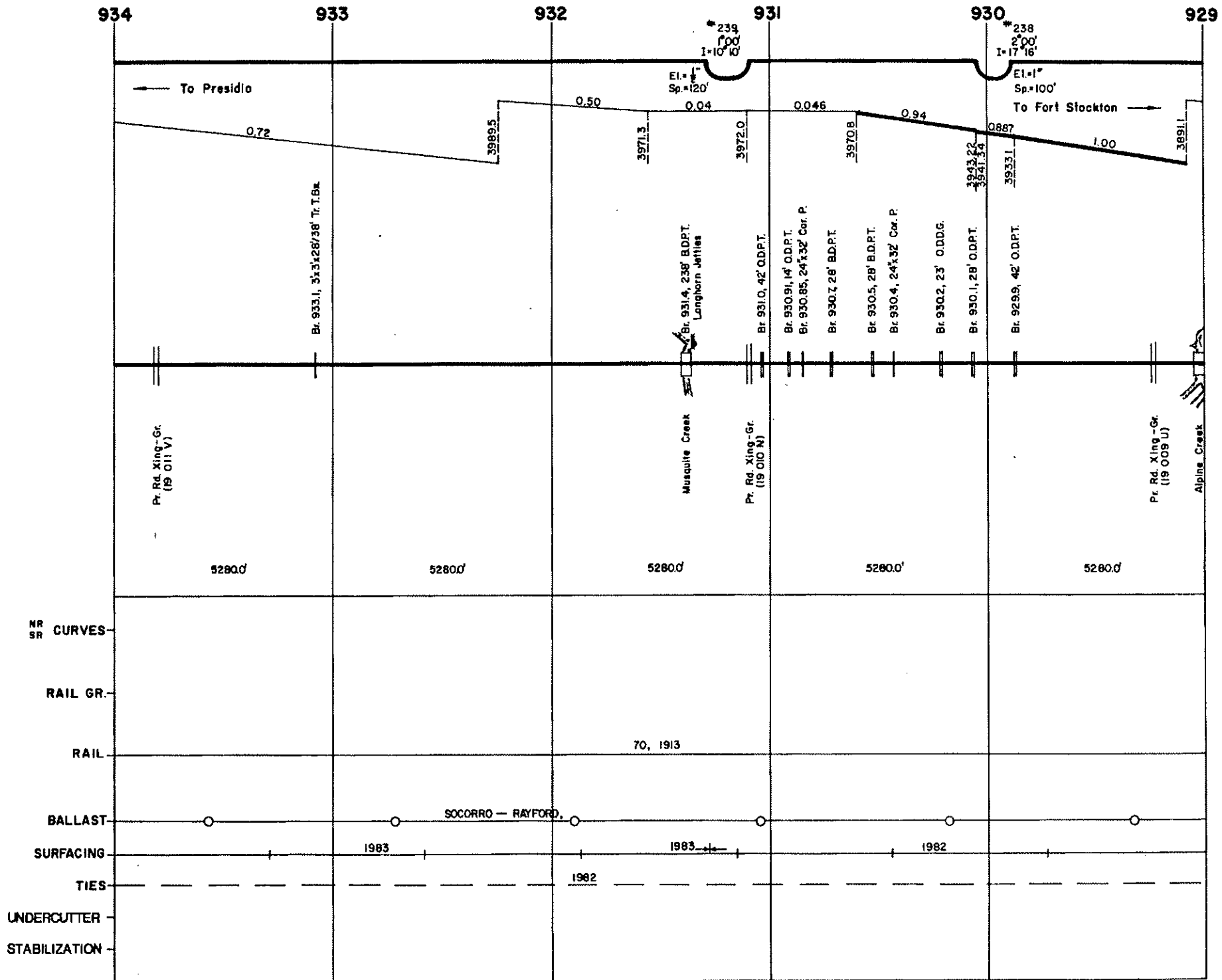
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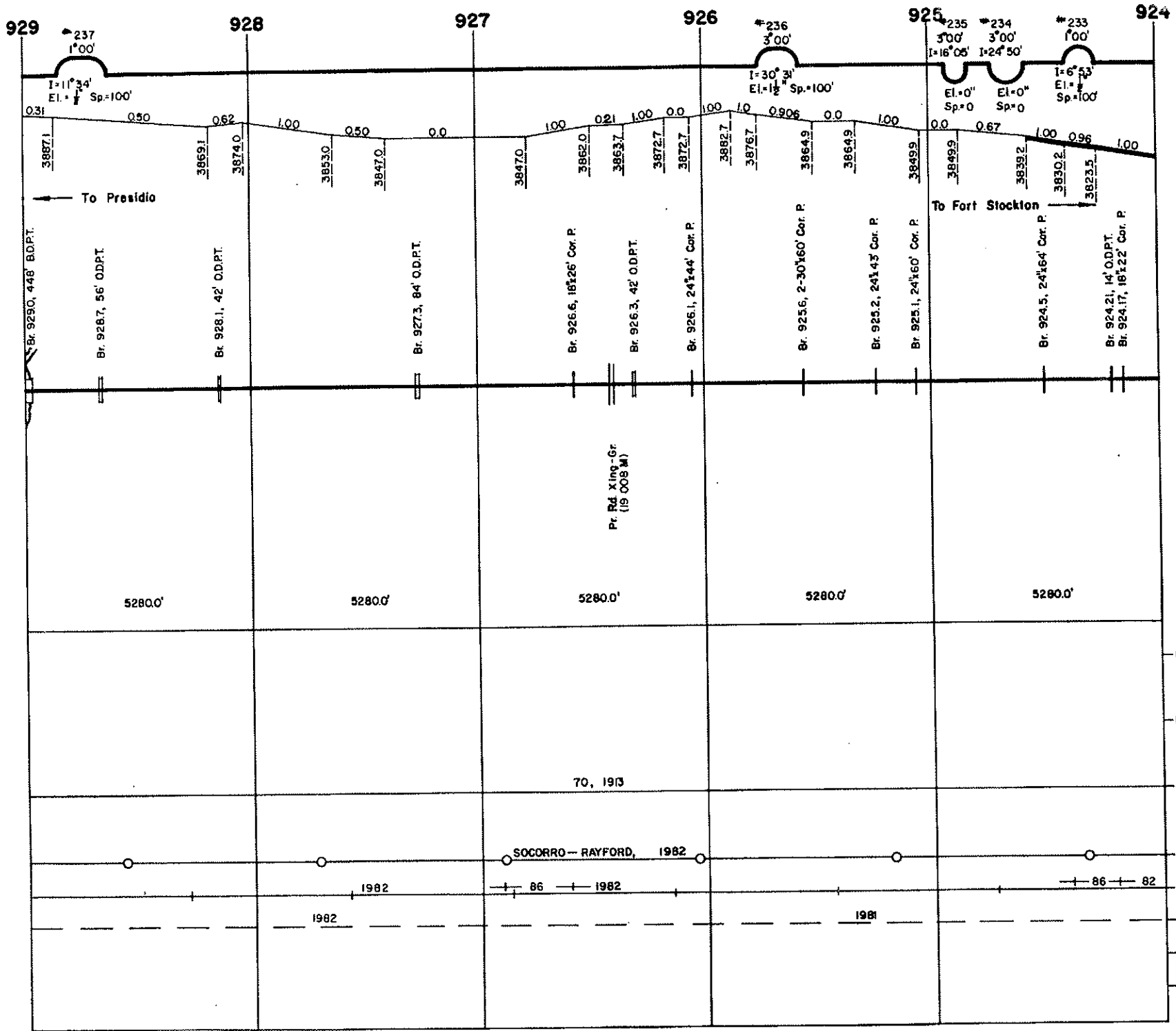
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TIES

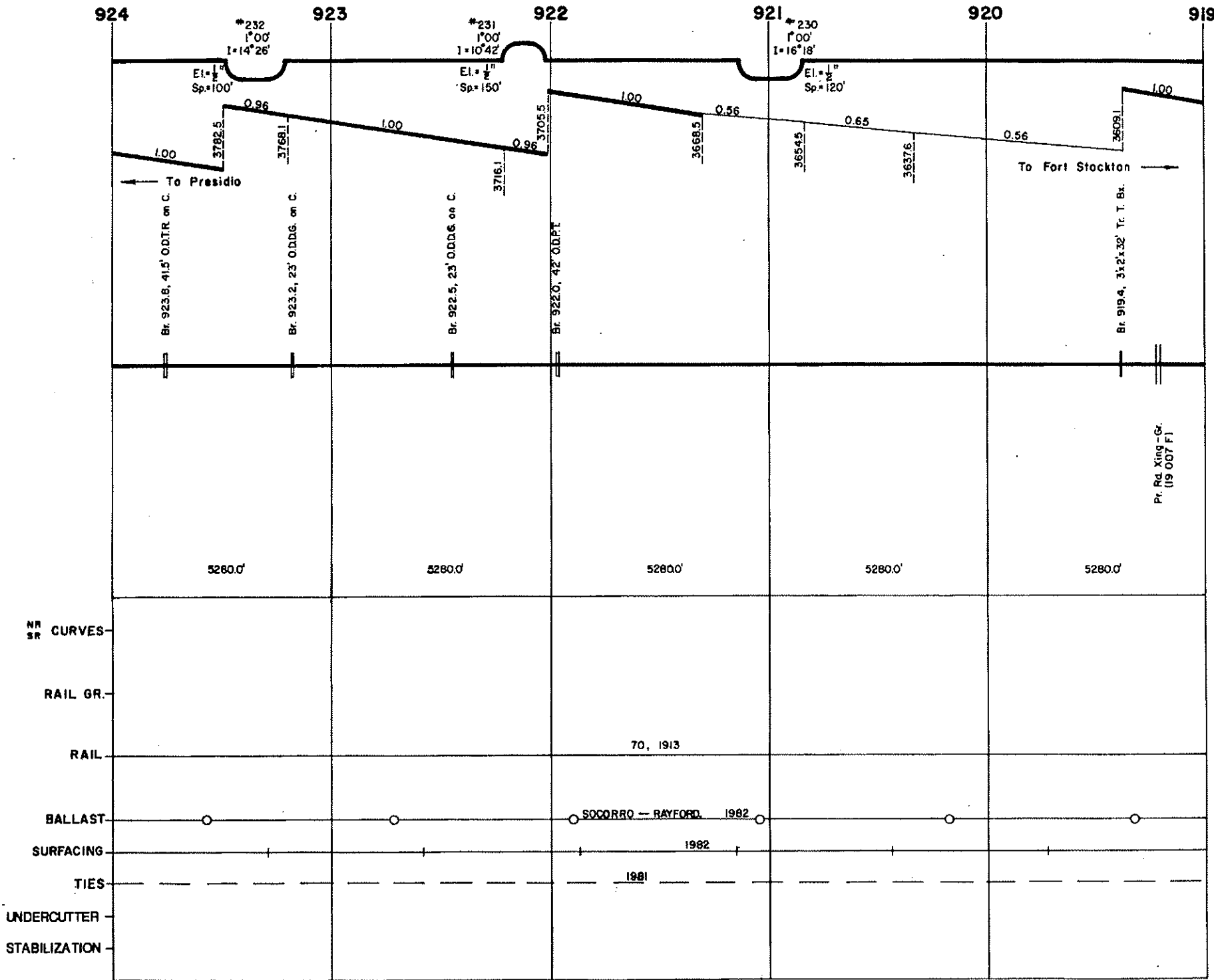
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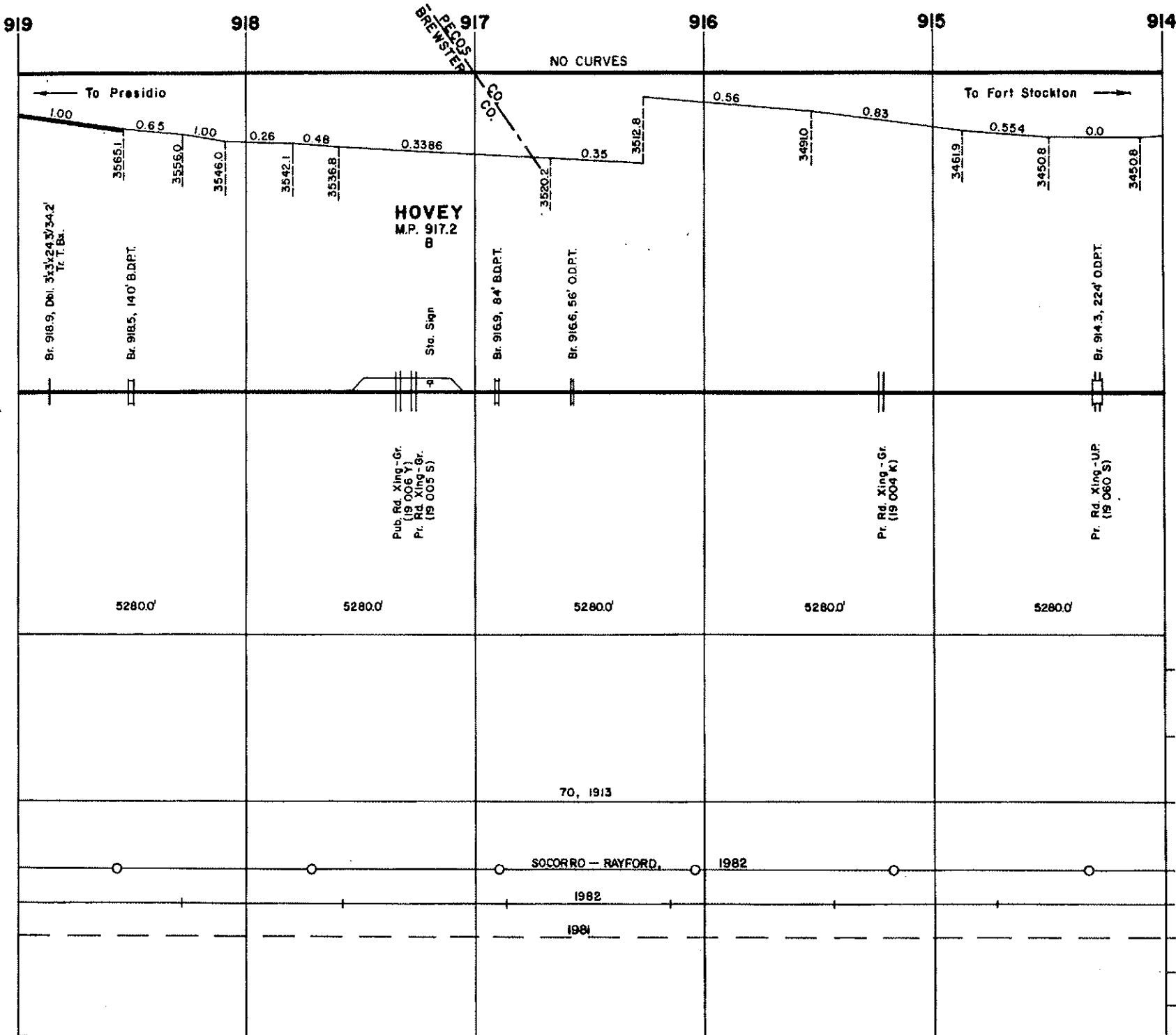
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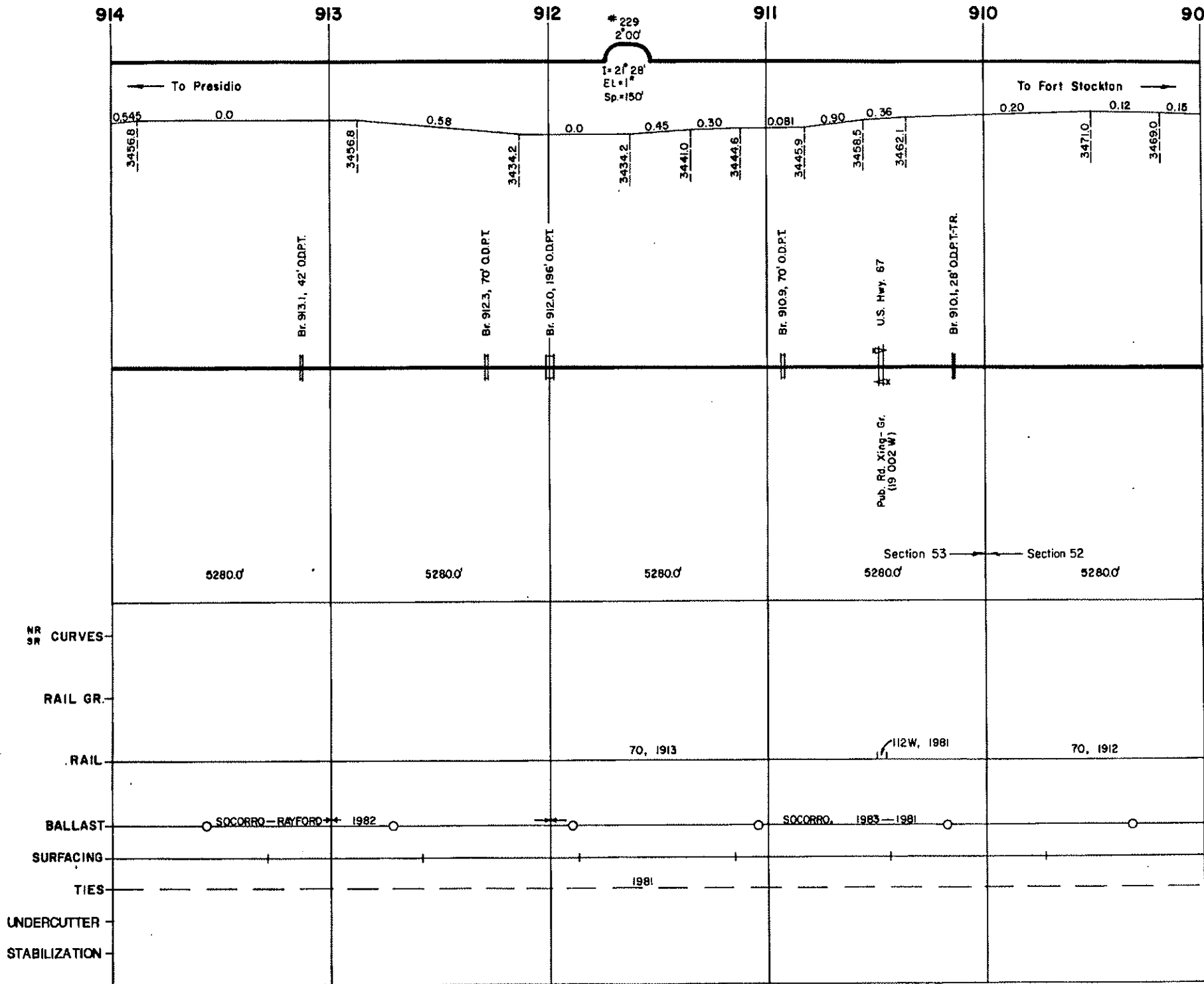


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- CURVES NR SR
- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION



909 908 907 906 905 904

NO CURVES

To Presidio ←

To Fort Stockton →

0.15 0.34 0.43

0.201

0.23

0.45

0.30

0.45

0.29

0.06

3464.7

3458.9

3452.1

3442.8

3433.1

3420.5

3411.5

3406.1

3397.7

CHANCELLOR  
M.P. 904.3

Br. 904.9, 56' ODPT.-TR.

Br. 904.4, 30'x32' Cor. P.



Sta. Sign

Pr. Rd. King-Gr.  
(19 001 P)

5280.0'

5280.0'

5280.0'

5280.0'

5280.0'

CURVES NR SR

RAIL GR.

RAIL

BALLAST

SURFACING

TIES

UNDERCUTTER

STABILIZATION

SOCORRO, 1981

RAYFORD - SAN ANTONIO, 1981

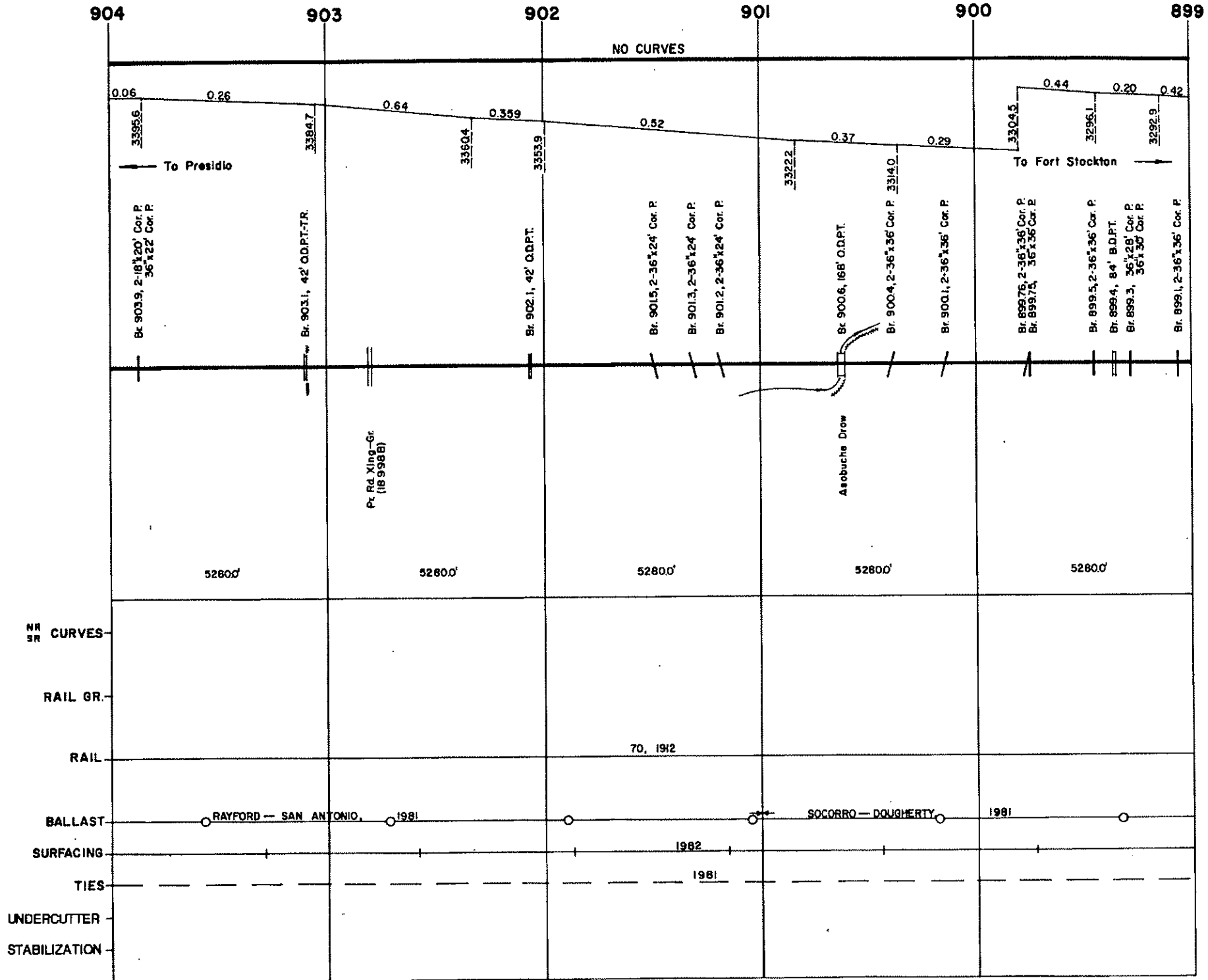
1982

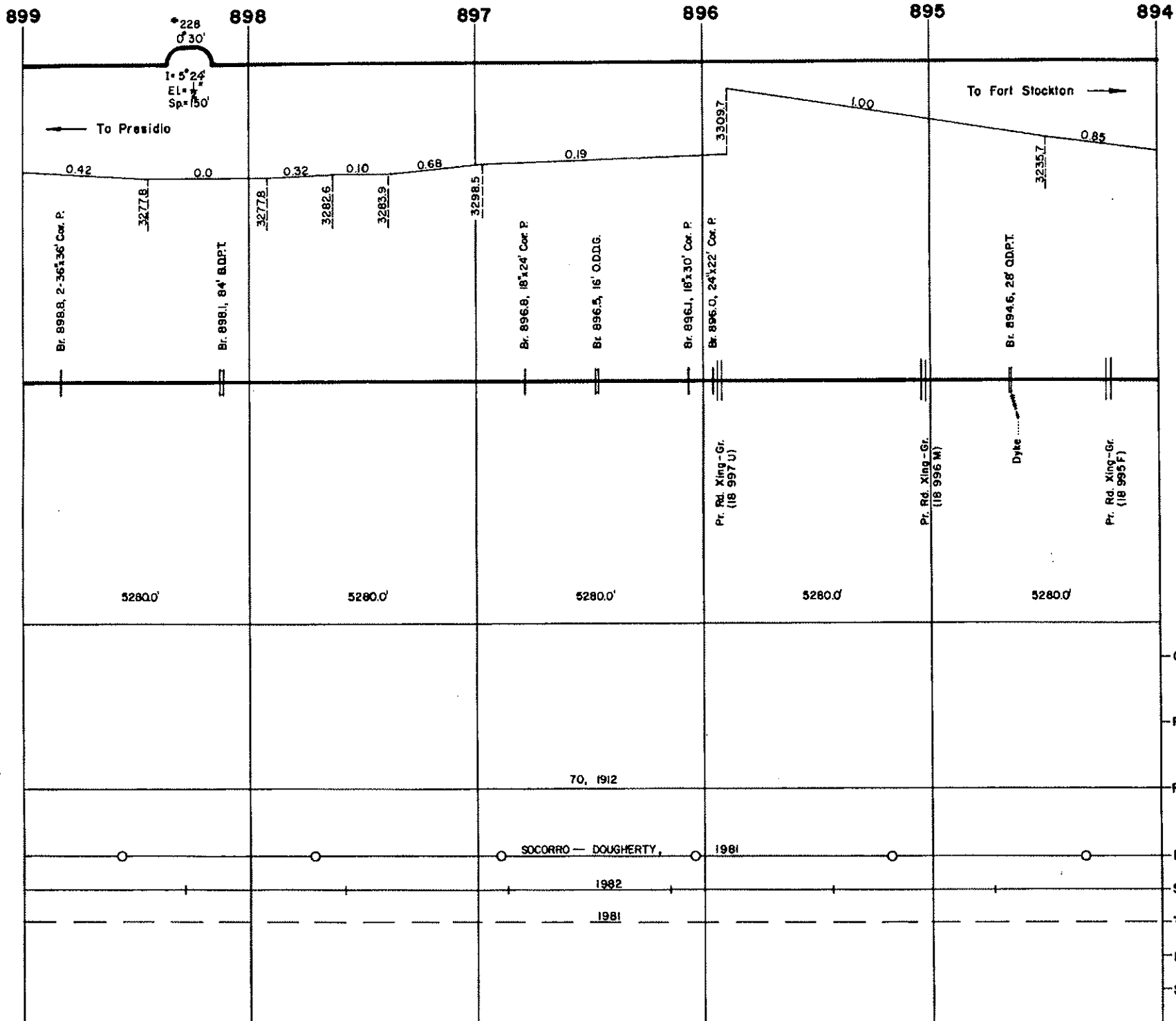
1981

70, 1912

85,







CURVES  $\frac{MM}{BR}$

RAIL GR.

RAIL

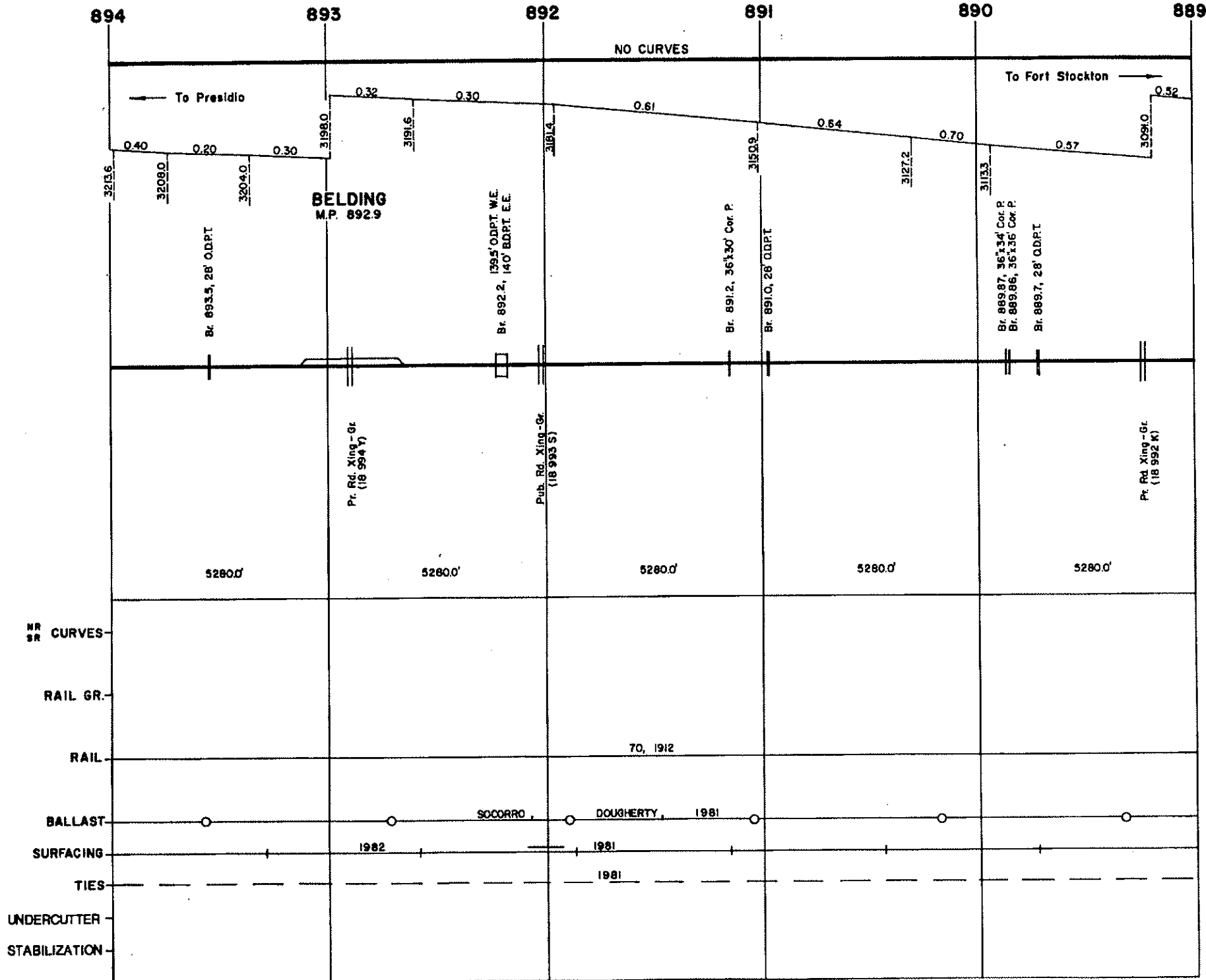
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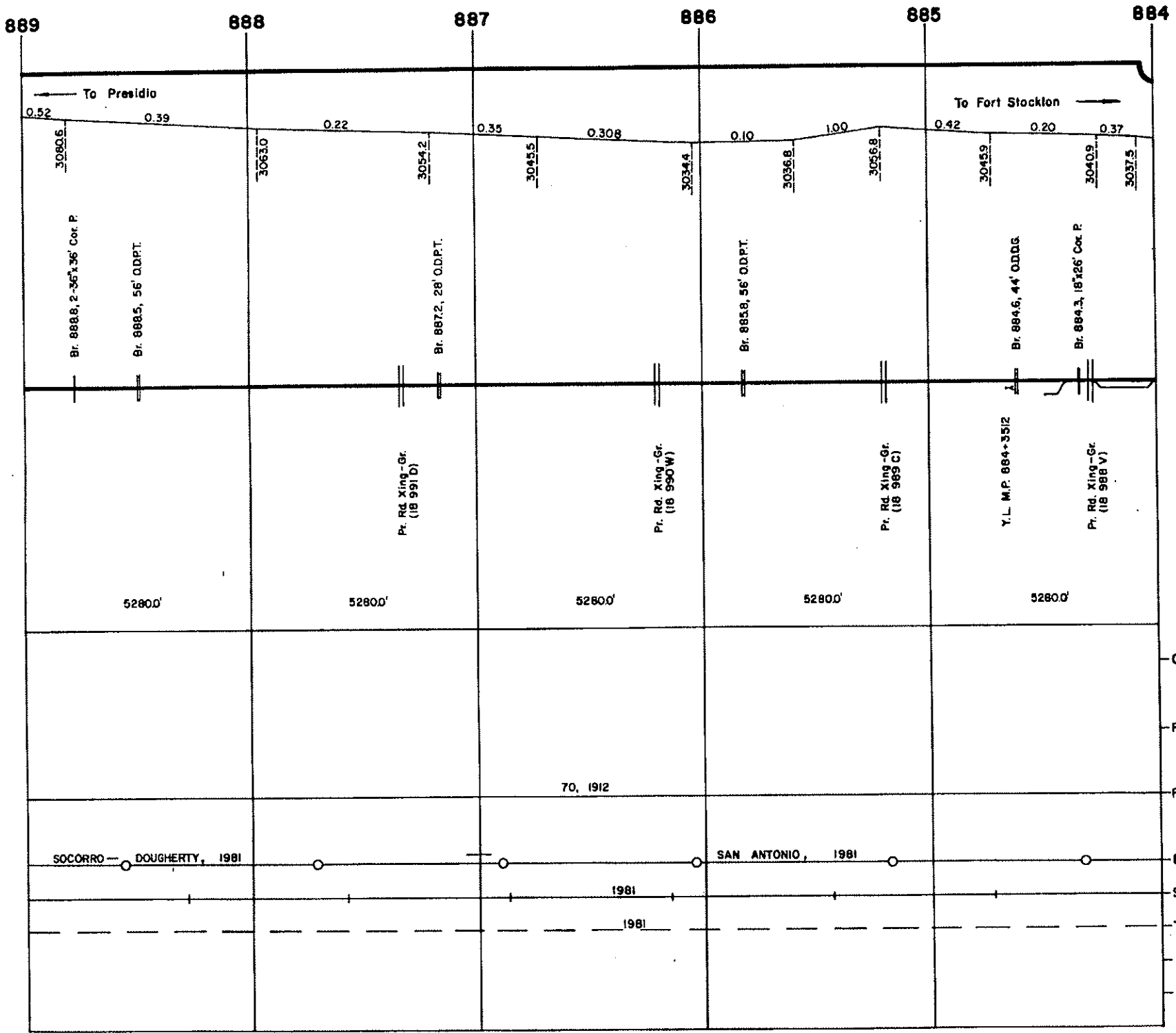
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TIES

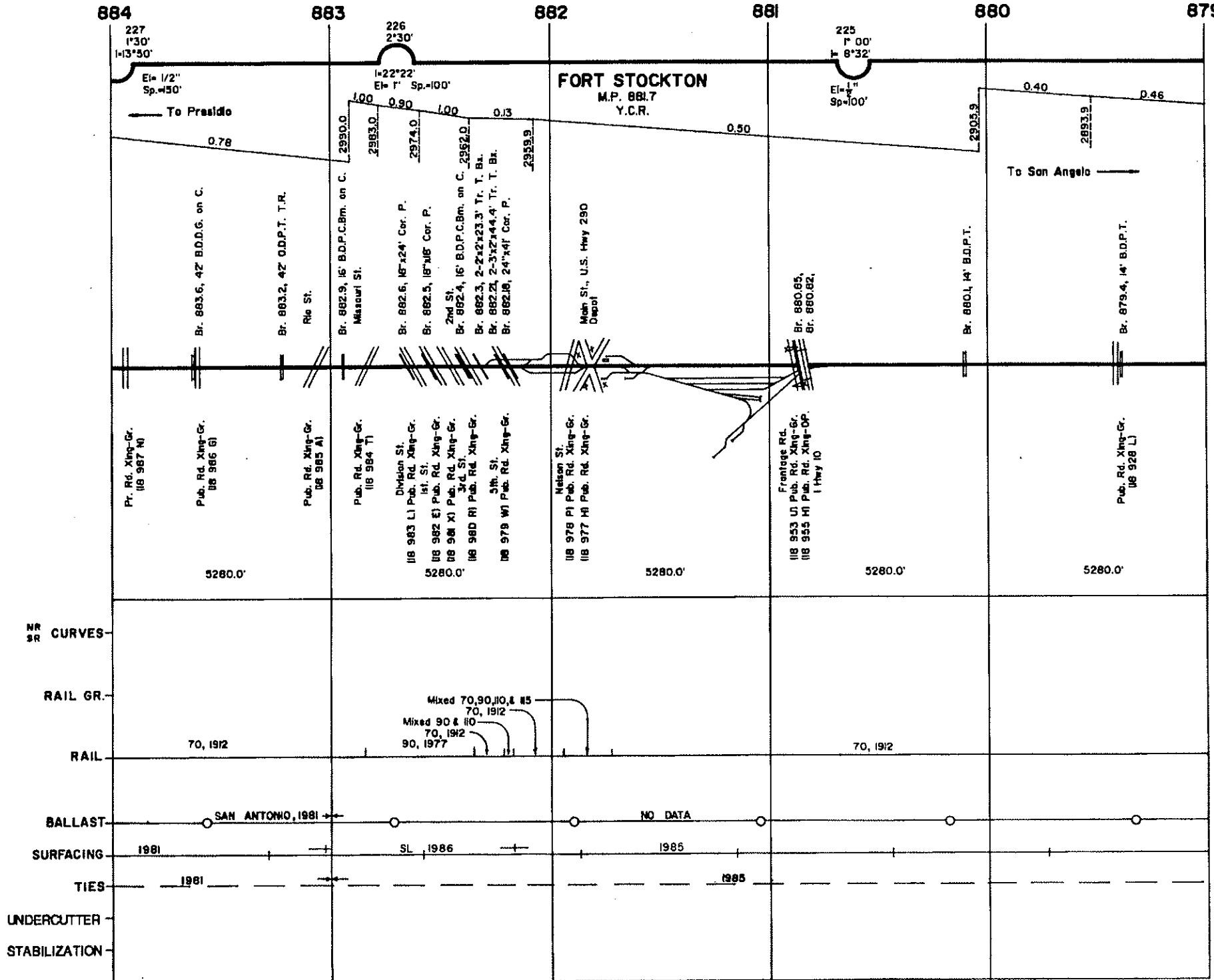
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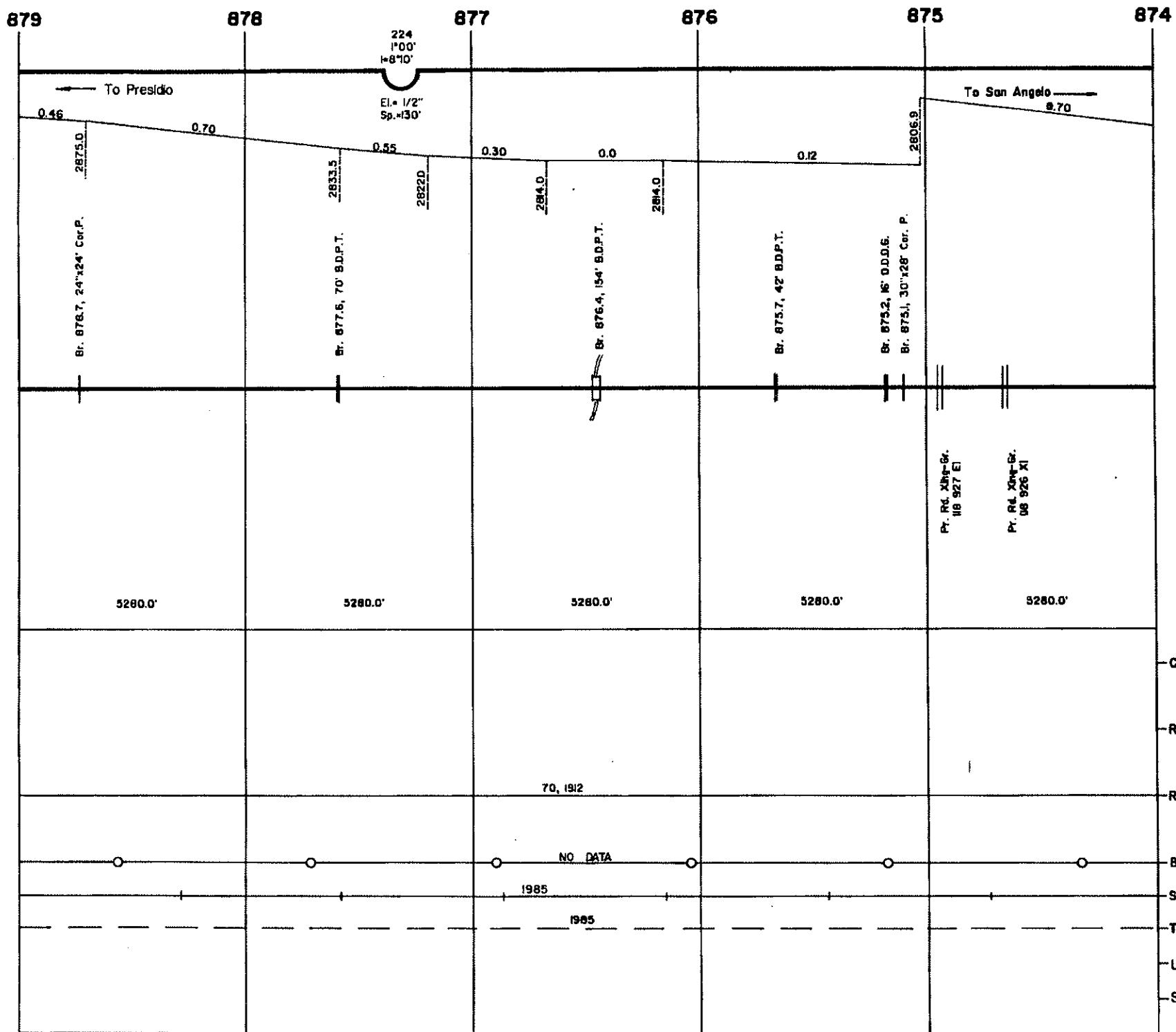
STABILIZATION

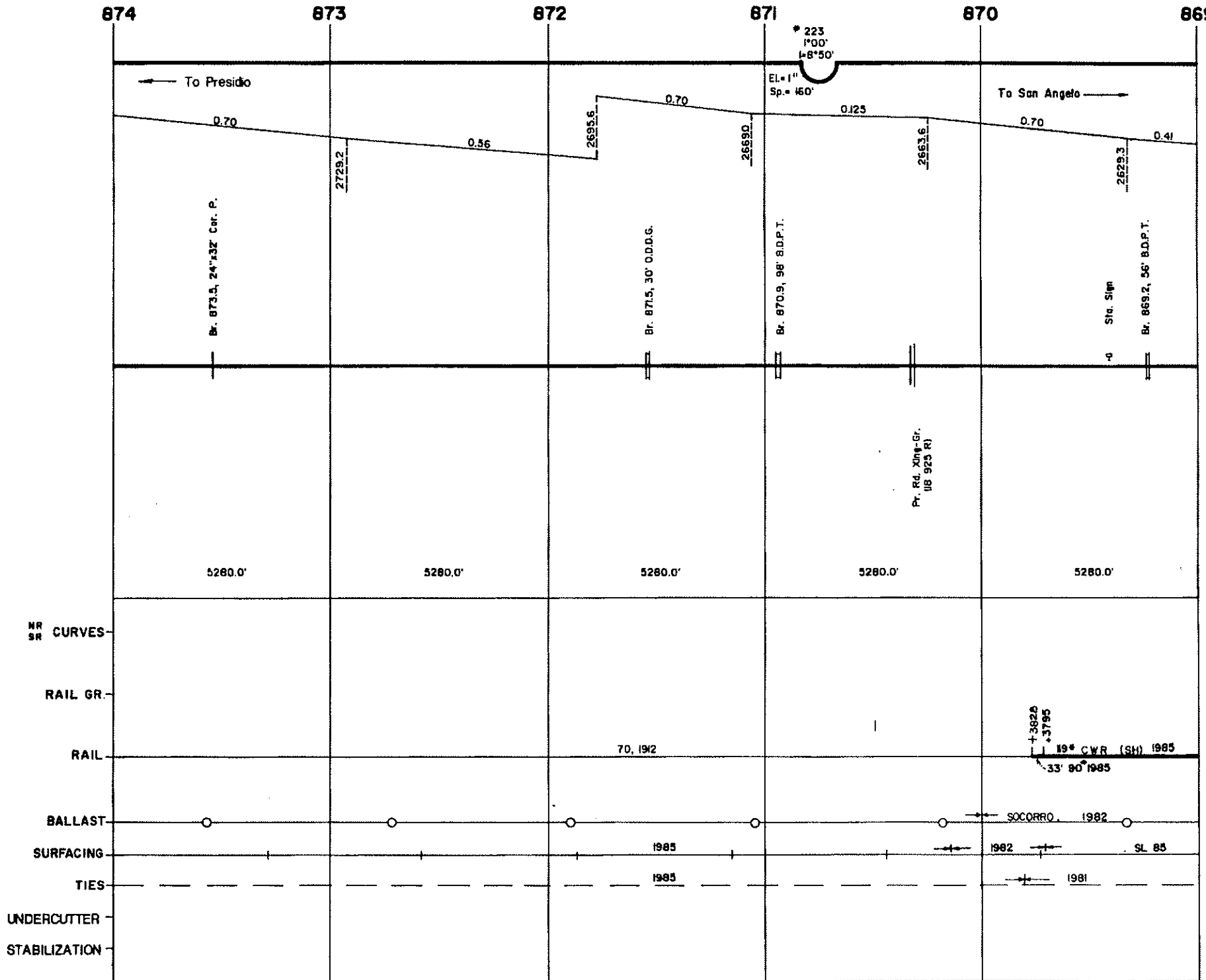


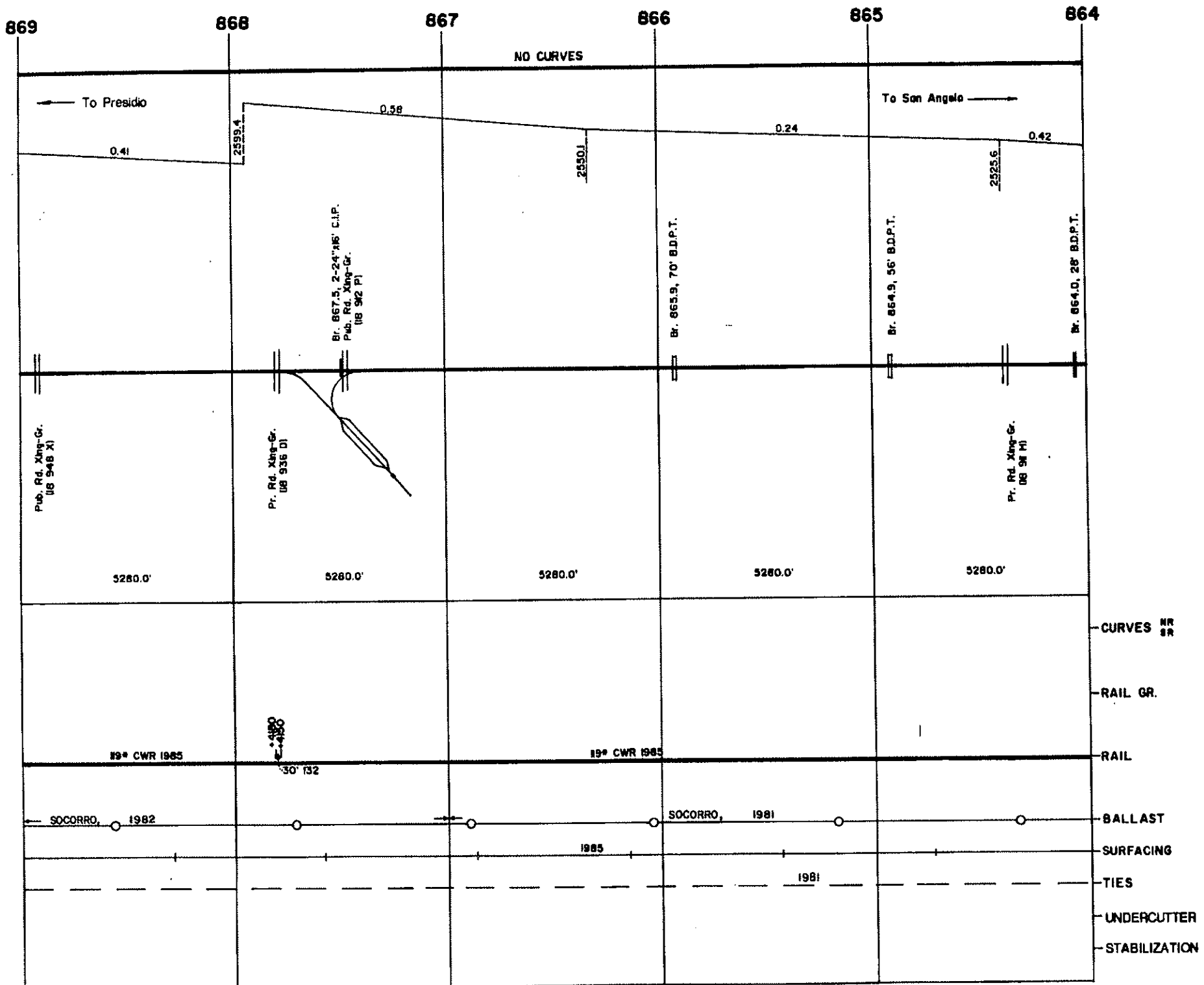


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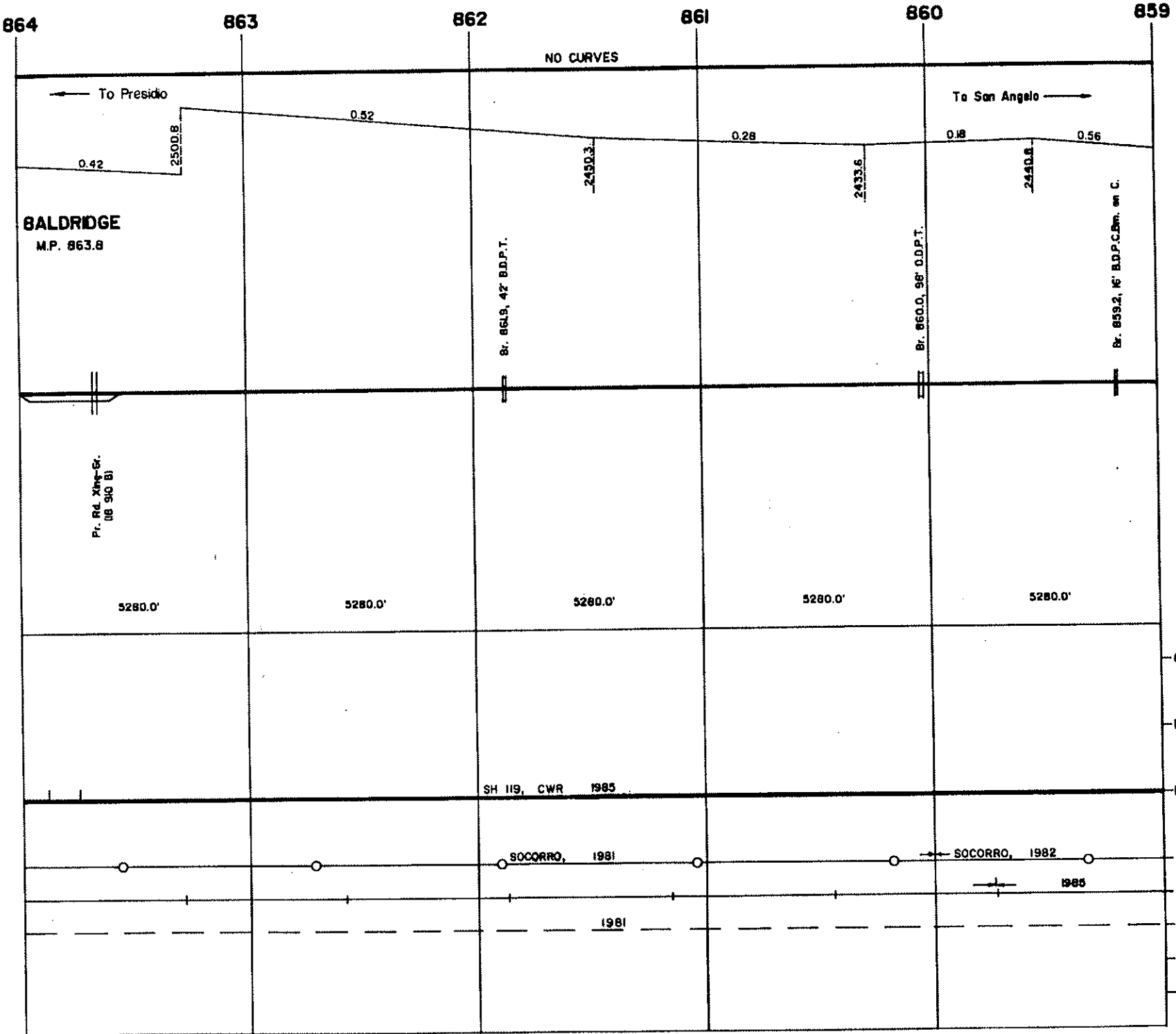


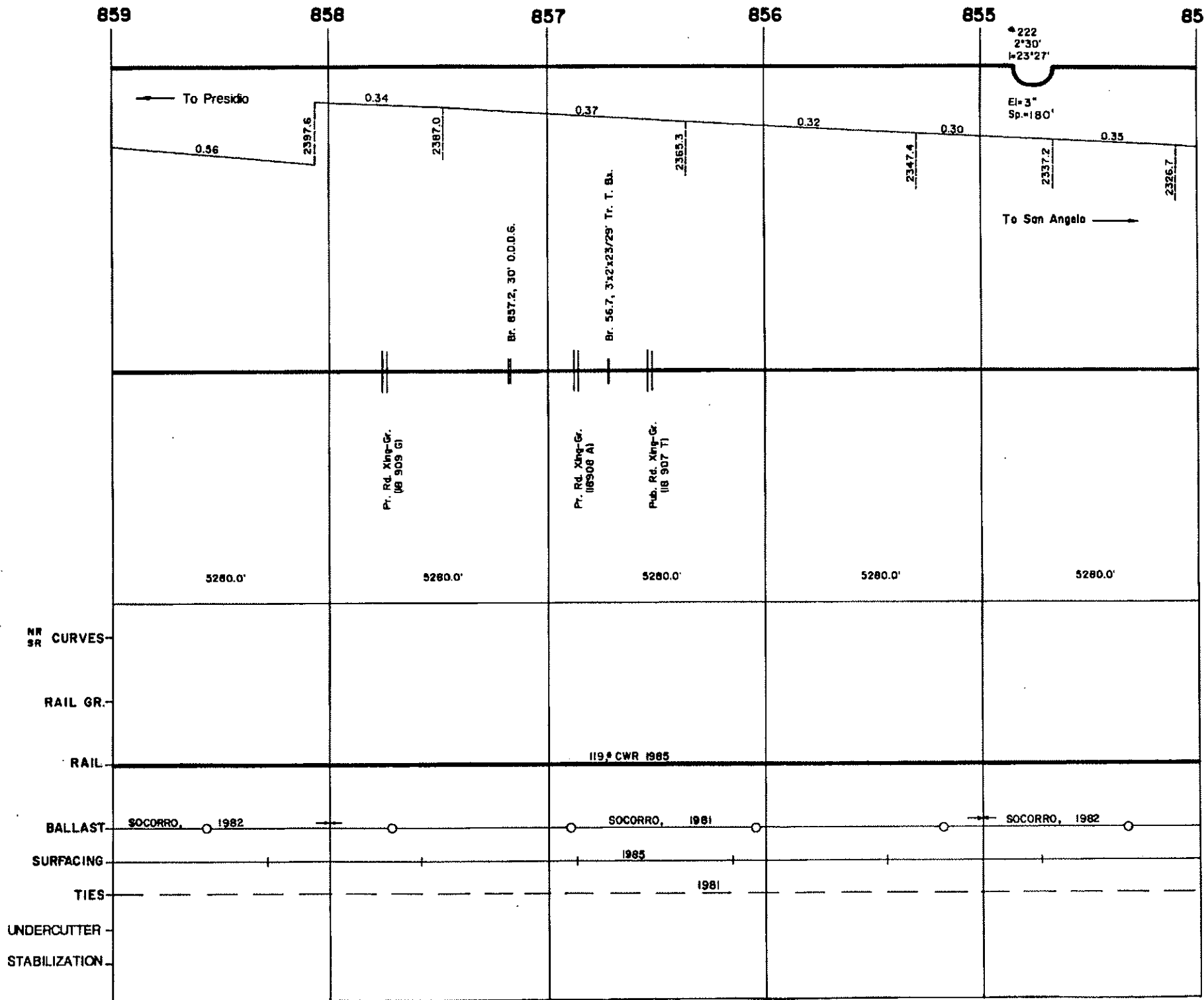


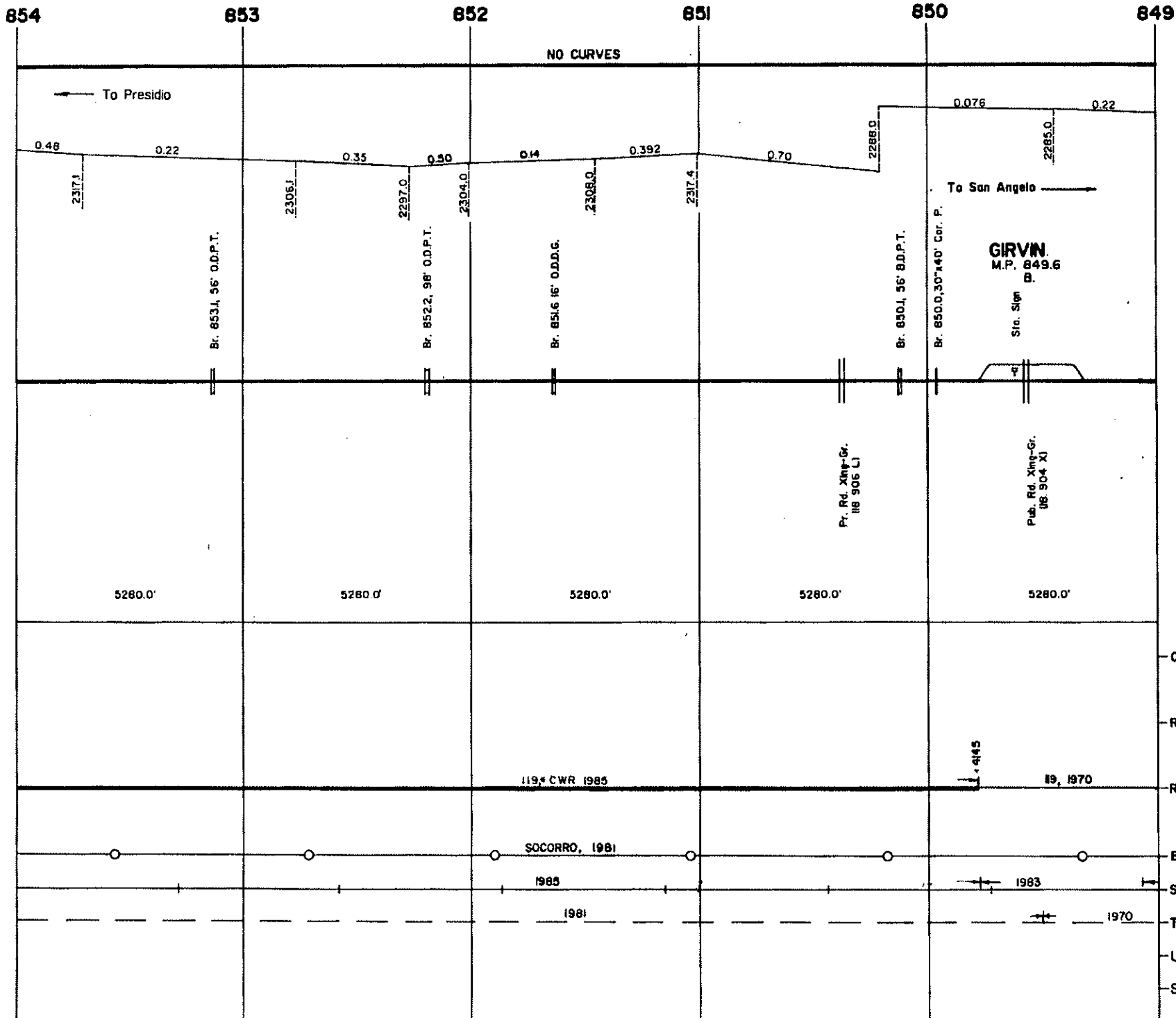


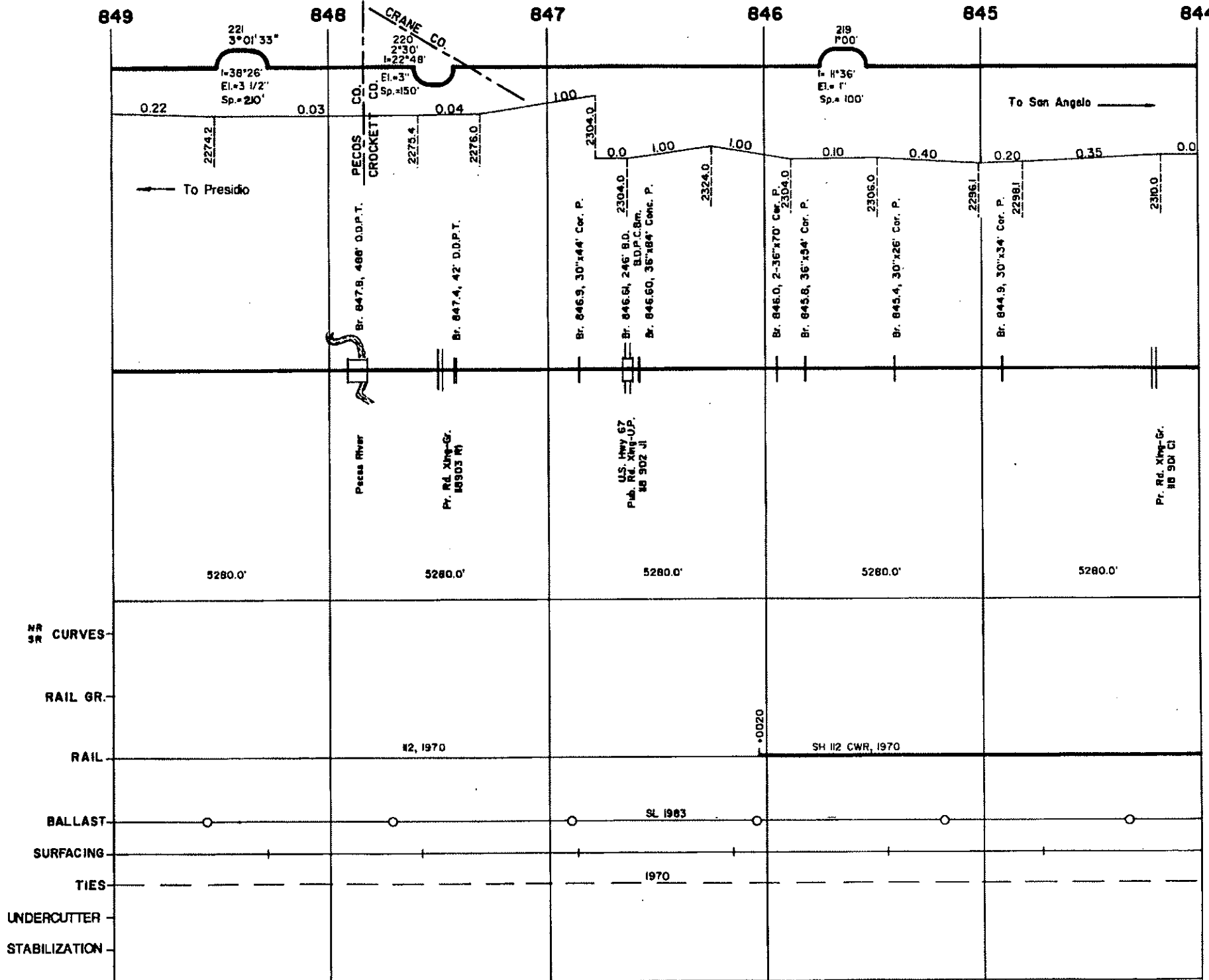


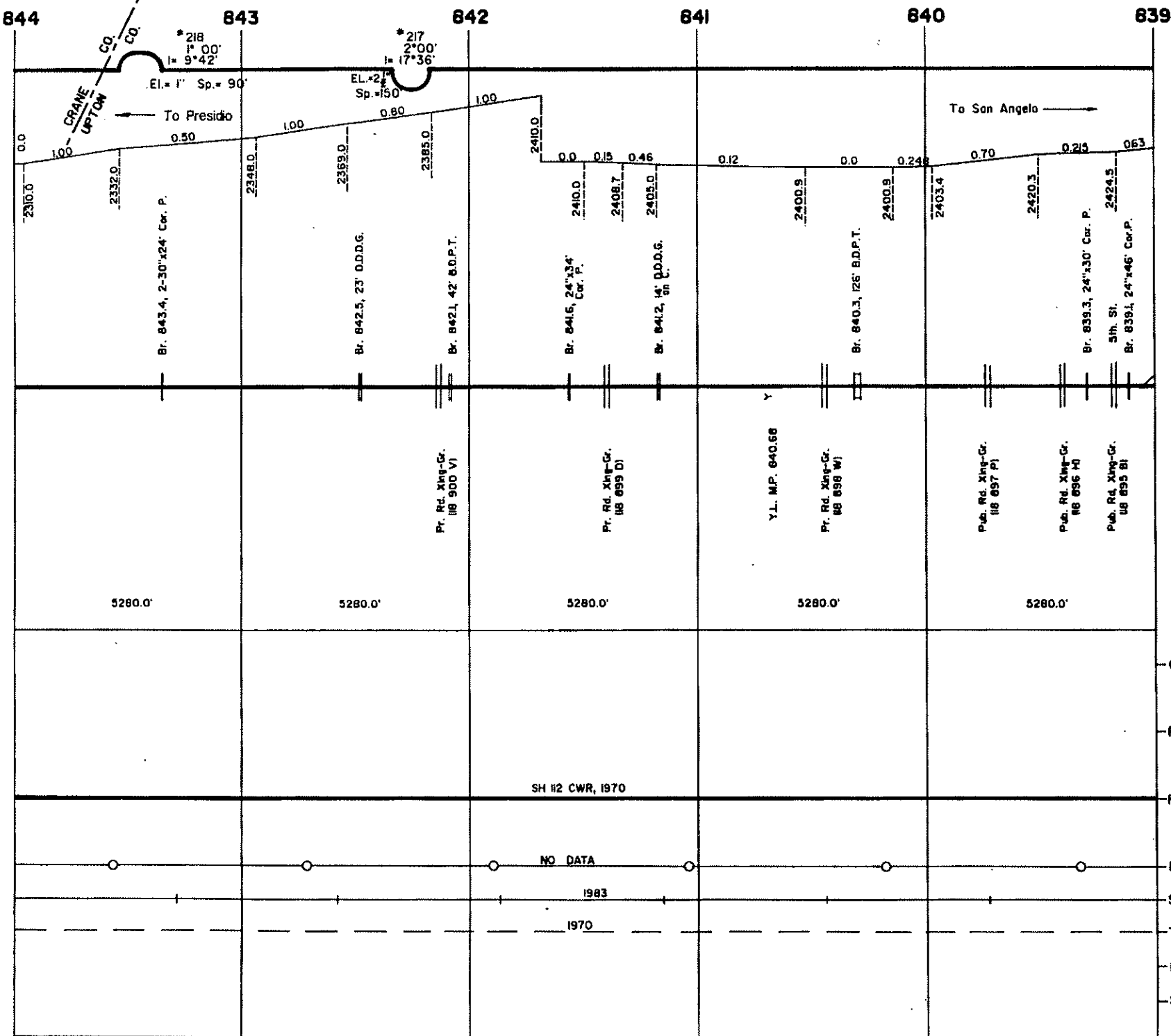
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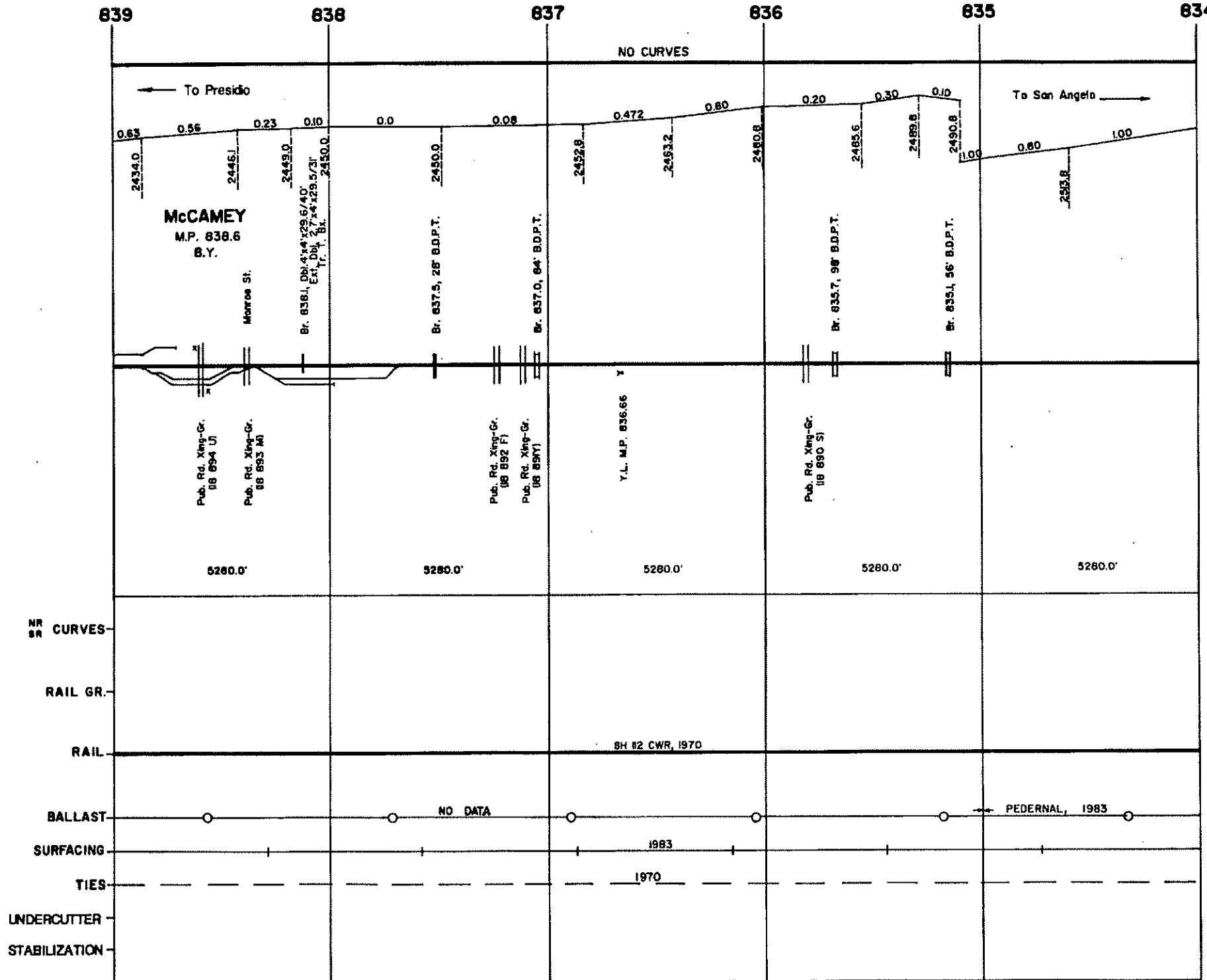


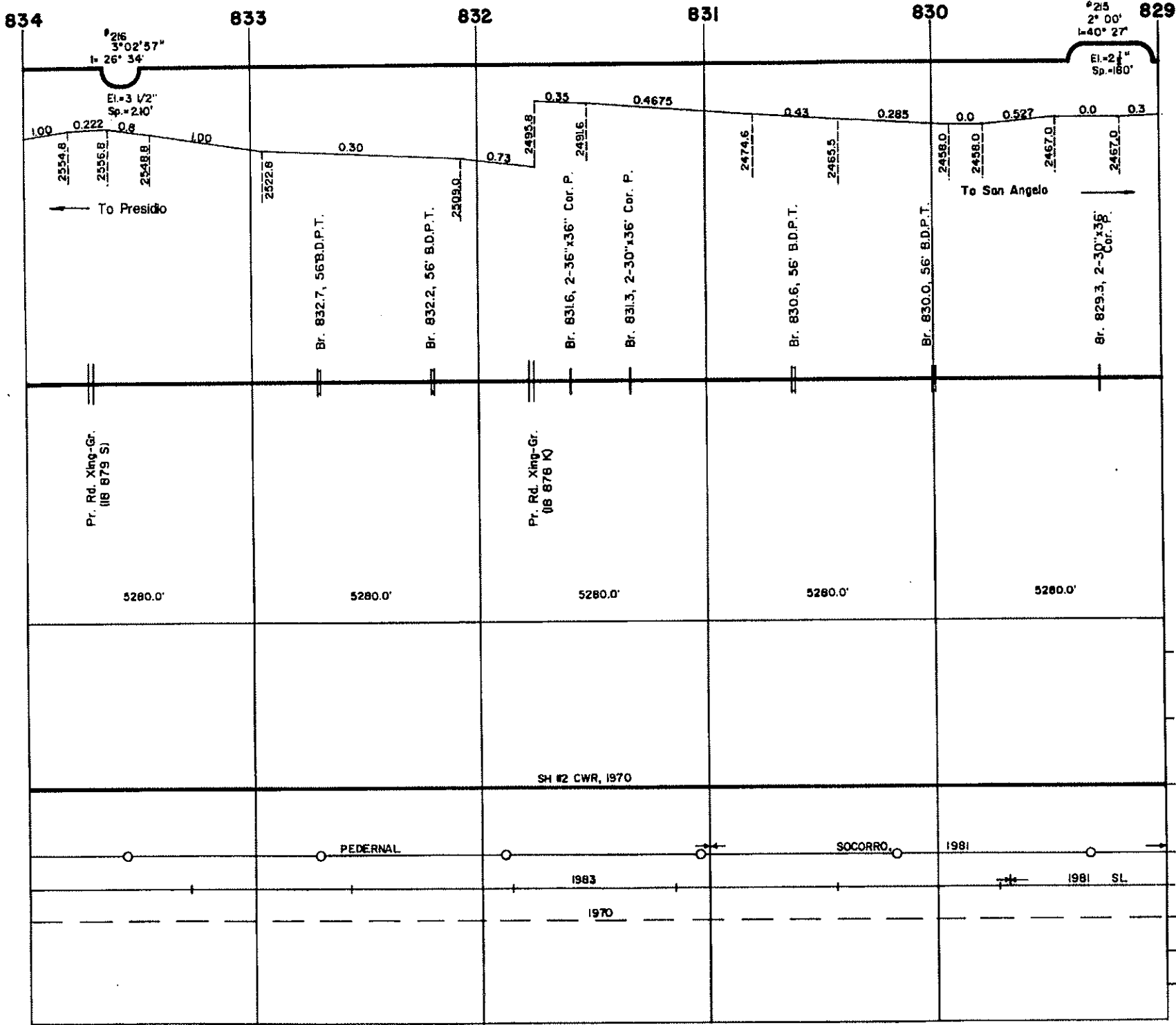






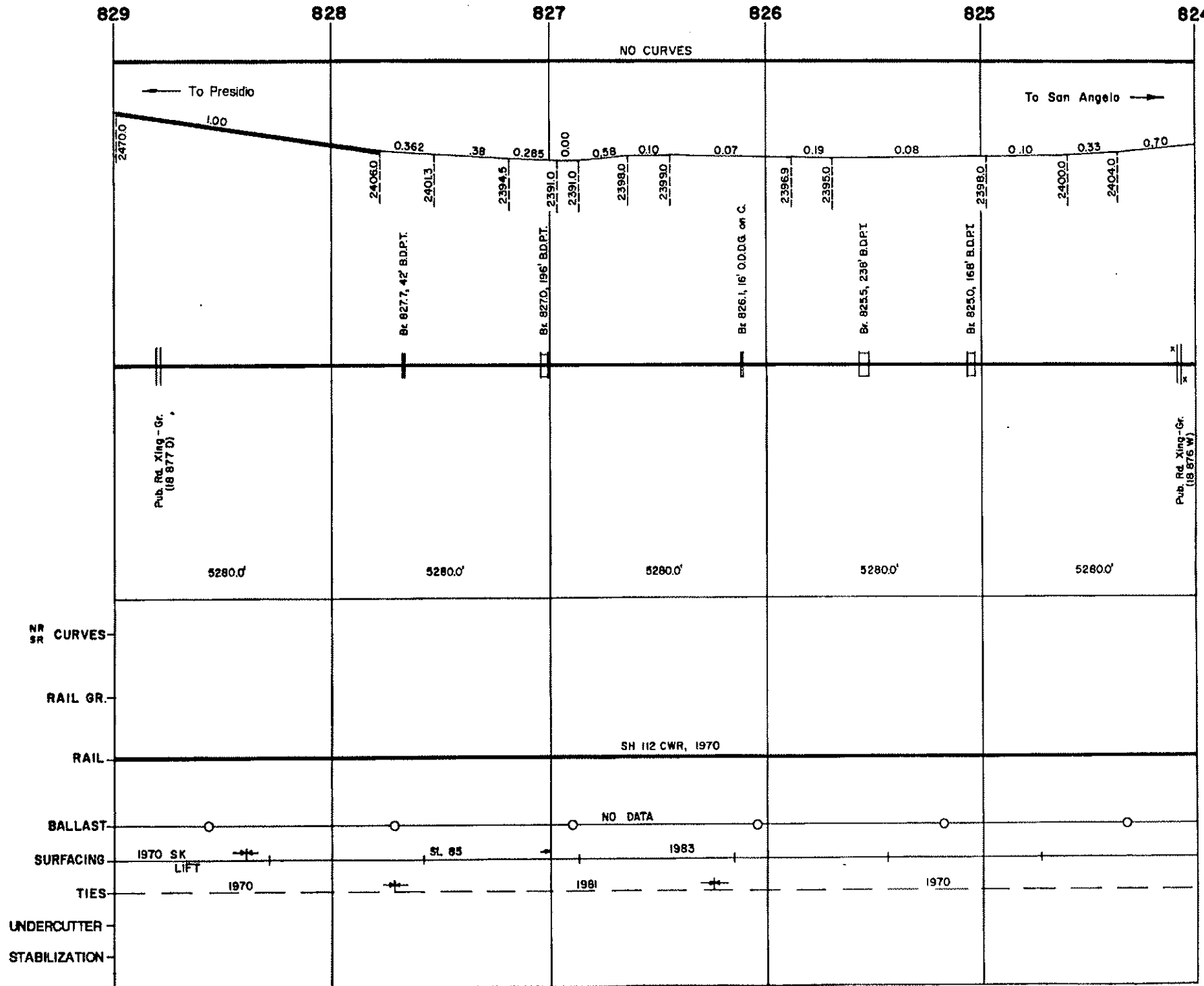


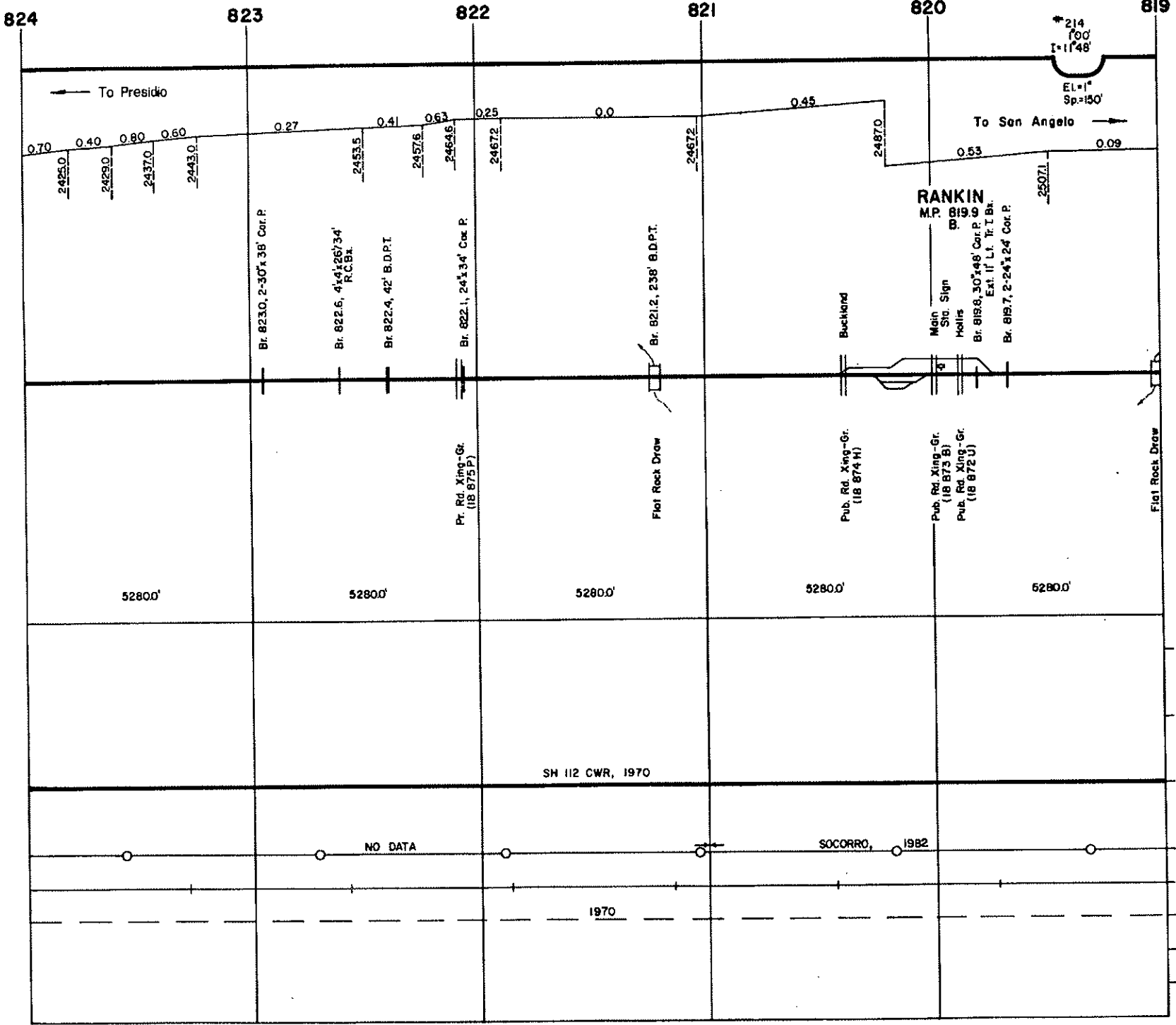




CURVES NR BR  
RAIL GR.  
RAIL  
BALLAST  
SURFACING  
TIES  
UNDERCUTTER  
STABILIZATION







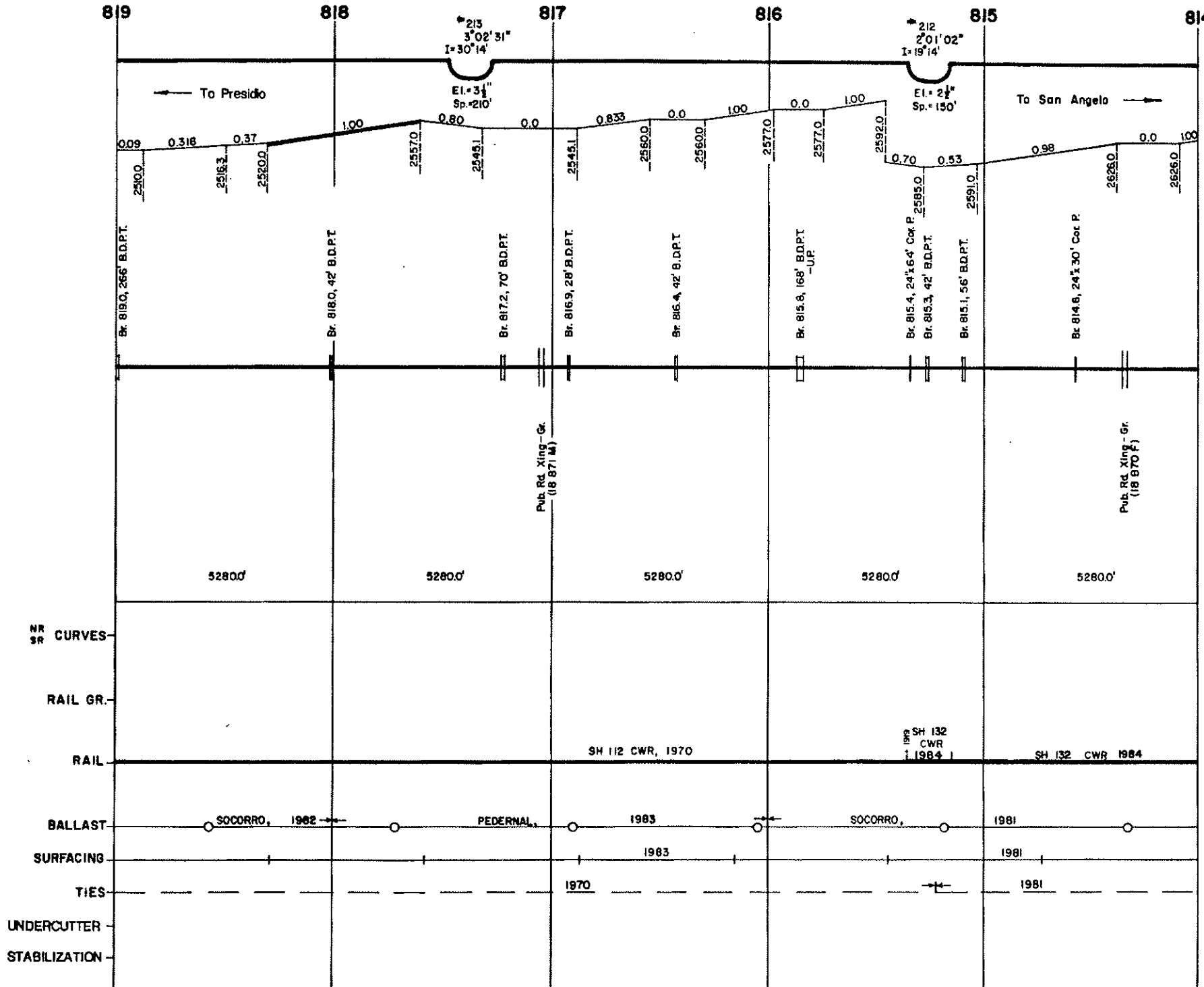
- CURVES NR SR
- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION

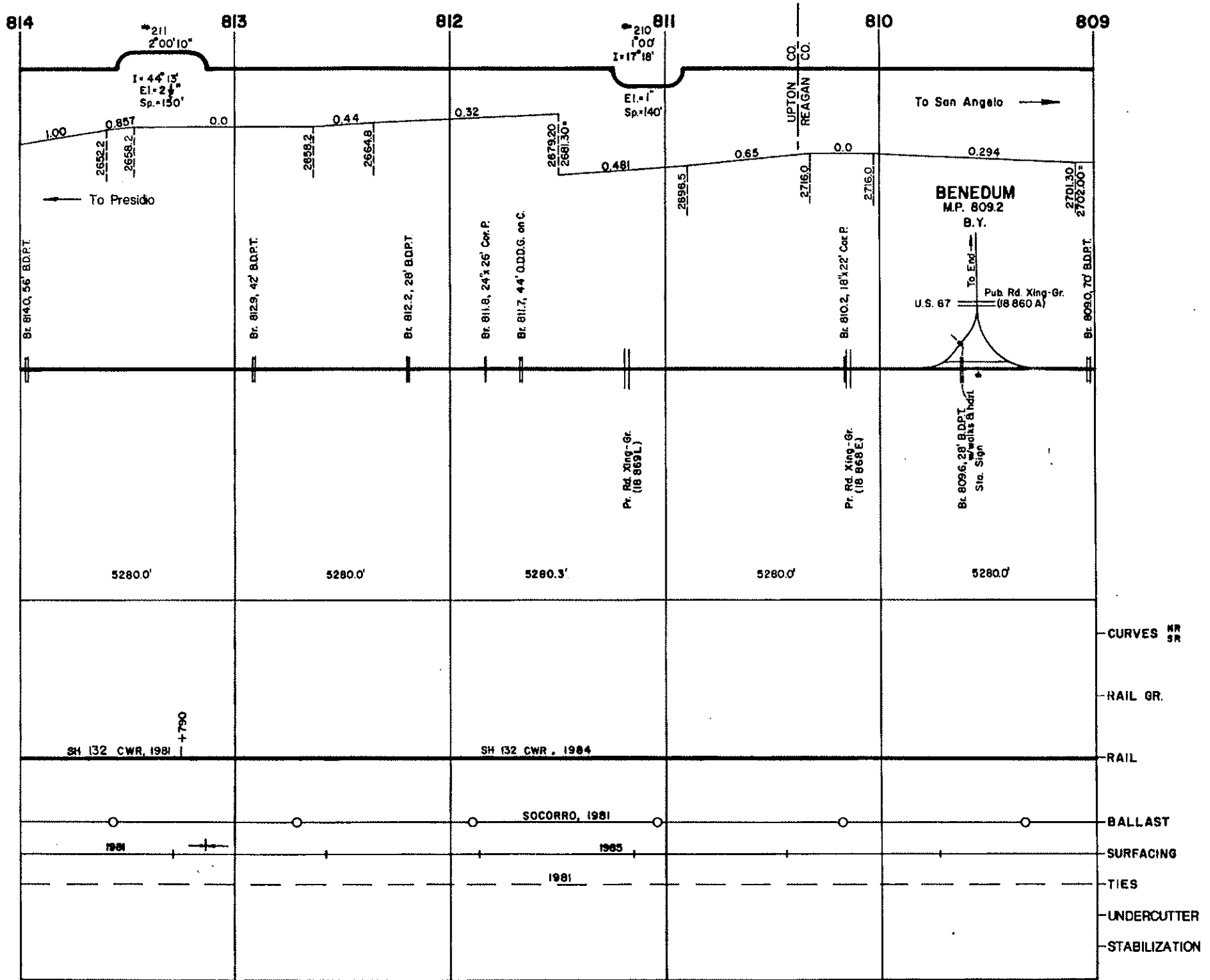
SH 112 CWR, 1970

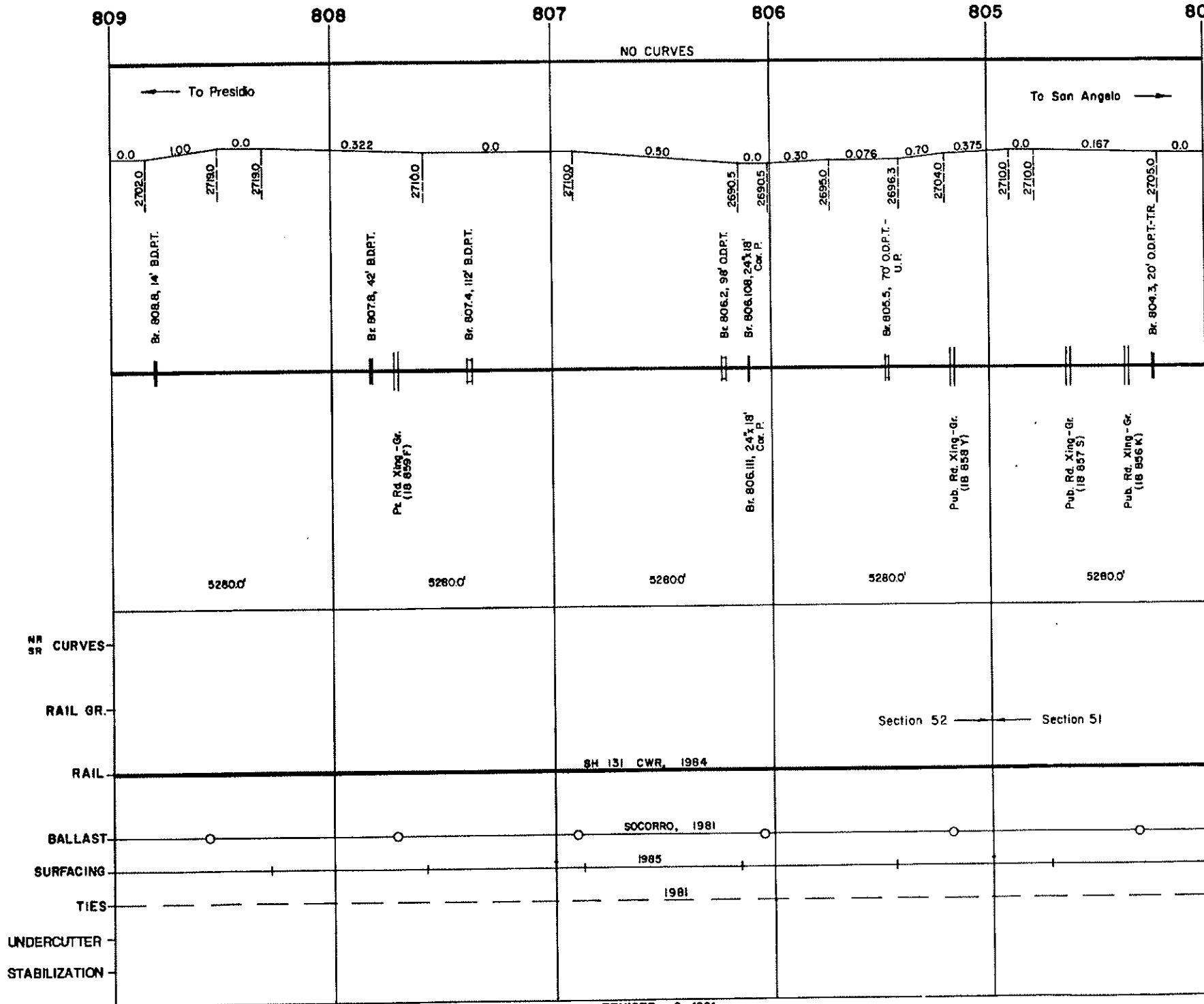
NO DATA

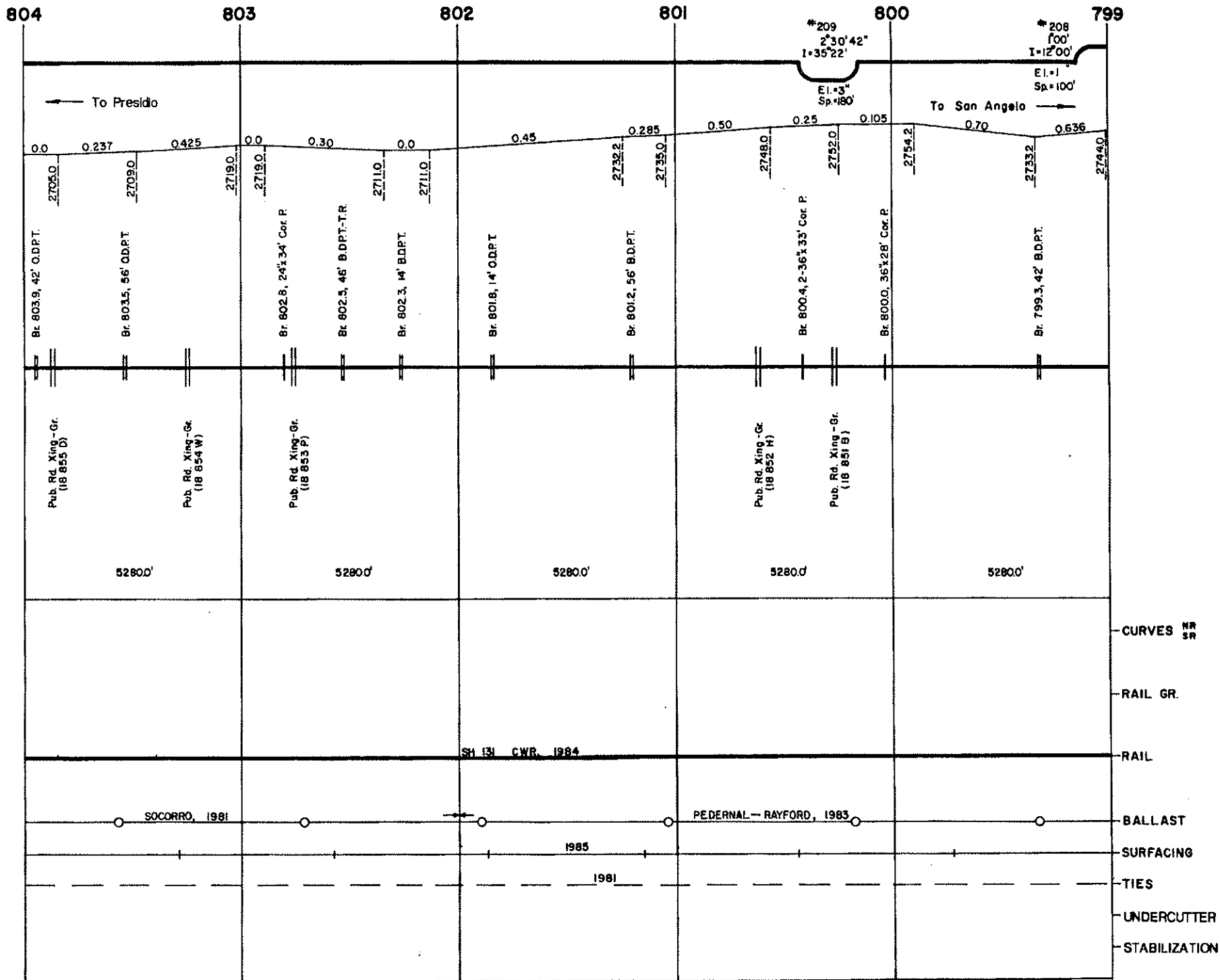
SOCORRO, 1982

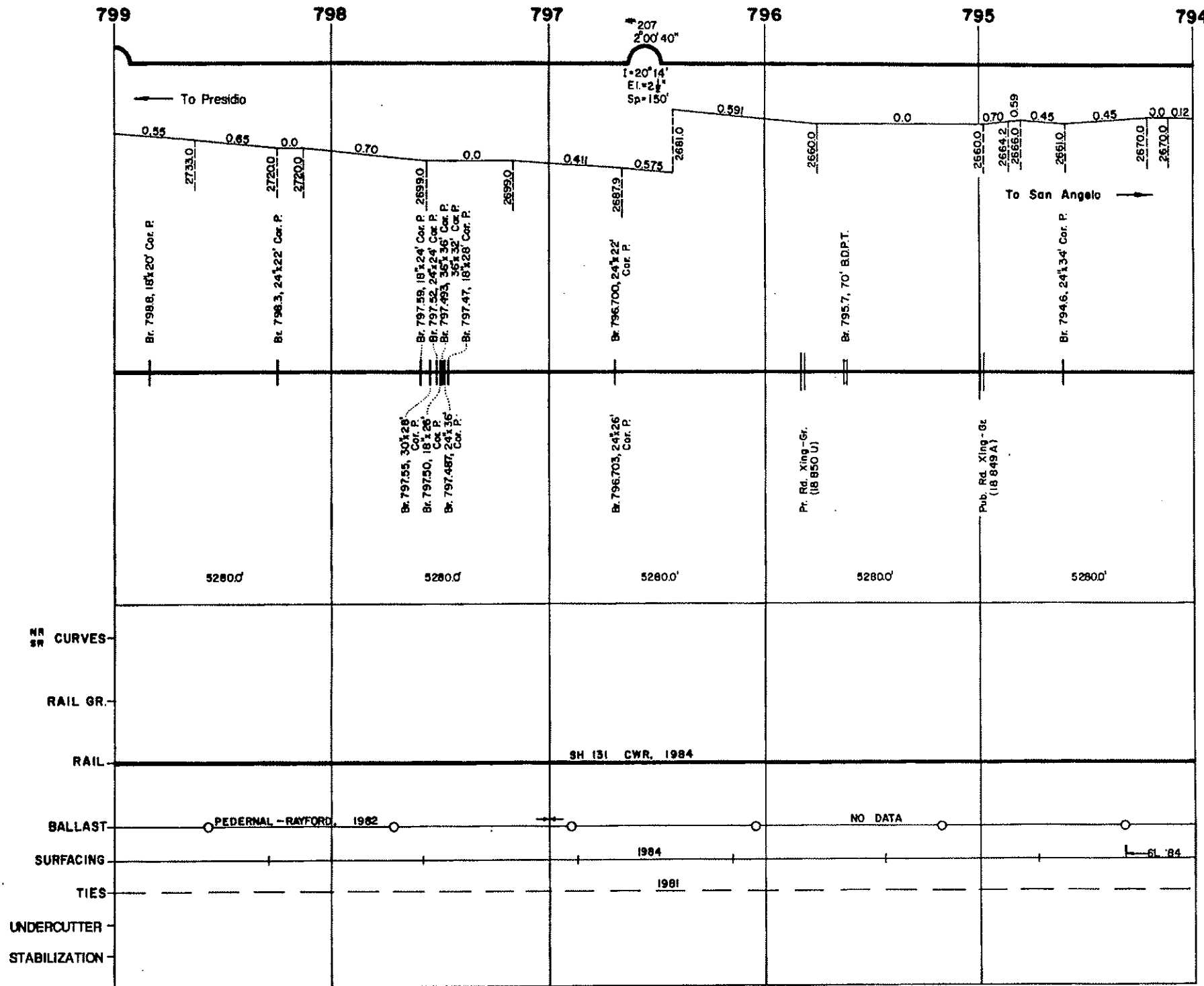
1970

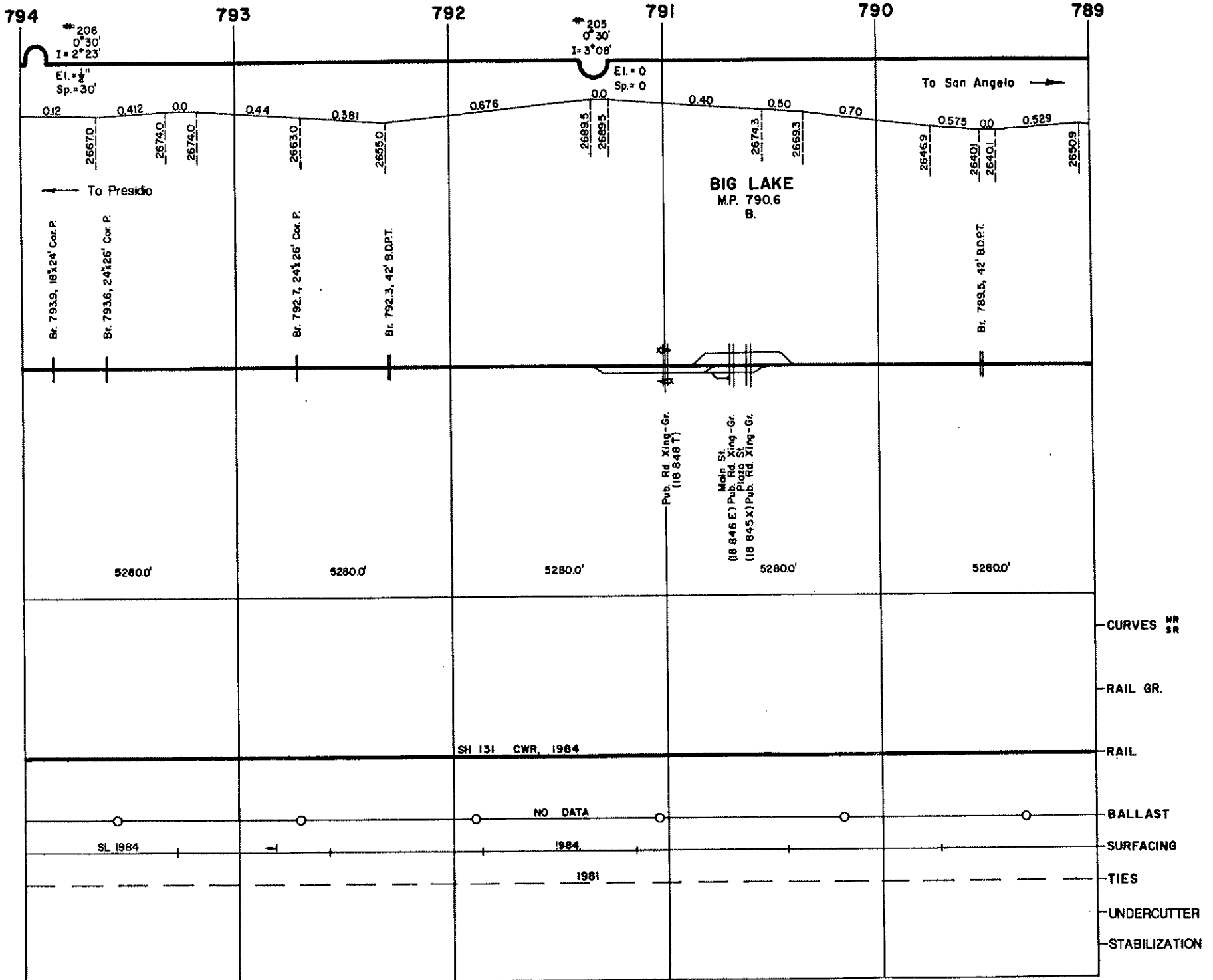




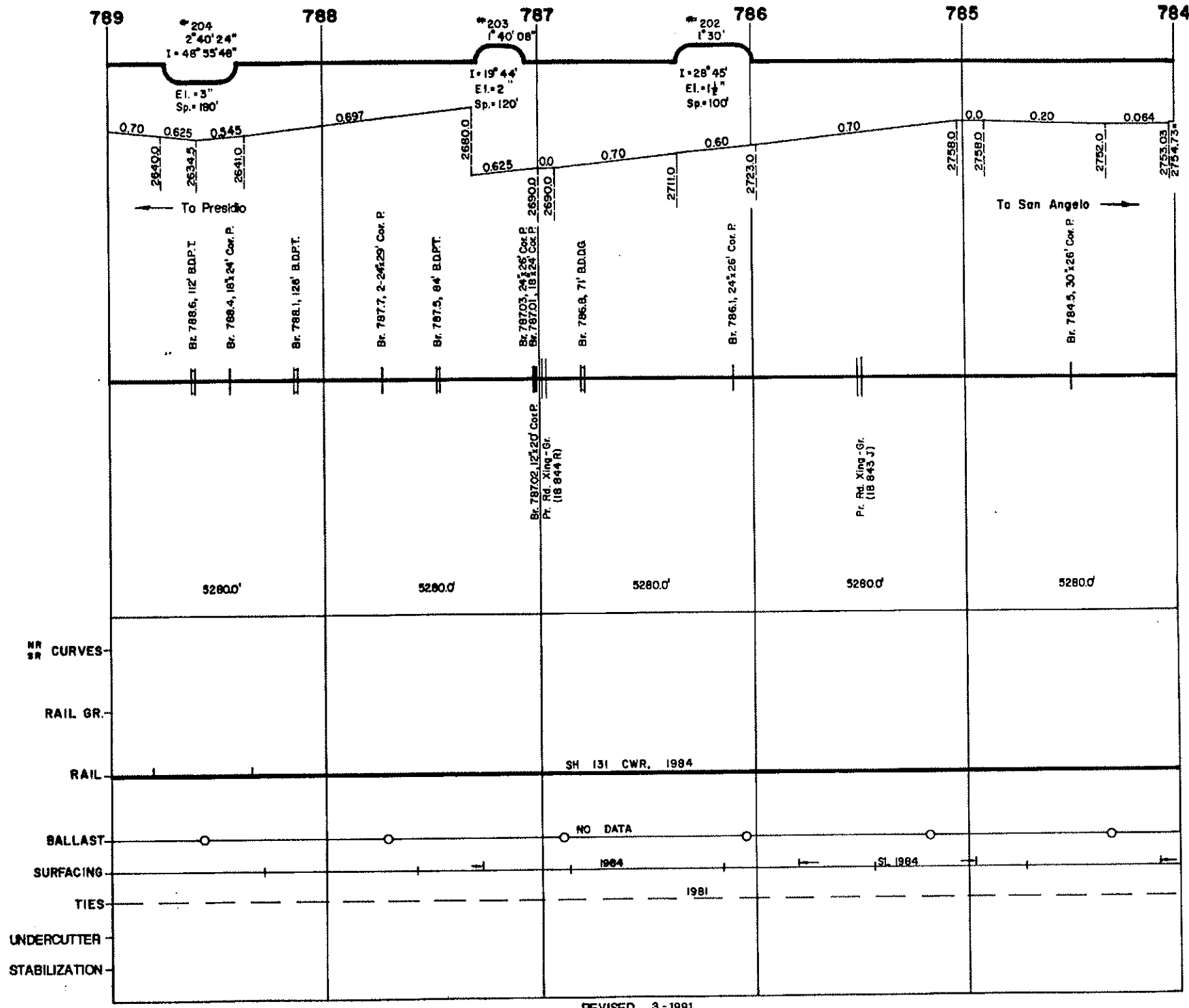


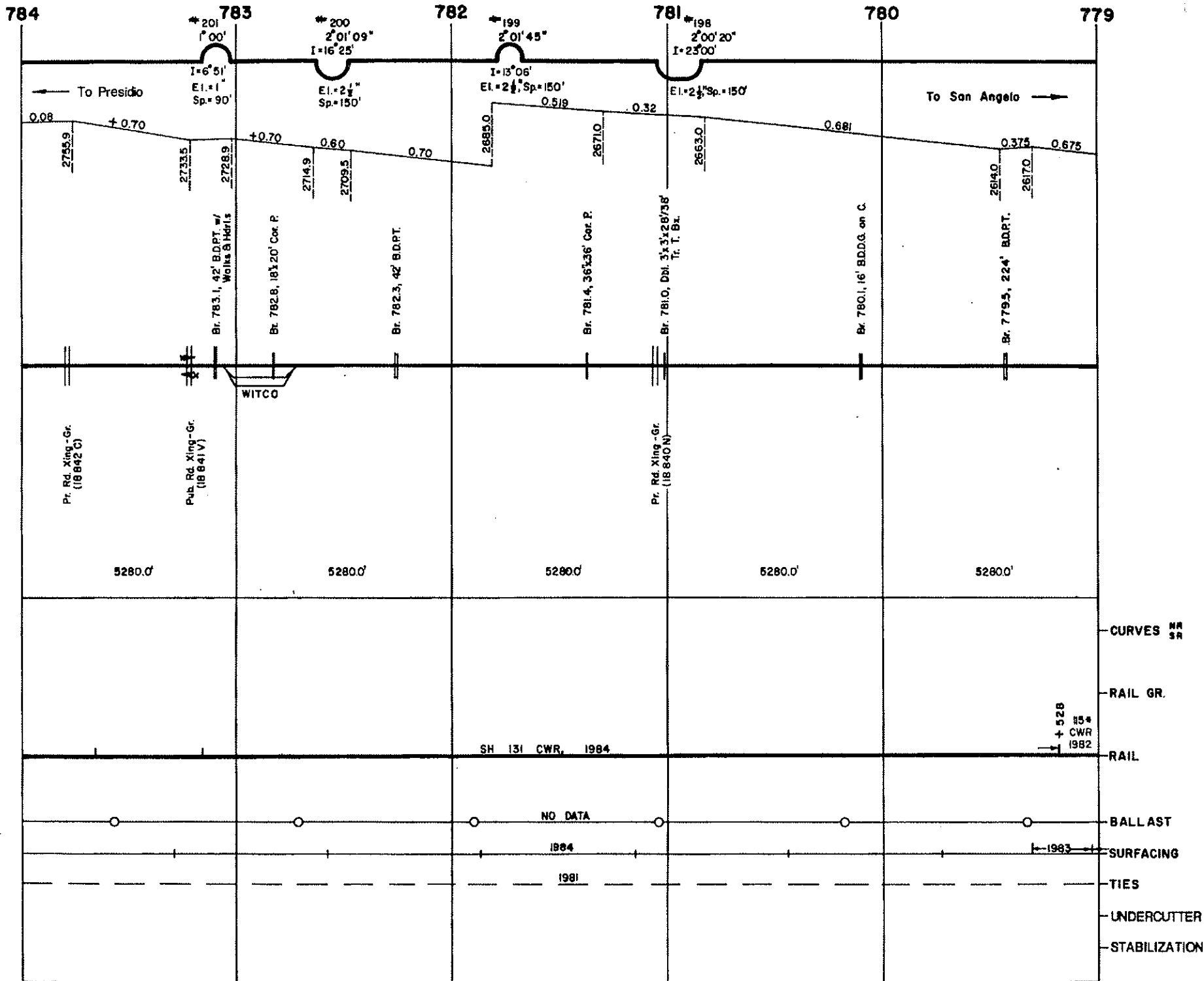


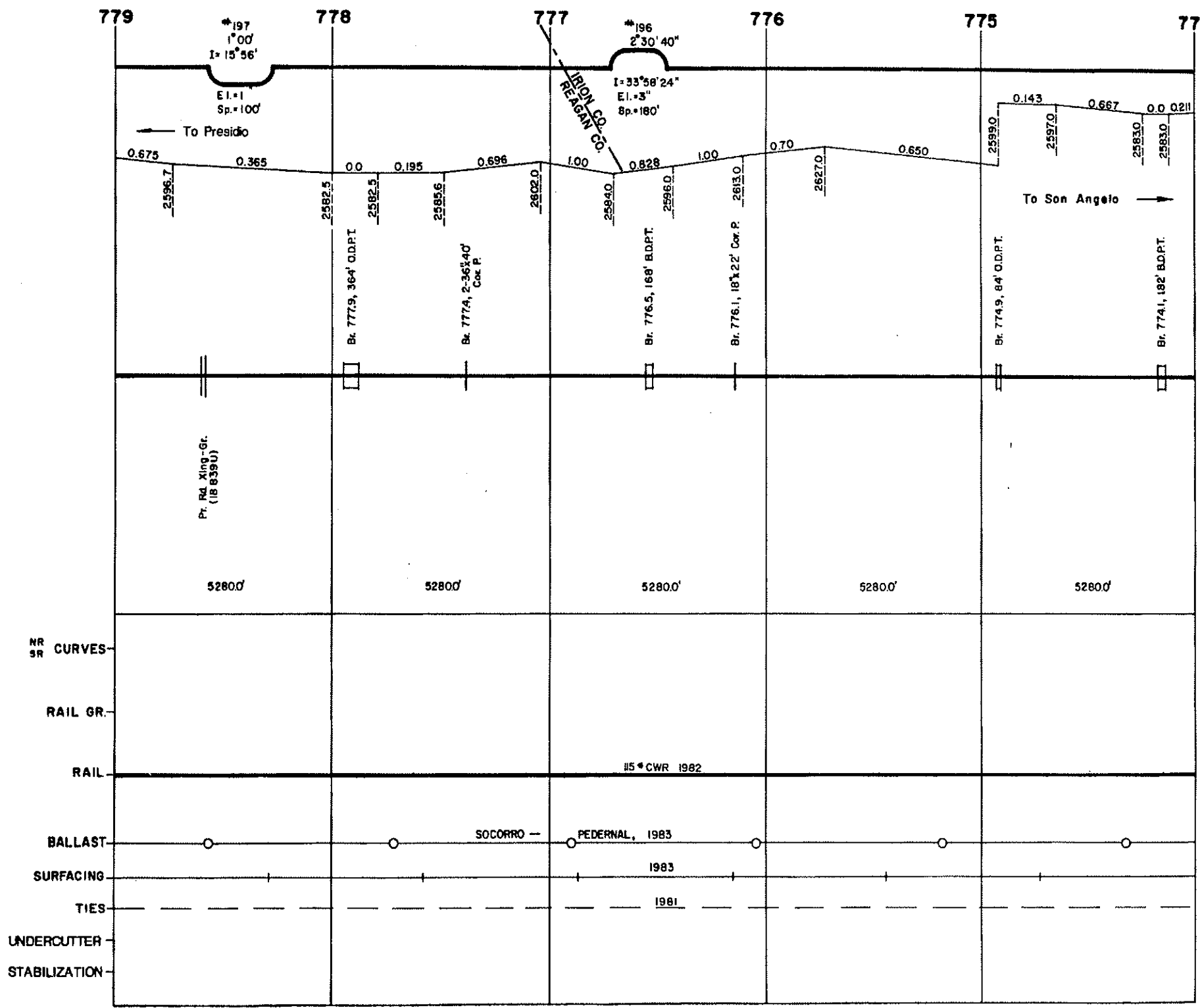


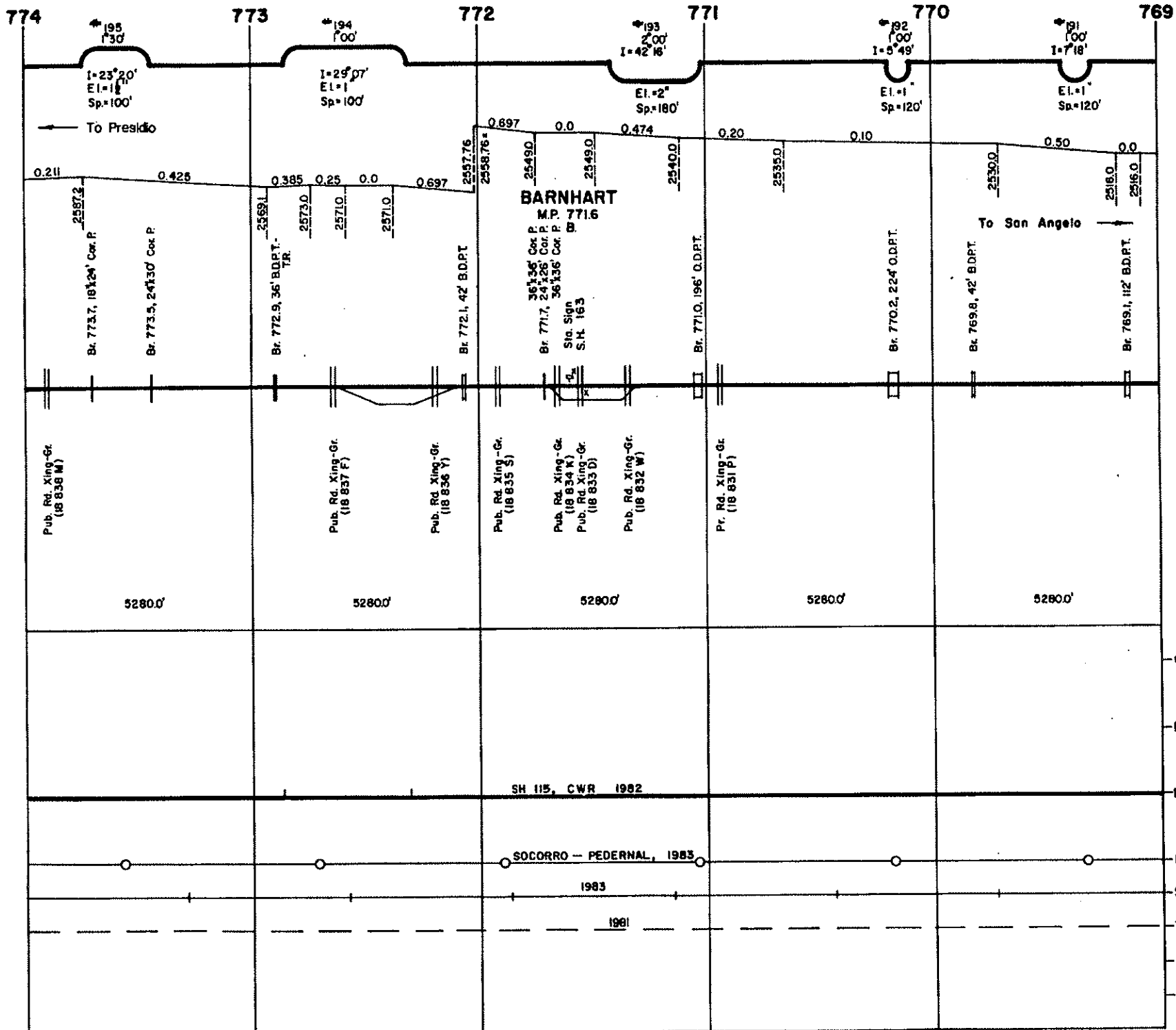


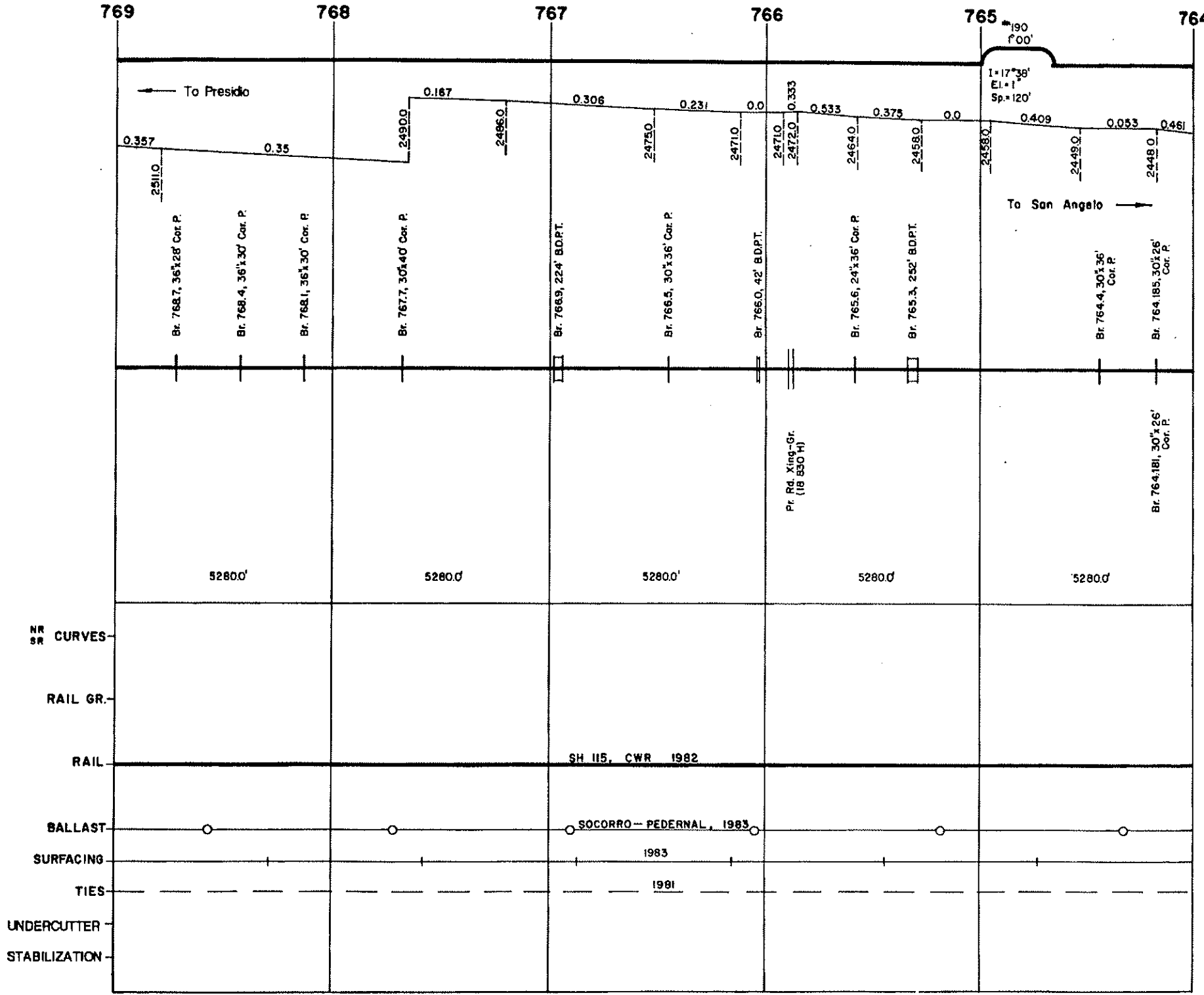


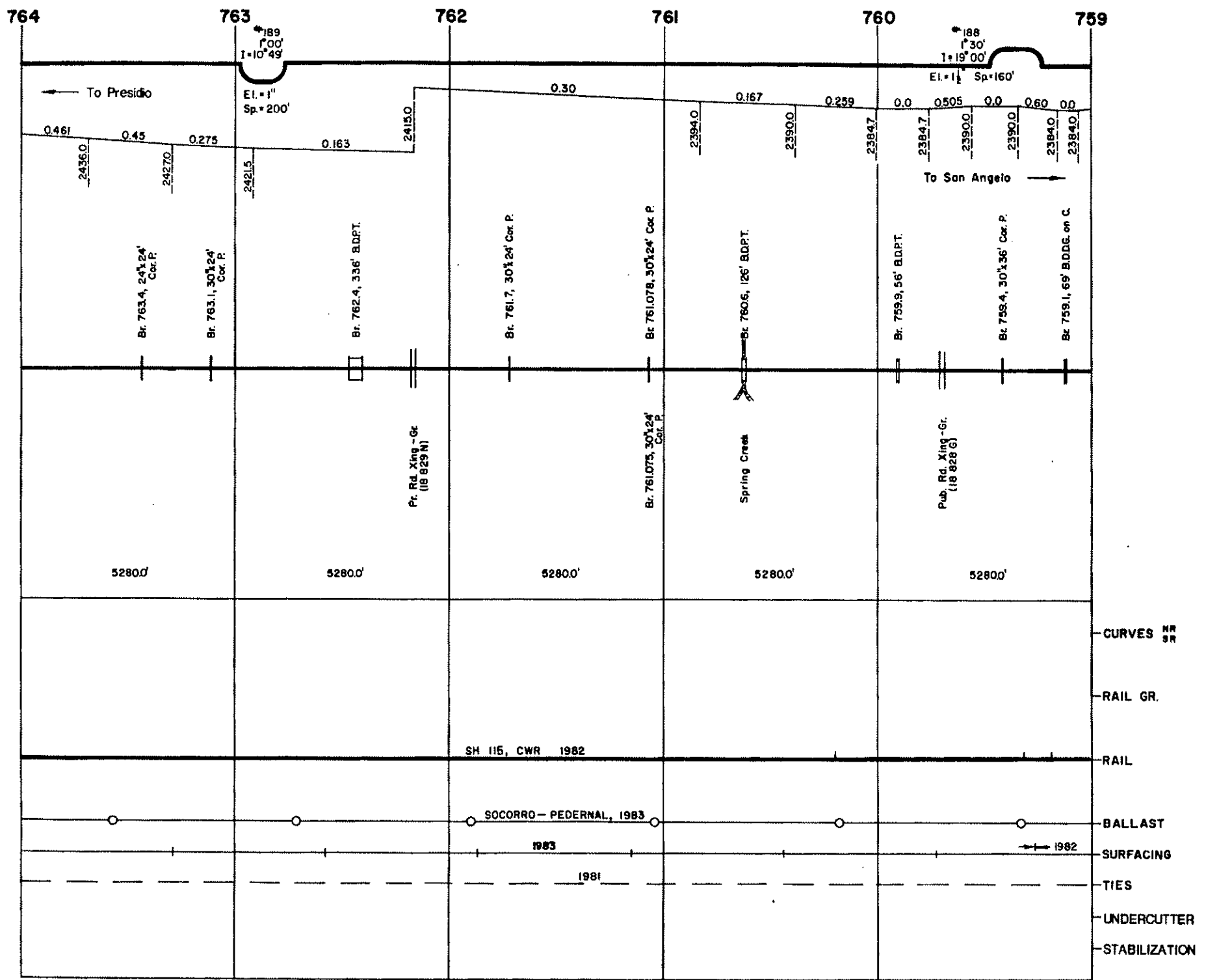




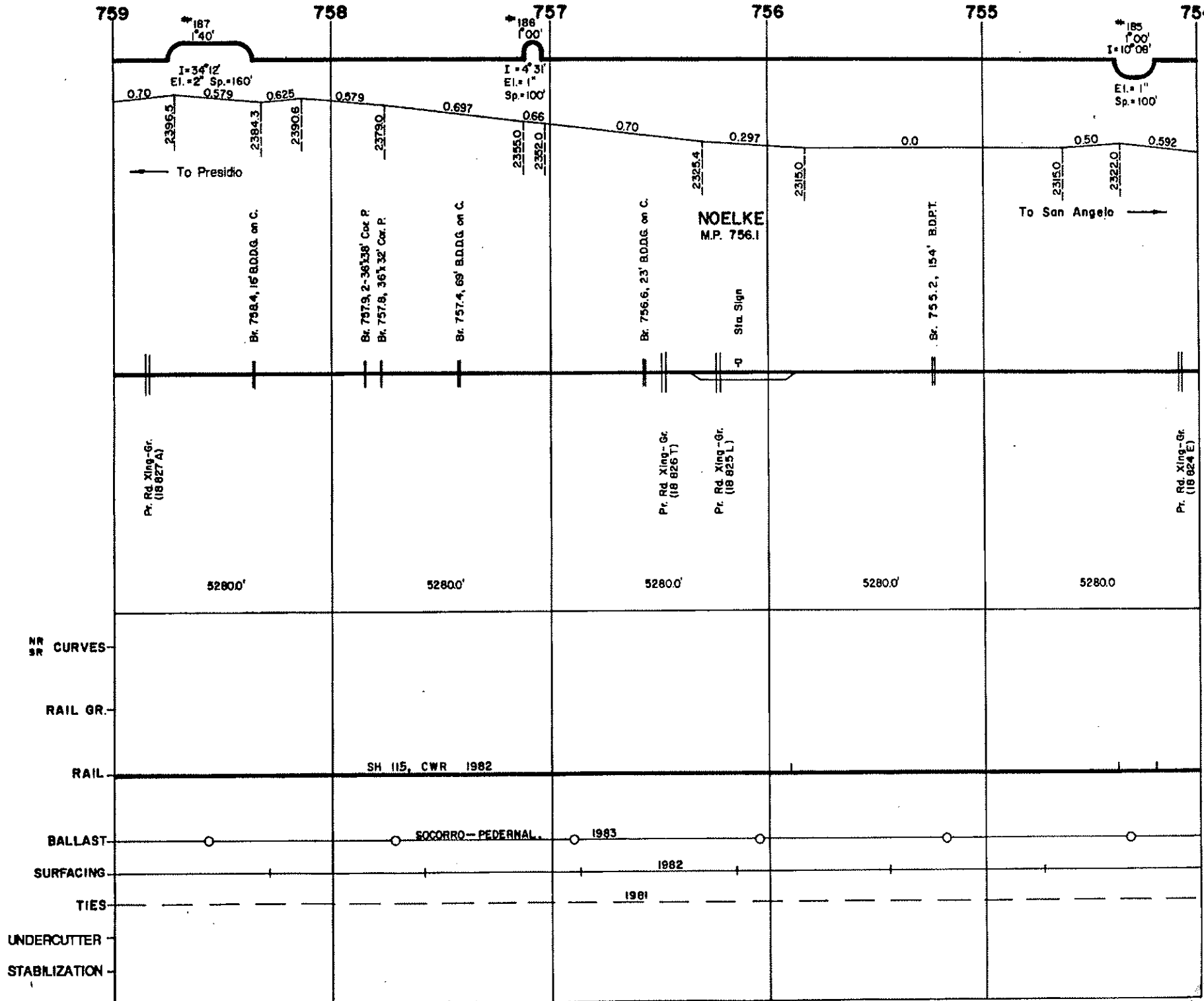


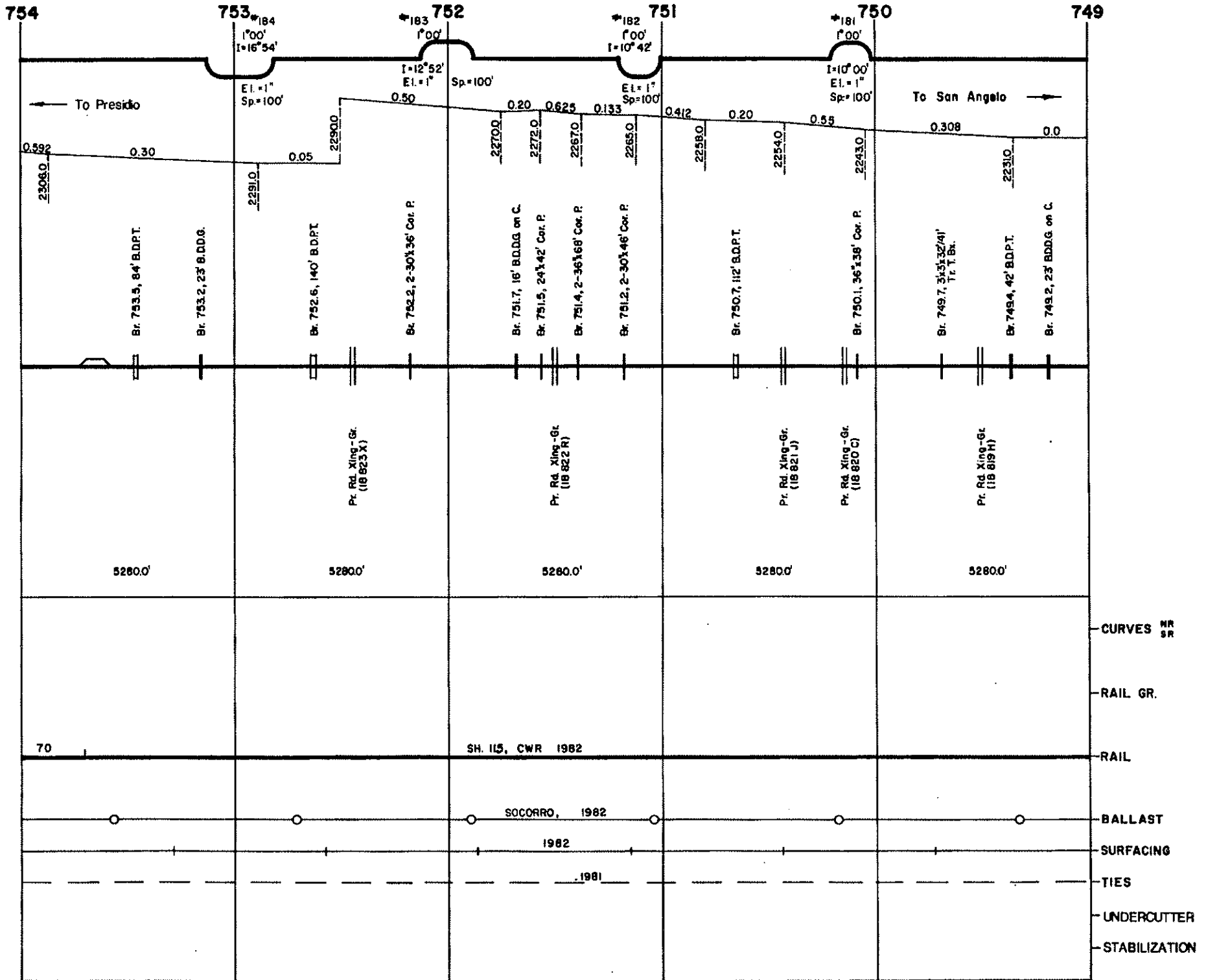




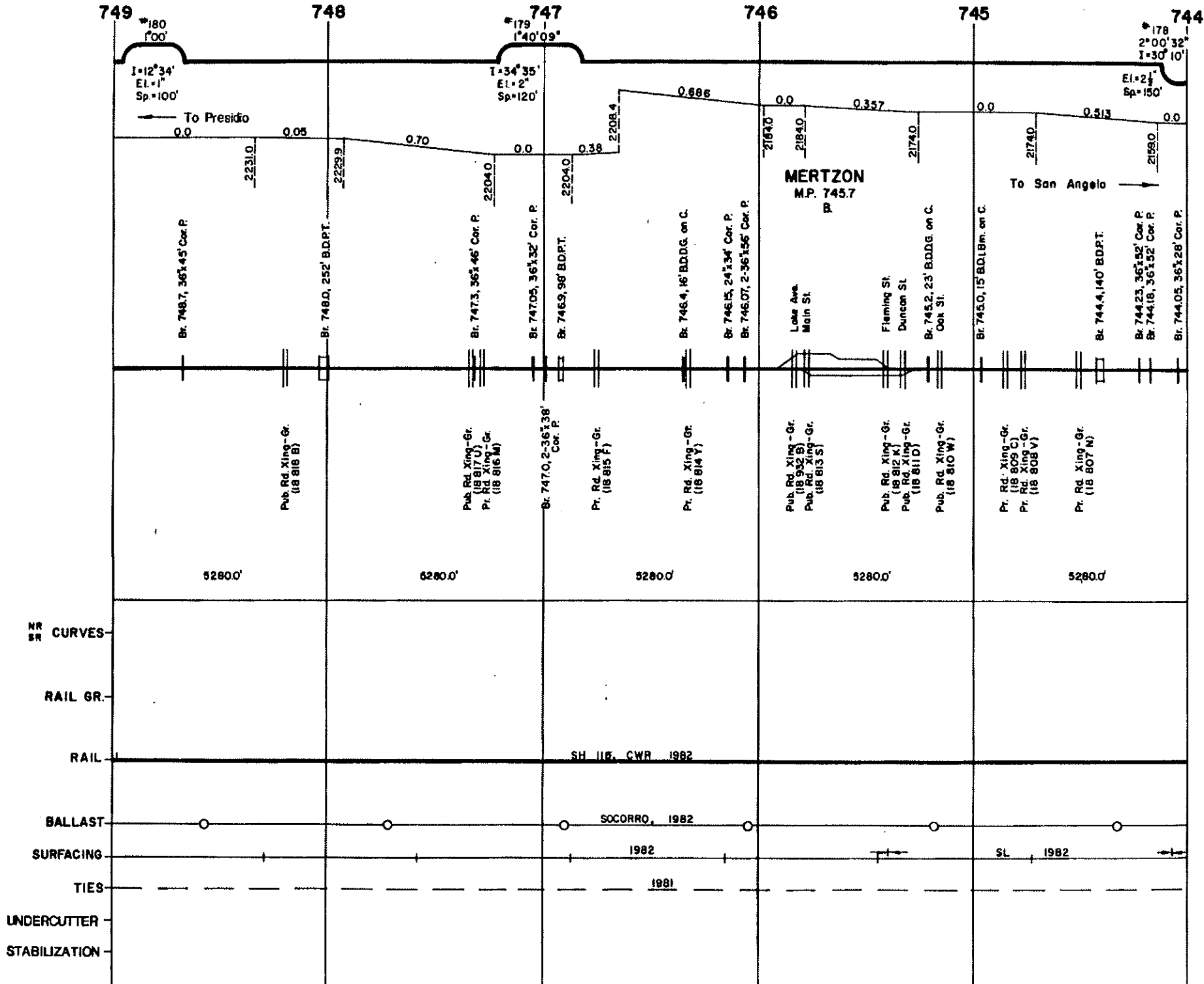


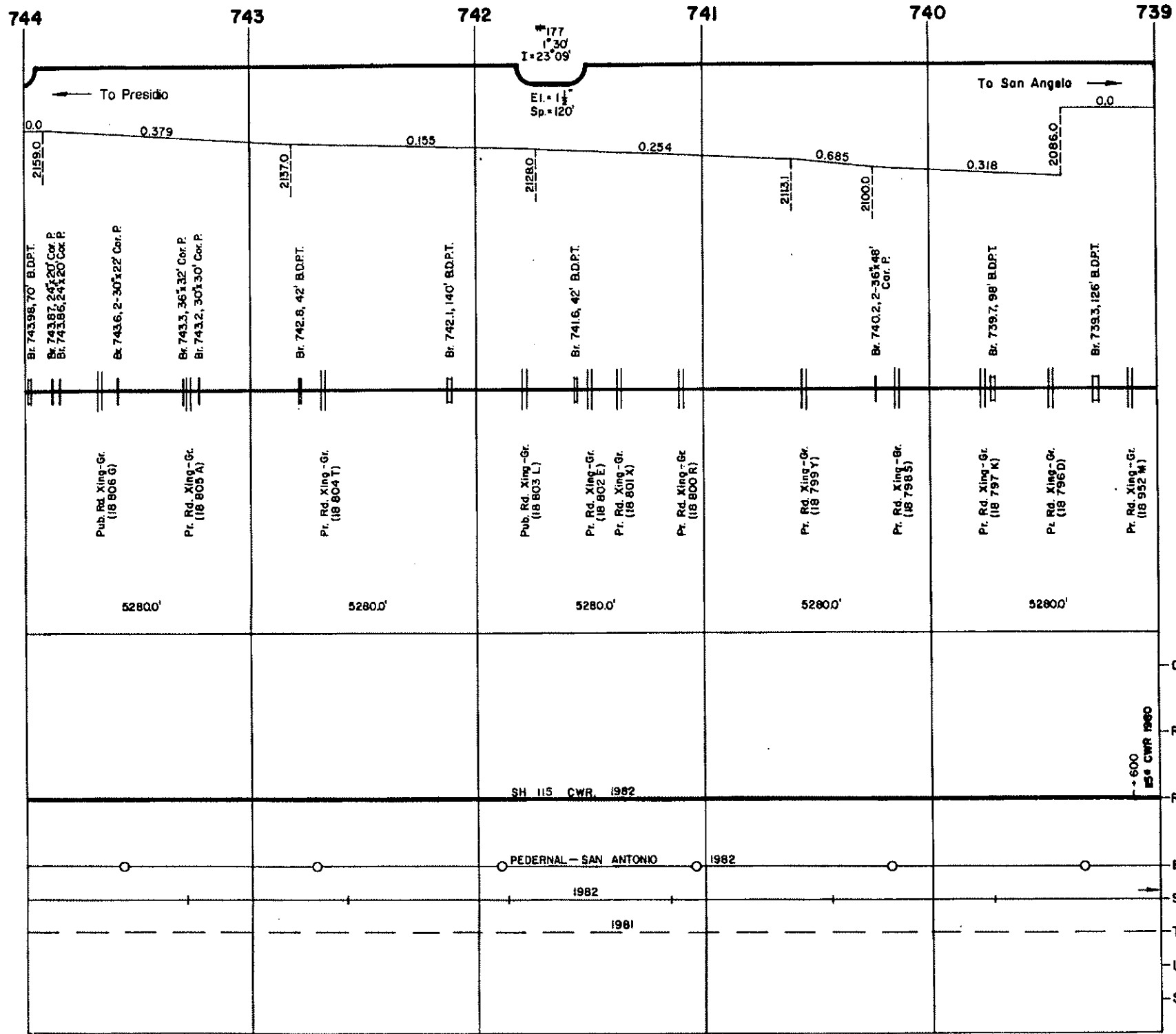
REVISED 3-1991



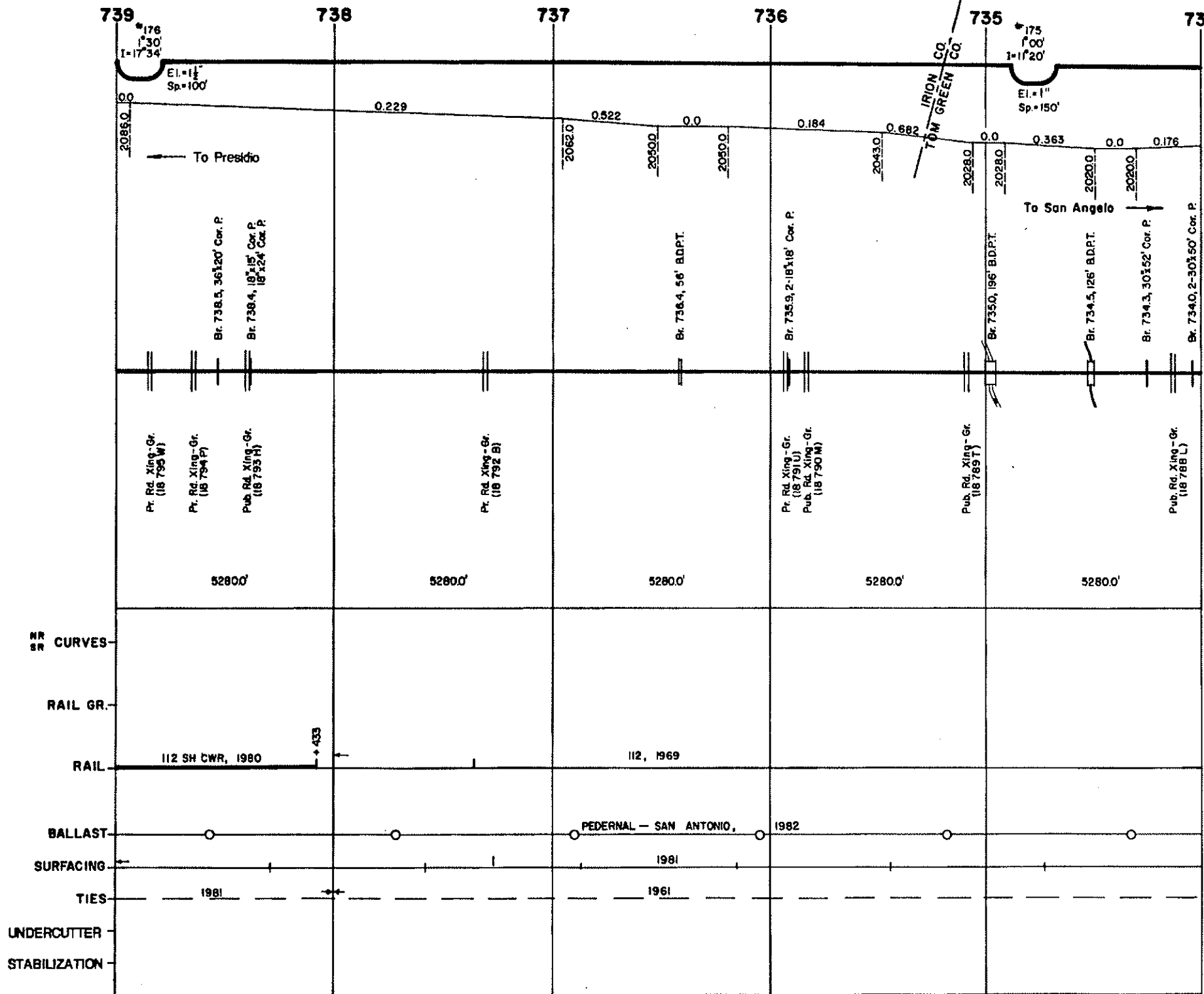


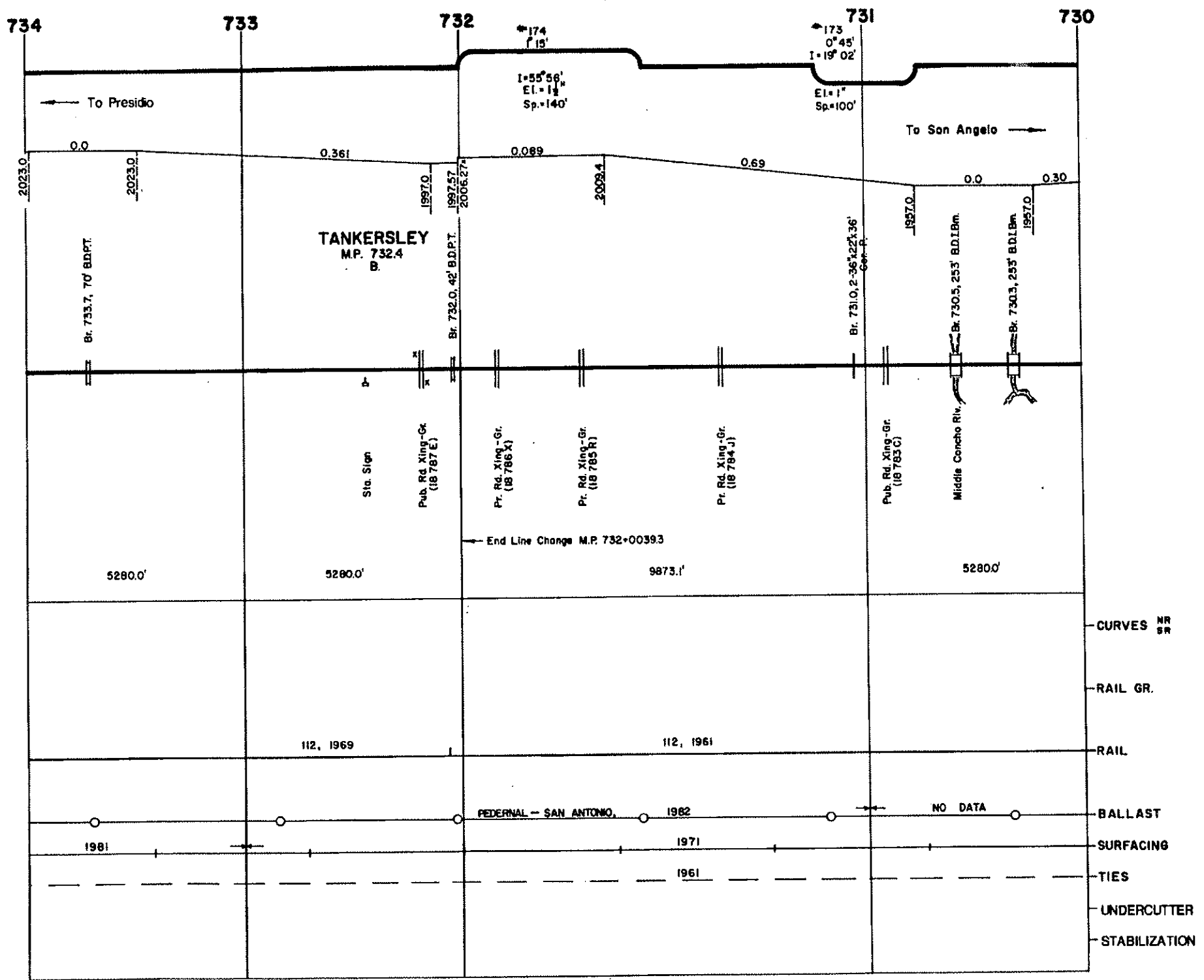


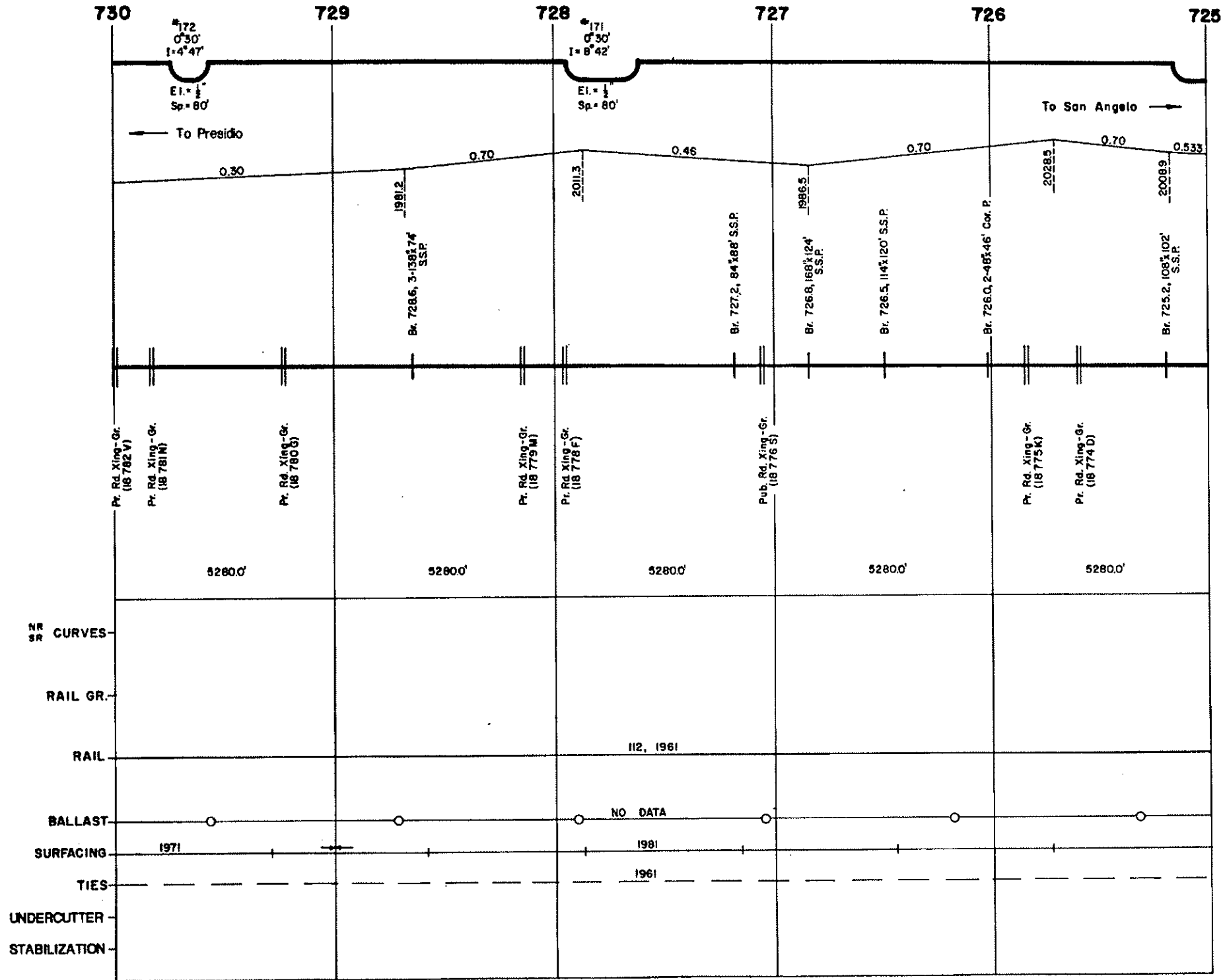


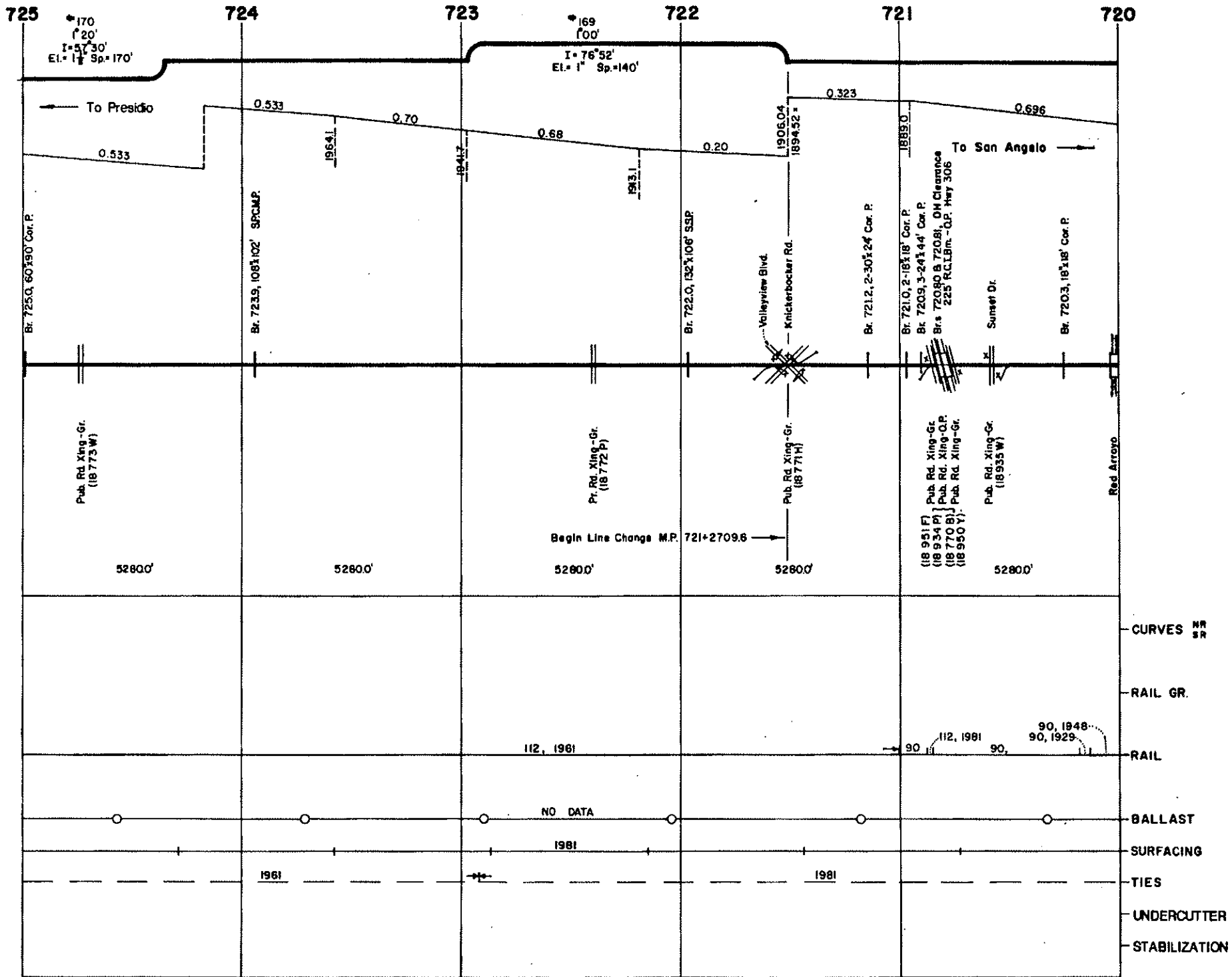


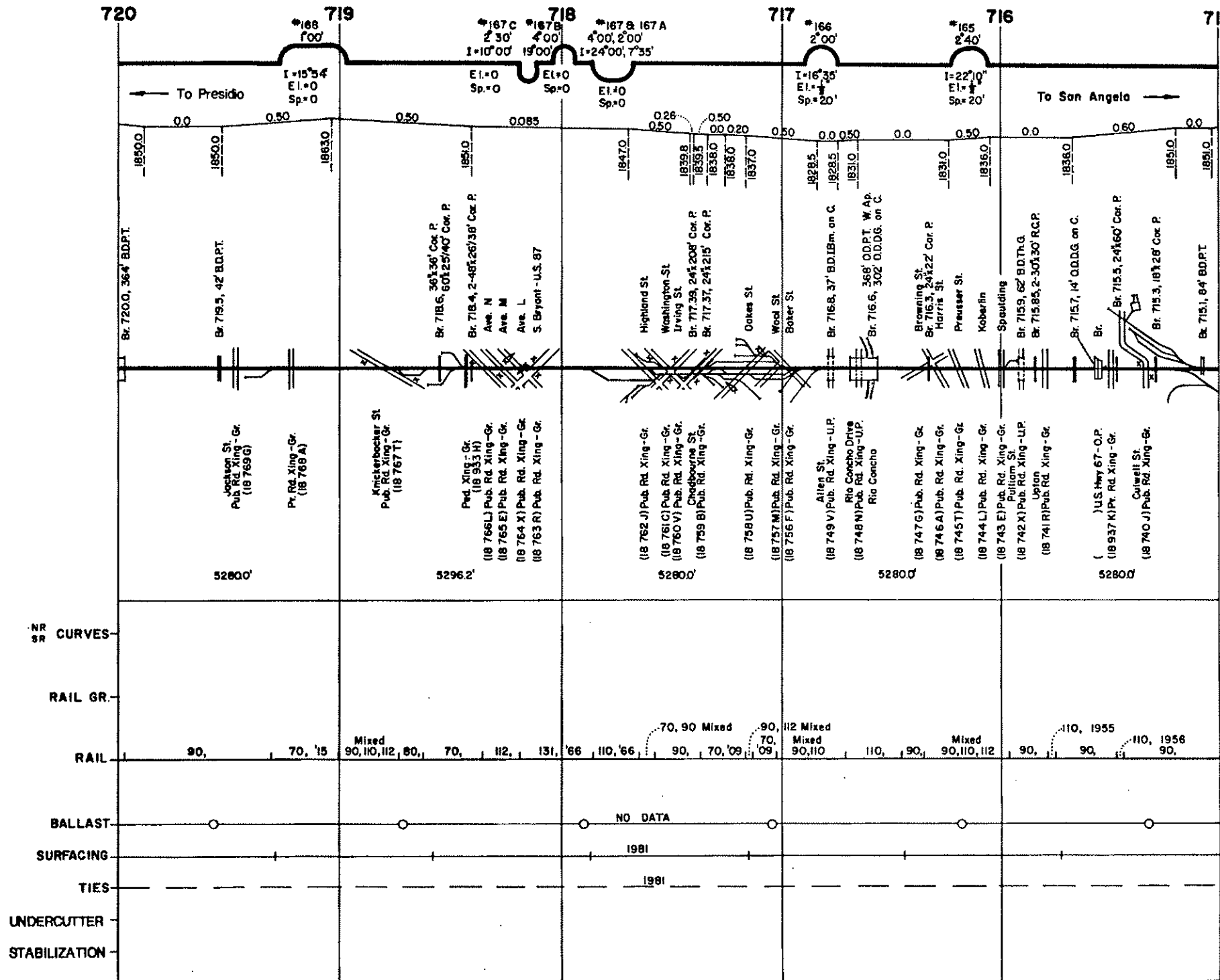
- CURVES NR SR
- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION

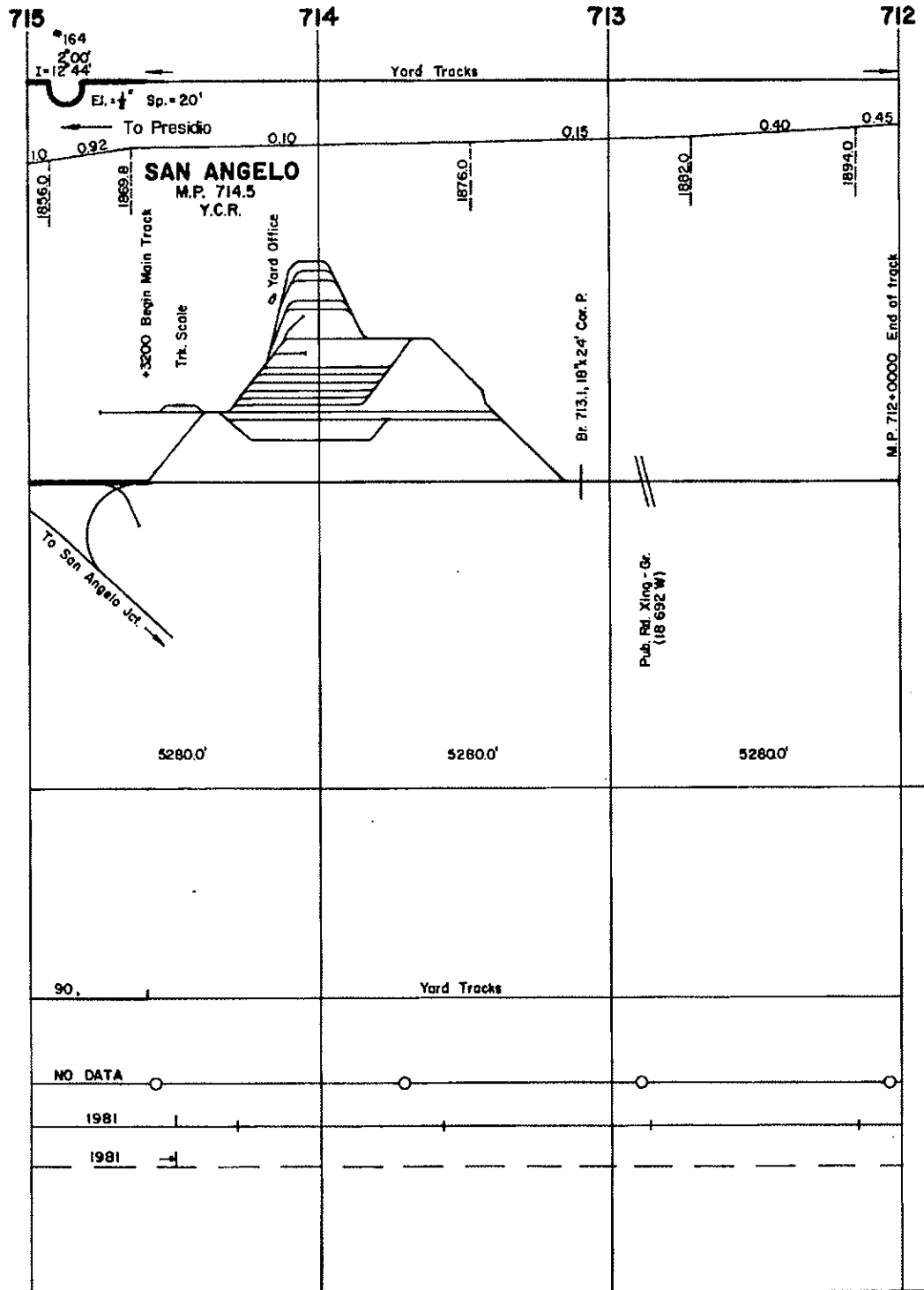








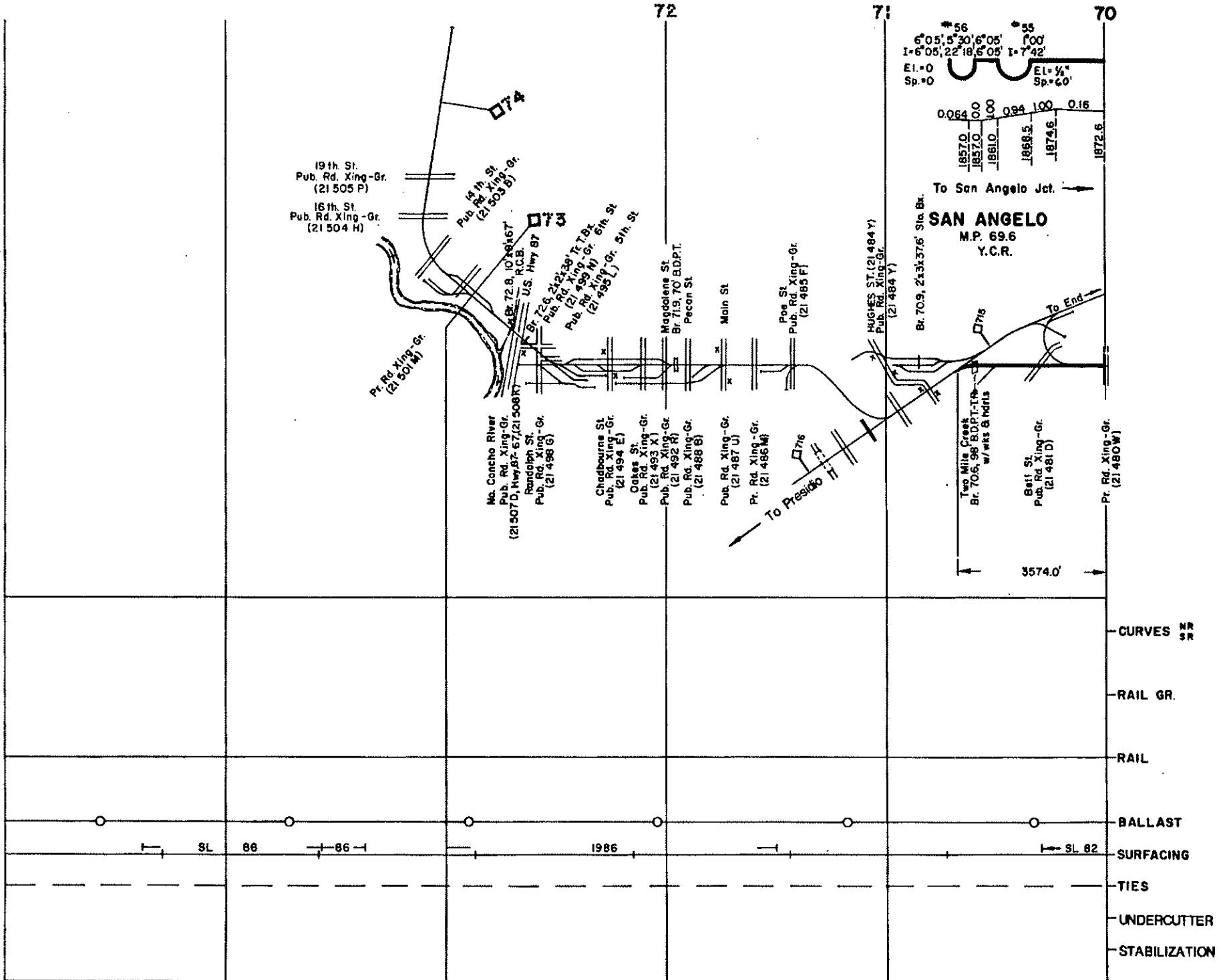


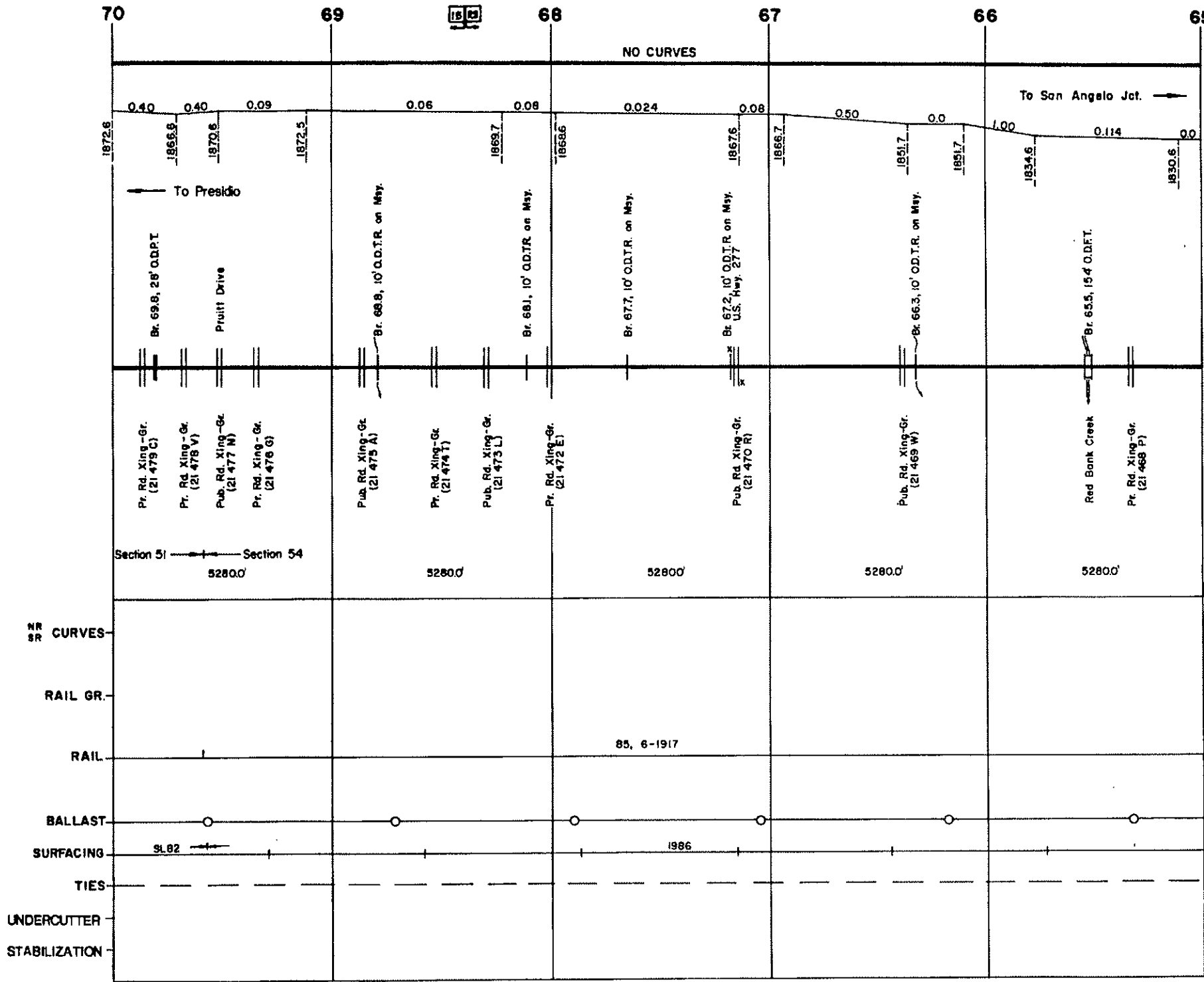


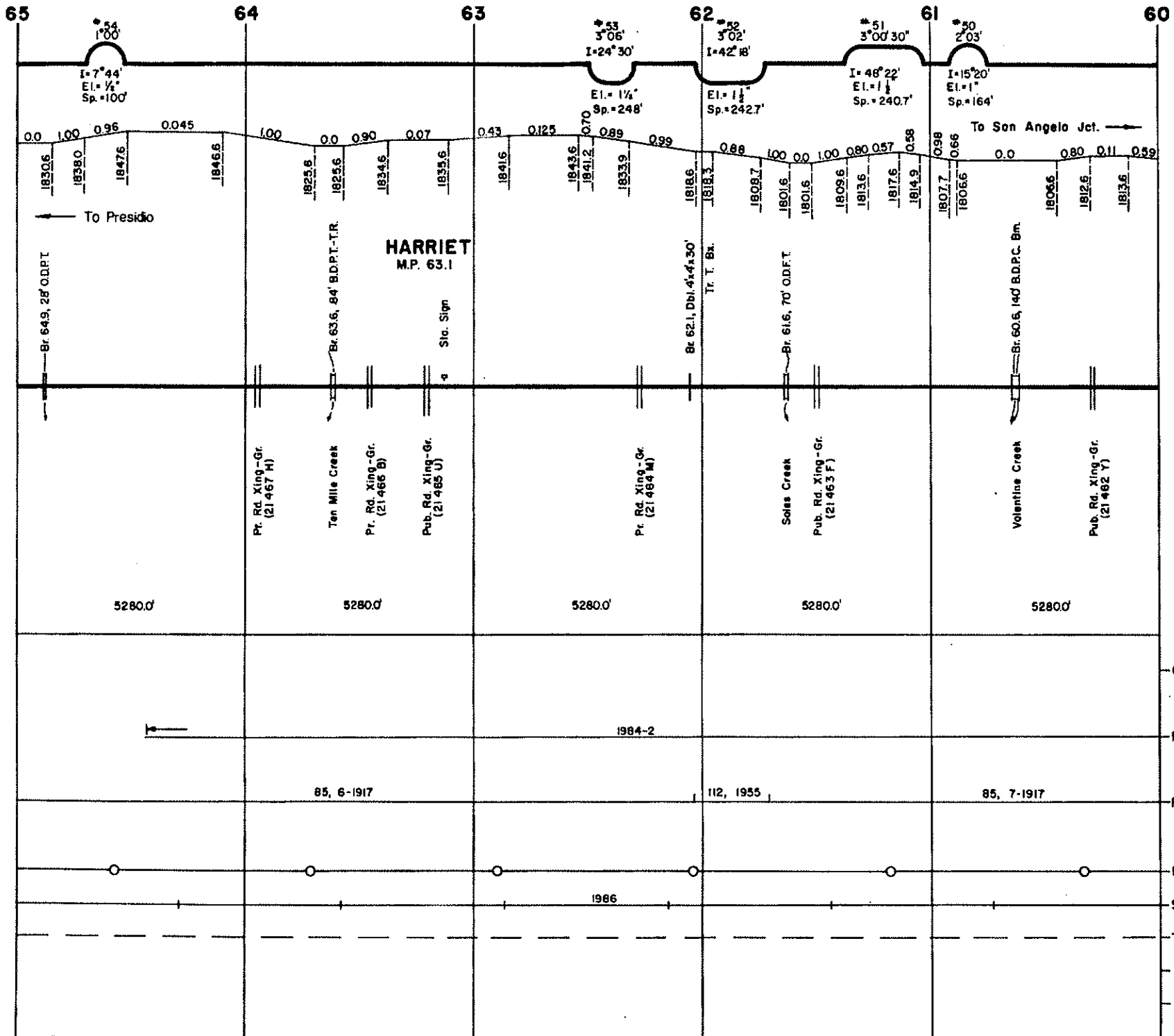
- CURVES NR SR
- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION

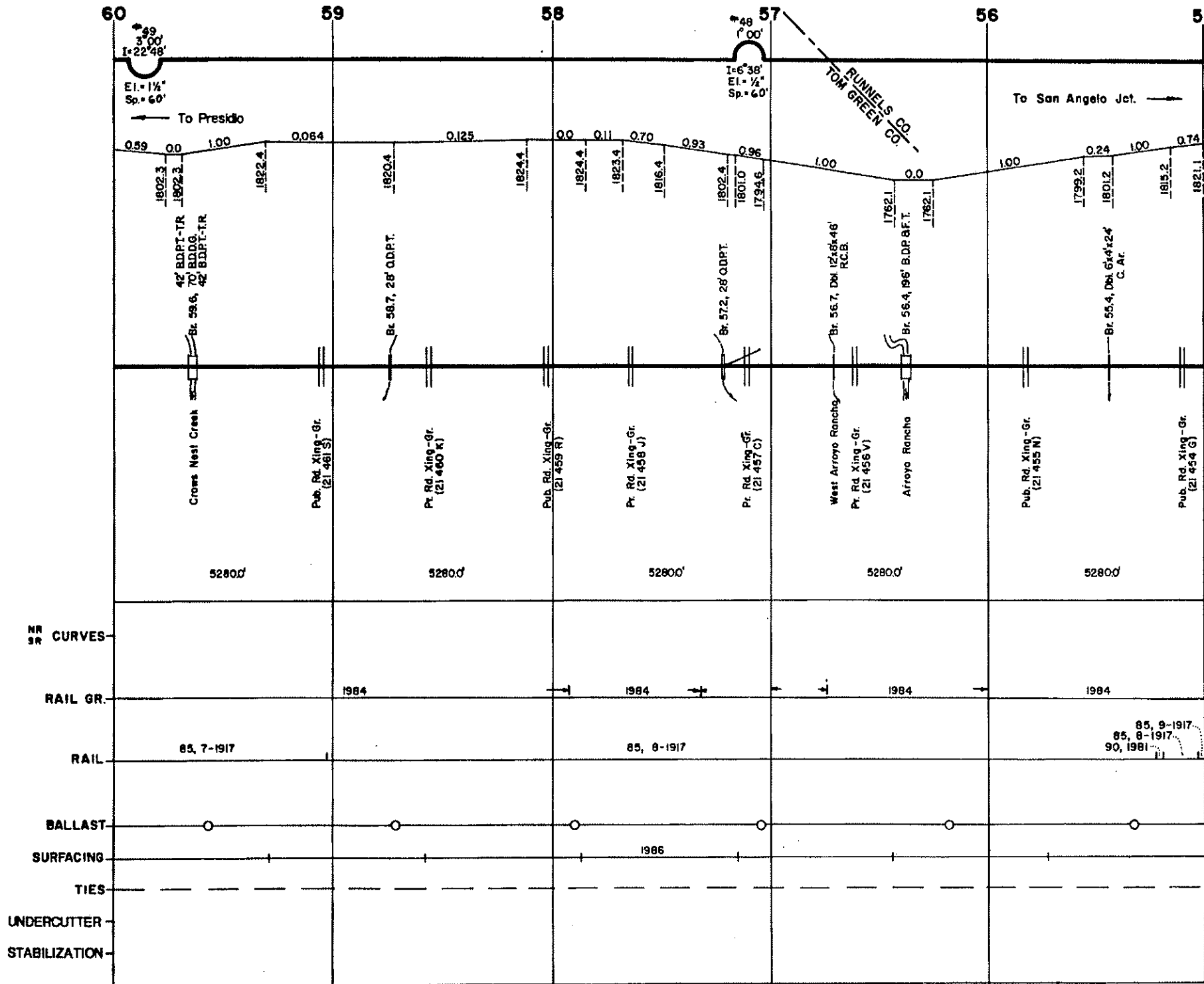


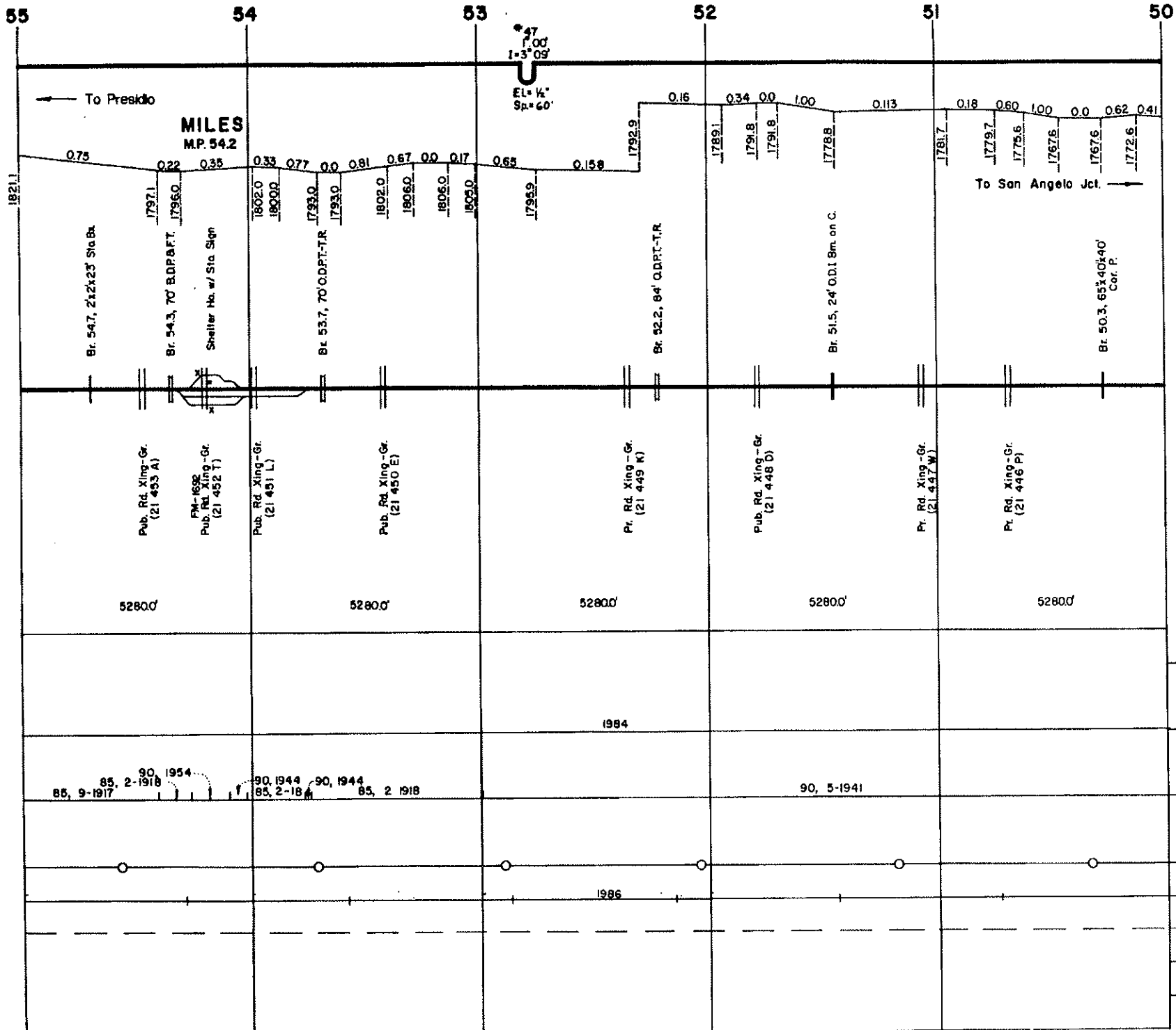
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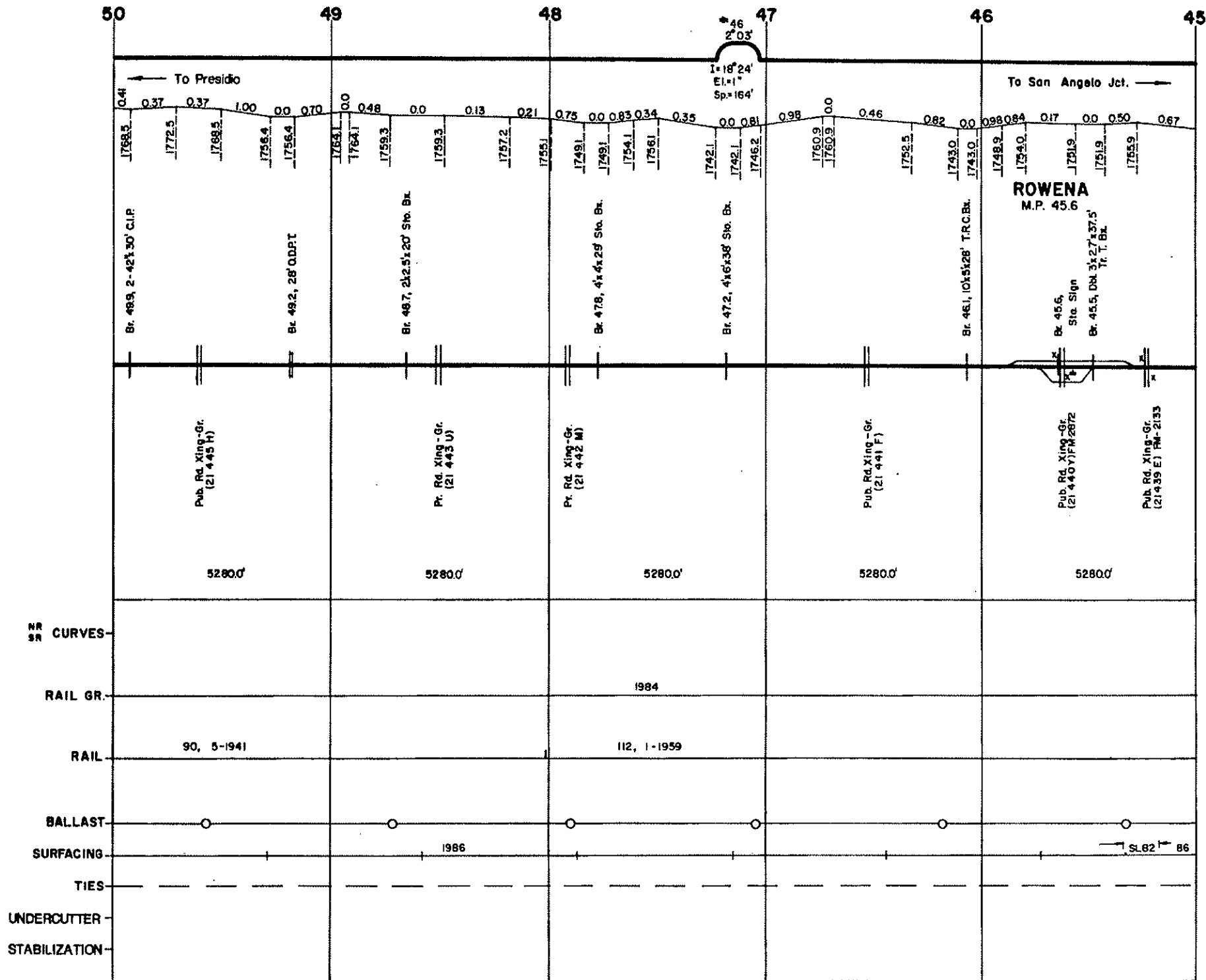


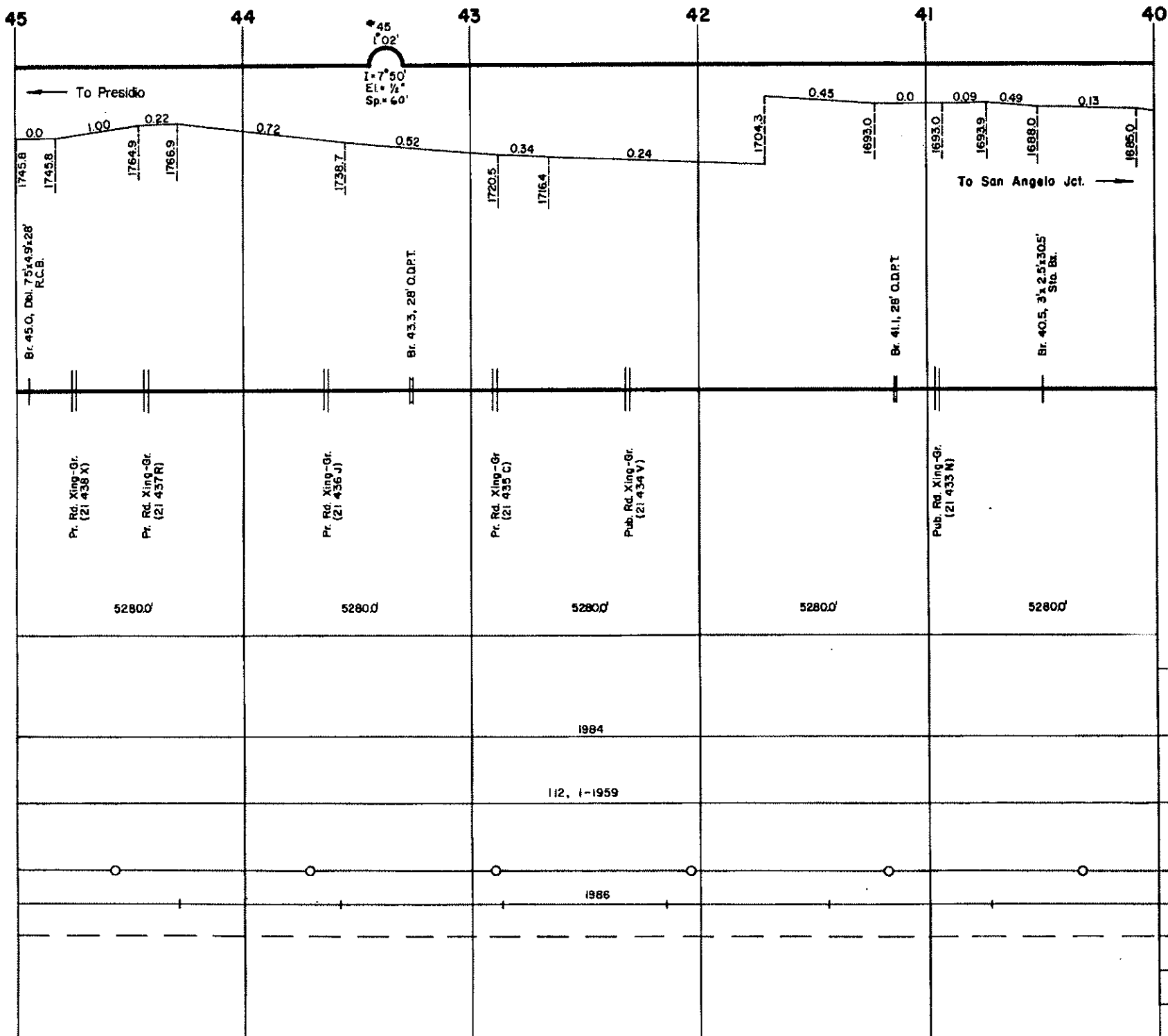












CURVES NR  
SR

RAIL GR.

RAIL

BALLAST

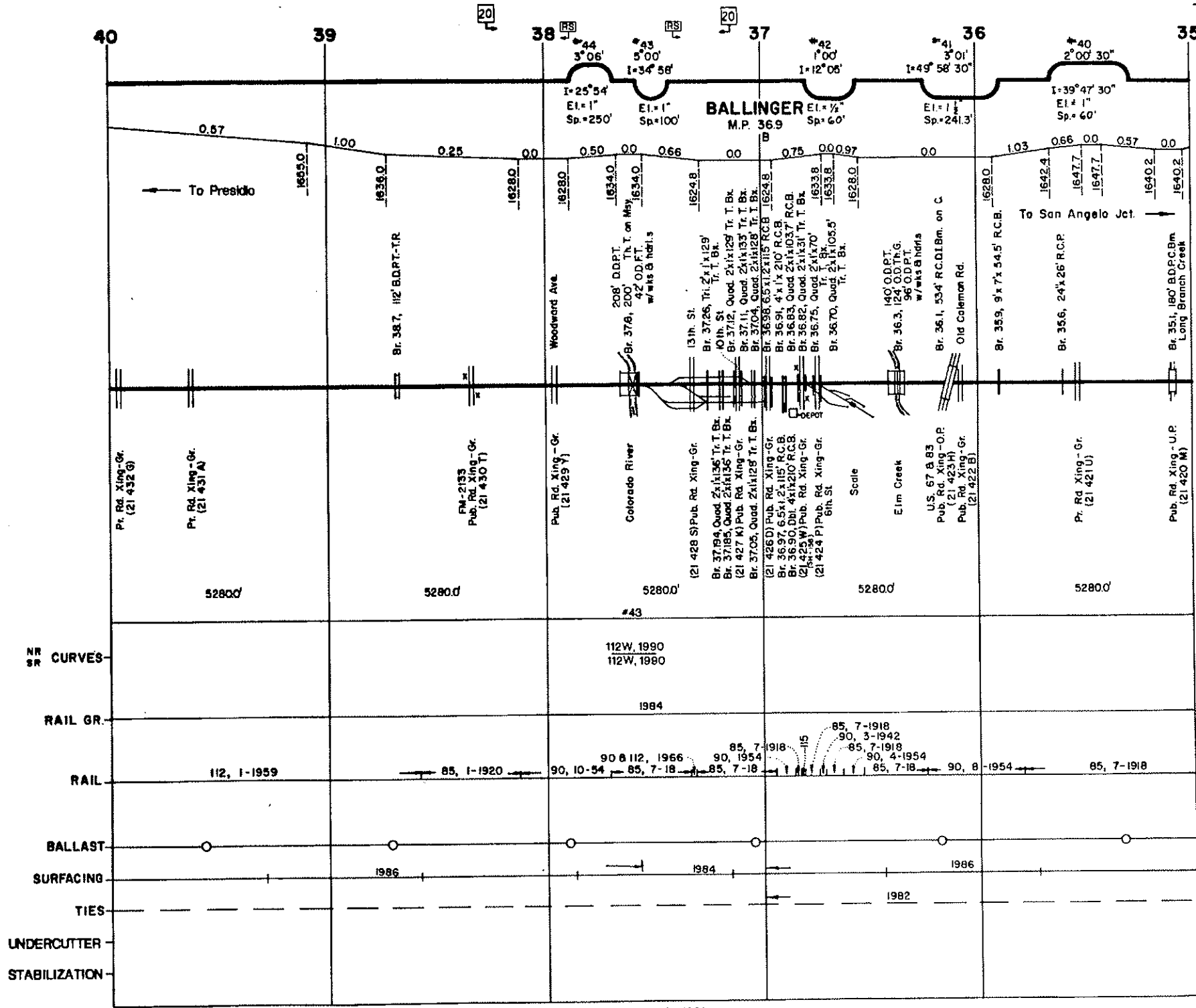
SURFACING

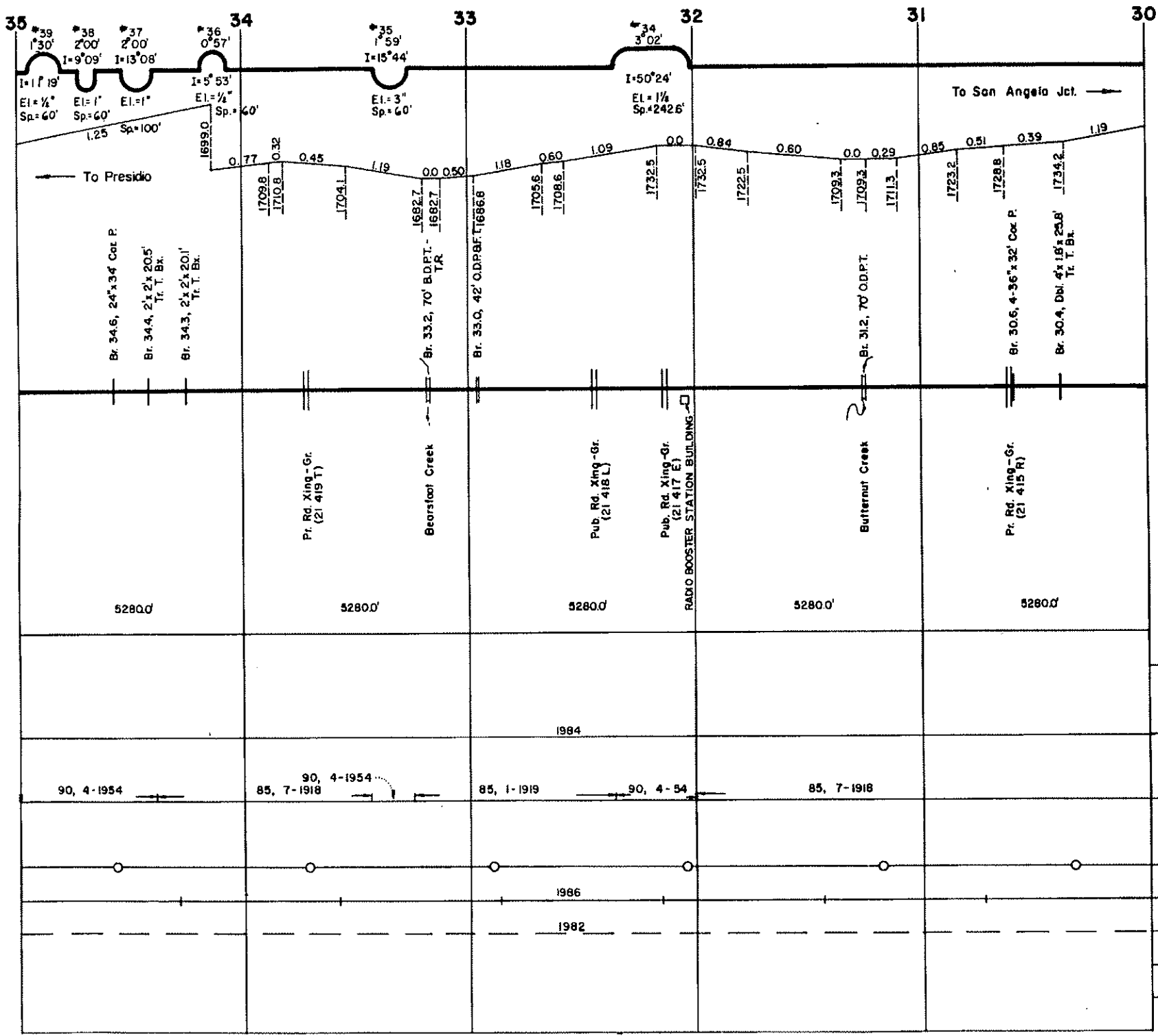
TIES

UNDERCUTTER

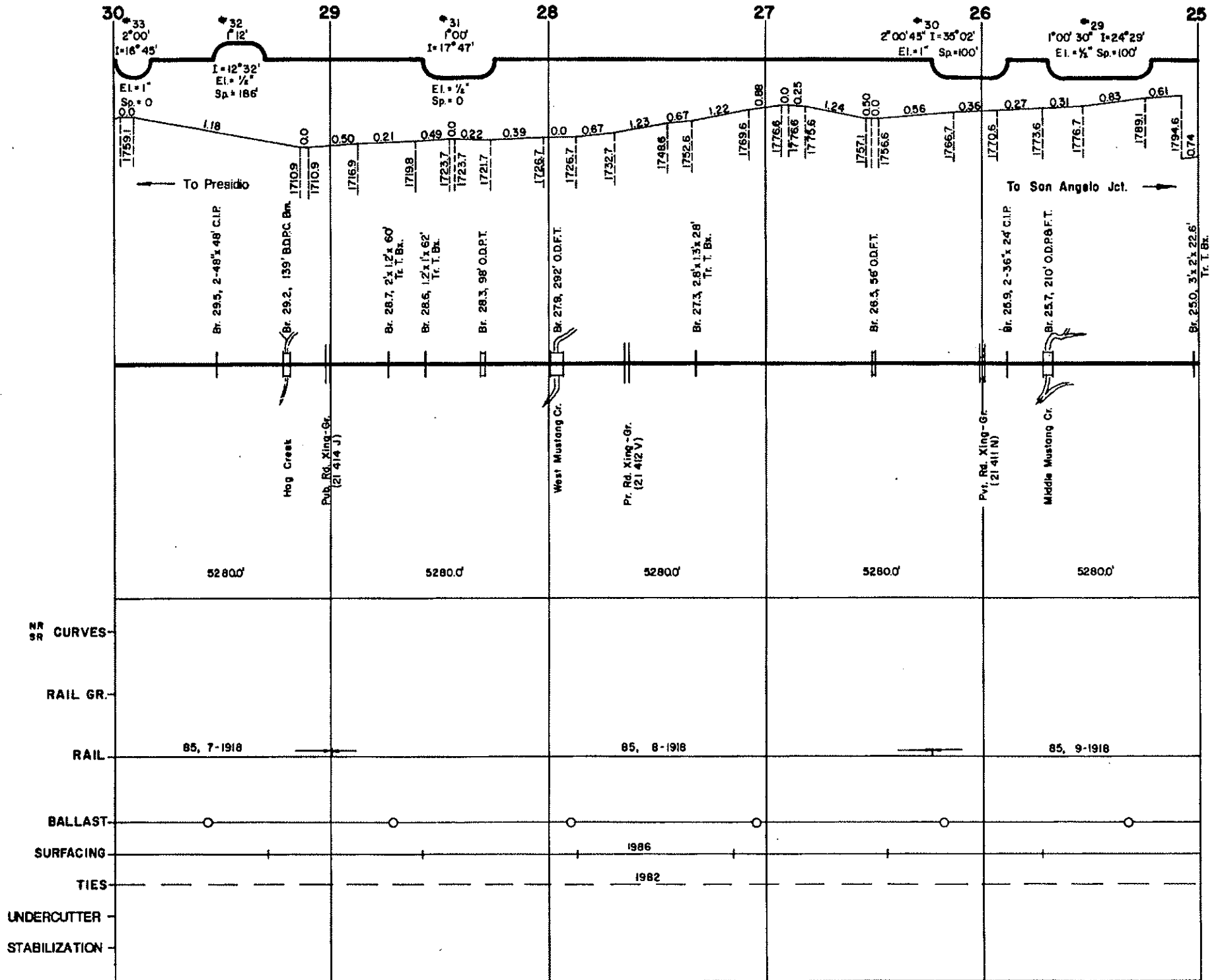
STABILIZATION

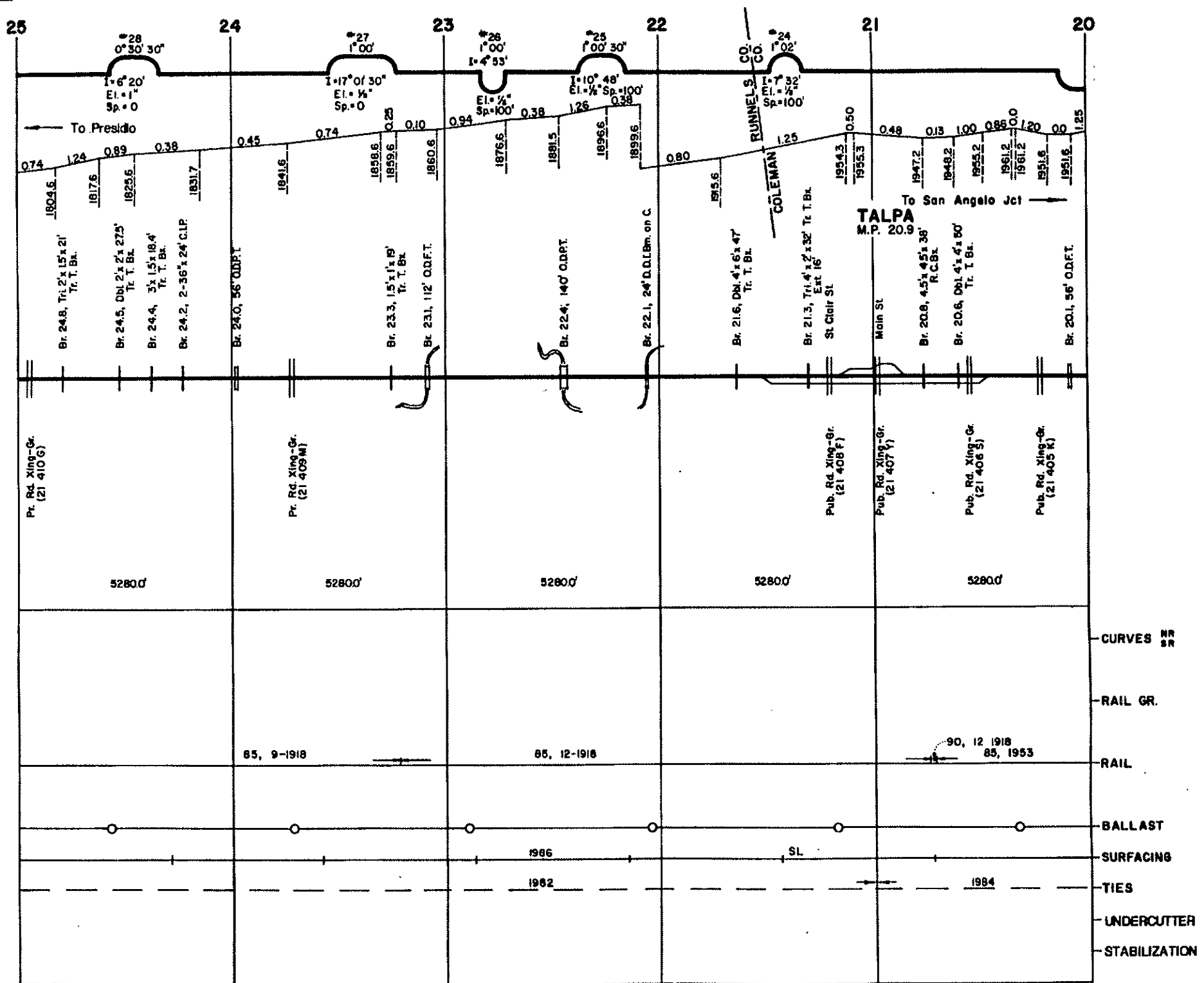


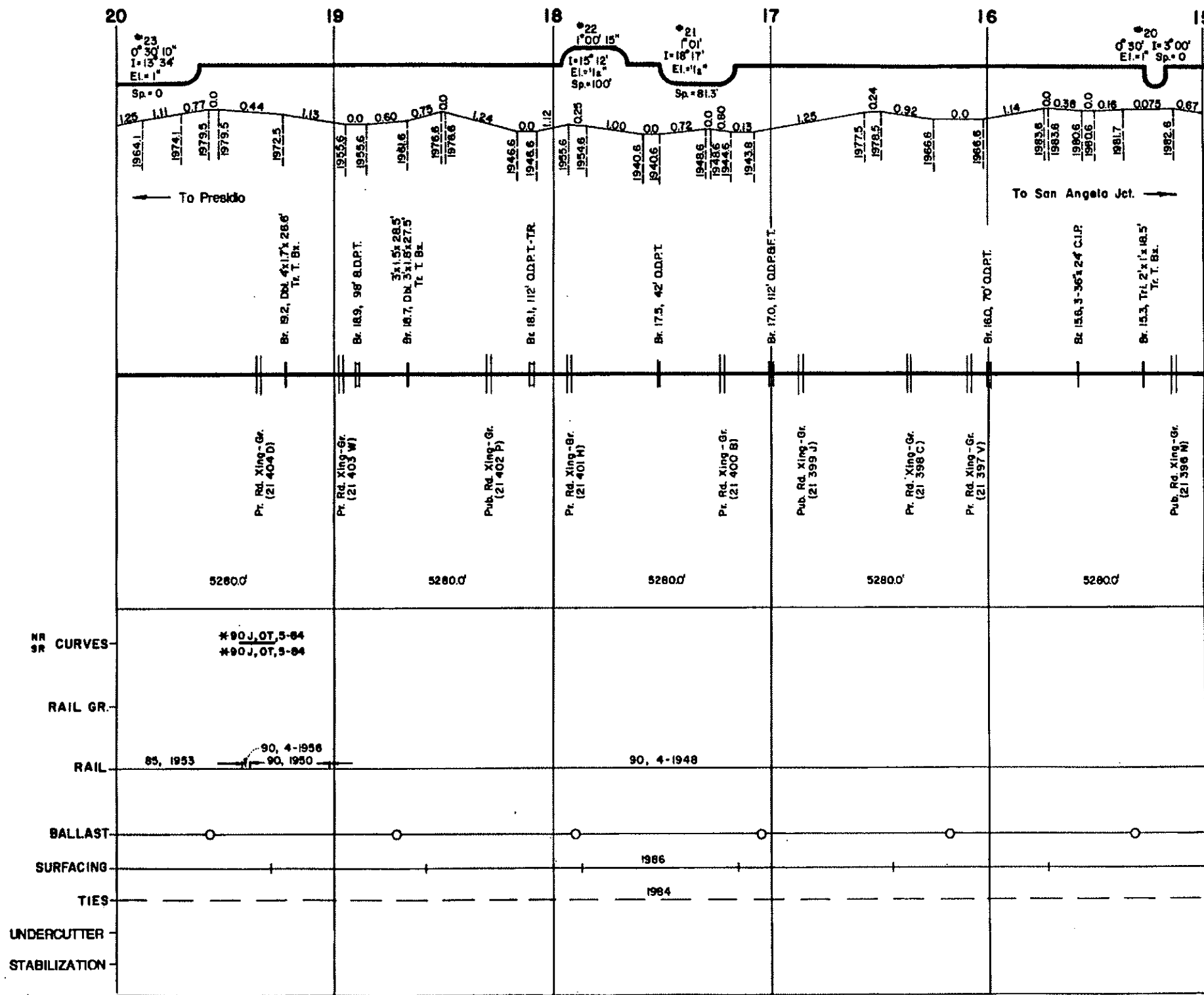


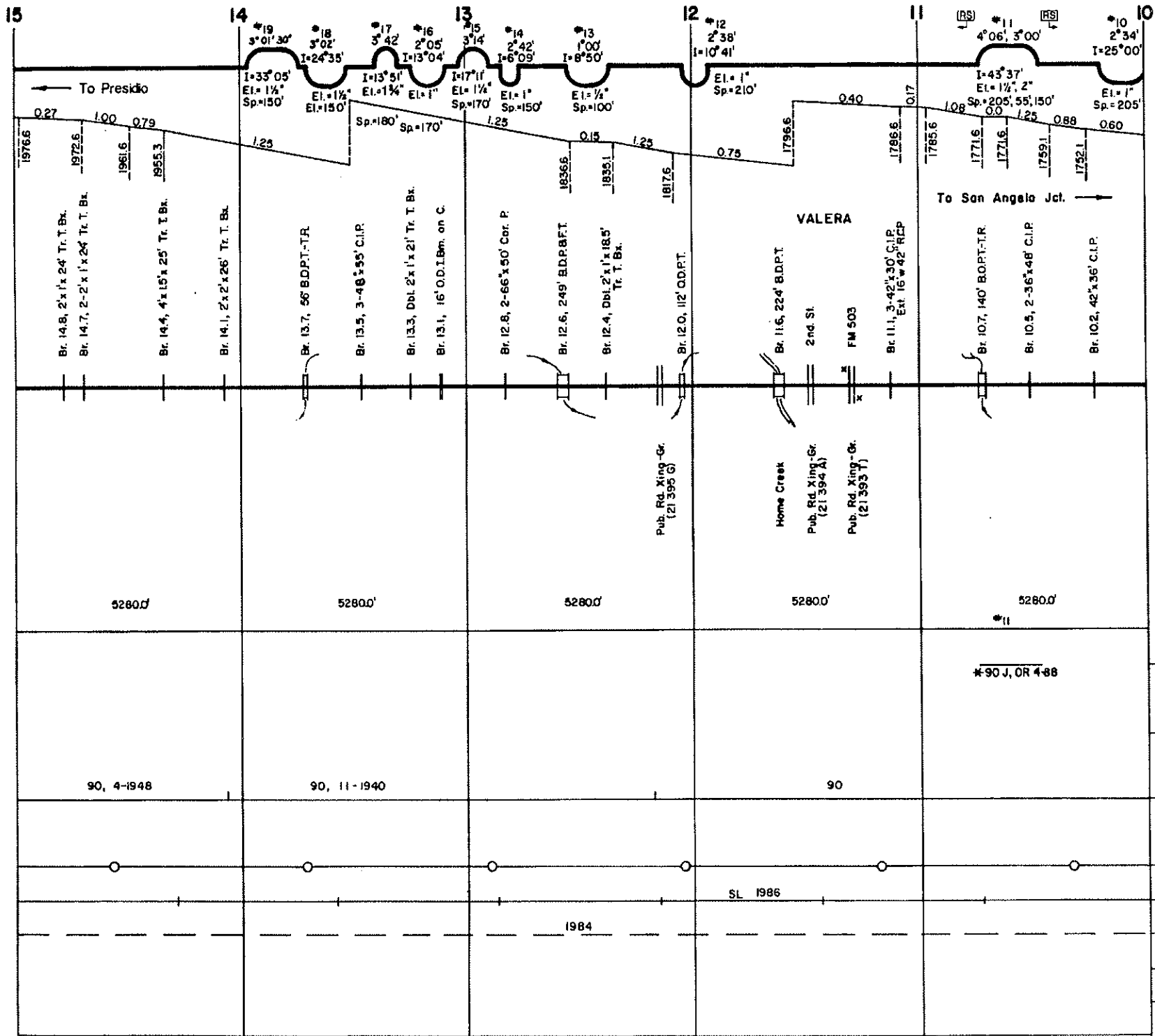


- CURVES NR 9R
- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION

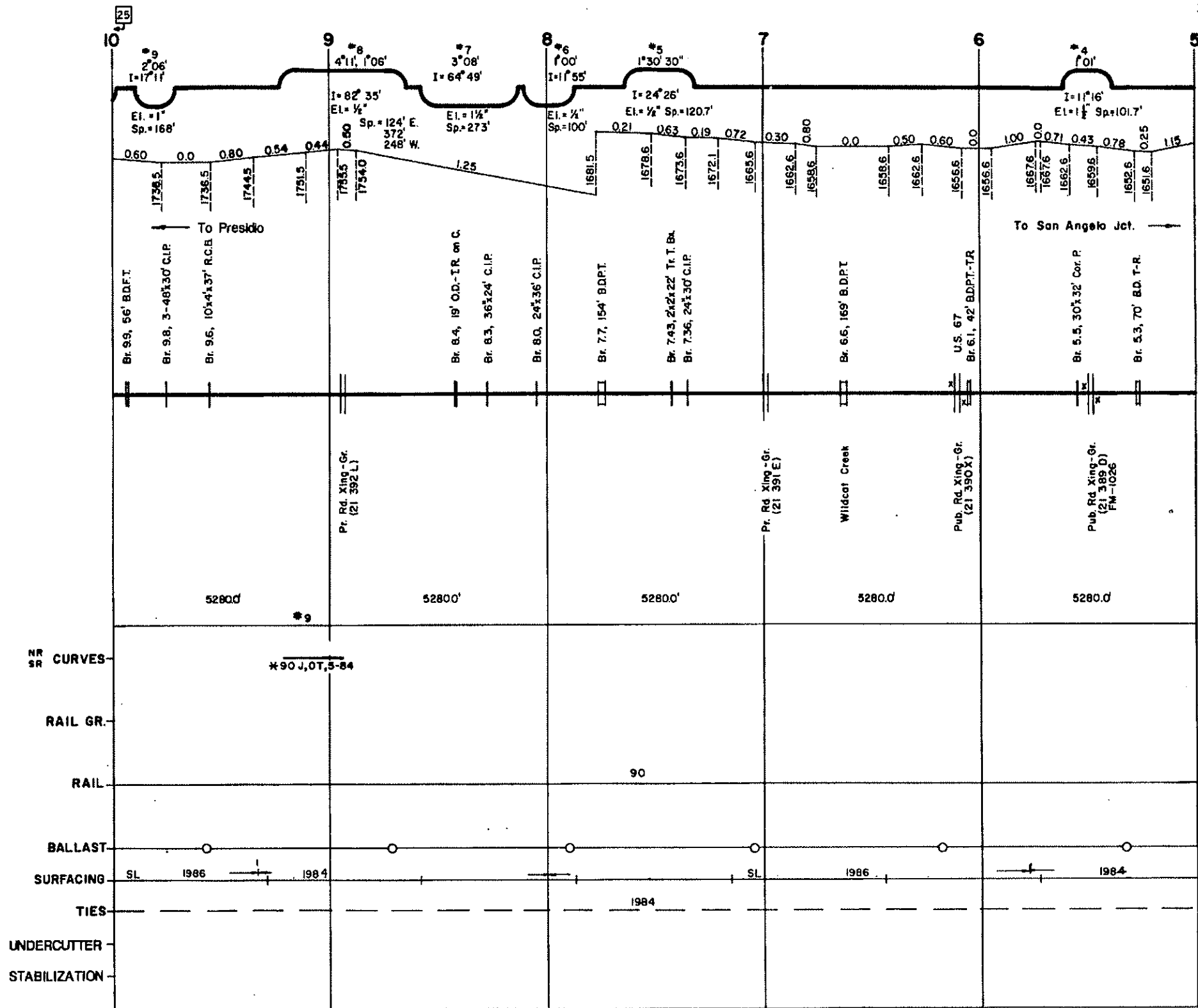


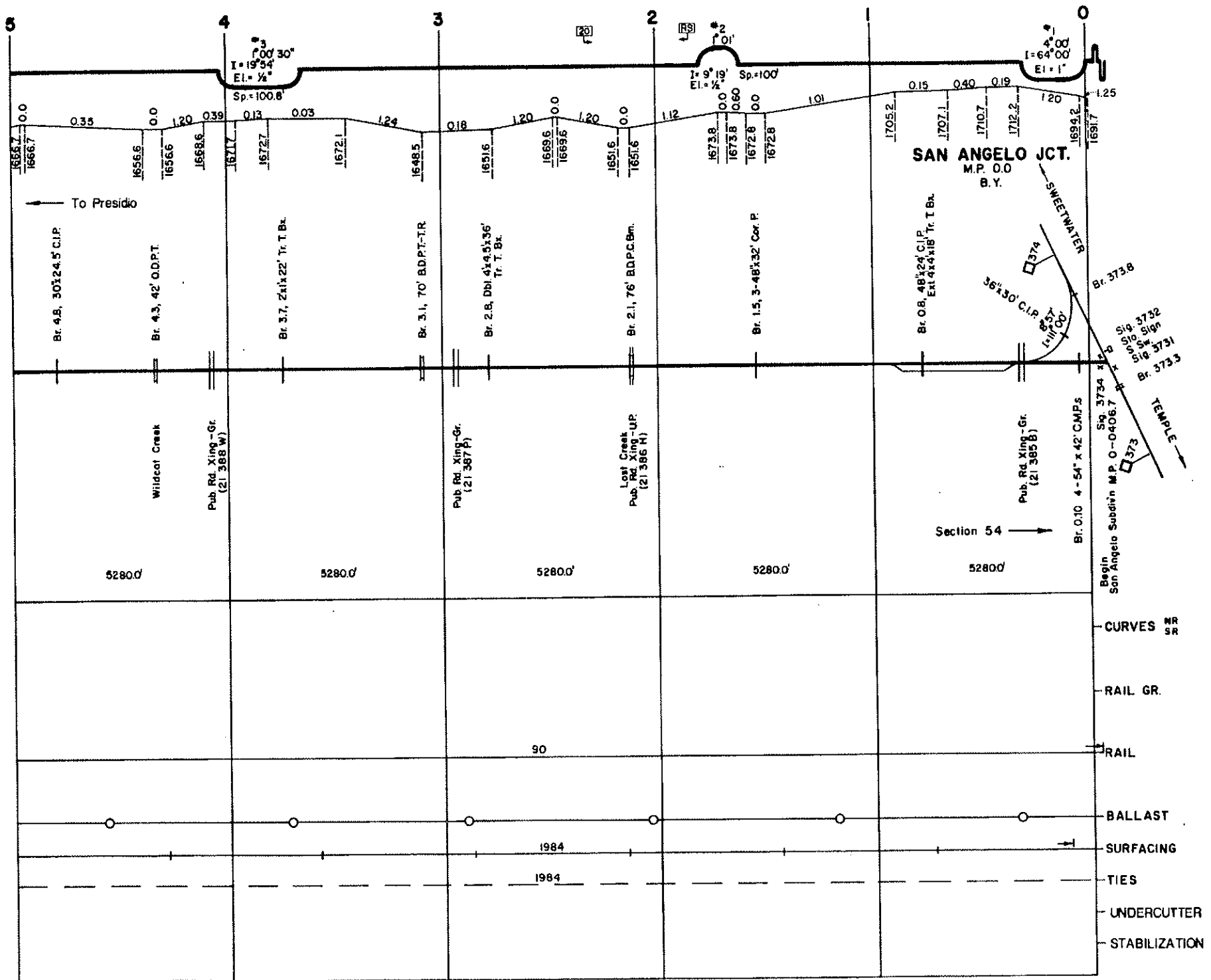






- CURVES <sup>NR</sup><sub>BR</sub>
- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION



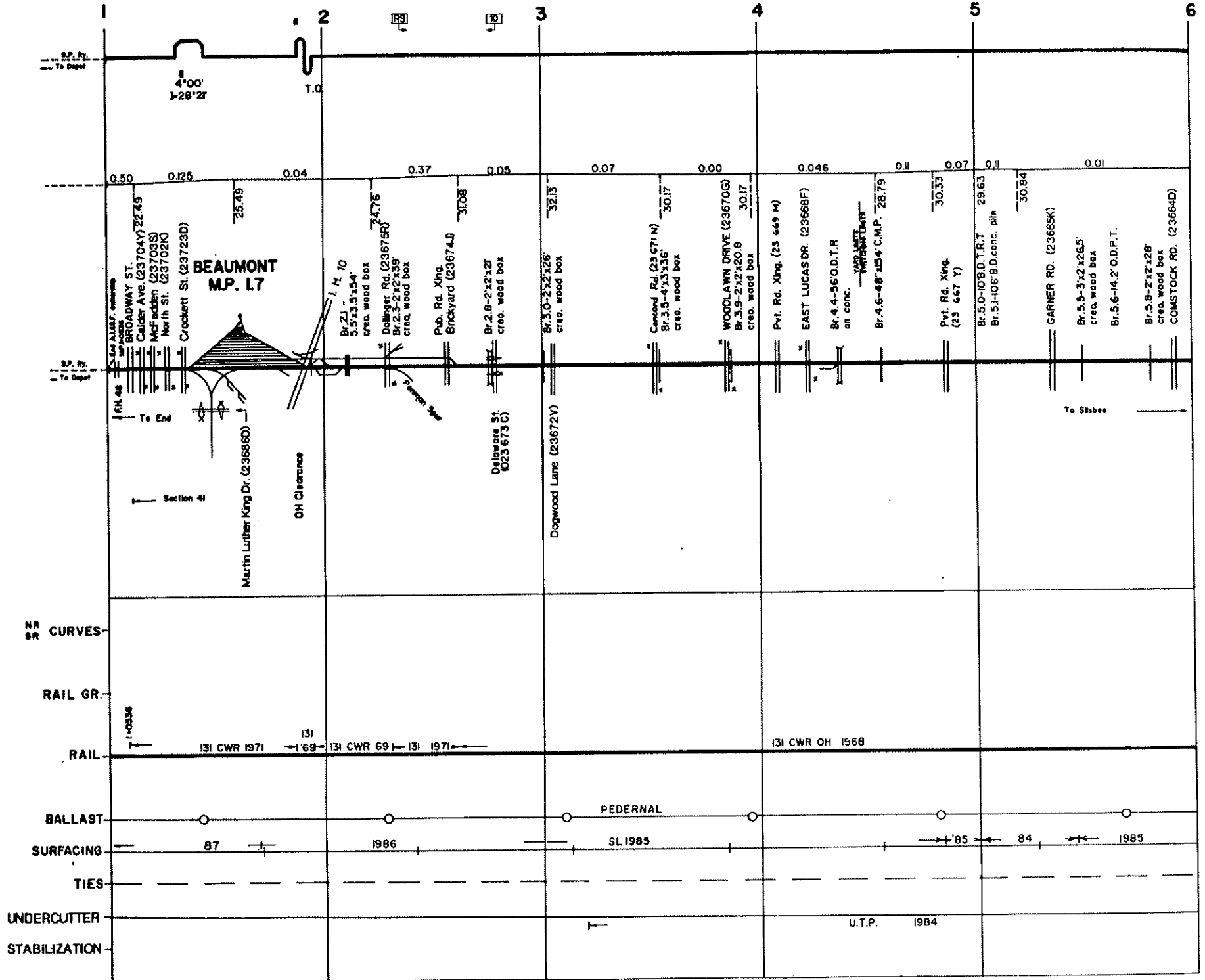


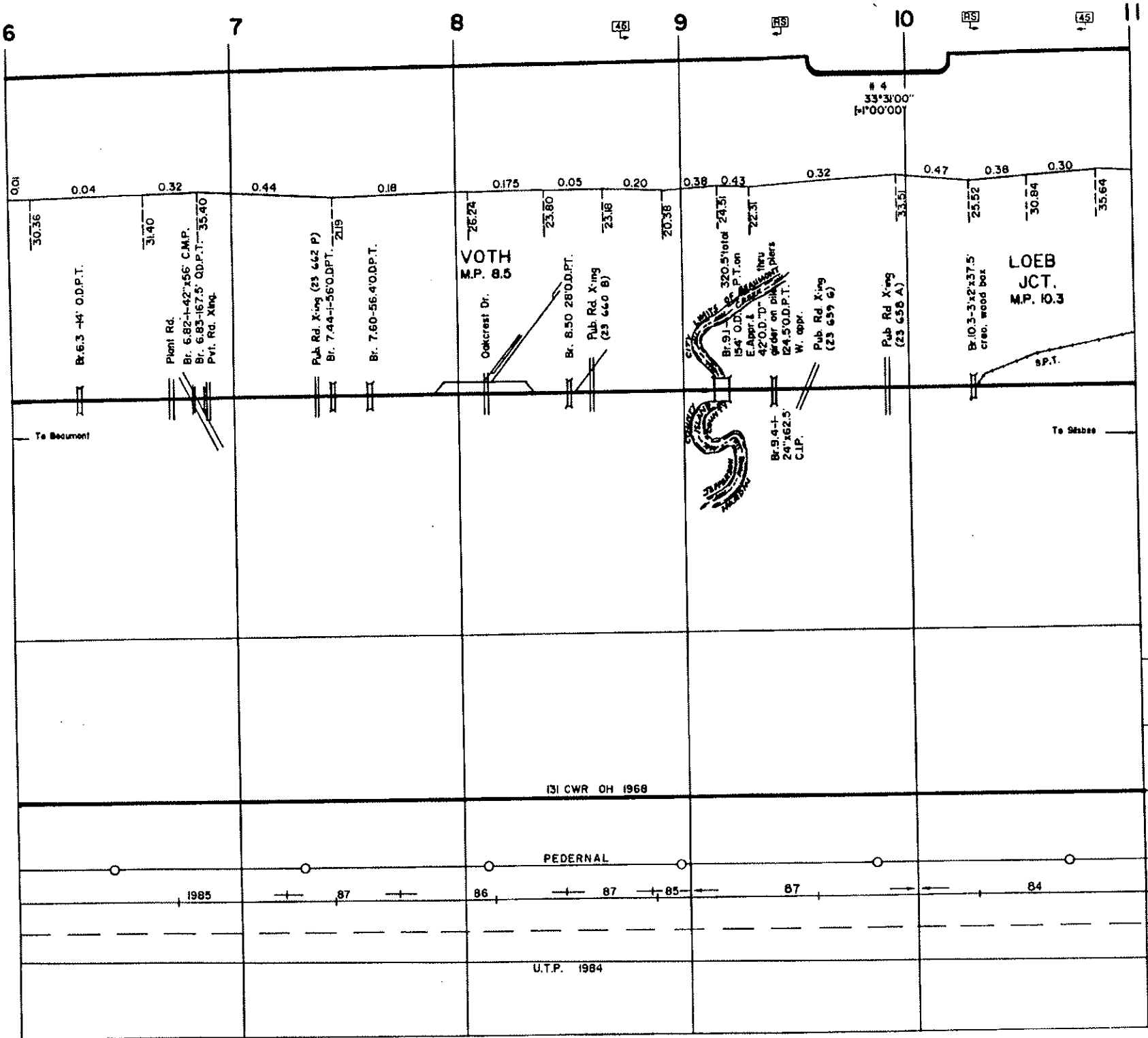


# San Angelo Subdivision

San Angelo Jct. (M.P. 0.0) to Presidio (M.P. 1029.1)

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REVISED 3 - 1991

- CURVES <sup>NR</sup> <sub>SR</sub>
- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION

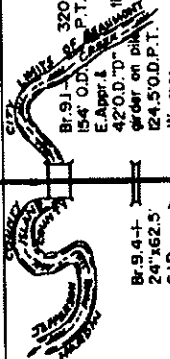
131 CWR OH 1968

PEDERNAL

U.T.P. 1984

VOTH  
M.P. 8.5

LOEB  
JCT.  
M.P. 10.3



# 4  
33°31'00"  
1-1°00'00"

Br. 6.3 -4' O.D.P.T.

Plant Rd.  
Br. 6.82-42'x56' C.M.P.  
Br. 6.83-167.5' O.D.P.T.  
Pub. Rd. Xing.

Pub. Rd. Xing (23 662 P)  
Br. 7.44-56' O.D.P.T. 2119

Br. 7.60-56.4' O.D.P.T.

Oakcrest Dr.

Br. 8.50 28' O.D.P.T.

Pub. Rd. Xing  
(23 660 B)

Br. 9.1-320.5' total 23.31  
154' O.D. C.M.P. thru  
E. Appro. &  
42' O.D. T-  
girders on piers  
124.5' O.D.P.T.  
W. appx.

Br. 9.4-  
24'x162.5'  
C.I.P.

Pub. Rd. Xing  
(23 659 G)

Pub. Rd. Xing  
(23 658 A)

Br. 10.3-3'x2'x37.5'  
creo. wood box

S.P.T.

To Beacmont

To Shesee

0.01 0.04 0.32 0.44 0.18 0.175 0.05 0.20 0.38 0.43 0.32 0.47 0.38 0.30

30.36 31.40 25.24 23.80 23.18 20.38 23.31 22.31 33.51 25.52 30.84 35.64

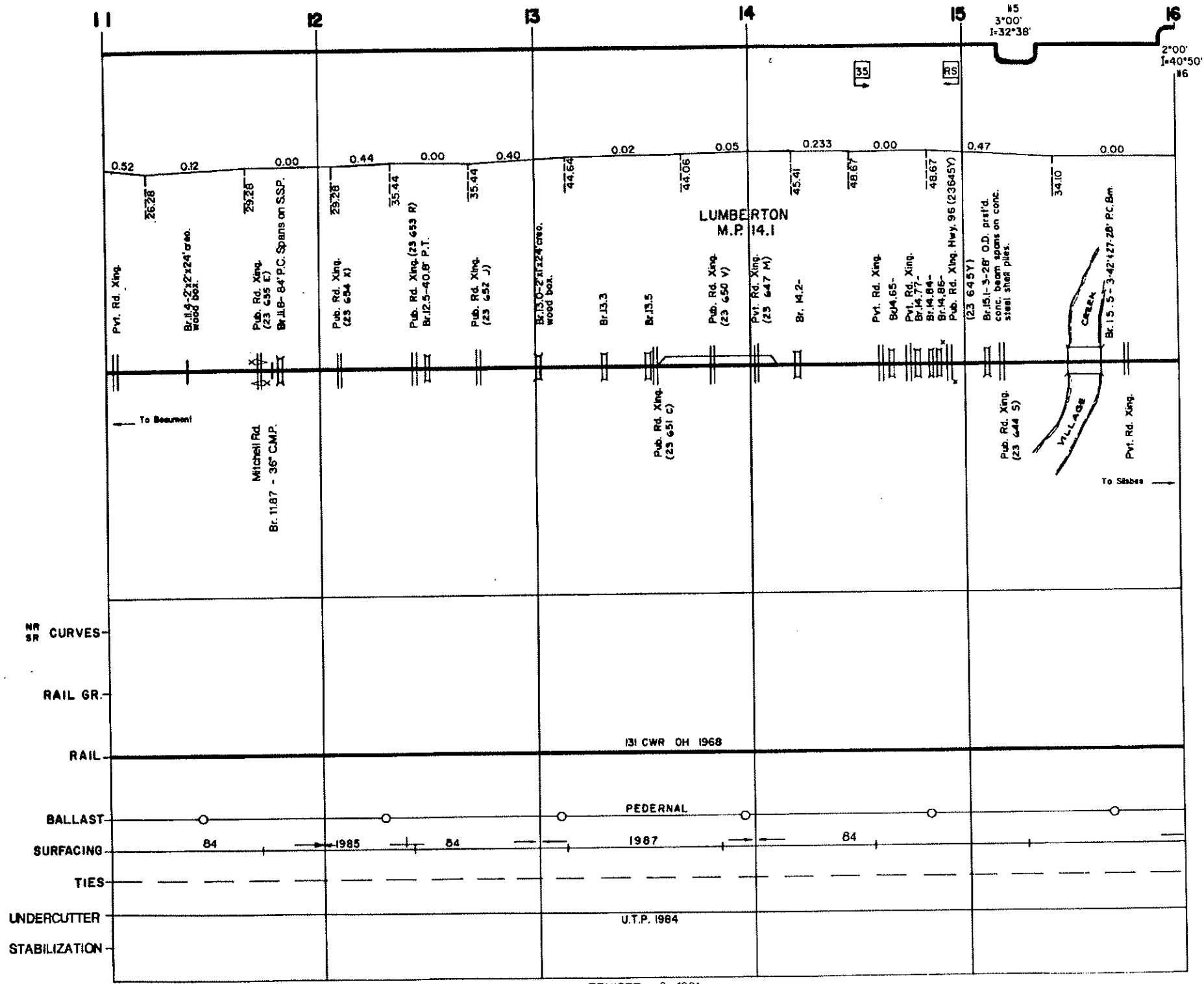
6 7 8 9 10 11

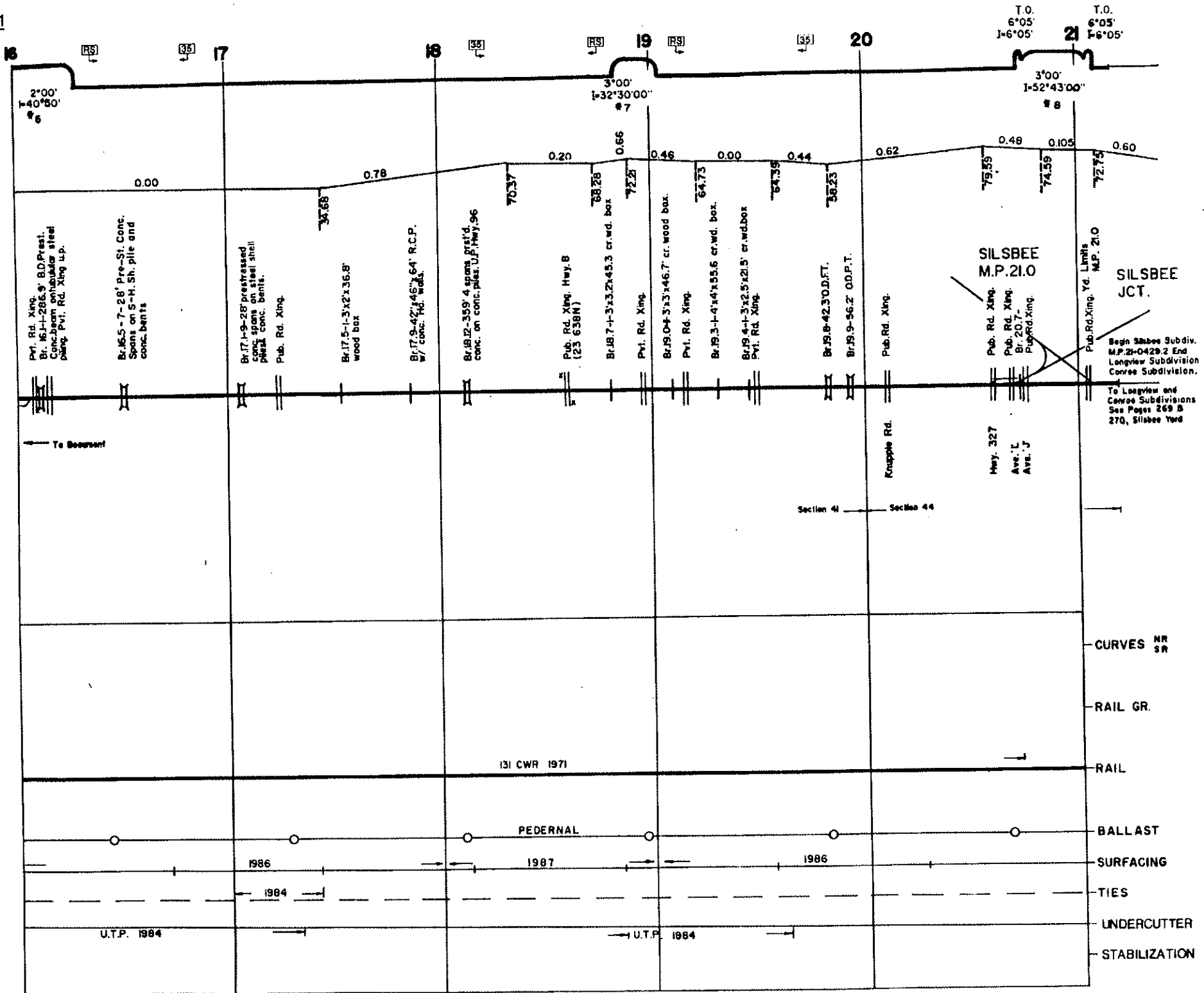
46

49

45

45





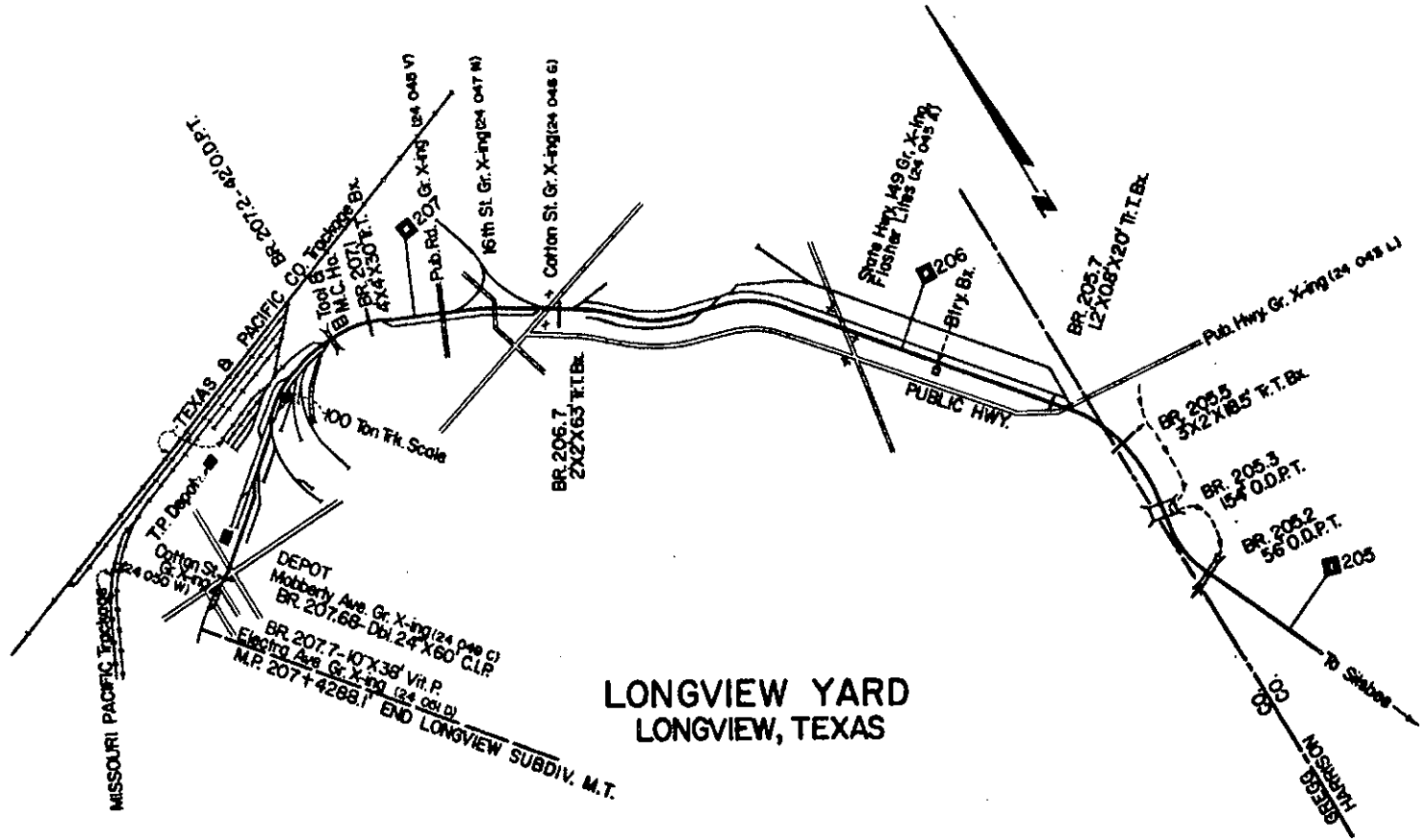
# Silsbee Subdivision

Silsbee (M.P. 21.1) to Beaumont (M.P. 1.1)

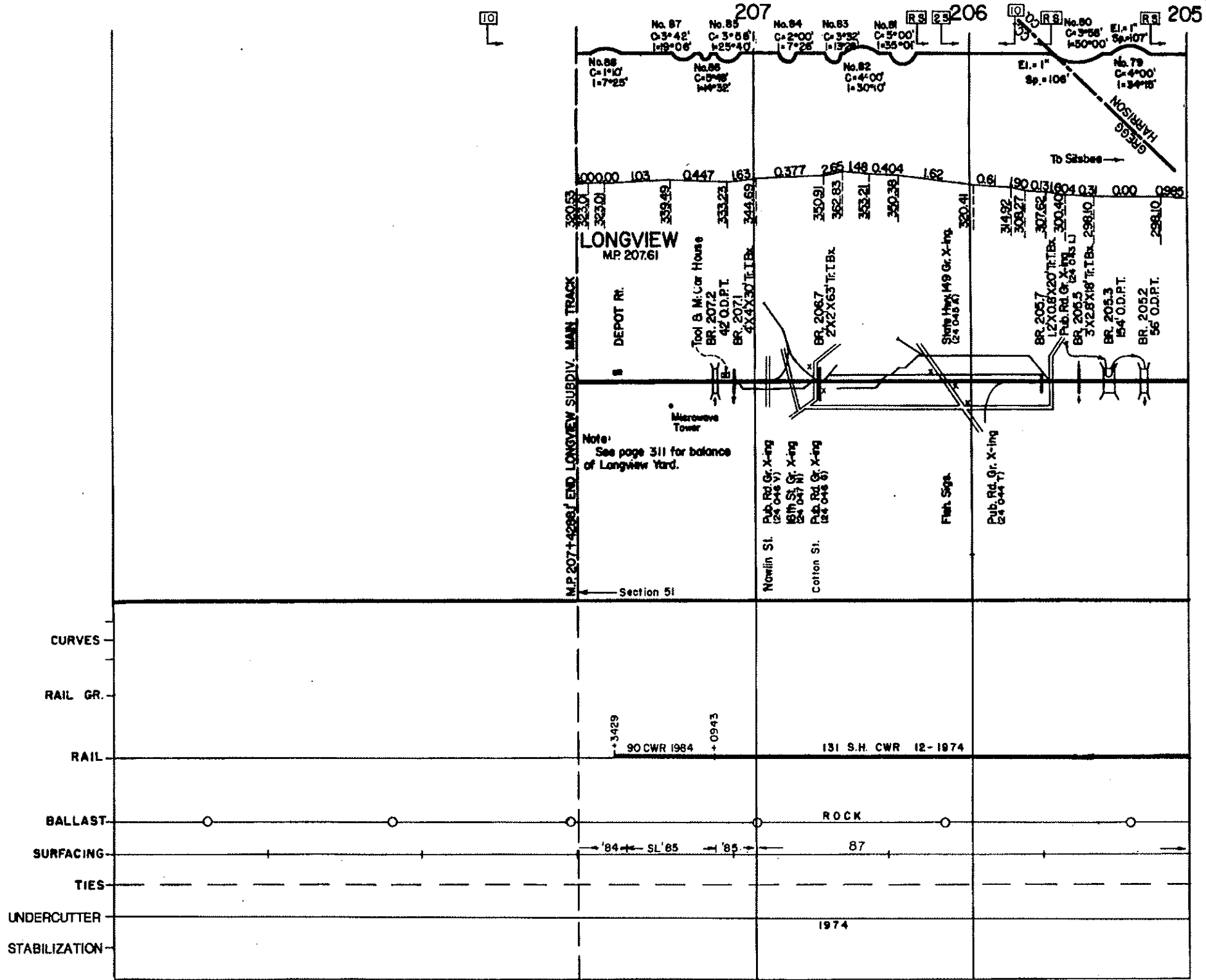
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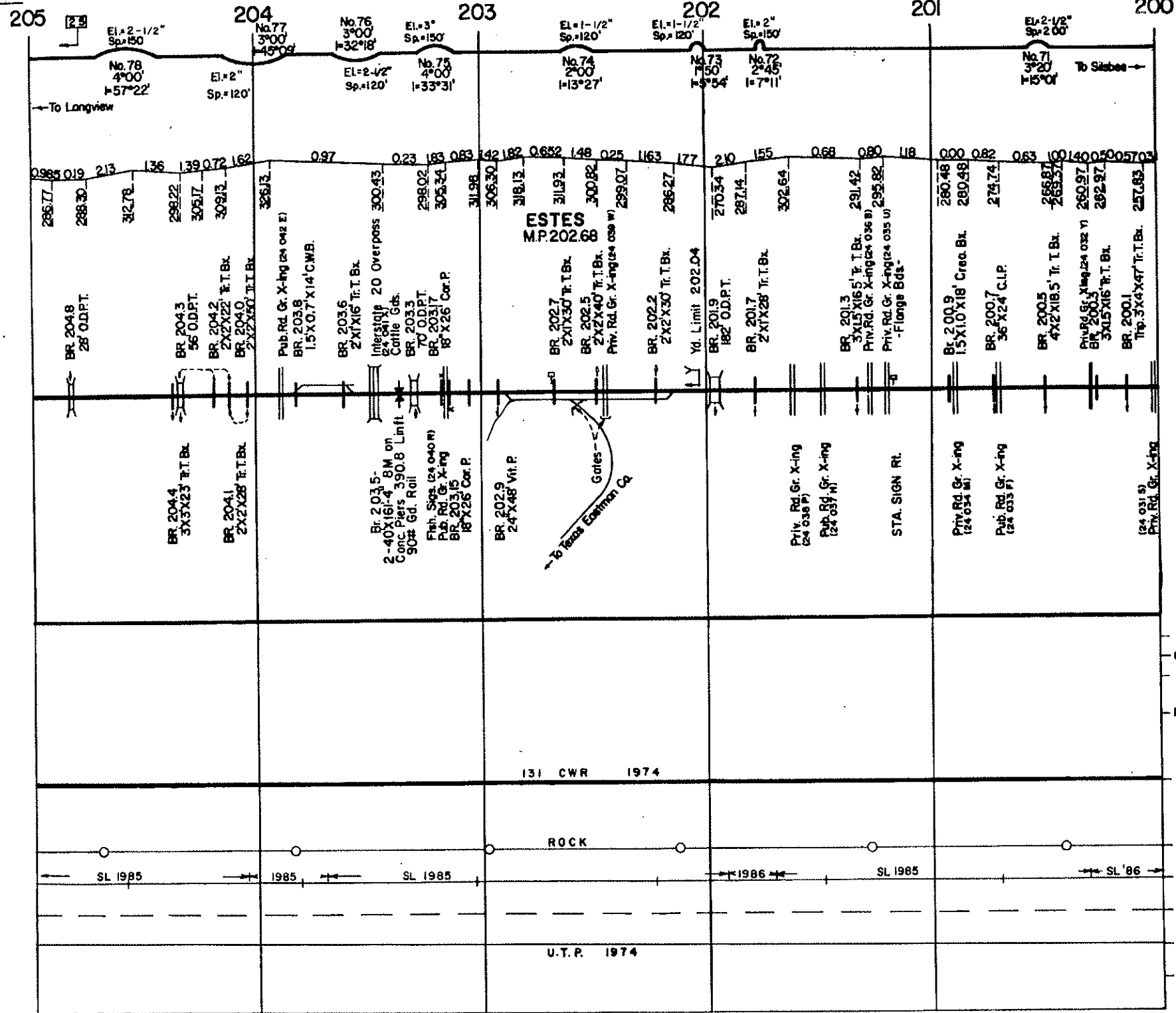


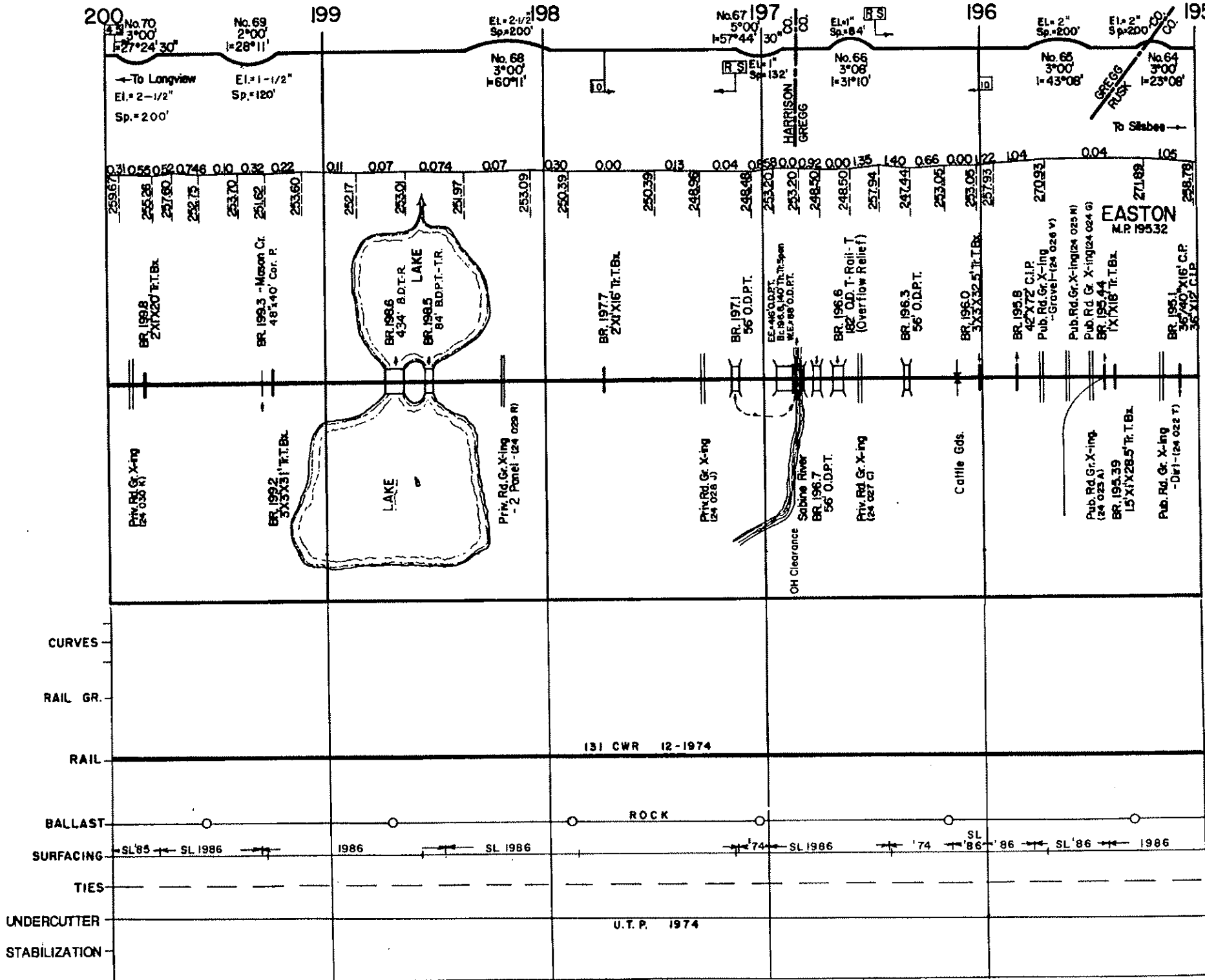
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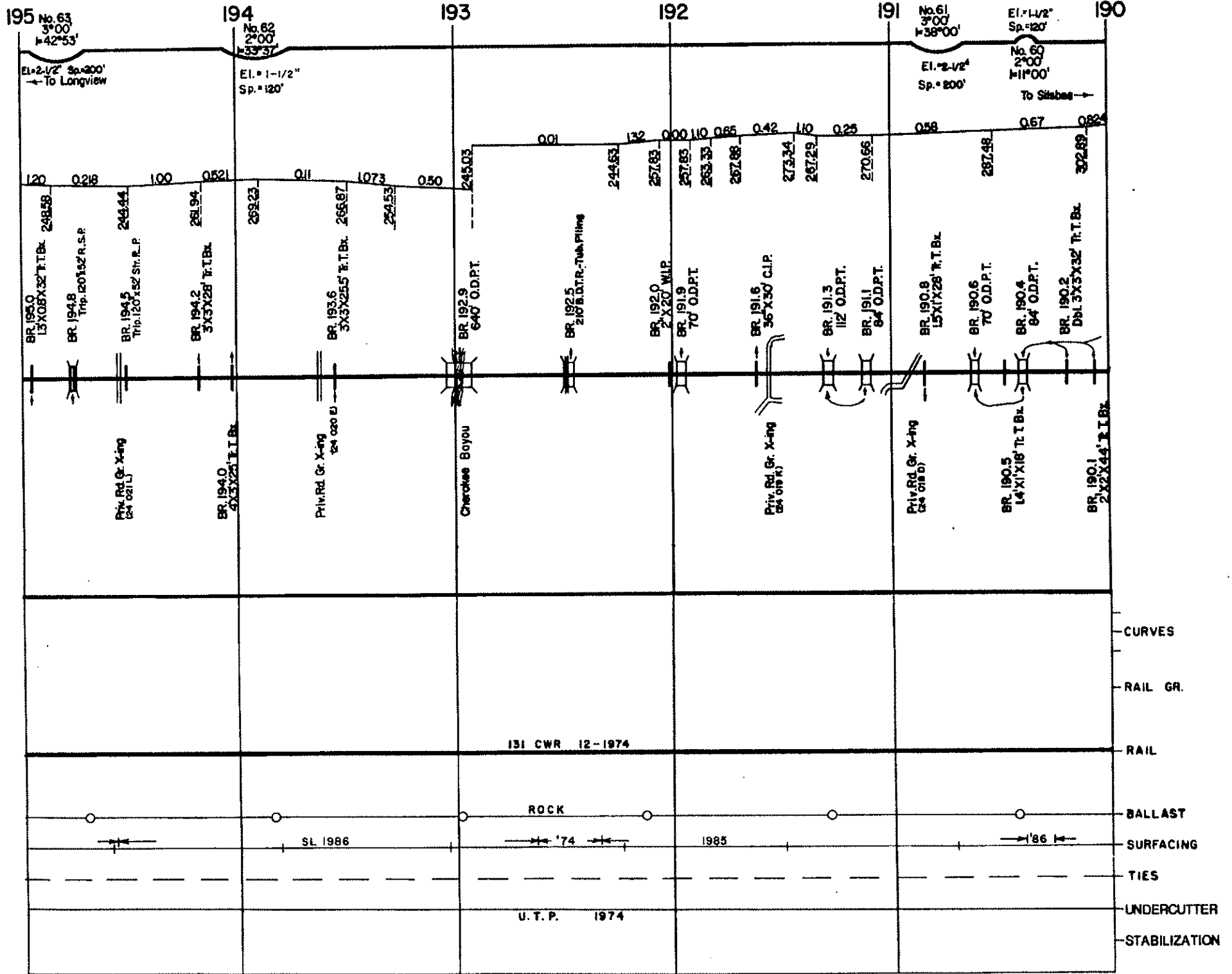


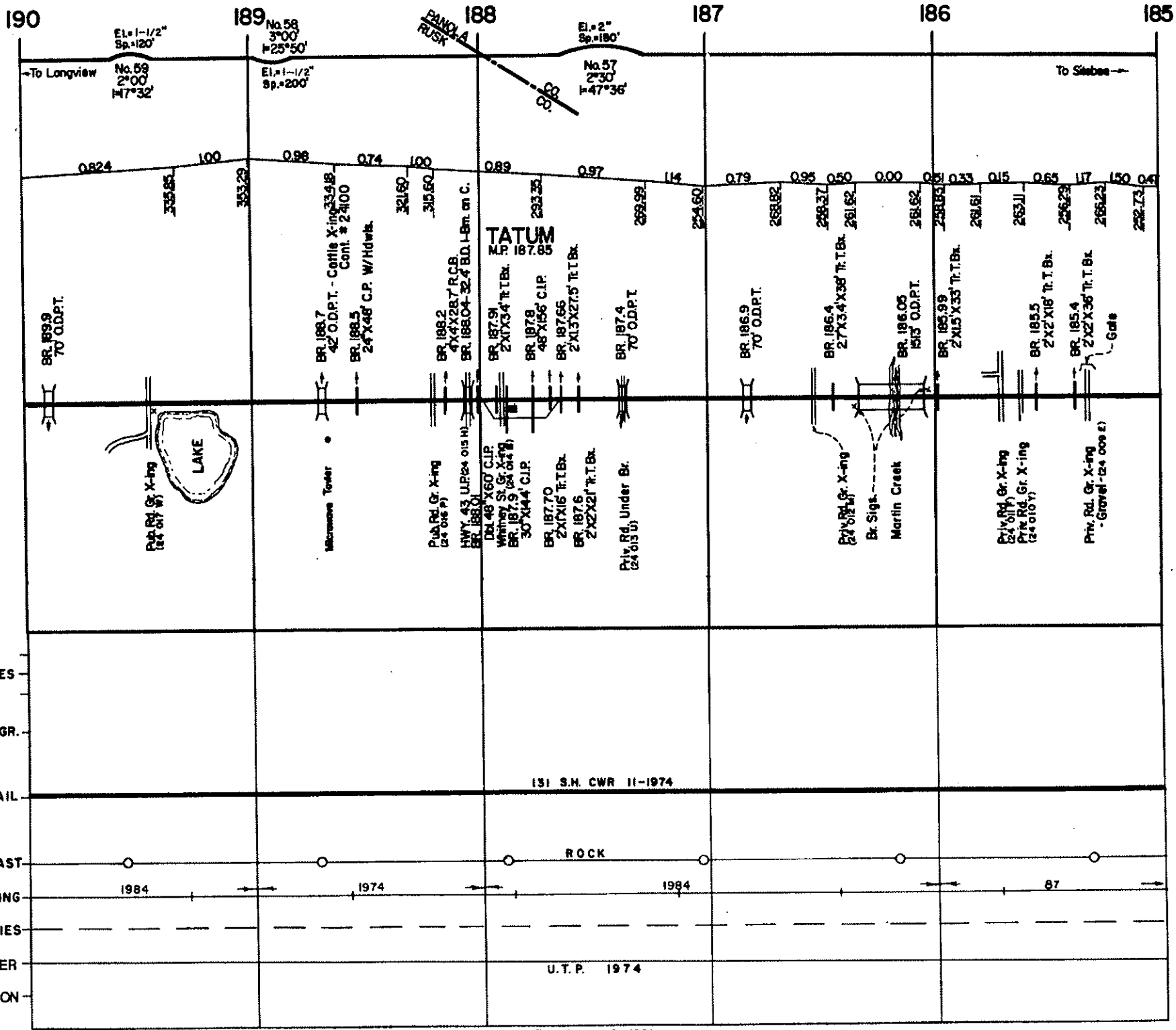
**LONGVIEW YARD  
LONGVIEW, TEXAS**

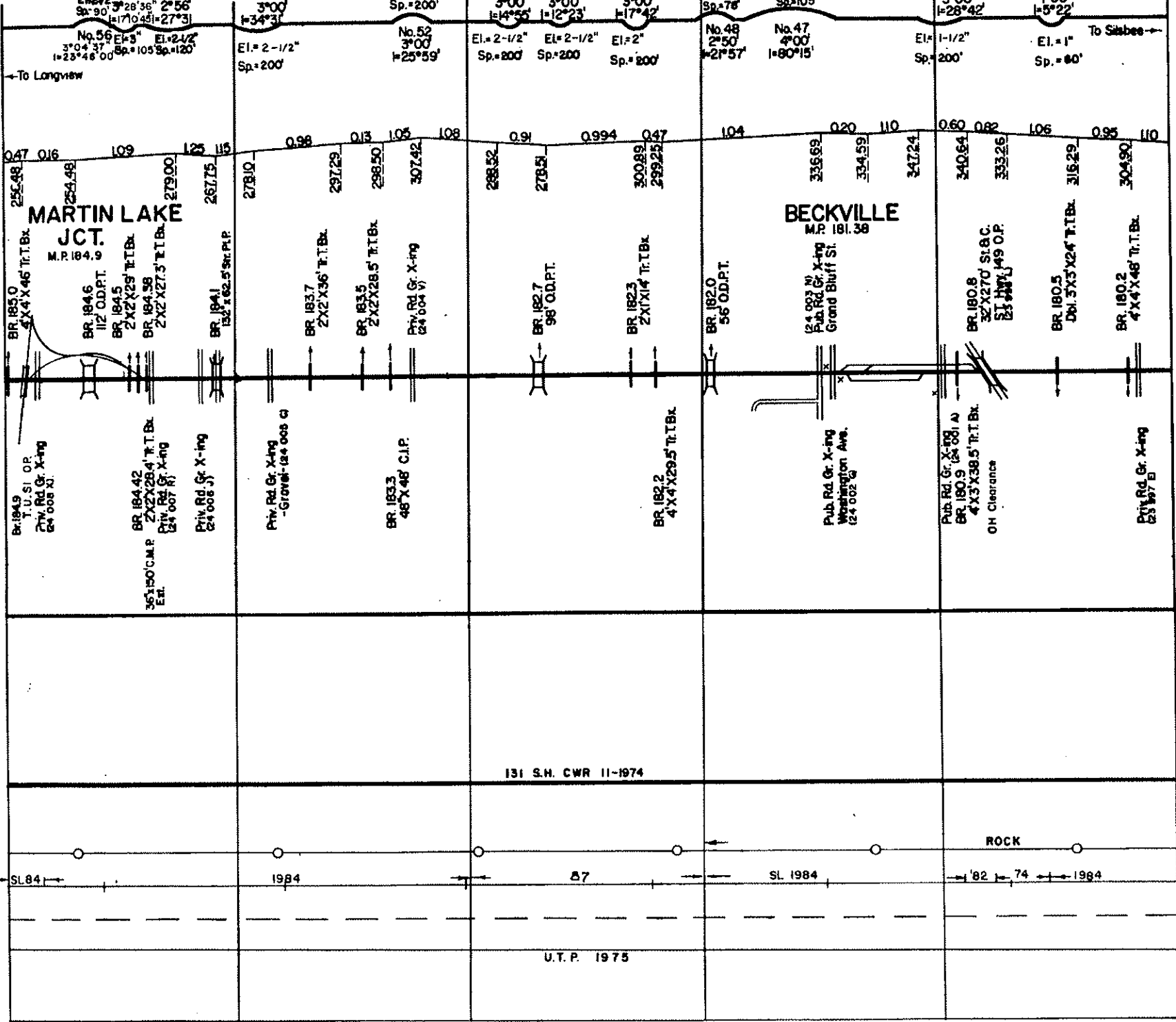




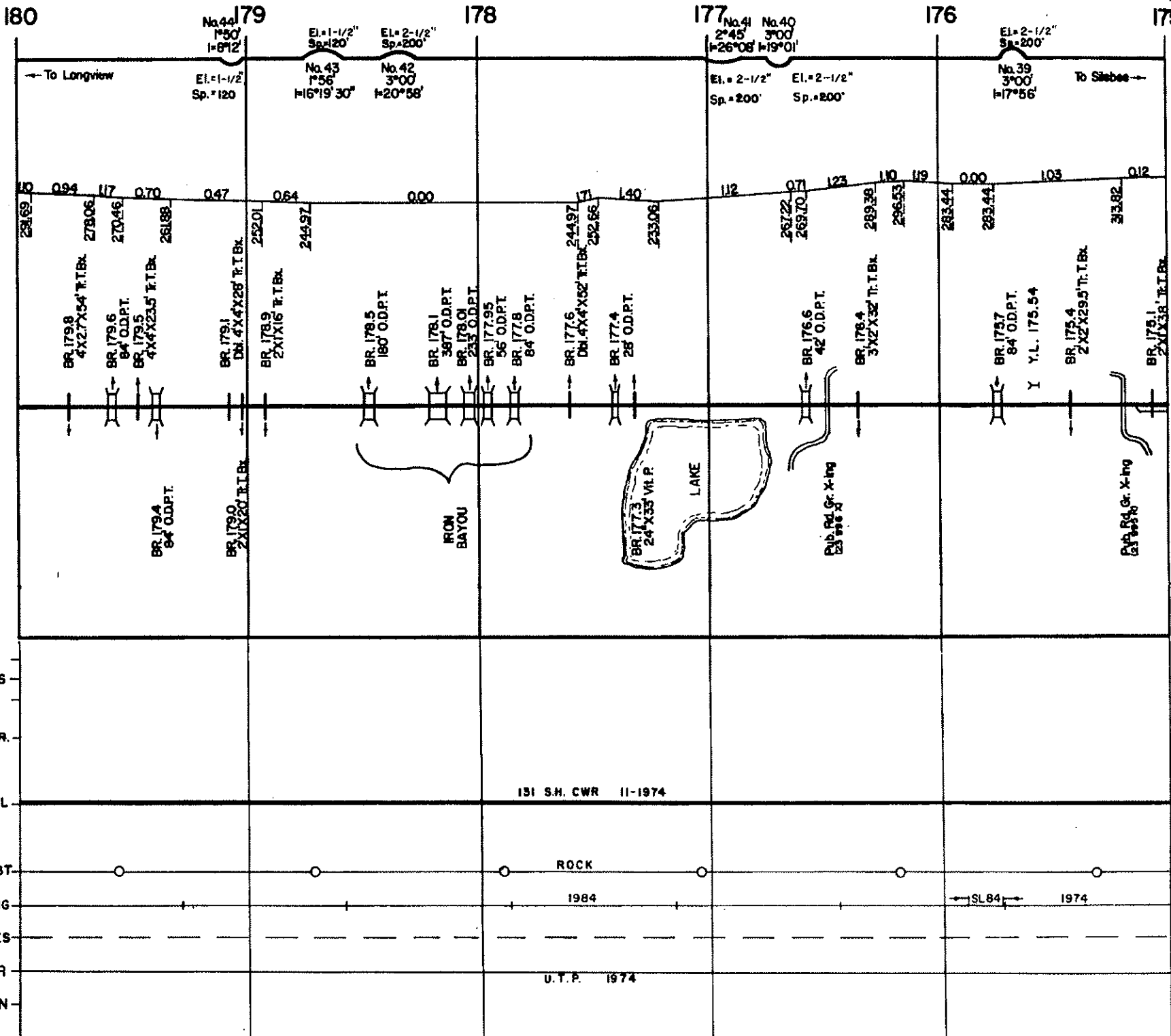












175

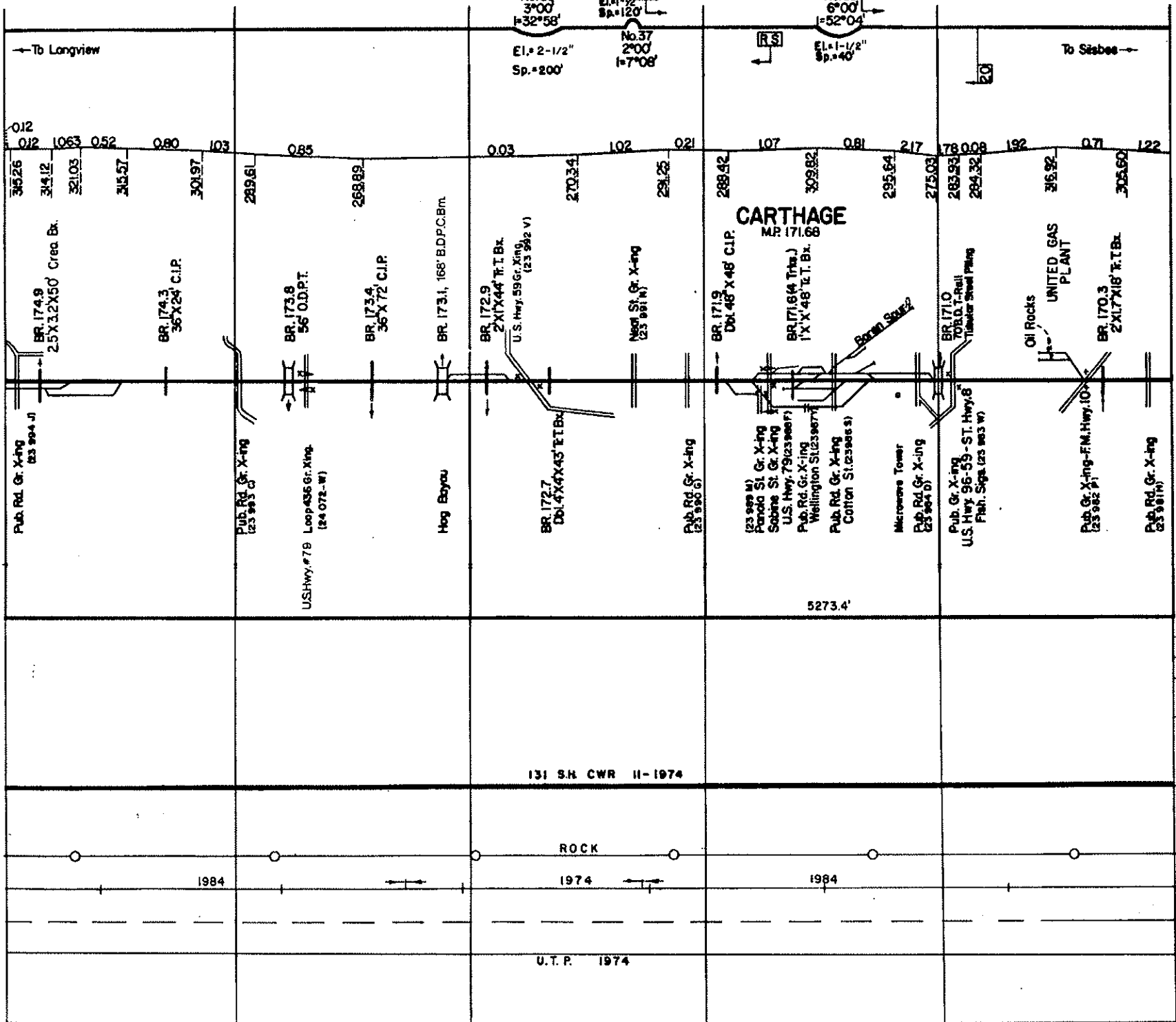
174

173

172

171

170



CURVES  
 RAIL GR.  
 RAIL  
 BALLAST  
 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION

170

No. 34  
2°08'  
T=13°28'30"

168

El. = 5"  
Sp. = 30'  
No. 33  
4°30'  
T=30°26'

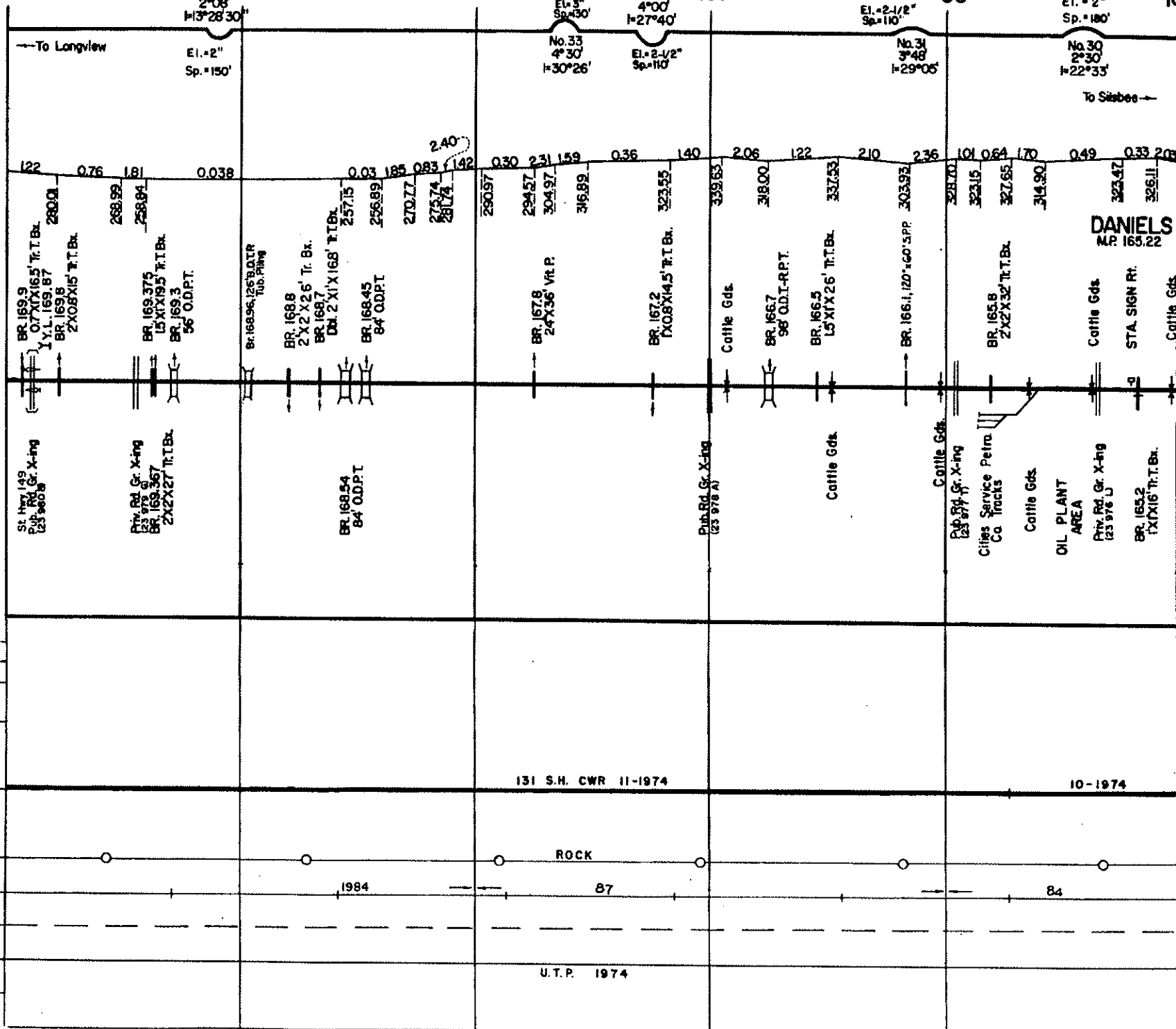
No. 32  
4°00'  
T=27°40'

El. = 2-1/2"  
Sp. = 110'

166

El. = 2"  
Sp. = 180'  
No. 30  
2°30'  
T=22°33'

165



To Longview

To Siassee

**DANIELS**  
M.P. 165.22

CURVES

RAIL GR.

RAIL

BALLAST

SURFACING

TIES

UNDERCUTTER

STABILIZATION

131 S.H. CWR II-1974

10-1974

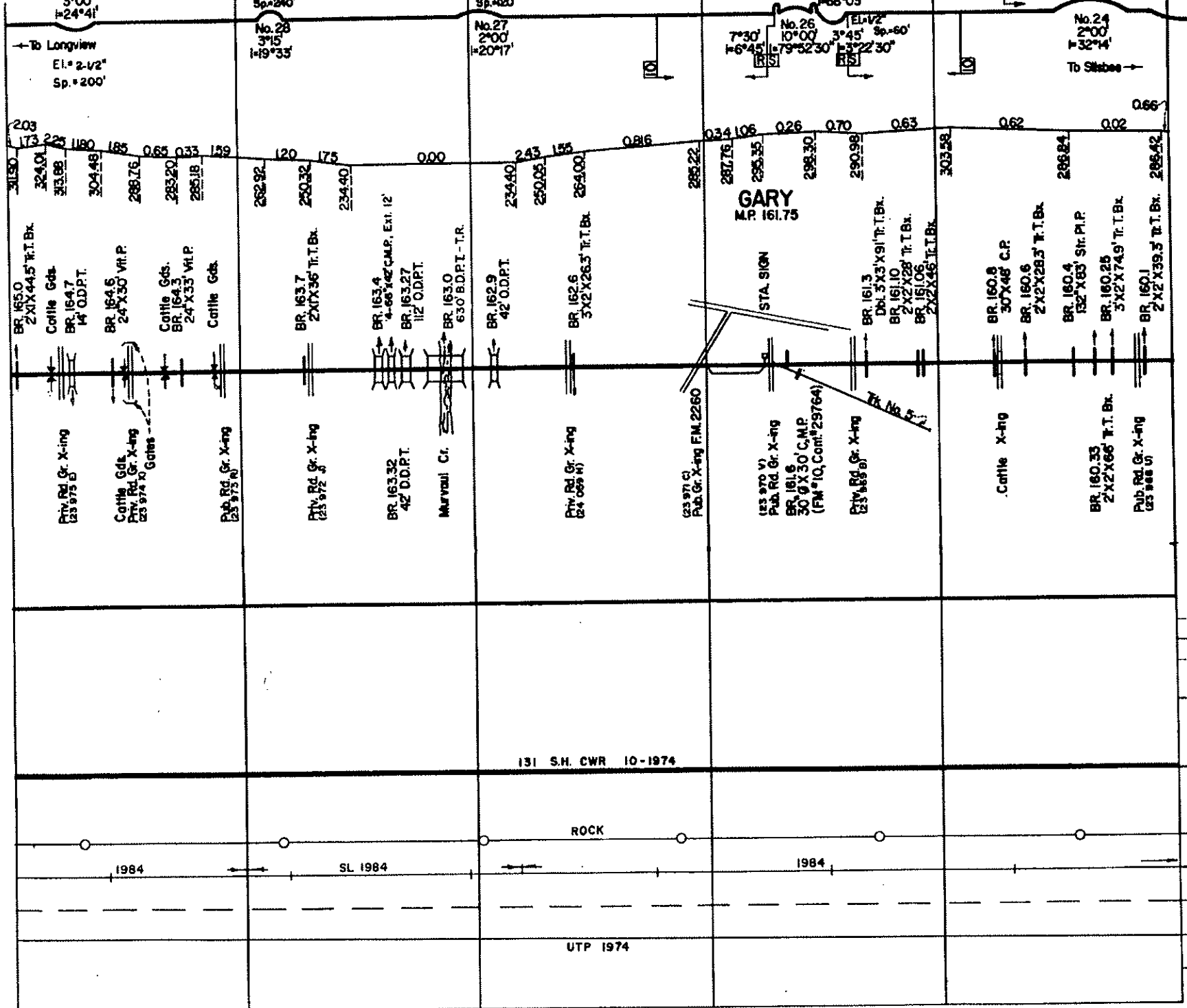
ROCK

1984

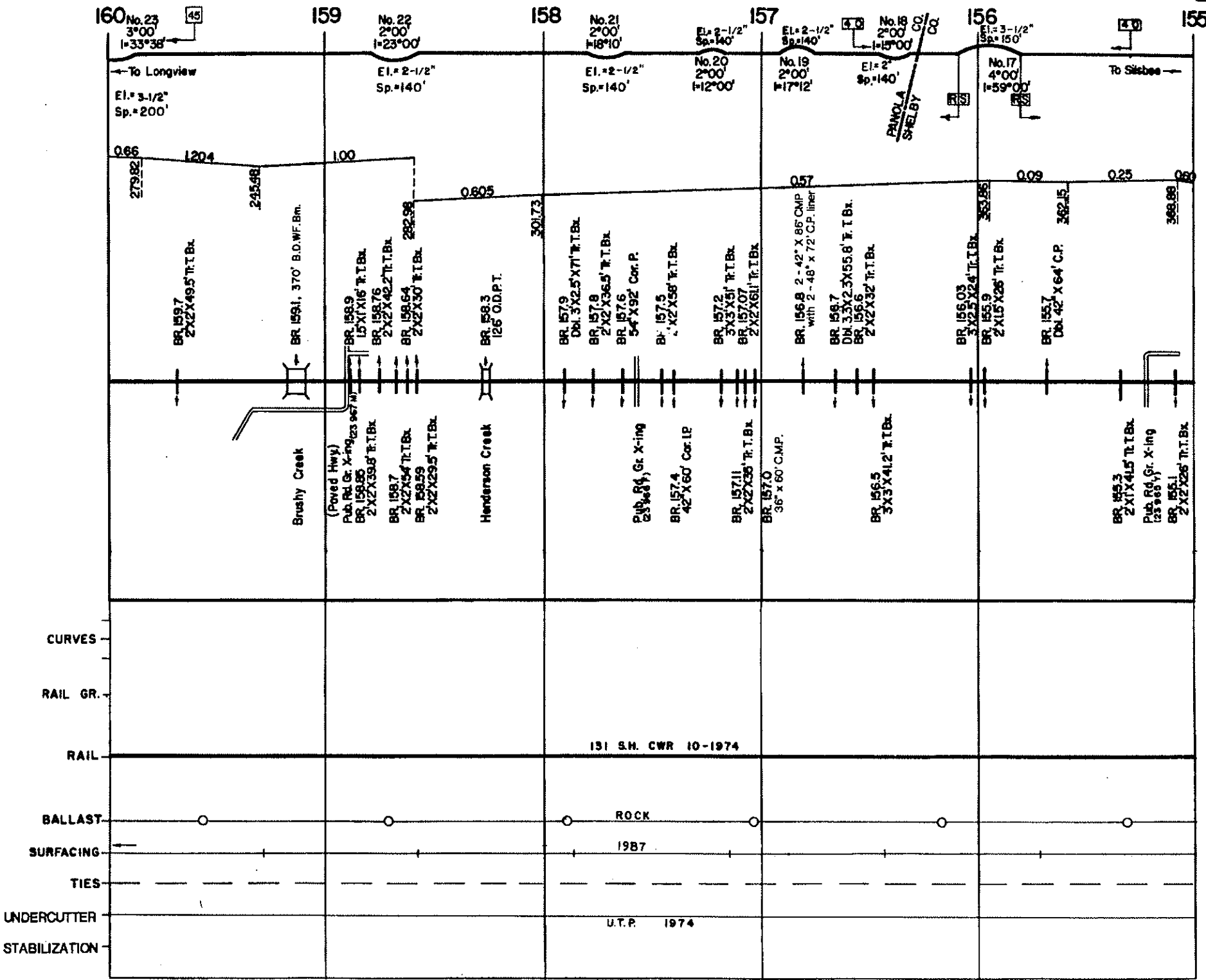
87

84

U.T.P. 1974



- CURVES
- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION



155

154

153

152

151

150

No. 16  
2°00'  
=23°24'

No. 15  
3°00'  
=23°24'

No. 14  
2°00'  
=32°00'

No. 13  
3°00'  
=39°00'

To Longview

To Stabes

El. = 2"  
Sp. = 120'

El. = 1"  
Sp. = 70'

El. = 2-1/2"  
Sp. = 200'

0.60 0.08 0.61 0.59 0.26 0.53 0.41 0.32 0.63 0.50 0.57

355.88

358.40

345.24

357.45

349.39

357.02

353.62

344.02

334.57

317.07

BR 154.6  
4' X 3' X 49' Tr. T. Bx.

BR 154.0  
36' X 48' R.C.P.

BR 153.8  
2' X 2' X 36' Tr. T. Bx.

BR 153.3  
2' X 2' X 42.5' Tr. T. Bx.

BR 152.6  
48' X 64' Bx. Conc. Pipe

BR 152.4  
3' X 3' X 76.5' Tr. T. Bx.

BR 152.0  
4' X 2' X 53.5' Tr. T. Bx.

TENAHA  
Md 151.63

BR 151.55  
5' X 92' R.C.P.

BR 151.0  
2' X 1' X 45' Tr. T. Bx.

BR 150.9  
36' X 36' Cor. P.

BR 150.6 - 42' X 60' C.M.P.

BR 150.43  
3' X 3' X 48' Tr. T. Bx.

BR 150.2  
3' X 3.5' X 52' Tr. T. Bx.

BR 150.0  
2' X 1.5' X 41' Tr. T. Bx.

Pub. Rd. Gr. X-ing  
FM # 947 (23 964 S)  
Priv. Rd. Gr. X-ing  
(23 963 N)  
Priv. Rd. Gr. X-ing  
(23 962 S)  
Priv. Rd. Gr. X-ing  
(23 961 N)

Yd. Limit 153.06  
Priv. Rd. Gr. X-ing  
(23 960 S)

Pub. Rd. Gr. X-ing  
(23 959 V)

BR 151.56, 168' U.P.  
B.D. 18m. on C.  
Hwy. 15, 59 8 84  
(23 957 S)

FM 947  
Pub. Rd. Gr. X-ing  
(23 966 T)

BR 150.39  
2' X 2' X 50' Tr. T. Bx.

BR 150.1  
42' X 72' C.P.



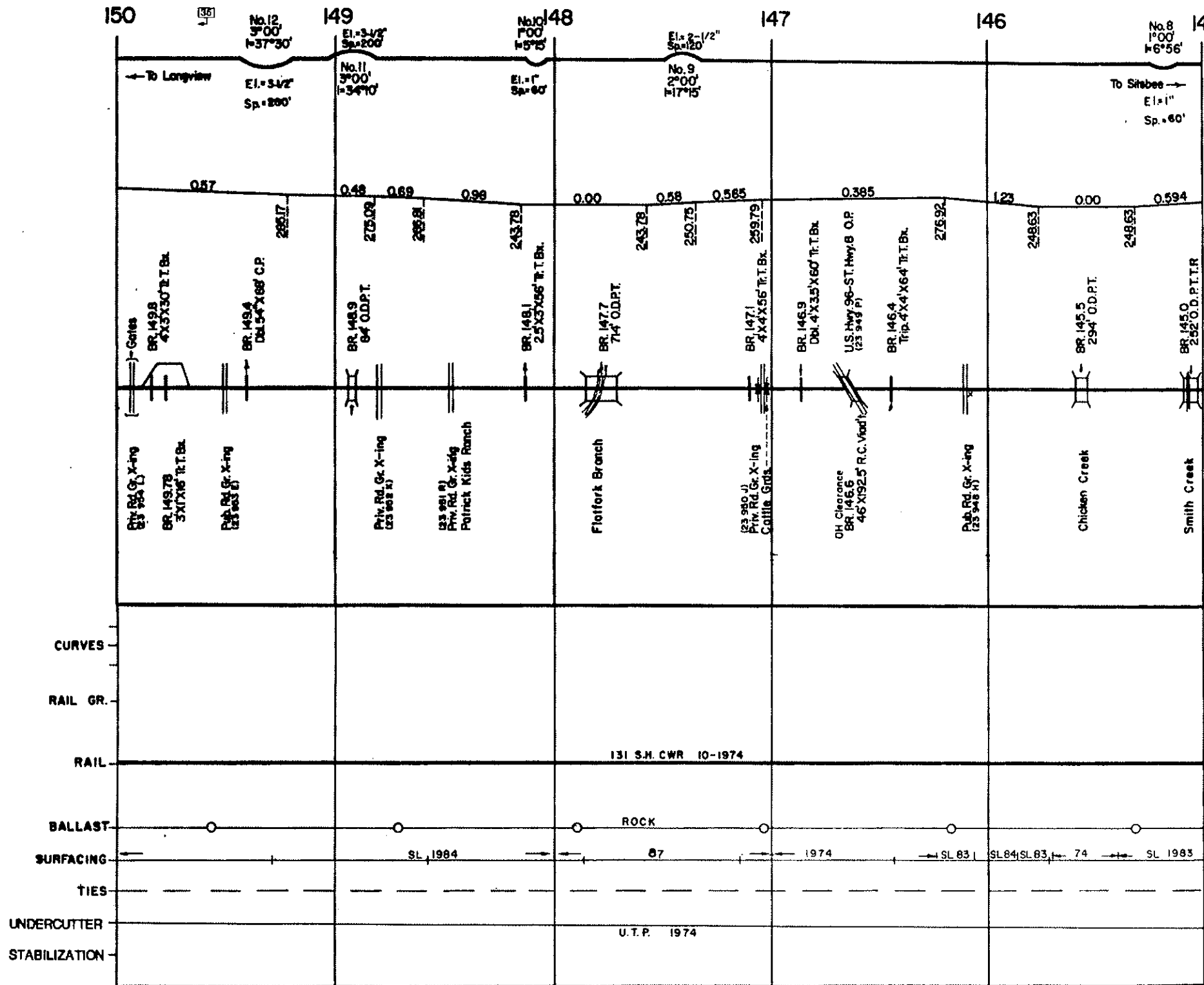
- CURVES
- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION

131 S.H. CWR 10-74

ROCK

1987

U.T.P. 1974



145

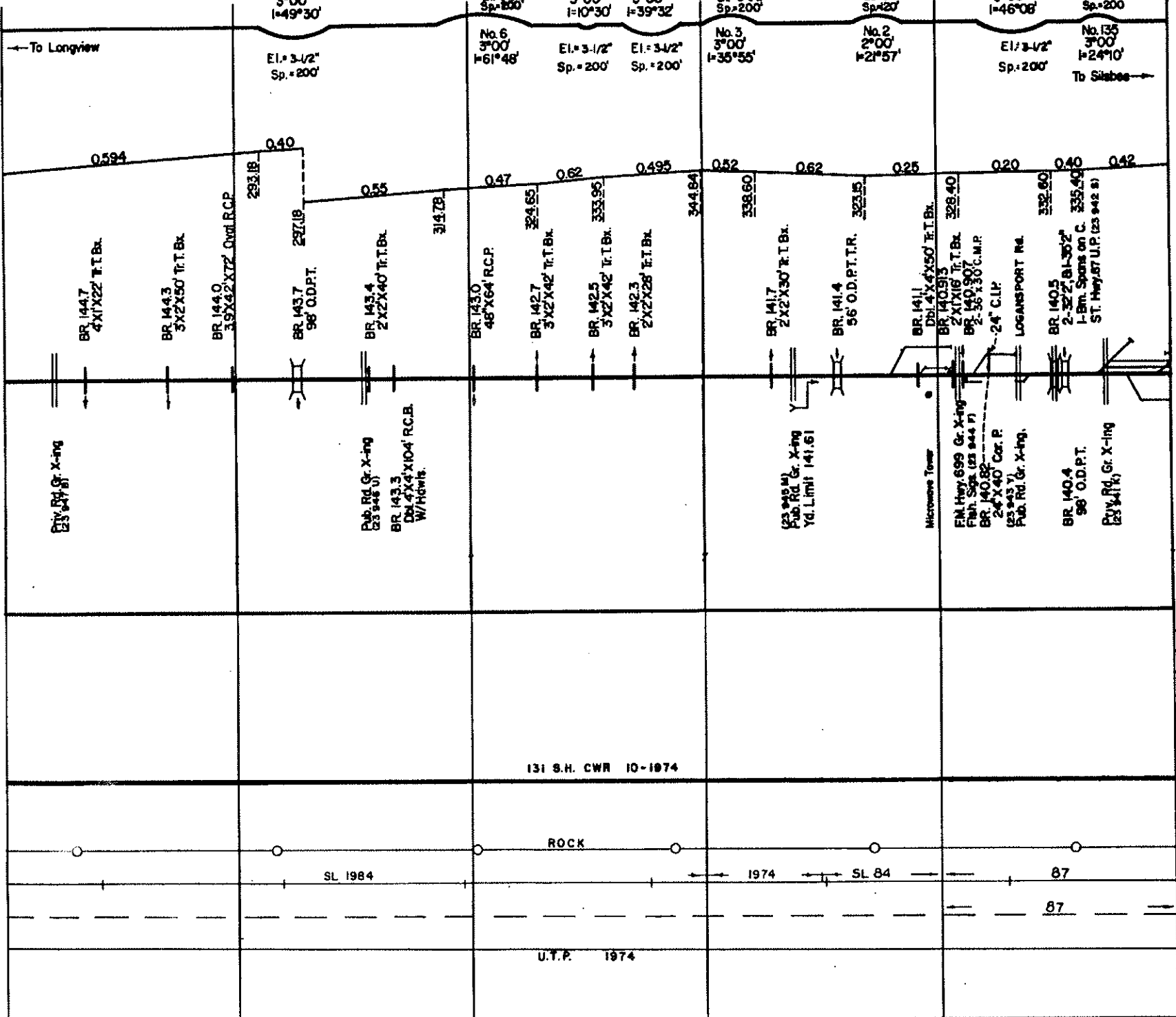
144

143

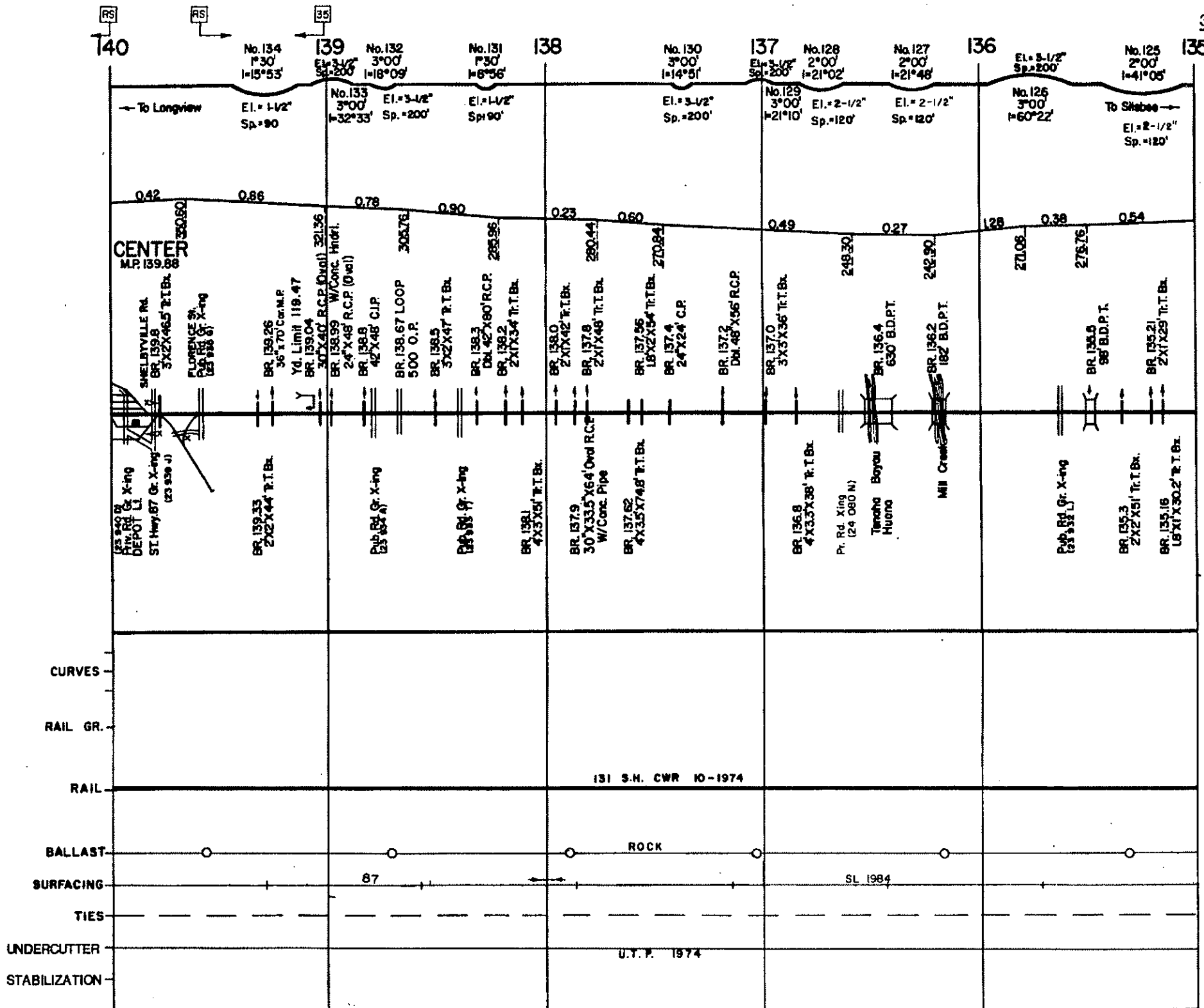
142

141

140







RS  
140

RS

35  
139

138

137

136

135

No. 134  
3°00'  
I=15°53'

No. 132  
3°00'  
I=16°09'

No. 131  
3°00'  
I=6°36'

No. 130  
3°00'  
I=14°51'

No. 128  
2°00'  
I=21°02'

No. 127  
2°00'  
I=21°48'

EL=3-1/2'  
Sp.=200'

No. 125  
2°00'  
I=41°05'

CENTER  
M.P. 139.88

CURVES

RAIL GR.

RAIL

BALLAST

SURFACING

TIES

UNDERCUTTER

STABILIZATION

131 S.H. CWR 10-1974

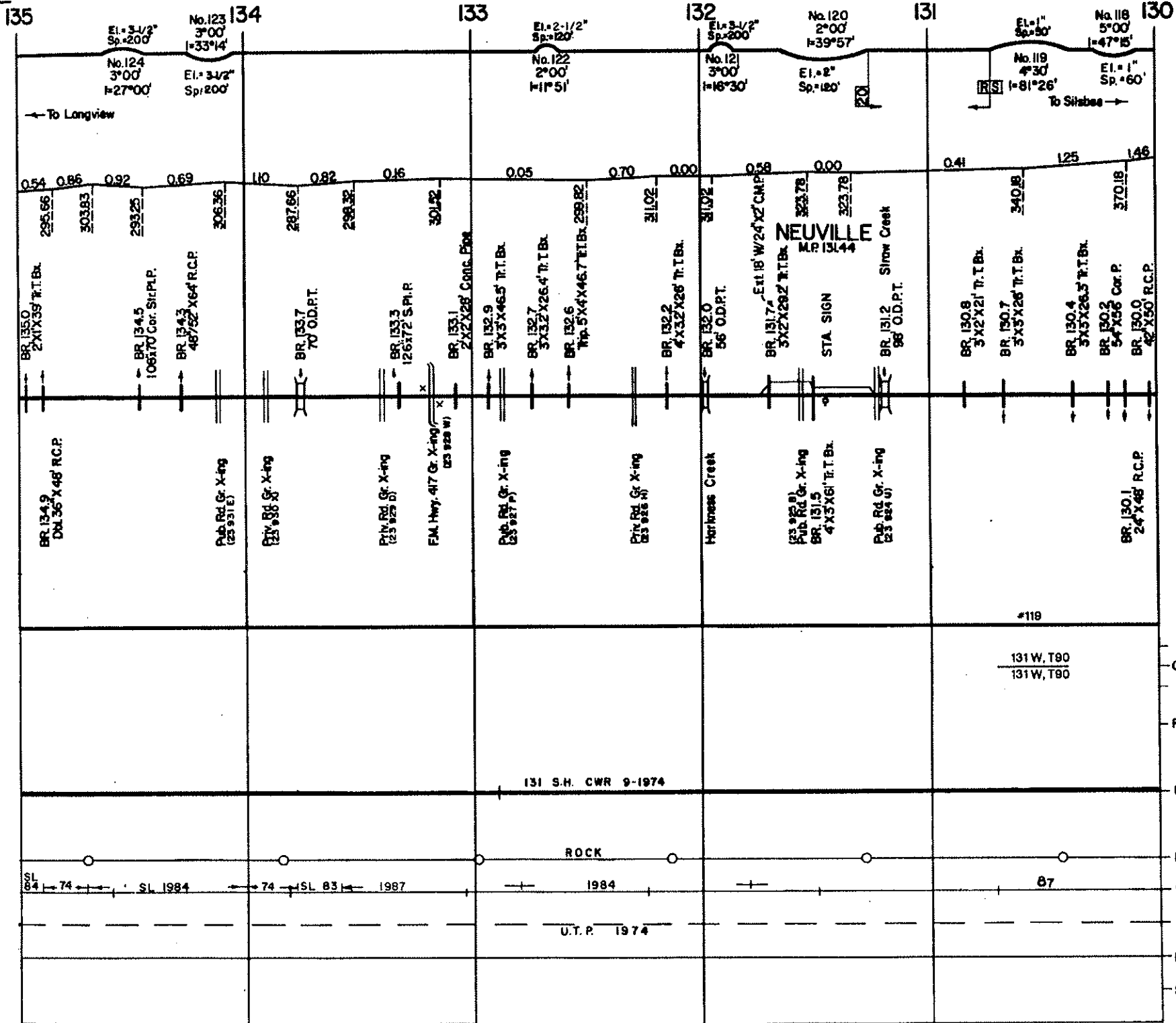
ROCK

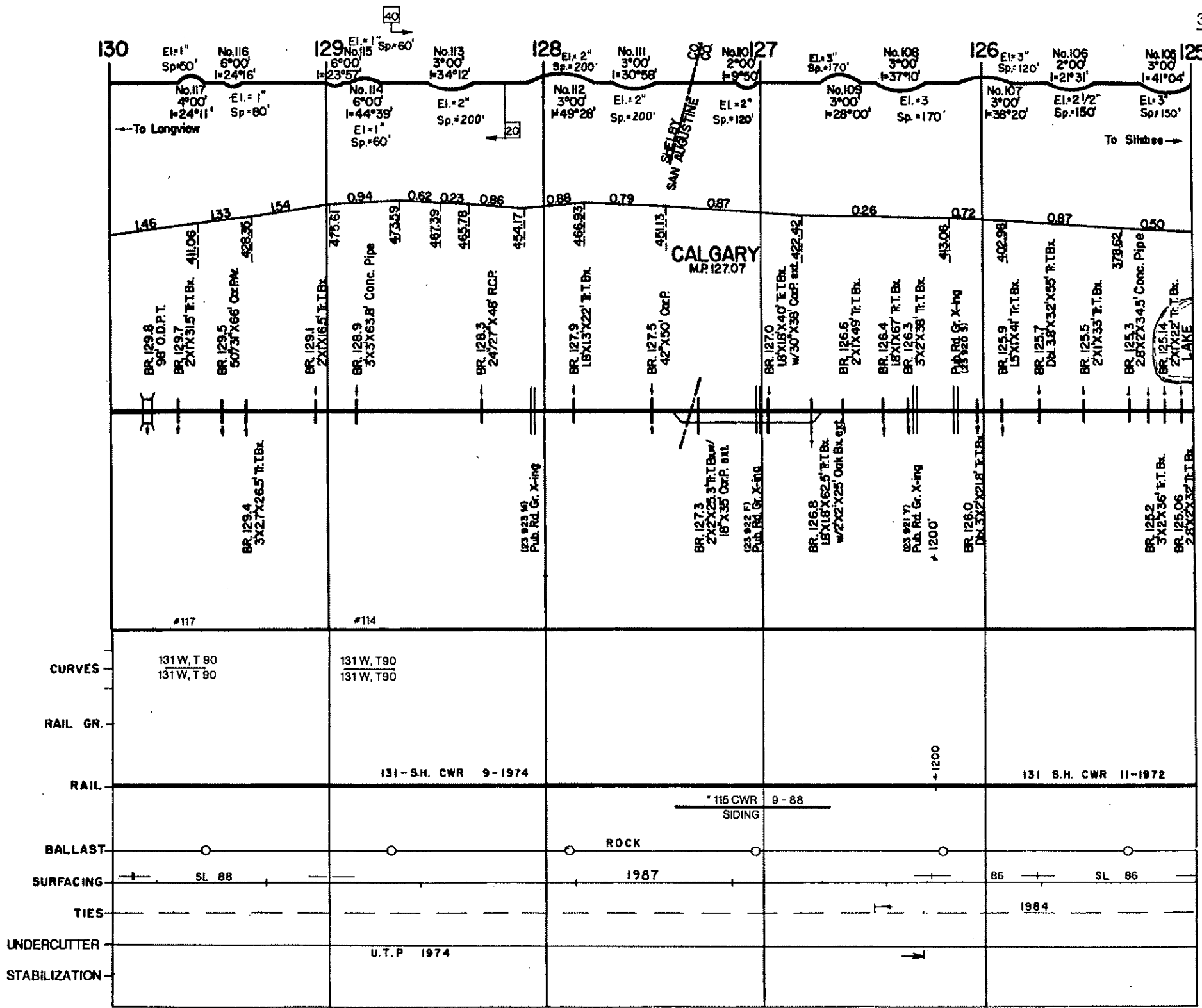
87

SL 1984

U.T.P. 1974

REVISED 3-1991





125

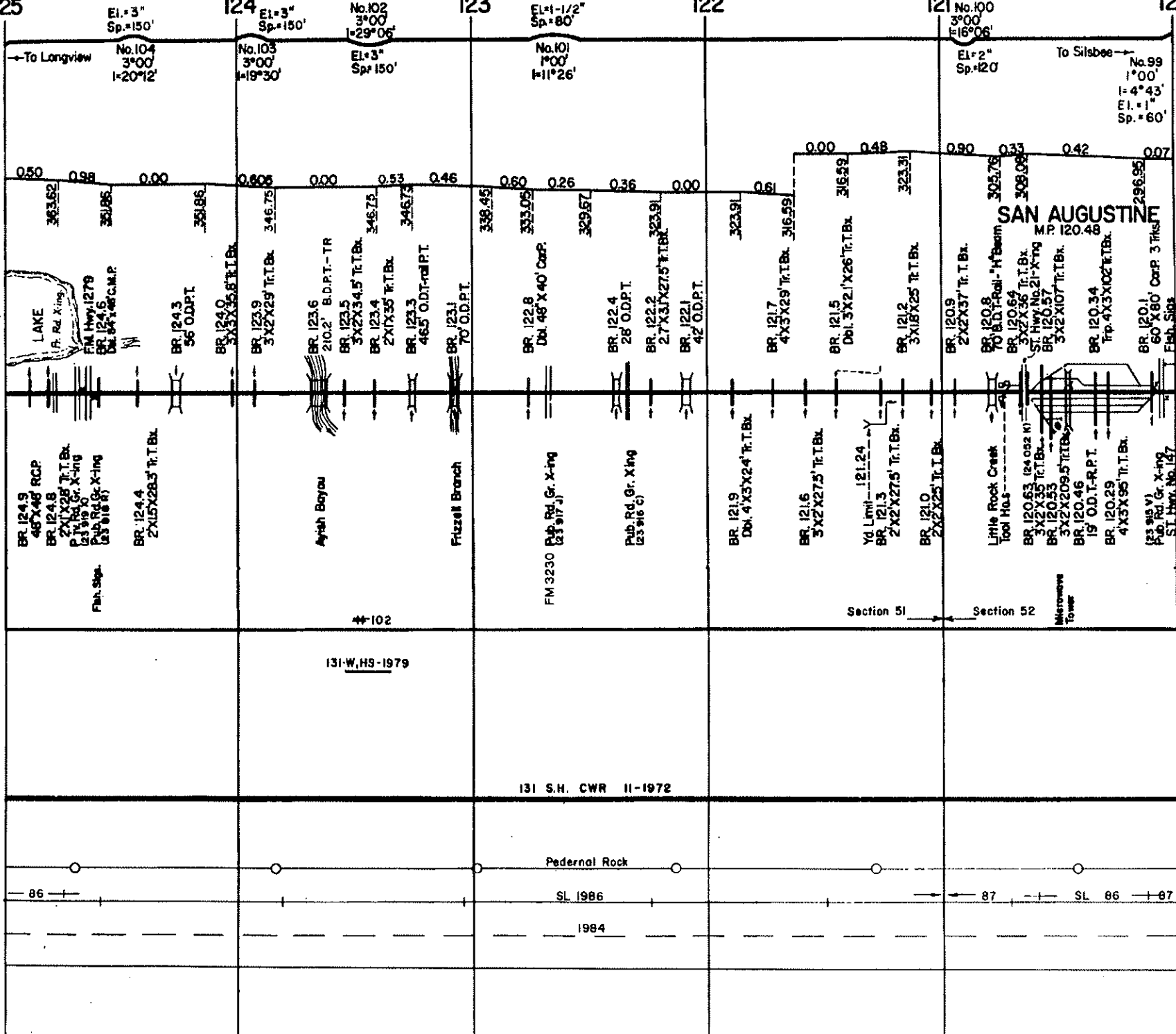
124

123

122

121

120



No.104  
 3°00'  
 I=20°12'  
 To Longview

No.103  
 3°00'  
 I=19°30'  
 No.102  
 3°00'  
 I=29°06'  
 No.101  
 3°00'  
 I=11°26'  
 No.100  
 3°00'  
 I=16°06'  
 To Silsbee

El. = 3"  
 Sp. = 150'  
 El. = 3"  
 Sp. = 150'  
 El. = 3"  
 Sp. = 150'  
 El. = 1-1/2"  
 Sp. = 80'  
 El. = 2"  
 Sp. = 120'  
 No.99  
 1°00'  
 I = 4°43'  
 F.I. = 1"  
 Sp. = 60'

0.50 0.98 0.00 0.606 0.00 0.53 0.46 0.60 0.26 0.36 0.00 0.00 0.48 0.90 0.33 0.42 0.07

343.62 351.85 351.86 346.75 346.75 346.75 346.75 339.45 333.05 329.57 323.91 323.91 316.59 316.59 323.91 323.91 305.76 308.08 296.95

BR. 124.9  
46'x46' R.C.P.  
BR. 124.8  
2'x12' T.T.Bx  
Pub. Rd. Gr. X-ing  
123 918 D  
Pub. Rd. Gr. X-ing  
123 918 R

Fish. Stp.

BR. 124.4  
2'x15'x28.5' T.T.Bx

BR. 124.3  
56' O.D.P.T.

BR. 124.0  
3'x3'x36.5' T.T.Bx

BR. 123.9  
3'x2'x29' T.T.Bx

Ayish Bayou

BR. 123.6  
210.2' B.D.P.T.-TR

BR. 123.5  
3'x2'x34.5' T.T.Bx

BR. 123.4  
2'x1'x35' T.T.Bx

BR. 123.3  
46.5' O.D.T.-R.P.T.

Frazell Branch

BR. 123.1  
70' O.D.P.T.

FM 3230  
Pub. Rd. Gr. X-ing  
123 917 J

BR. 122.8  
Dbl. 46'x40' CorP.

BR. 122.4  
28' O.D.P.T.

BR. 122.2  
2.7'x3'x27.5' T.T.Bx

BR. 122.1  
42' O.D.P.T.

BR. 121.9  
Dbl. 4'x3'x24' T.T.Bx

BR. 121.6  
3'x2'x27.5' T.T.Bx

Yd. Limit  
121.24

BR. 121.3  
2'x2'x27.5' T.T.Bx

BR. 121.0  
2'x2'x25' T.T.Bx

BR. 121.7  
4'x3'x29' T.T.Bx

BR. 121.5  
Dbl. 3'x2'x26' T.T.Bx

BR. 121.2  
3'x1.8'x25' T.T.Bx

BR. 120.9  
2'x2'x37' T.T.Bx

BR. 120.8  
70' B.D.T.-Rail-H'Beam

BR. 120.64  
3'x2'x36' T.T.Bx

BR. 120.57  
St. Hwy. No. 21 X-ing

BR. 120.57  
3'x2'x20.5' T.T.Bx

BR. 120.46  
19' O.D.T.-R.P.T.

BR. 120.29  
4'x5'x95' T.T.Bx

BR. 120.34  
Trp. 4'x3'x102' T.T.Bx

BR. 120.1  
60'x60' CorP. 3' H'Beam

ST. Hwy. No. 147

Little Rock Creek  
Tool Has

BR. 120.63 (24 032 M)  
3'x2'x35' T.T.Bx

BR. 120.53  
3'x2'x20.5' T.T.Bx

BR. 120.46  
19' O.D.T.-R.P.T.

BR. 120.29  
4'x5'x95' T.T.Bx

Microwave Tower

Section 51

Section 52

131-W, H9-1979

131 S.H. CWR II-1972

Pedernal Rock

SL 1986

1984

NR  
CURVES  
SR

RAIL GR

RAIL

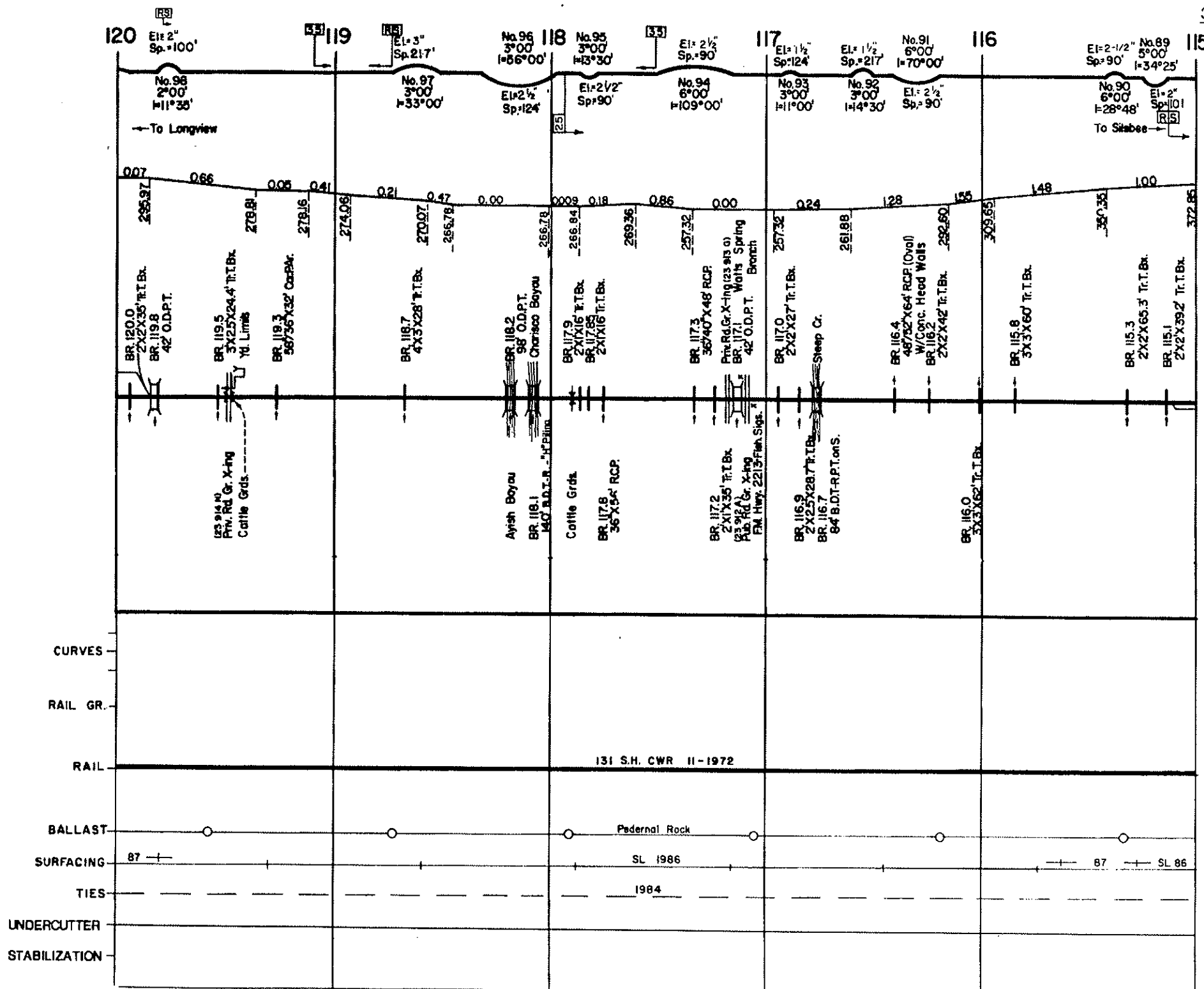
BALLAST

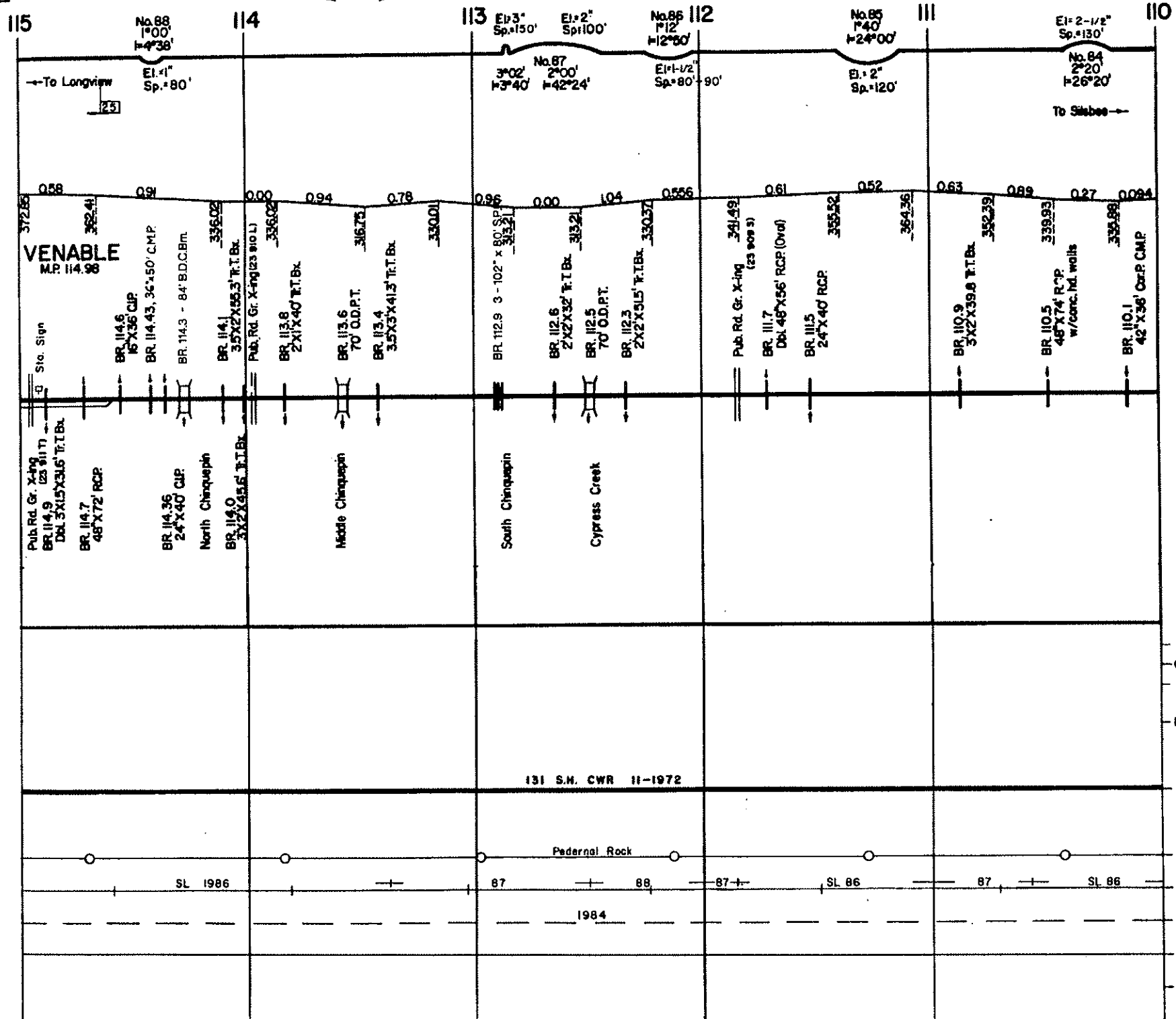
SURFACING  
86 87 88 89 90 91 92 93 94 95 96 97

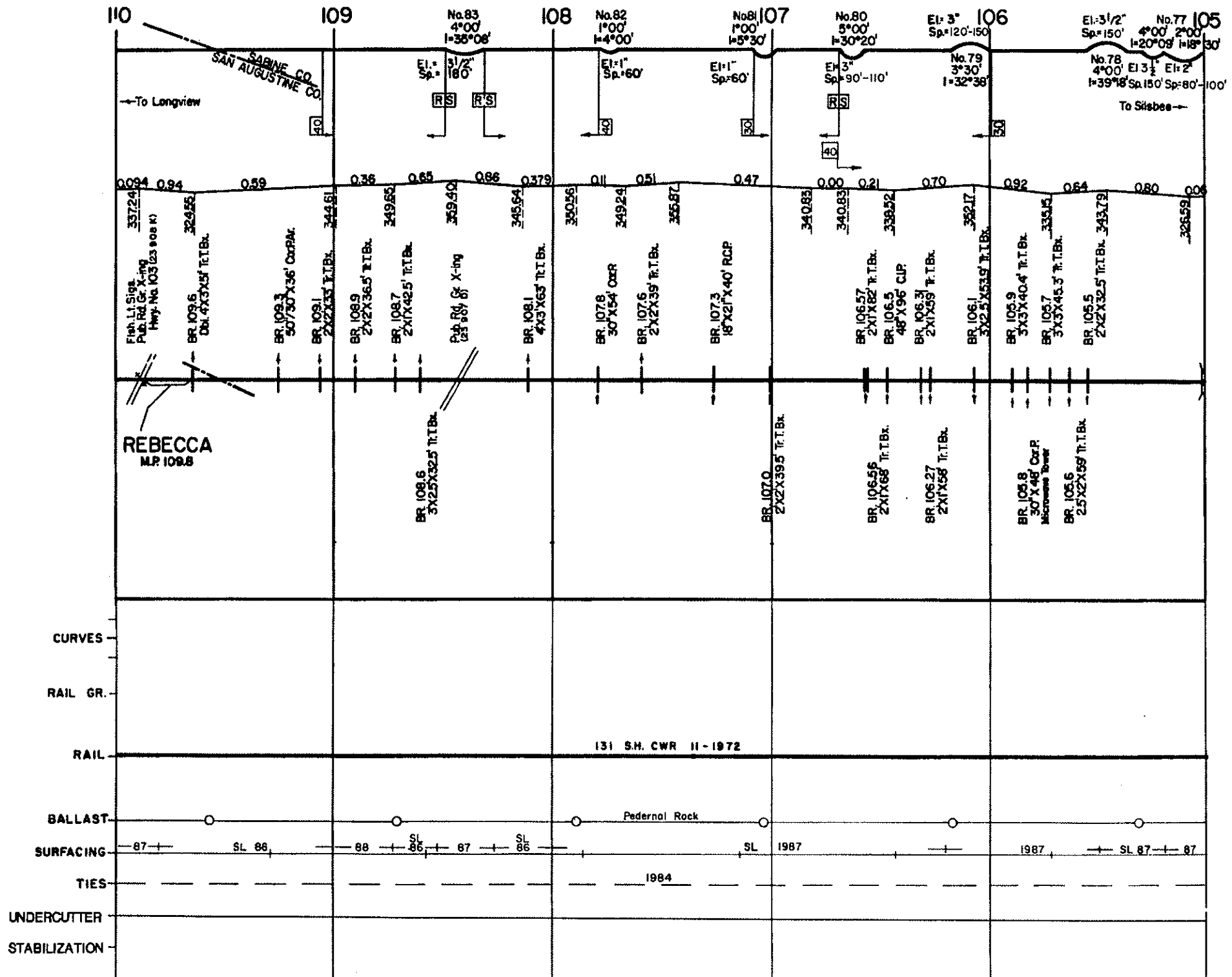
TIES

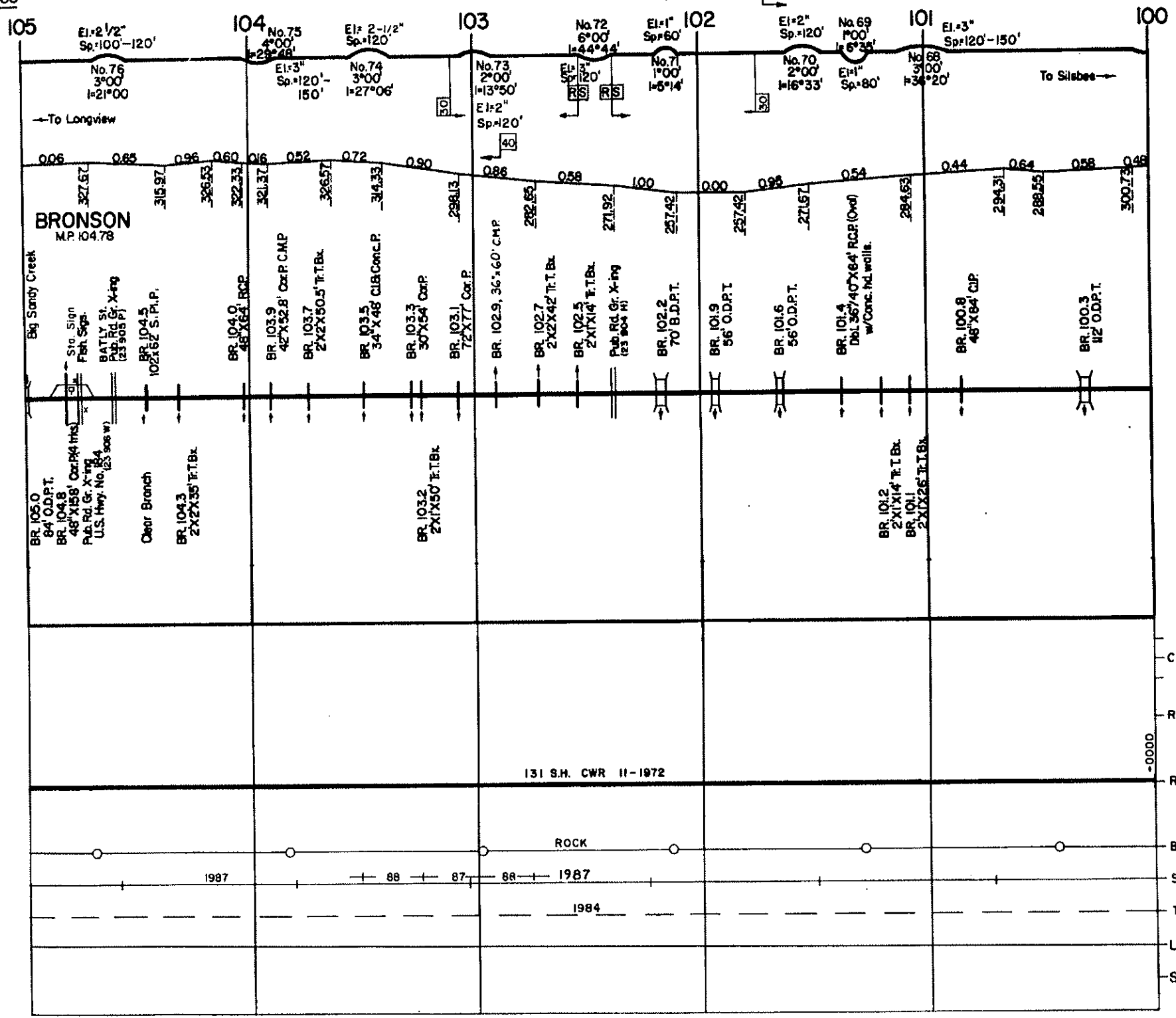
UNDERCUTTER

STABILIZATION



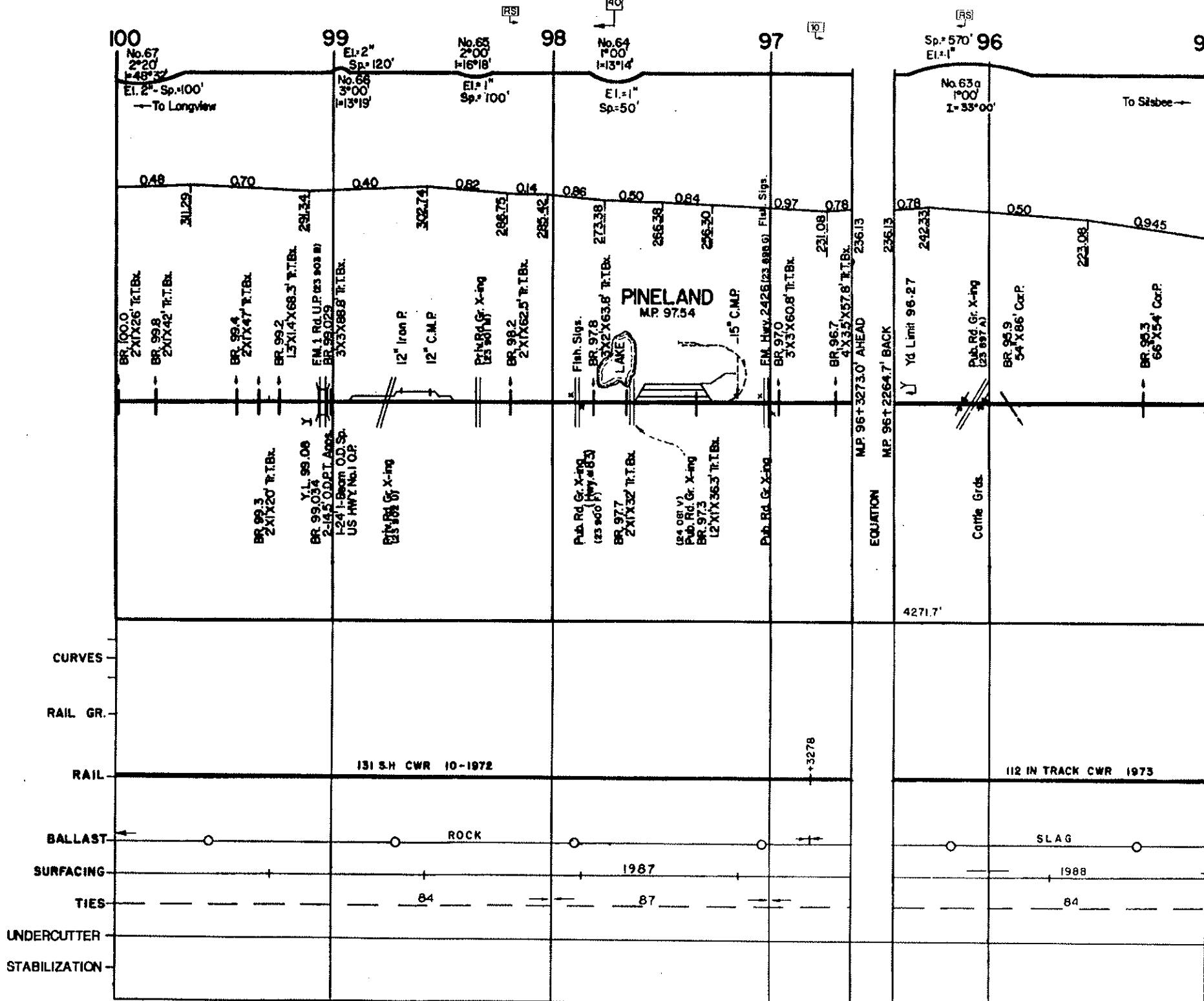


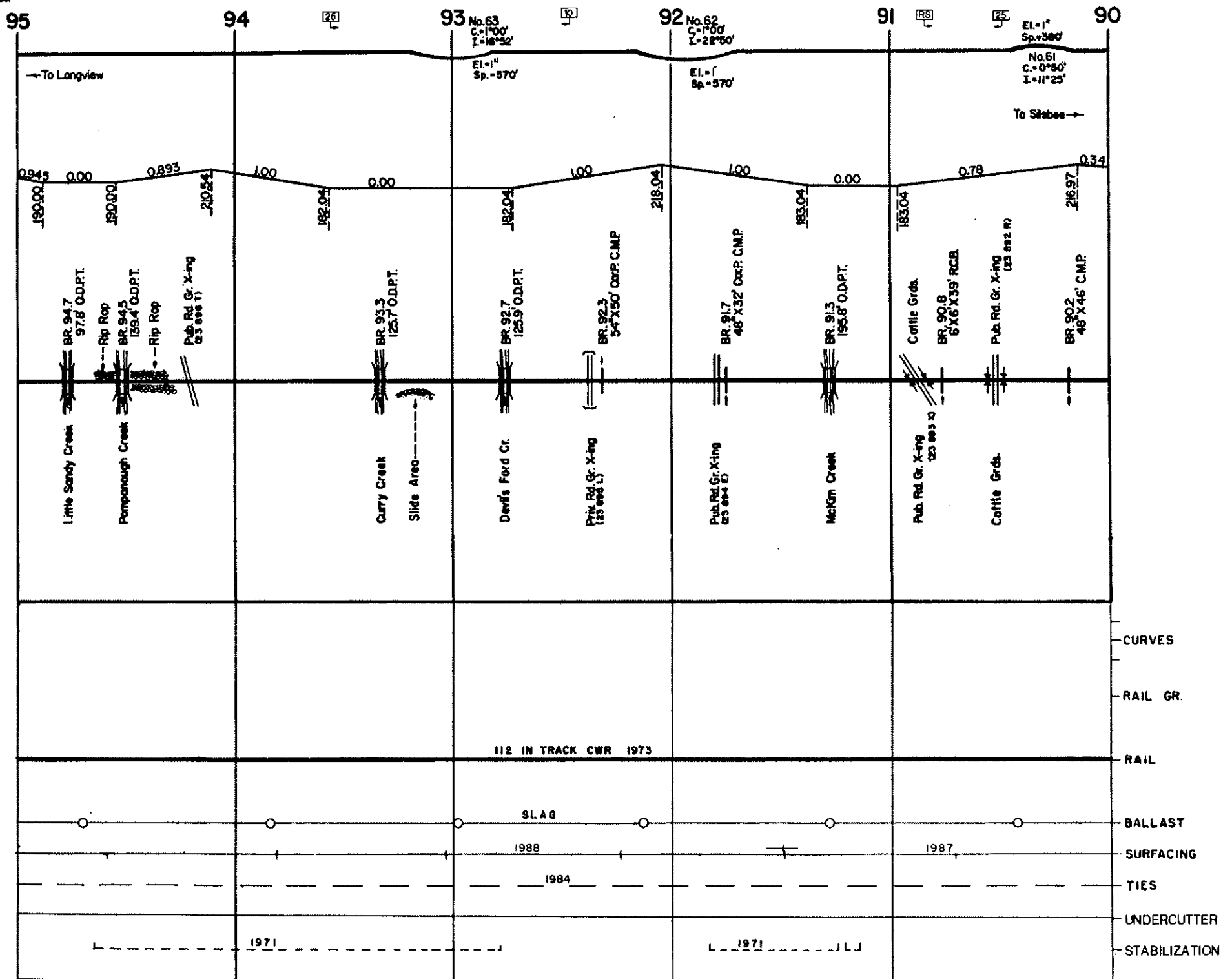


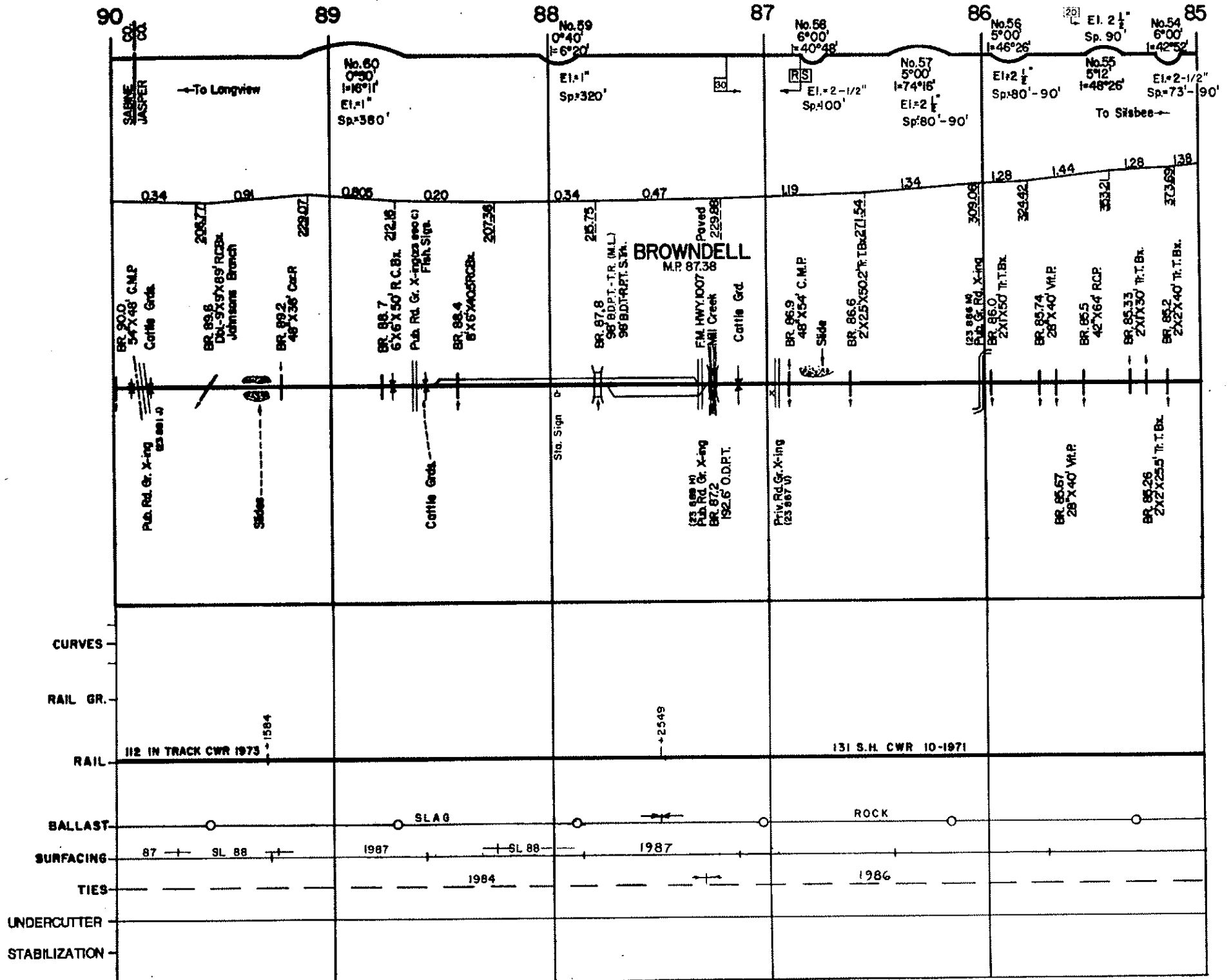


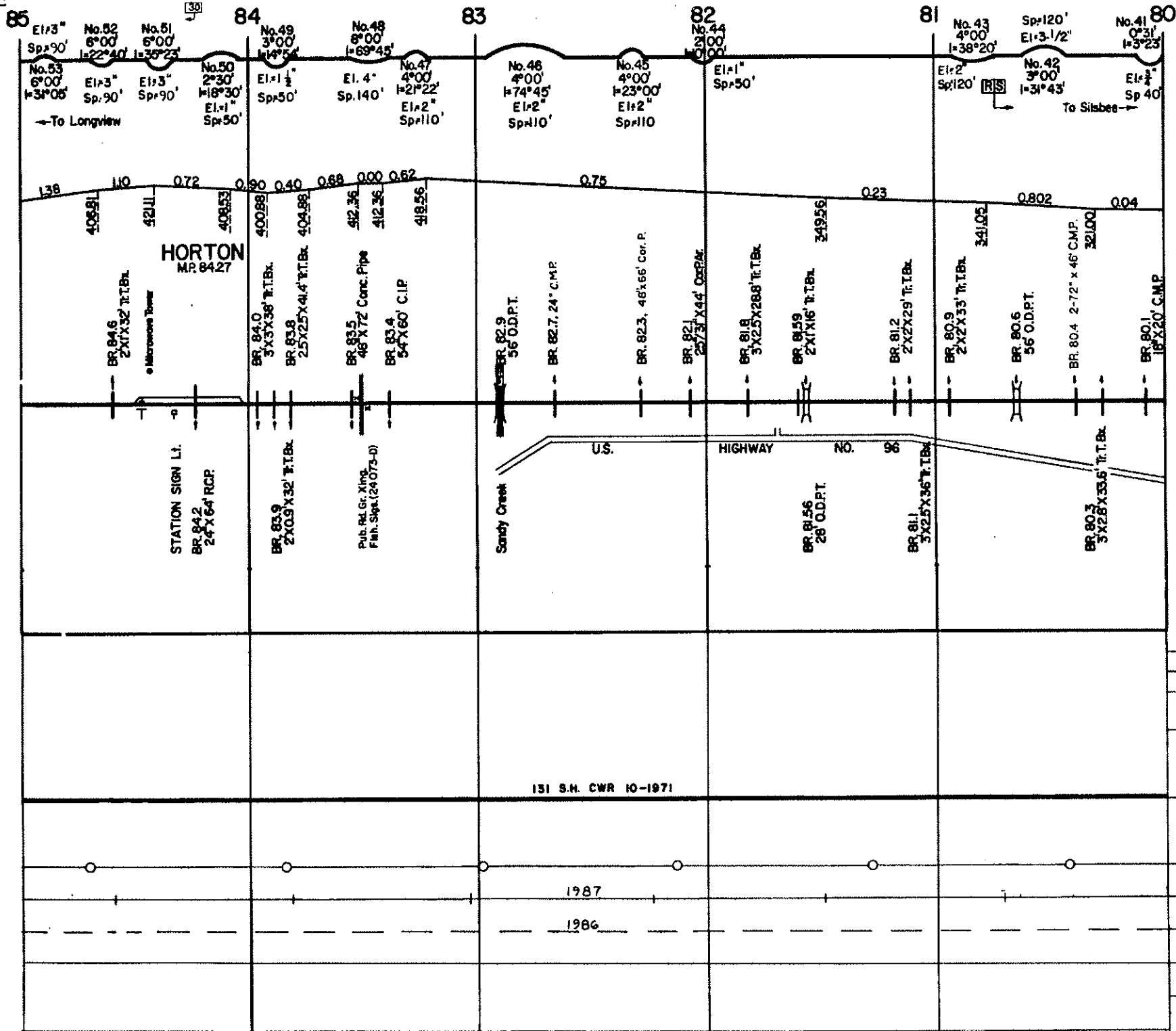
CURVES  
 RAIL GR.  
 RAIL  
 BALLAST  
 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION

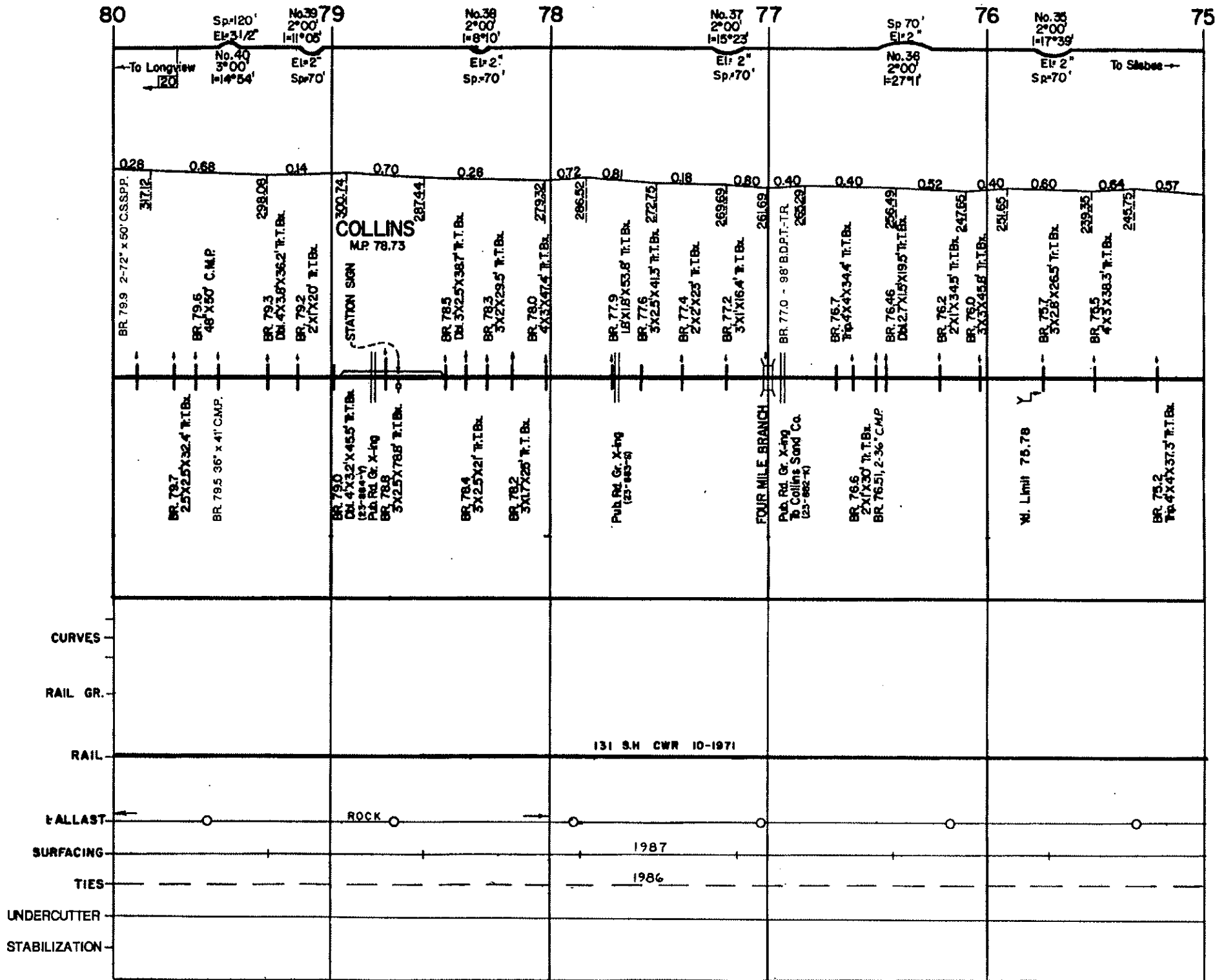


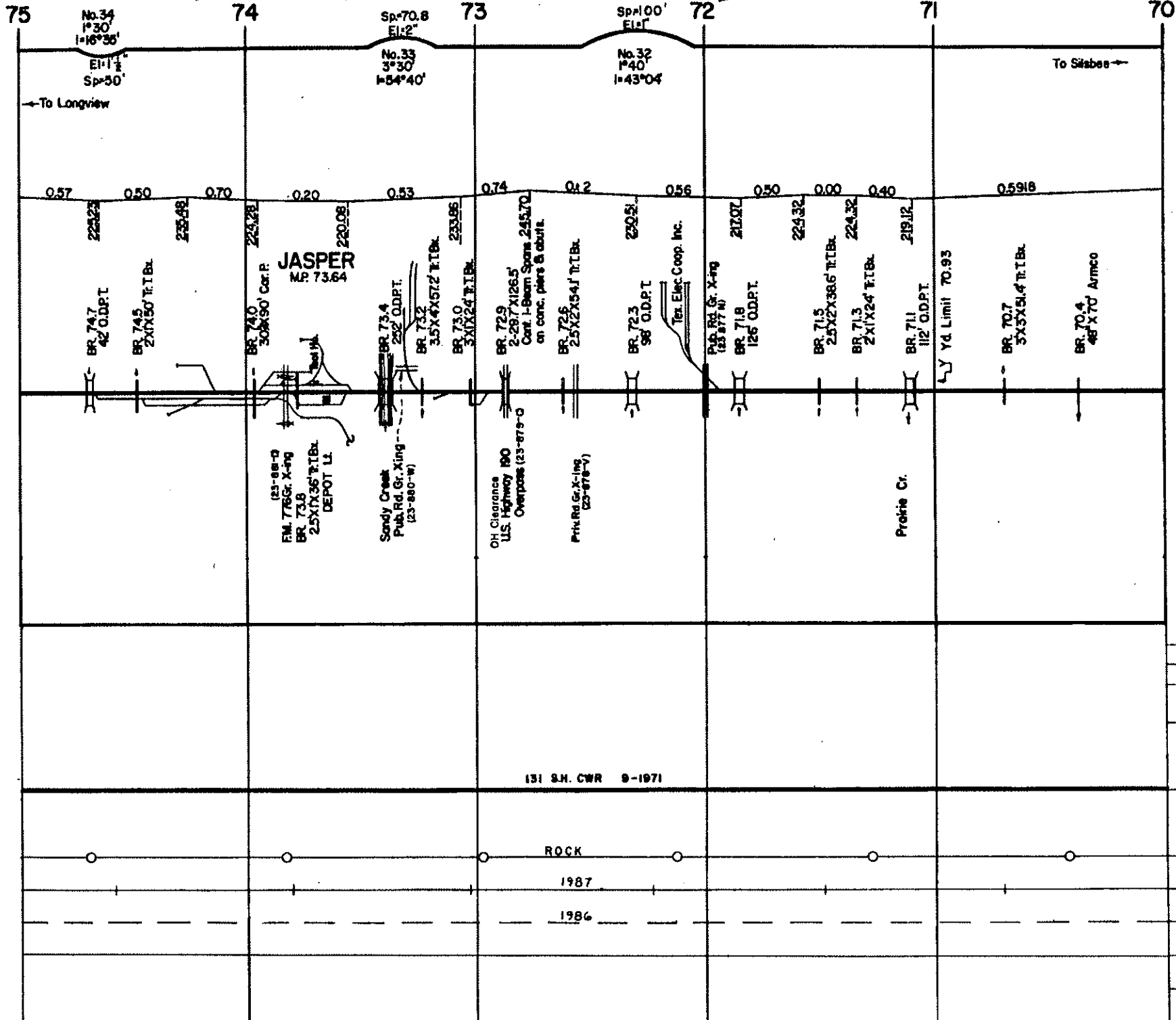


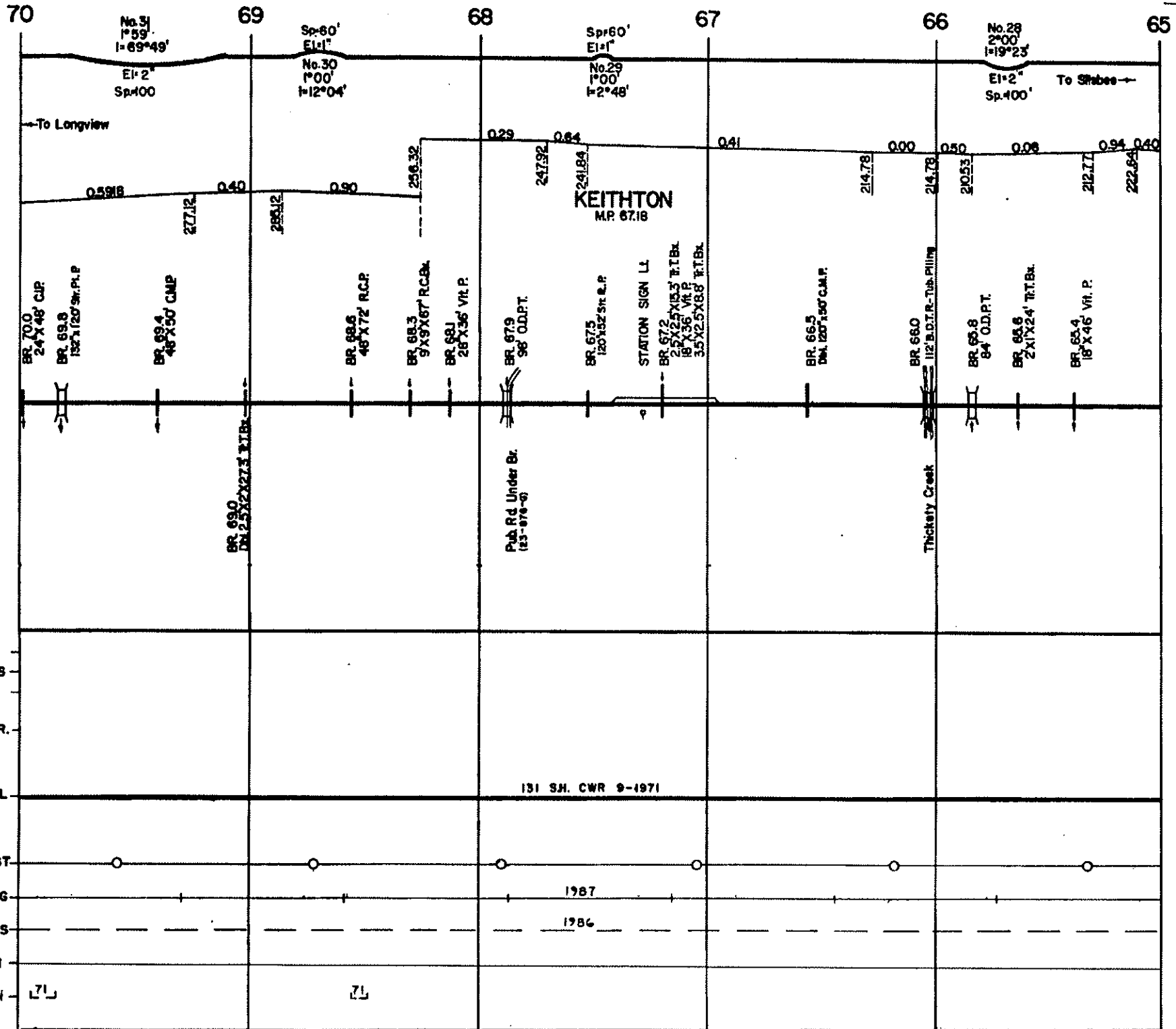




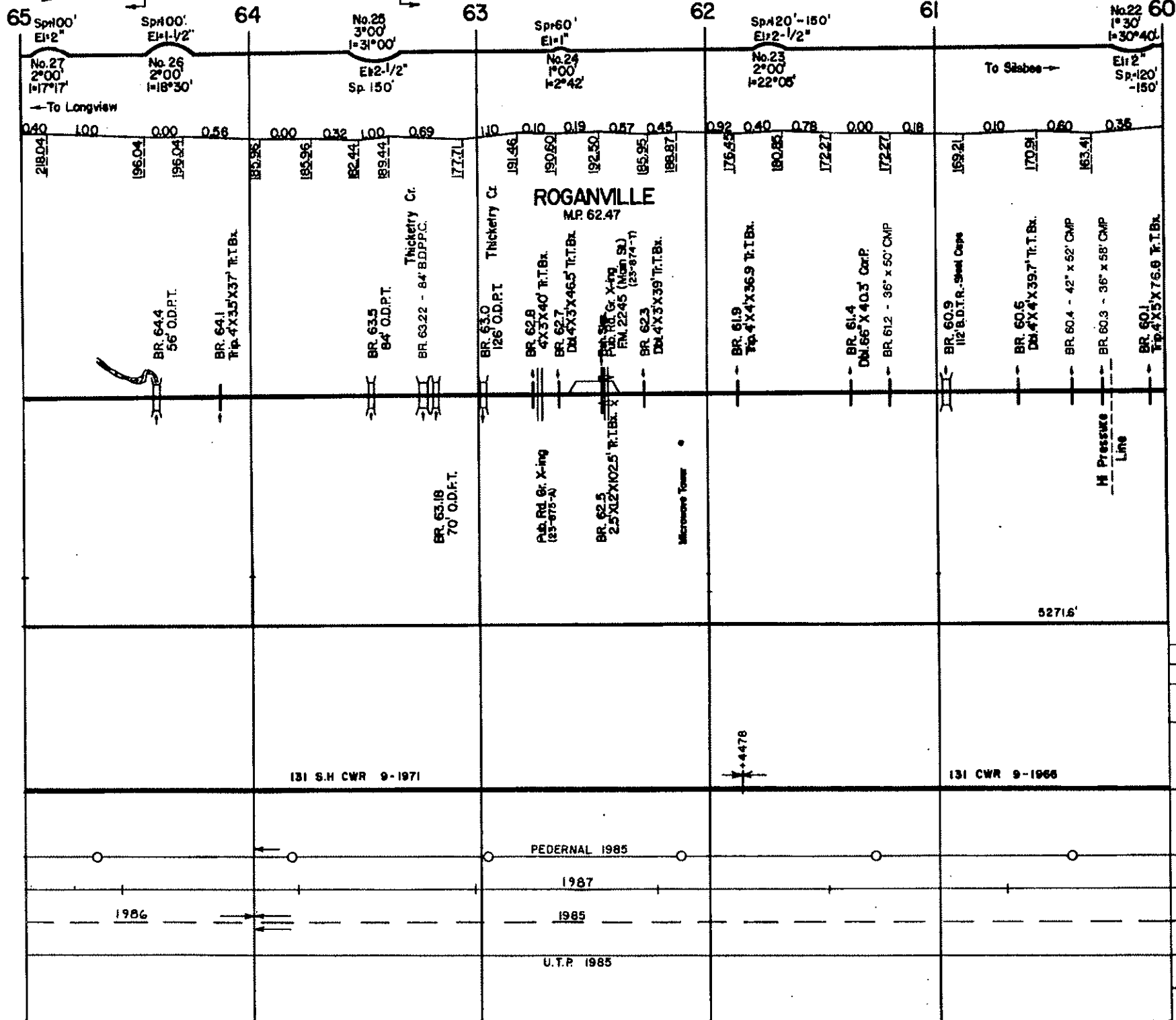








347



REVISED 3-1991

- CURVES
- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION

131 S.H CWR 9-1971

131 CWR 9-1966

PEDERNAL 1985

1987

1985

U.T.P. 1985

+4478

5271.6'

### ROGANVILLE

M.P. 62.47

Thicketry Cr.  
84' B.D.P.P.C.

Thicketry Cr.  
126' O.D.P.T.

BR. 62.8  
4'X3'X40' T.T.Bx.

BR. 62.7  
DM. 4'X3'X46.5' T.T.Bx.

Pub. Rd. Gr. X-ing  
RM. 22.45 (Main SJ)  
(23'-87.4'-1)

BR. 62.3  
DM. 4'X3'X39' T.T.Bx.

BR. 61.9  
Top. 4'X4'X36.9' T.T.Bx.

BR. 61.4  
DM. 6.6' X 40.3' Carp.

BR. 61.2 - 36' x 50' CMP

BR. 60.9  
112' B.D.T.R. - Sheet Cape

BR. 60.5  
DM. 4'X4'X39.7' T.T.Bx.

BR. 60.4 - 42' x 52' CMP

BR. 60.3 - 36' x 58' CMP

BR. 60.1  
Top. 4'X5'X76.9' T.T.Bx.

H Pressure  
Line

65 Spr 100' El: 2" No. 27 2°00' I=17°17' - To Longview  
 Spr 100' El: 1-1/2" No. 26 2°00' I=18°30'  
 No. 25 3°00' I=31°00' El: 2-1/2" Sp. 150'  
 63 Spr 60' El: 1" No. 24 1°00' I=2°42'  
 62 Spr 20' - 150' El: 2-1/2" No. 23 2°00' I=22°05'  
 61 To Slabes  
 No. 22 1°30' I=30°40' El: 2" Sp. 120' - 150'

0.40 1.00 0.00 0.56 0.00 0.32 1.00 0.69 1.10 0.10 0.19 0.67 0.45 0.92 0.40 0.78 0.00 0.18 0.10 0.60 0.35

218.04 196.04 196.04 189.92 189.96 182.44 189.44 177.71 191.46 190.60 192.50 185.95 188.87 176.45 180.86 172.27 172.27 169.21 170.91 163.41

BR. 64.4  
56' O.D.P.T.

BR. 64.1  
Top. 4'X3.5'X37' T.T.Bx.

BR. 63.5  
84' O.D.P.T.

BR. 63.22 - 84' B.D.P.P.C.

BR. 63.18  
70' O.D.P.T.

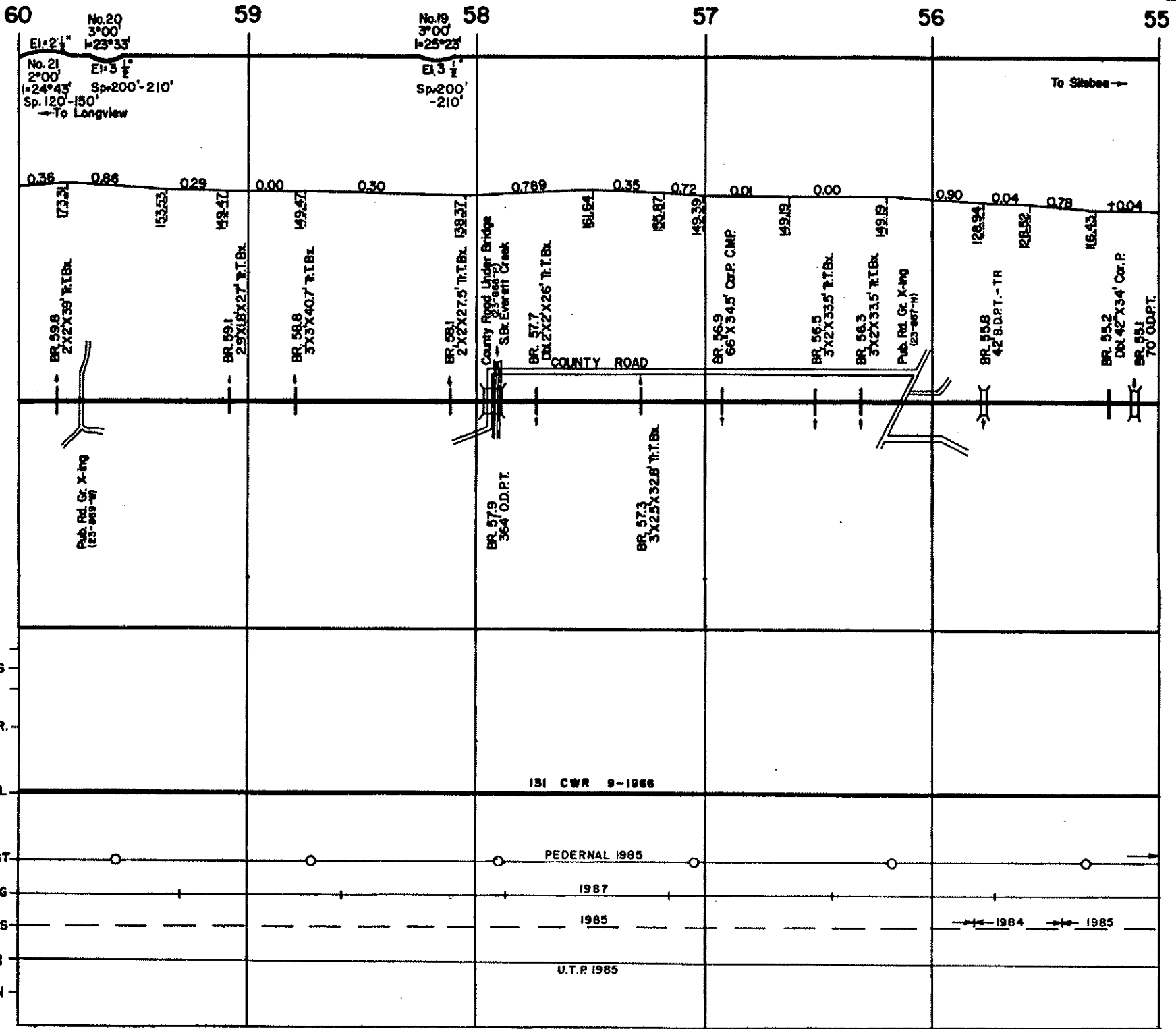
Pub. Rd. Gr. X-ing  
(23'-87.4'-1)

BR. 62.5  
2.5'X12'X102.5' T.T.Bx.

Microwave Tower







60

59

58

57

56

55

No. 20  
3°00'  
I=23°33'

No. 21  
2°00'  
I=24°43'  
Sp. 200'-210'  
Sp. 120'-150'  
→ To Longview

No. 19  
3°00'  
I=25°23'

El. 3 1/2  
Sp. 200'  
-210'

0.36 0.86 0.29 0.00 0.30 0.769 0.55 0.72 0.01 0.00 0.90 0.04 0.78 1.004

173.21 153.53 149.47 149.57 139.37 161.64 155.87 149.39 149.19 149.19 128.94 128.92 16.43 10.04

BR. 59.8 2'X2'X36' T.T.Bx  
BR. 59.1 2.5'X1.8'X27' T.T.Bx  
BR. 59.8 3'X3'X40.7' T.T.Bx  
BR. 59.1 2'X2'X27.5' T.T.Bx 139.37  
County Road Under Bridge  
→ S.Br. Everett Creek  
BR. 57.7 2'X2'X26' T.T.Bx  
BR. 56.9 6.6' X 3.4.5' ODP. C.M.P.  
BR. 56.5 3'X2'X33.5' T.T.Bx  
BR. 56.3 3'X2'X33.5' T.T.Bx  
Pub. Rd. Gr. X-179 (23-867-11)  
BR. 55.8 42' S.D.P.T.-TR  
BR. 55.2 2'X42'X34' Cor.P.  
BR. 55.1 TO O.D.P.T.

COUNTY ROAD

CURVES  
RAIL GR.  
RAIL  
BALLAST  
SURFACING  
TIES  
UNDERCUTTER  
STABILIZATION

131 CWR 9-1966

PEDERNAL 1985

1987

1985

1984 1985

U.T.P 1985

55

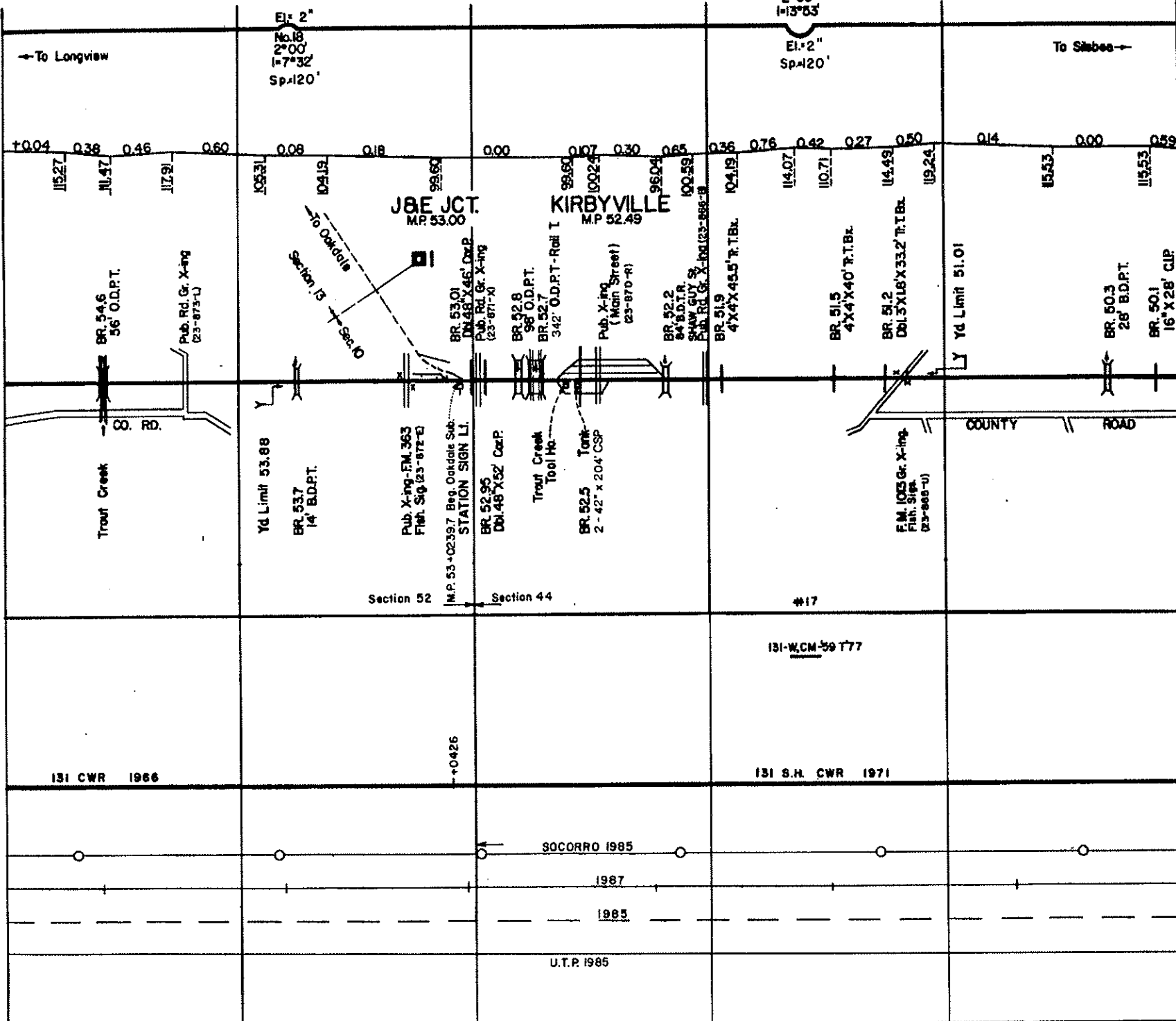
54

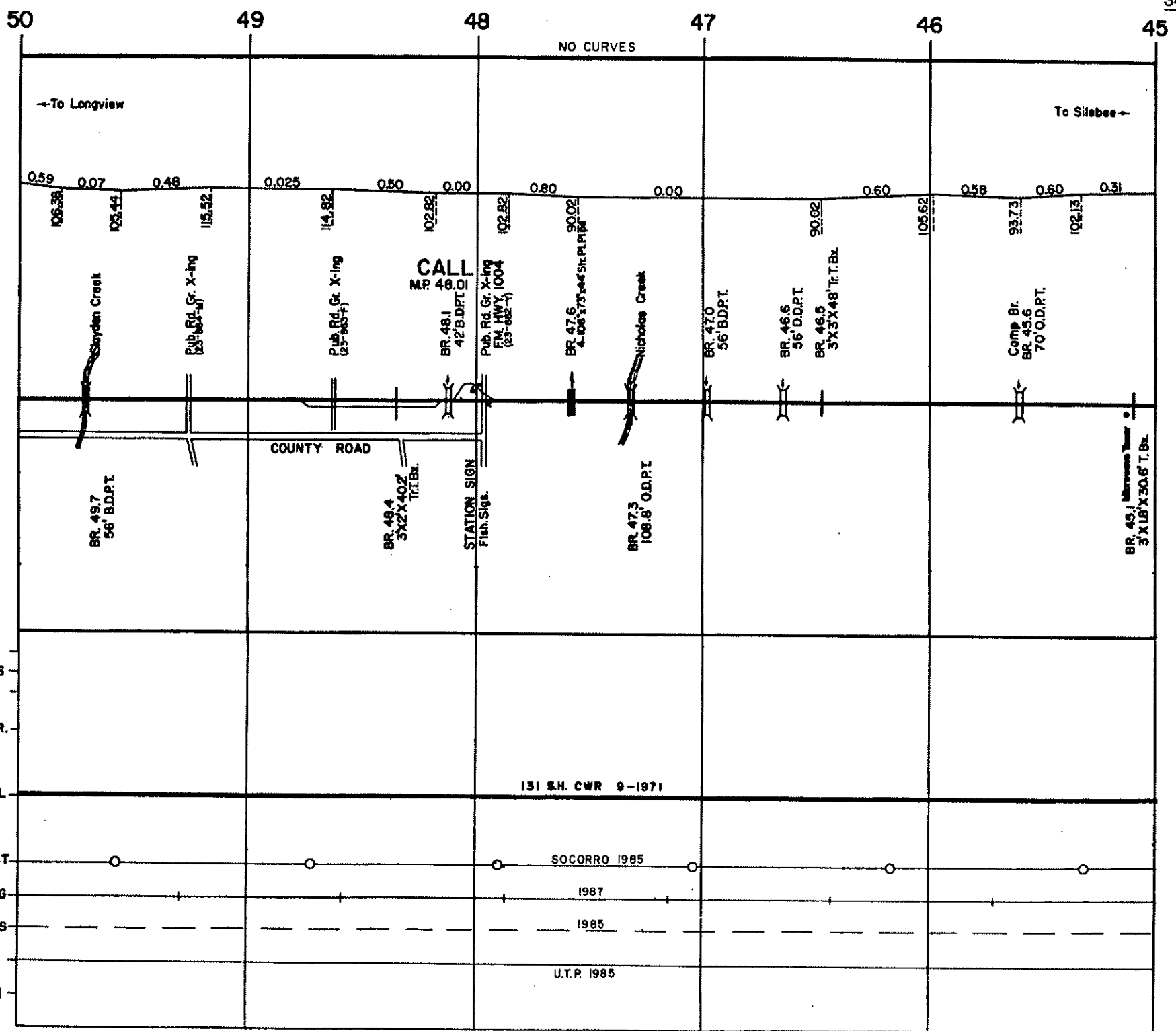
53

52

51

50





45

44

43

42

41

40

NO CURVES

← To Longview

To Siles →

0.31 0.02 0.58 0.04 0.20 0.47 0.00 0.192 0.24 0.00 0.60 0.57 0.05

109.88 110.24 98.06 99.30 107.40 98.00 98.00 101.66 96.86 96.86 85.16 78.60

**LE VERTE**  
M.P. 43.23

Pub. Rd. Gr. X-ing  
(23-860-K)

Br. 44.0 - 70' B.D.T.R

STATION SIGN Rt.  
Pub. Rd. Gr. X-ing  
BR. 43.2 (23-861-3)  
2.5' X 3.4' T.T.Bx

Pr. Gr. X-ing  
(23-863-70)

Canal

BR. 42.5  
29'7 1/2" X 32' CorRAc

BR. 42.2  
28' C.D.P.T.

BR. 42.0  
Dn. 3' X 2' X 30.5' T.T.Bx

BR. 41.8  
Trn. 3' X 2' X 29' T.T.Bx

Pub. Rd. Gr. X-ing  
(23-858-4)

BR. 41.1  
Dn. 3' X 2' X 29.3' T.T.Bx

BR. 40.9  
3' X 2' X 30' T.T.Bx

BR. 40.6  
2.8' X 1.5' X 40' T.T.Bx

Pub. Rd. Gr. X-ing  
(23-857-C)

BR. 40.1  
Dn. 4' X 3.5' X 40' T.T.Bx

CURVES

RAIL GR.

RAIL

BALLAST

SURFACING

TIES

UNDERCUTTER

STABILIZATION

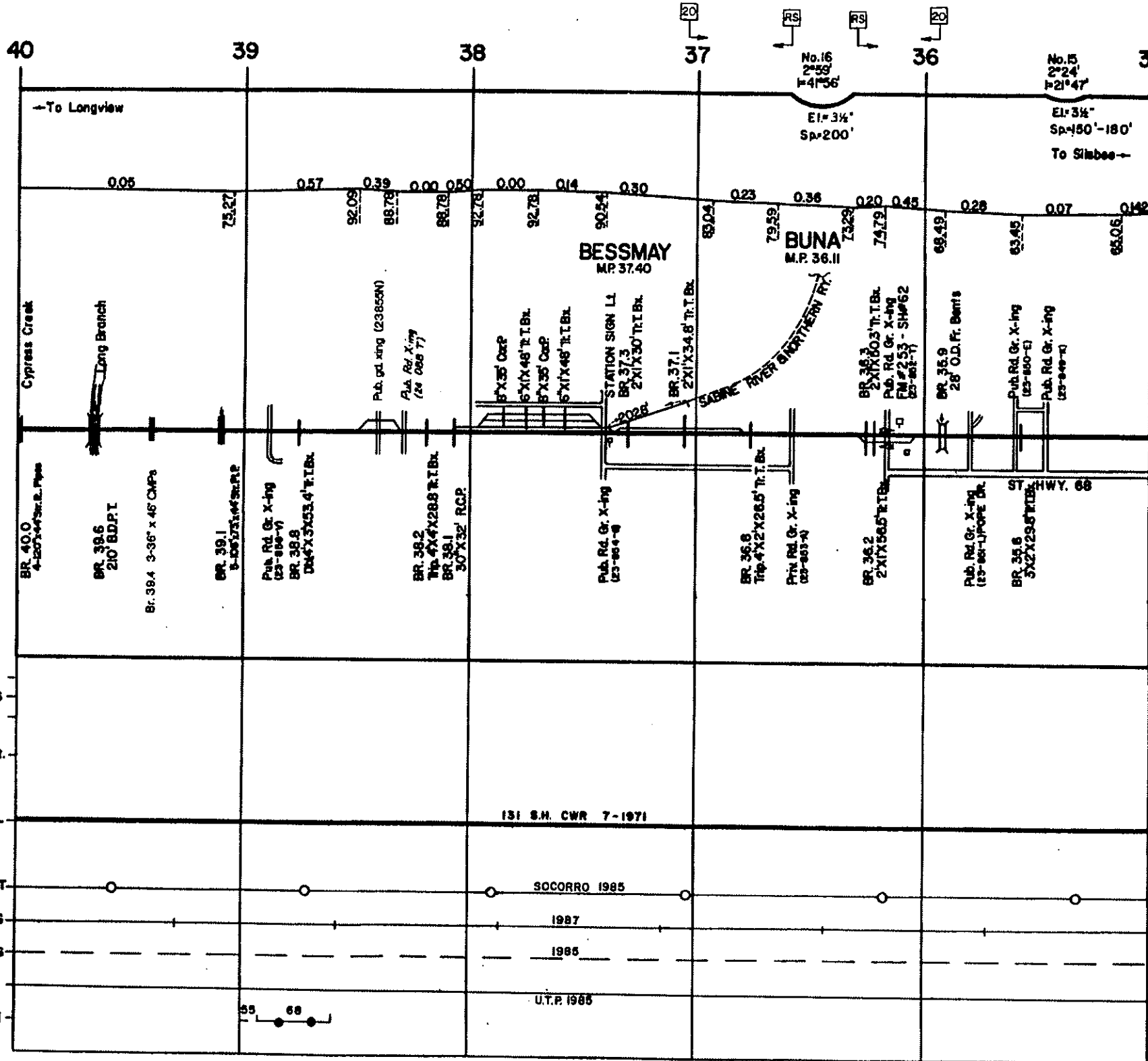
131 S.H. CWR 7-8-1971

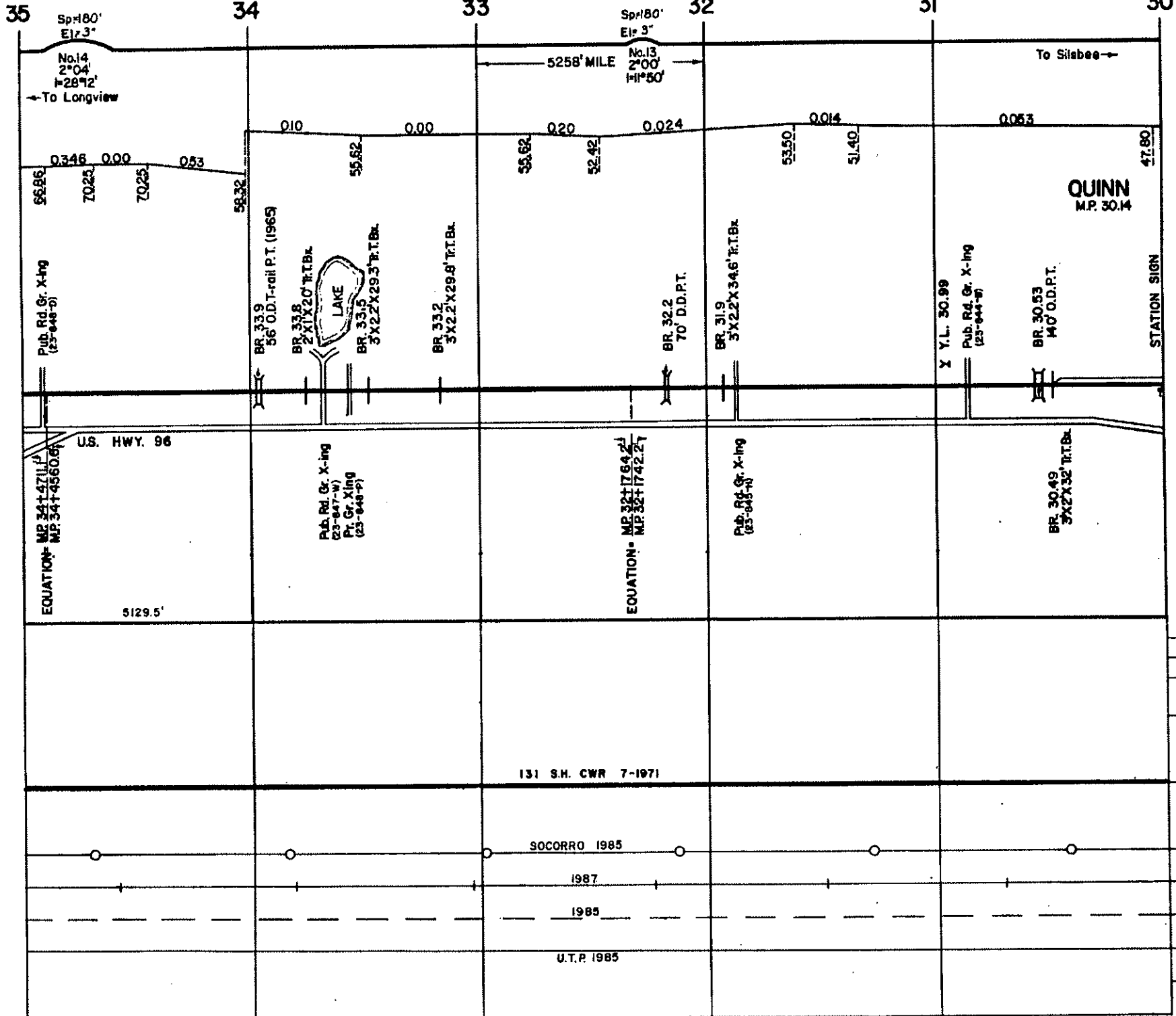
SOCORRO 1985

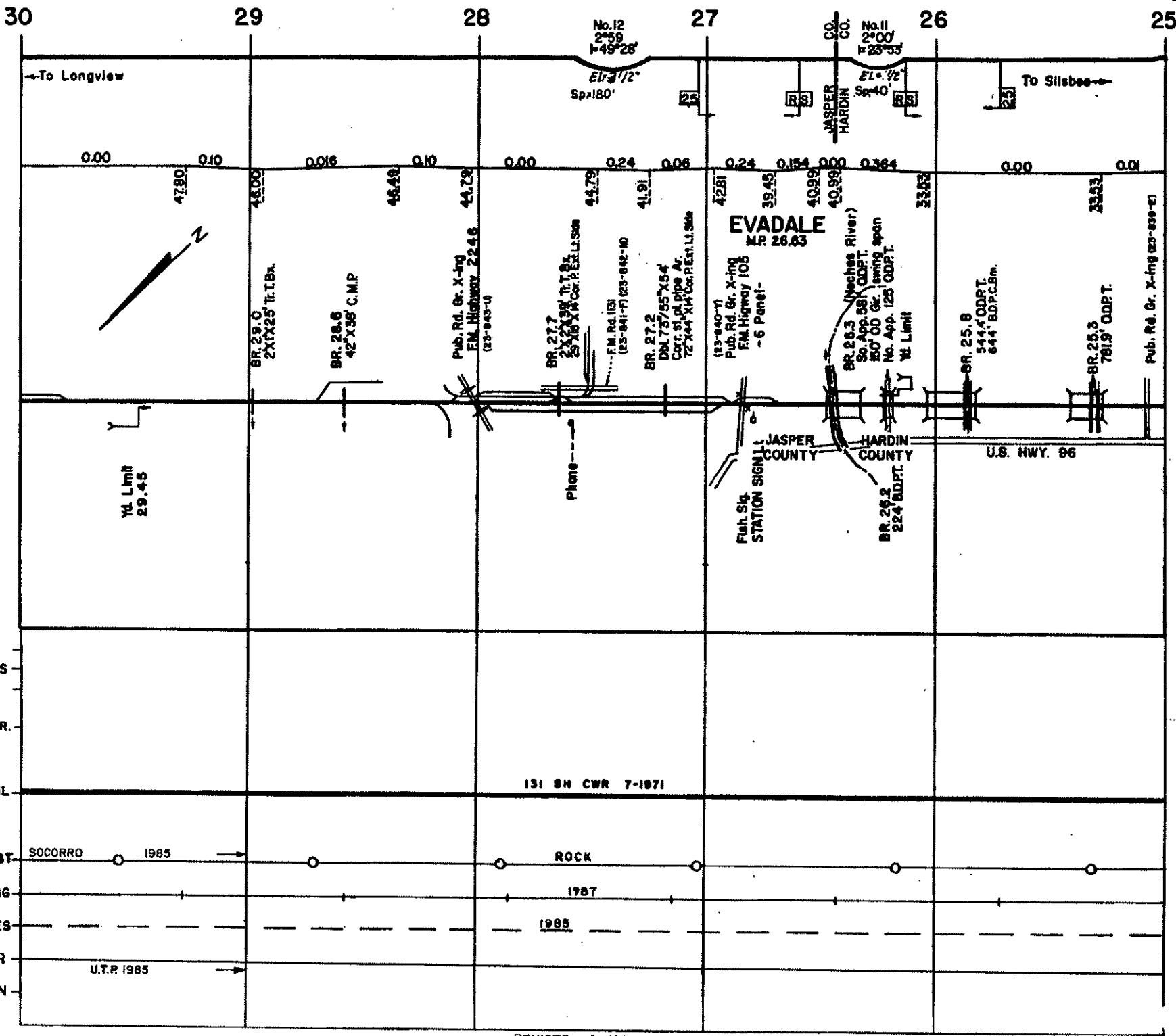
1987

1985

U.T.P 1985

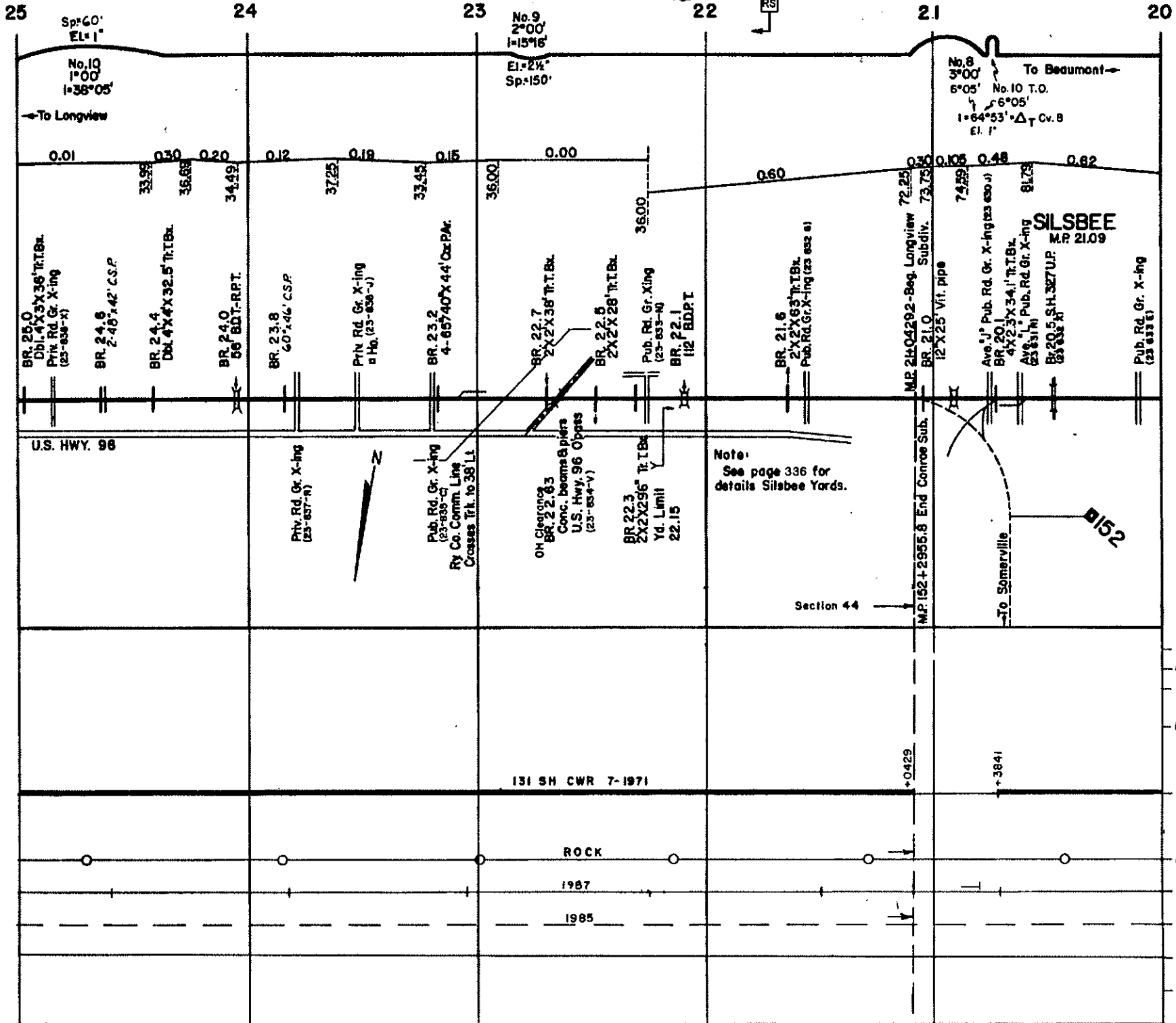






CURVES  
 RAIL GR.  
 RAIL  
 BALLAST  
 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION

SOCORRO	1985				
			ROCK		
				1987	
					1985
U.T.P. 1985					

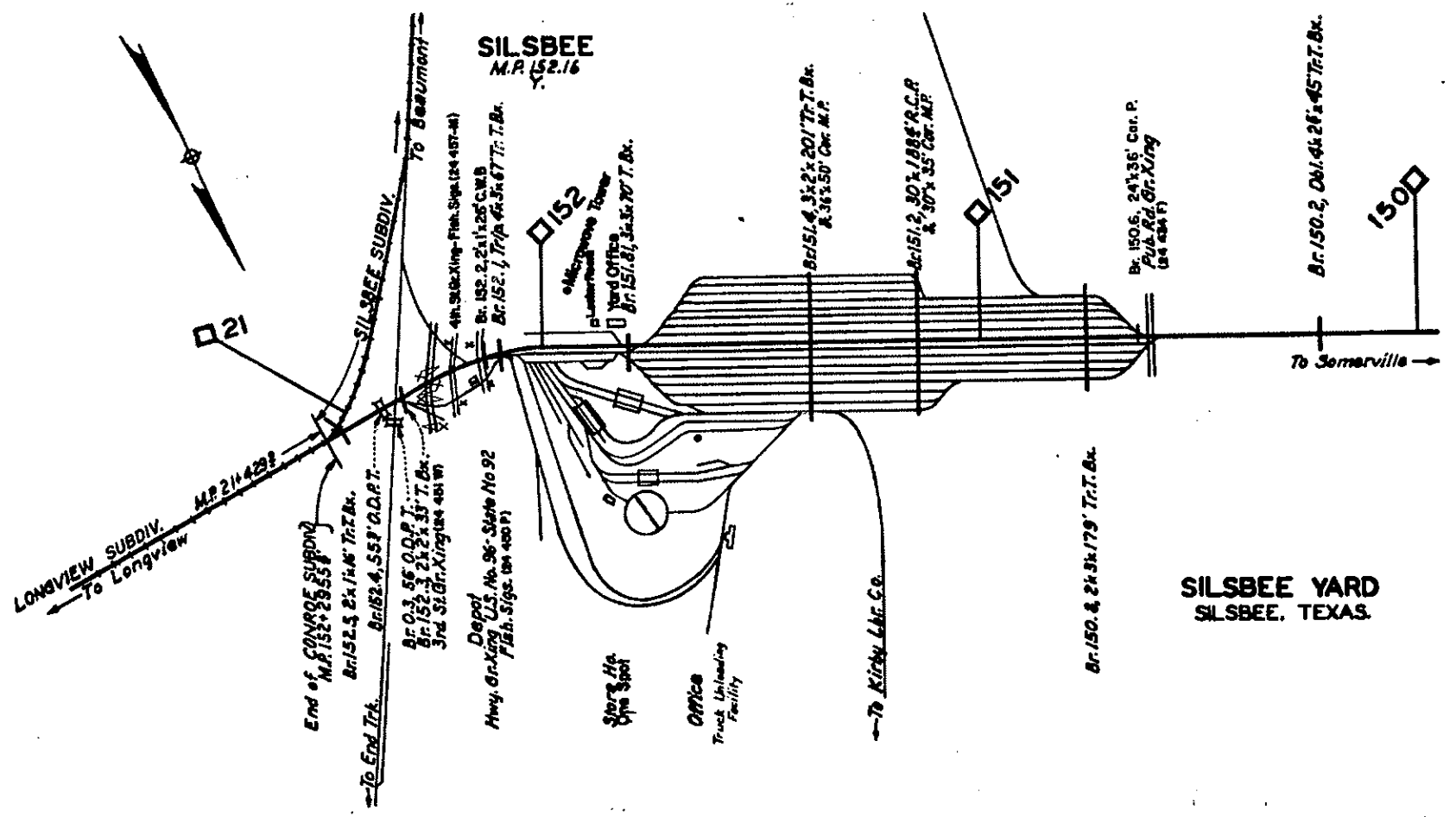




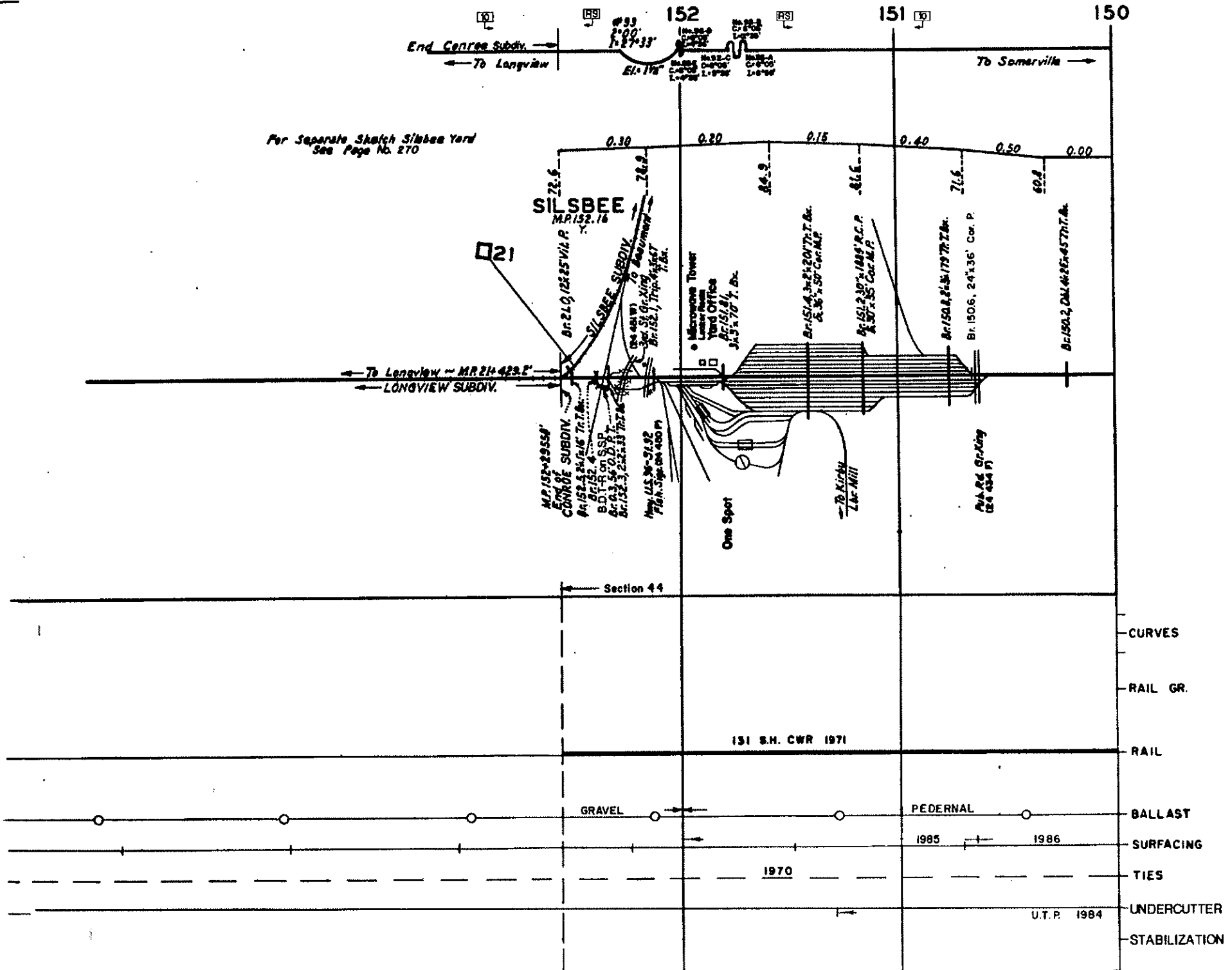
# Longview Subdivision

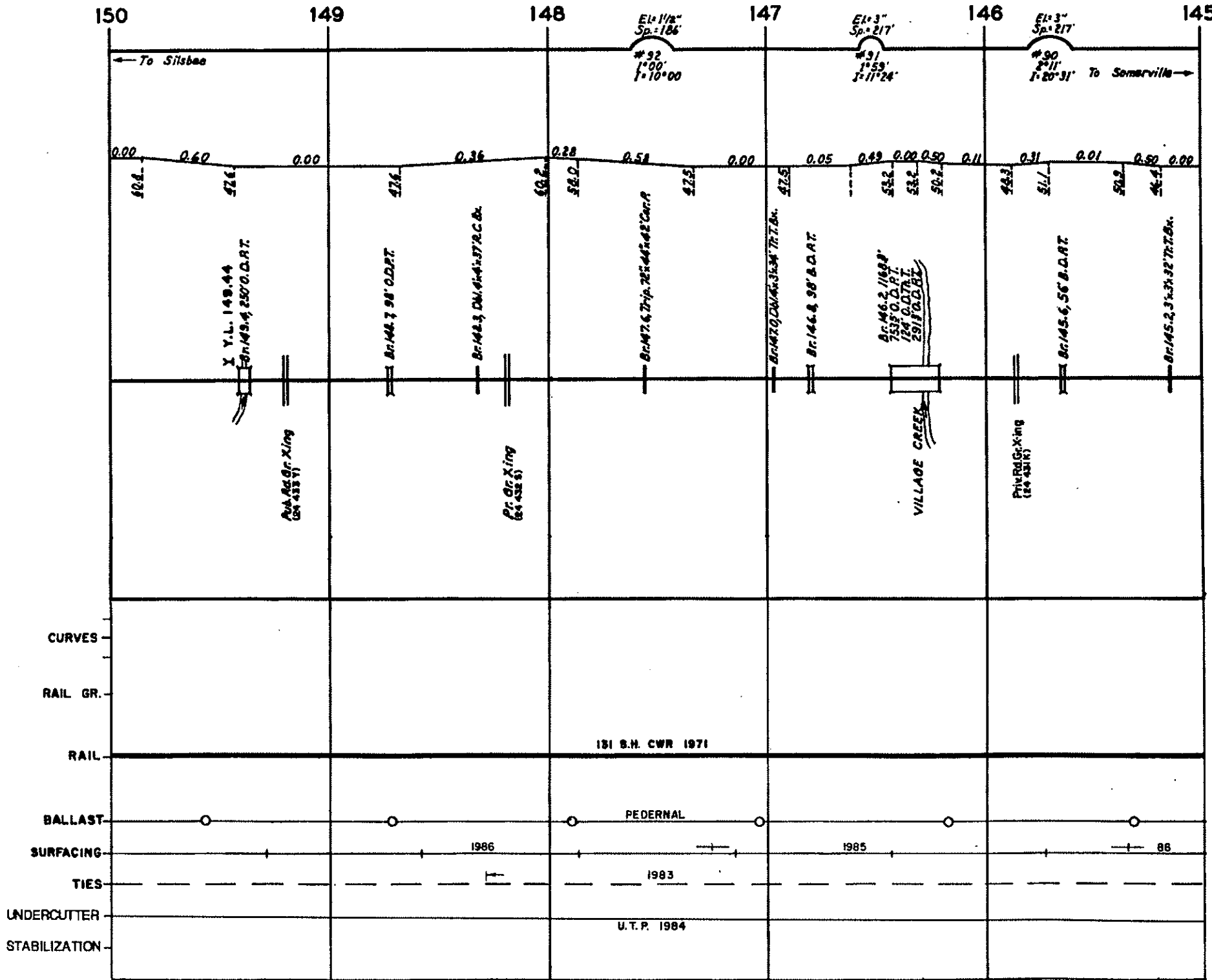
Silsbee (M.P. 21.1) to Longview (M.P. 207.8)

*Intentionally Blank*



**SILSBEE YARD**  
SILSBEE, TEXAS.





145

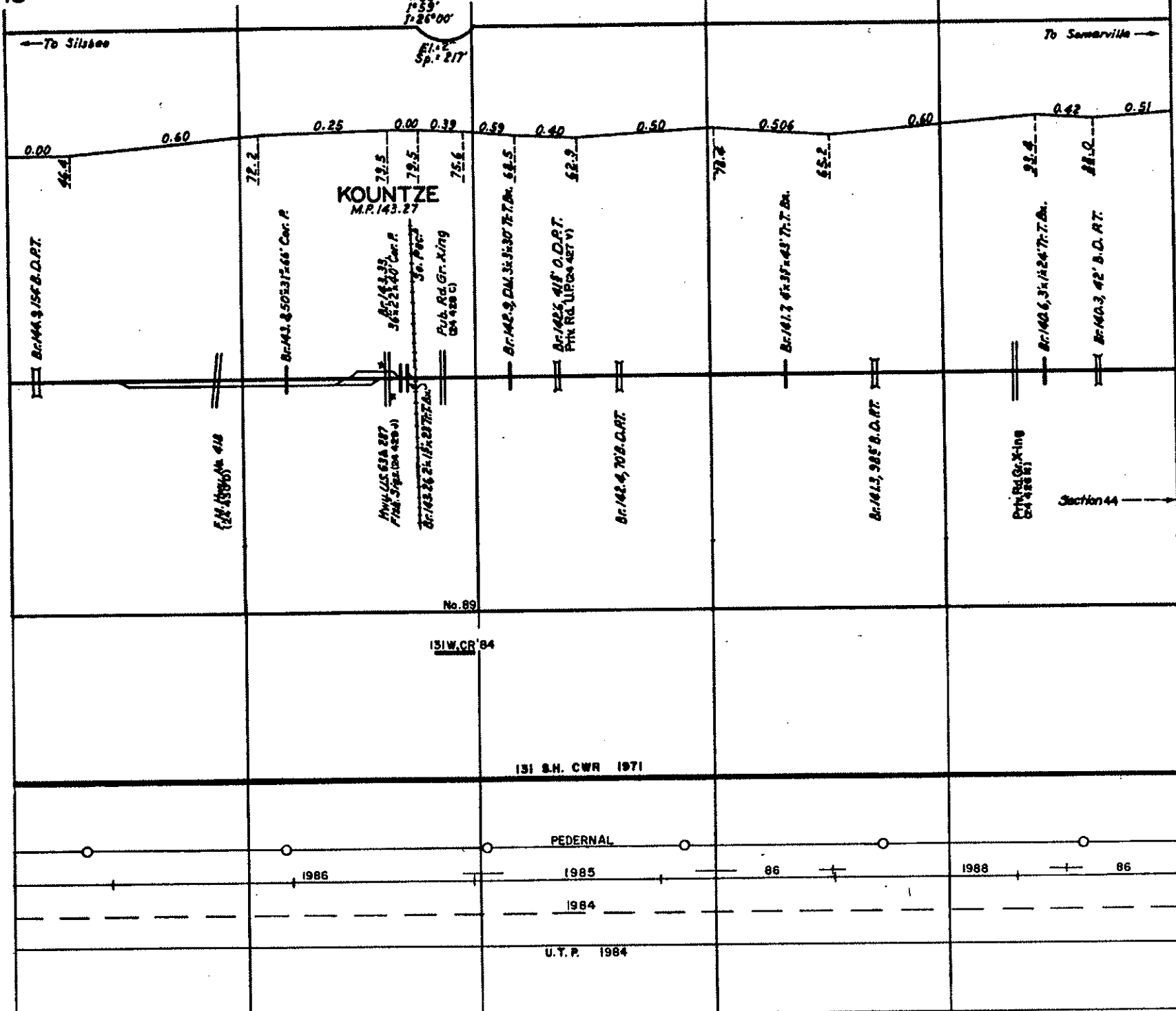
144

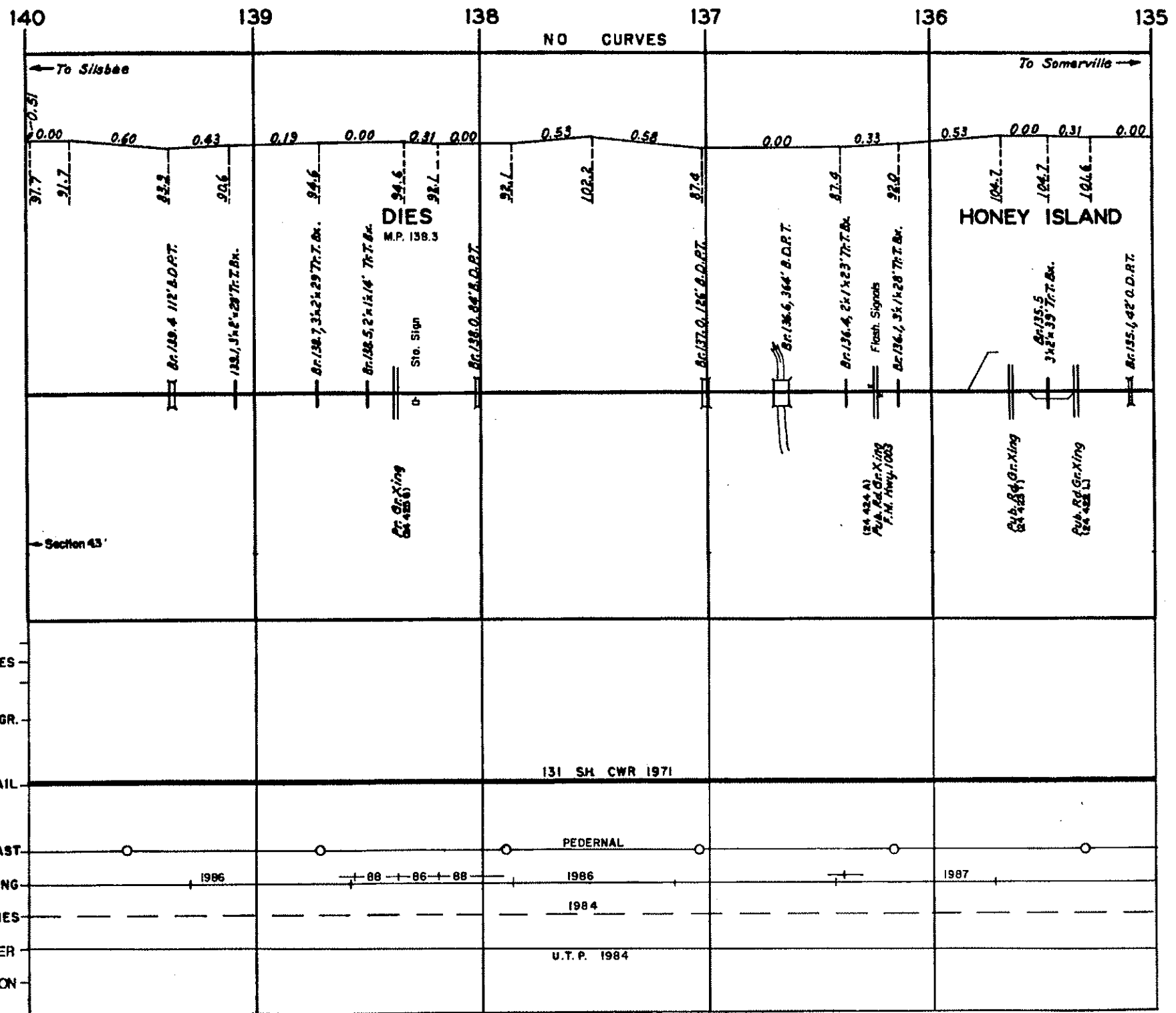
143

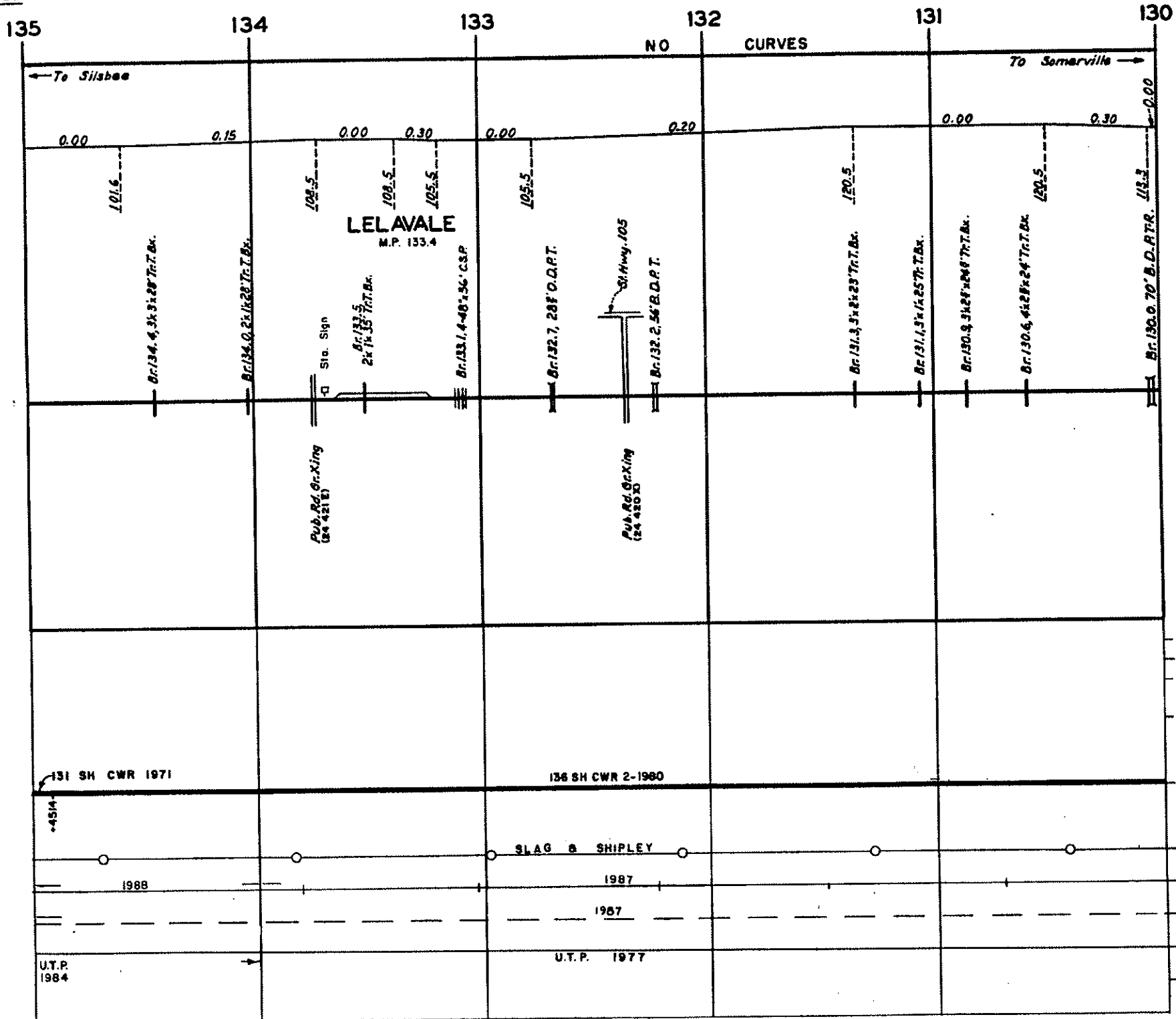
142

141

140

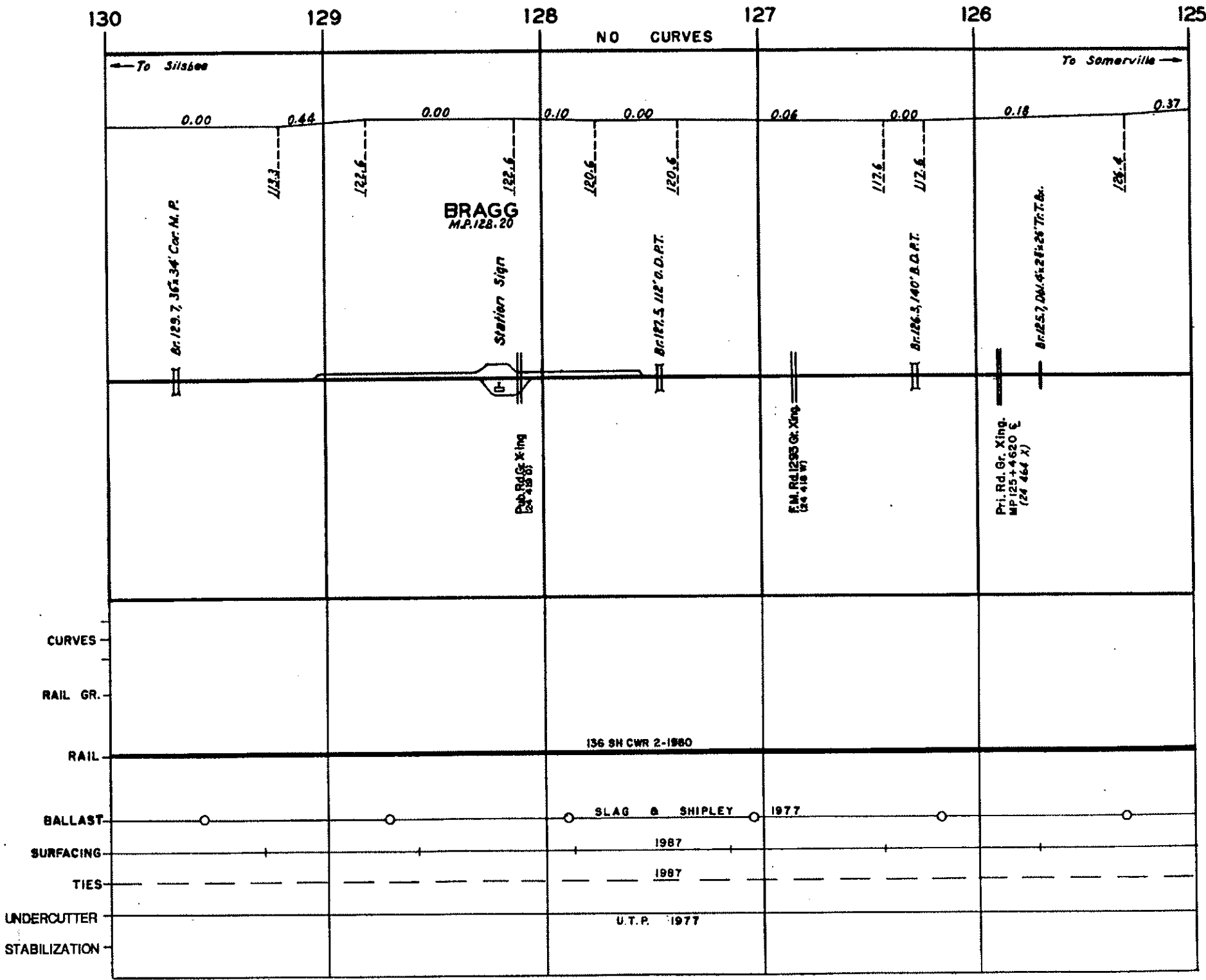


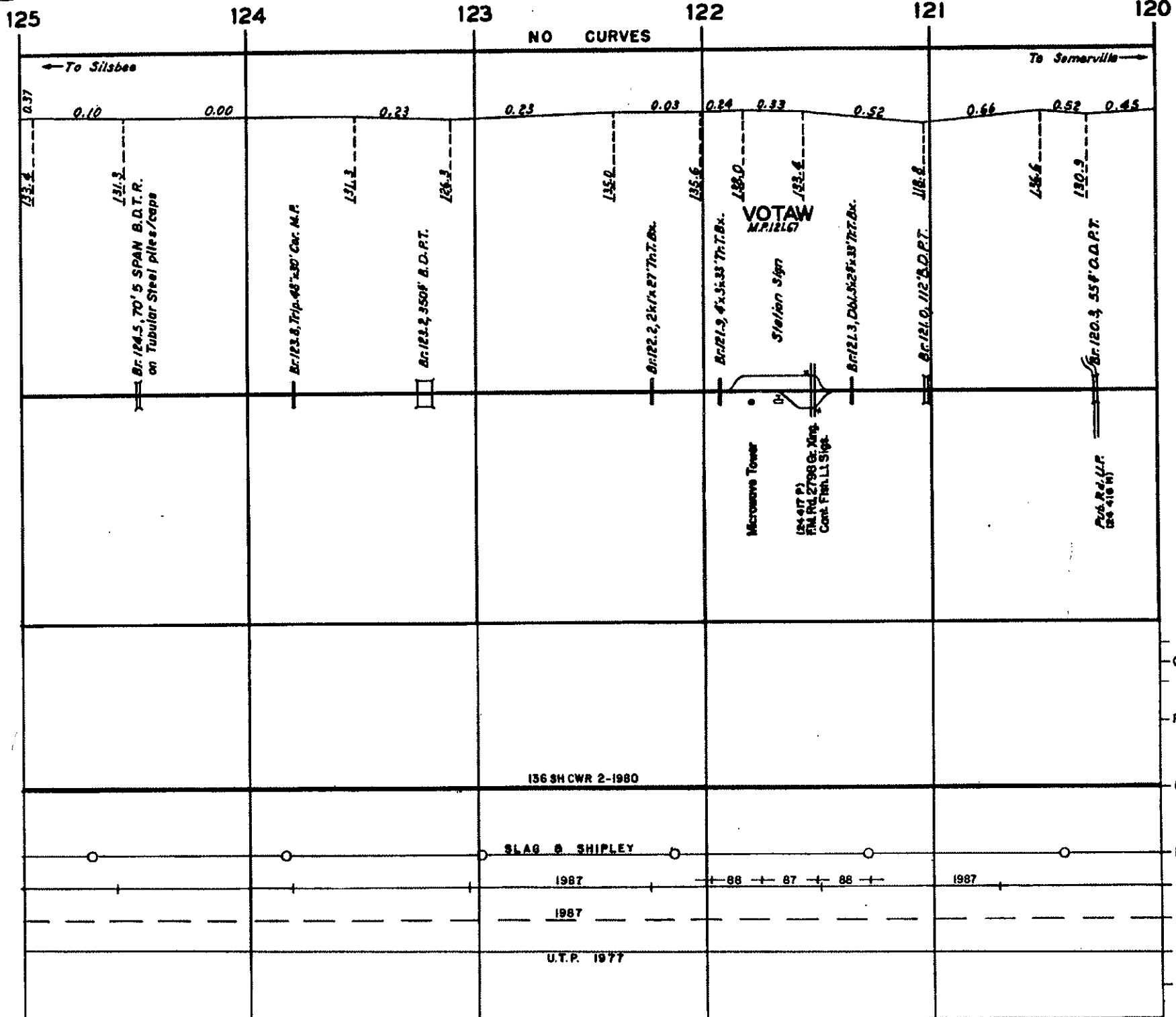


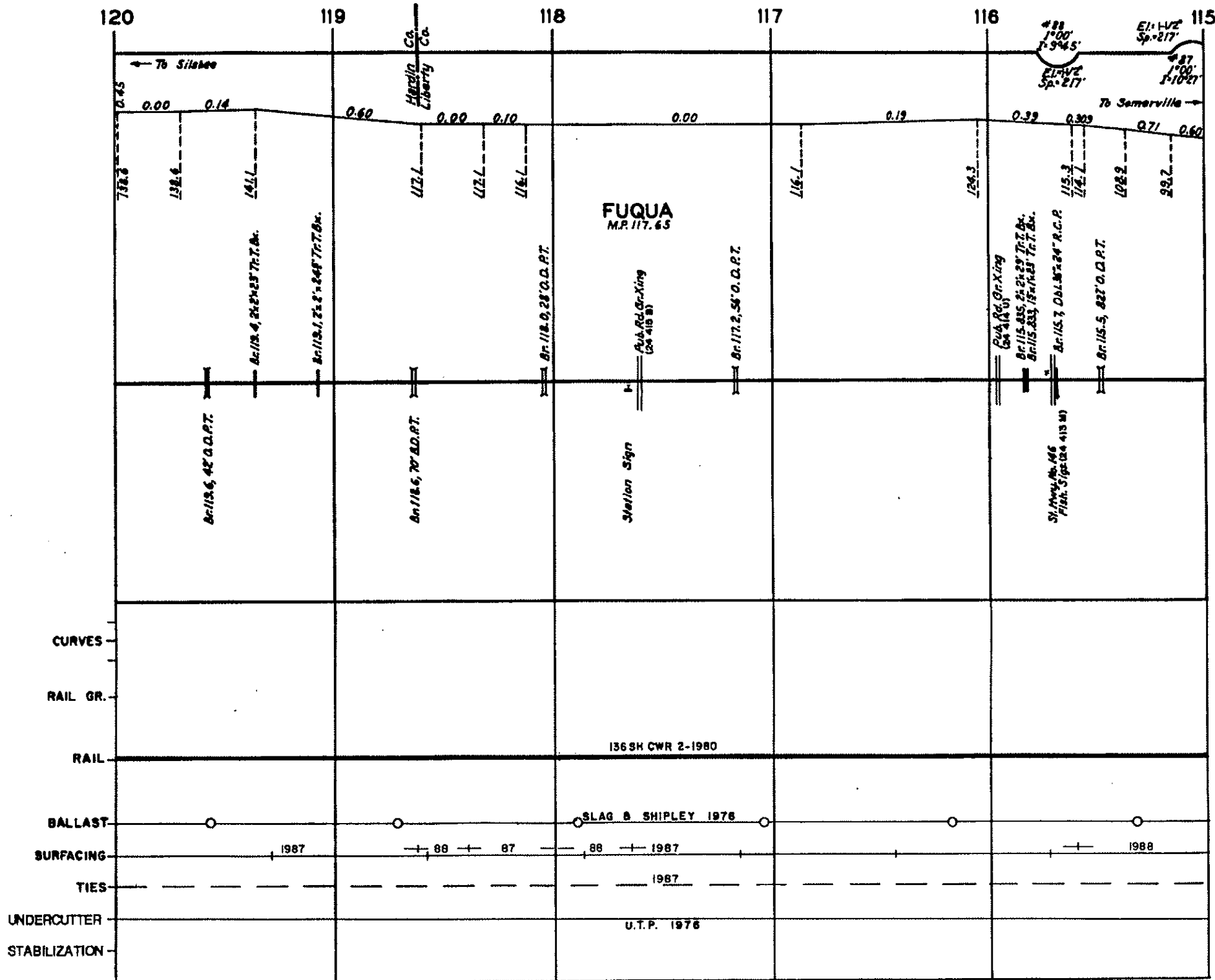


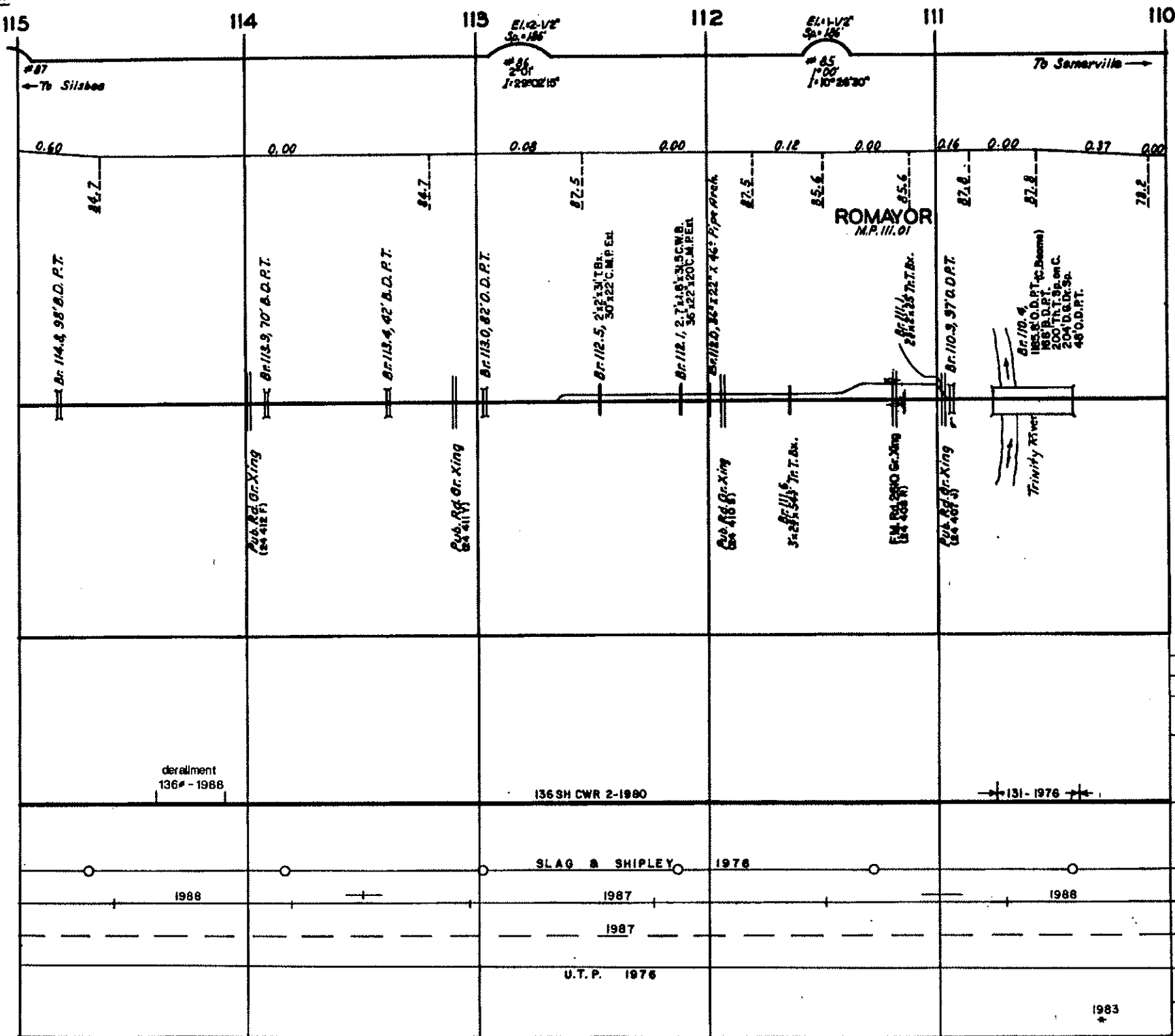
- CURVES
- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION

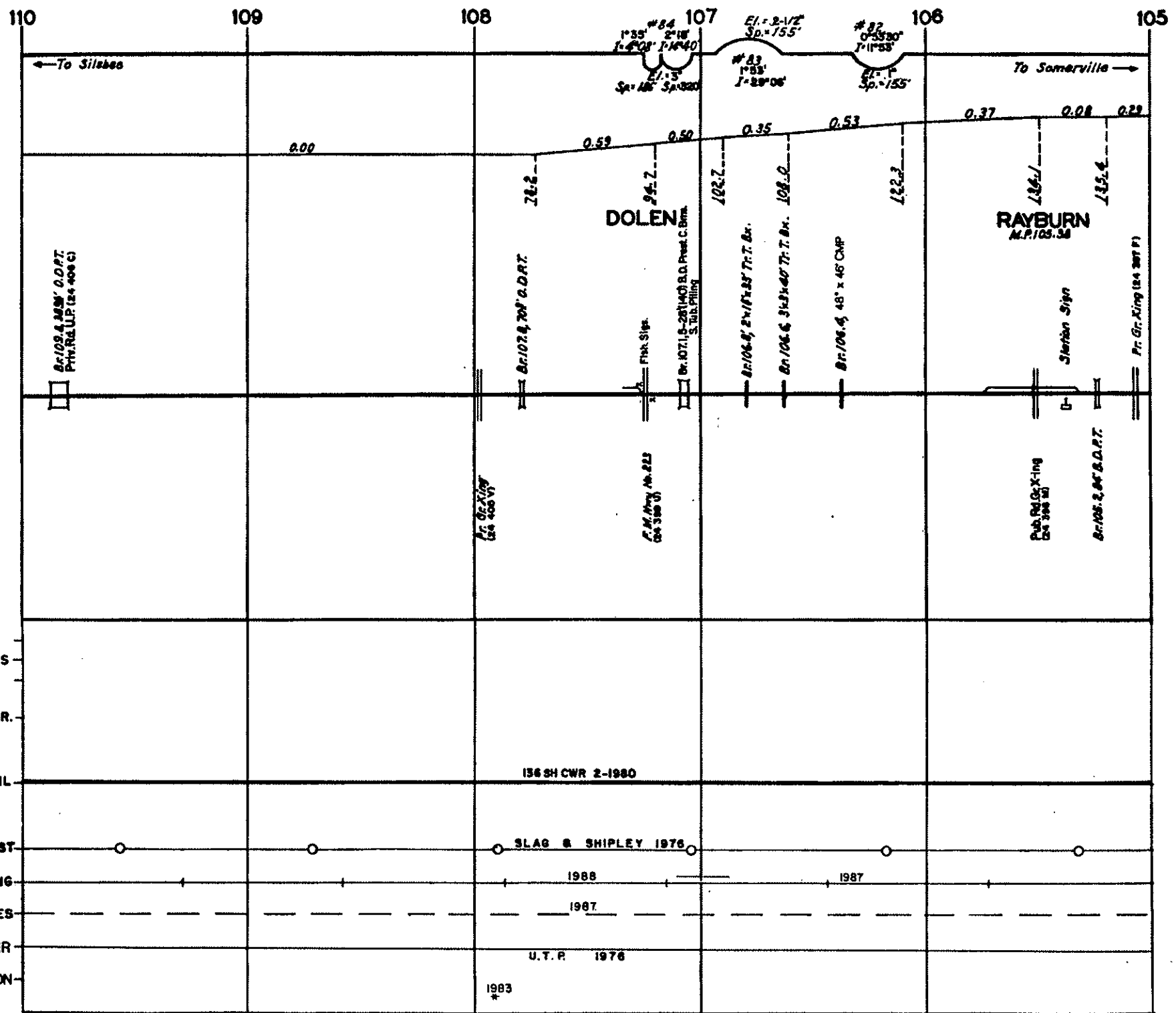


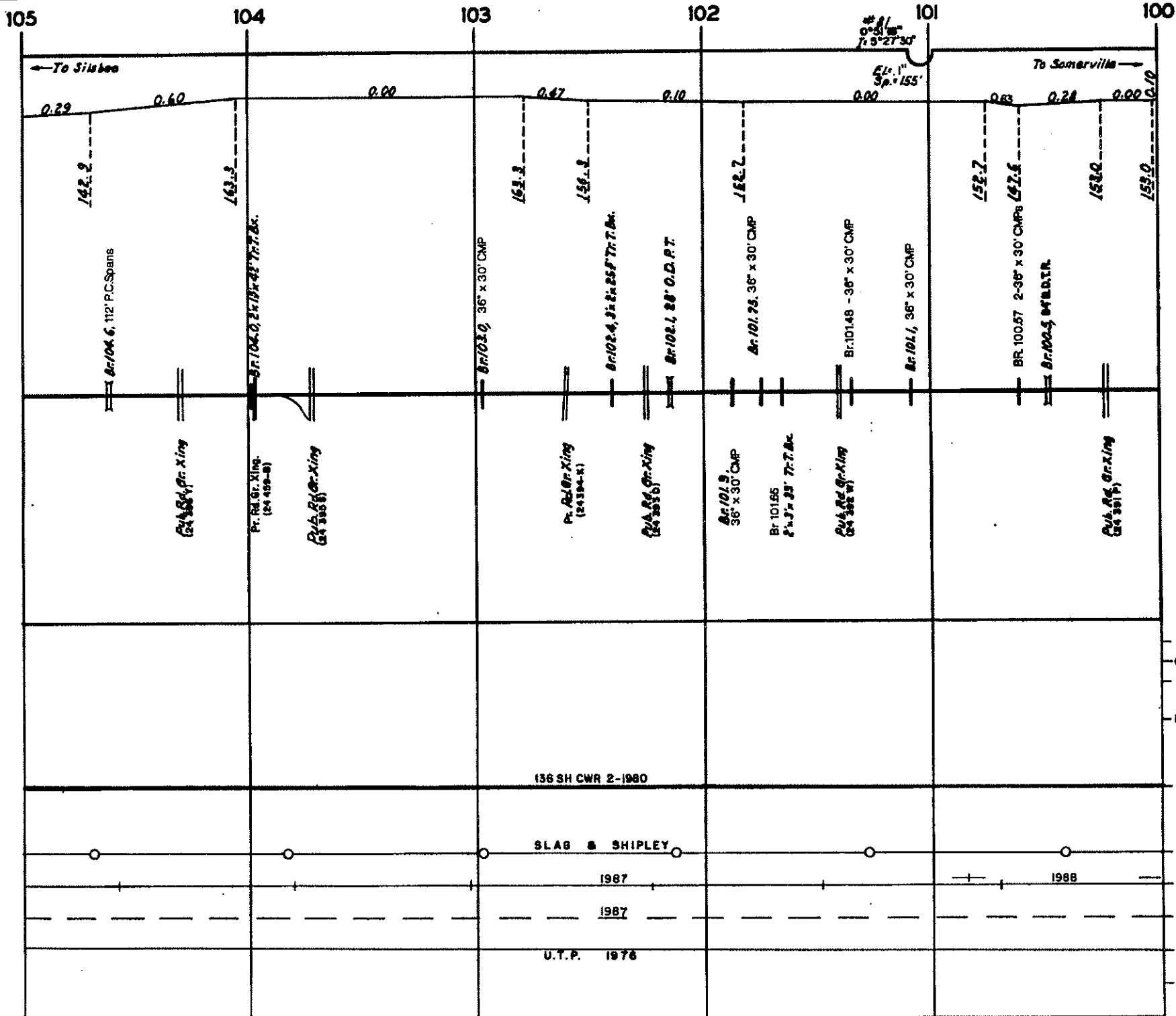


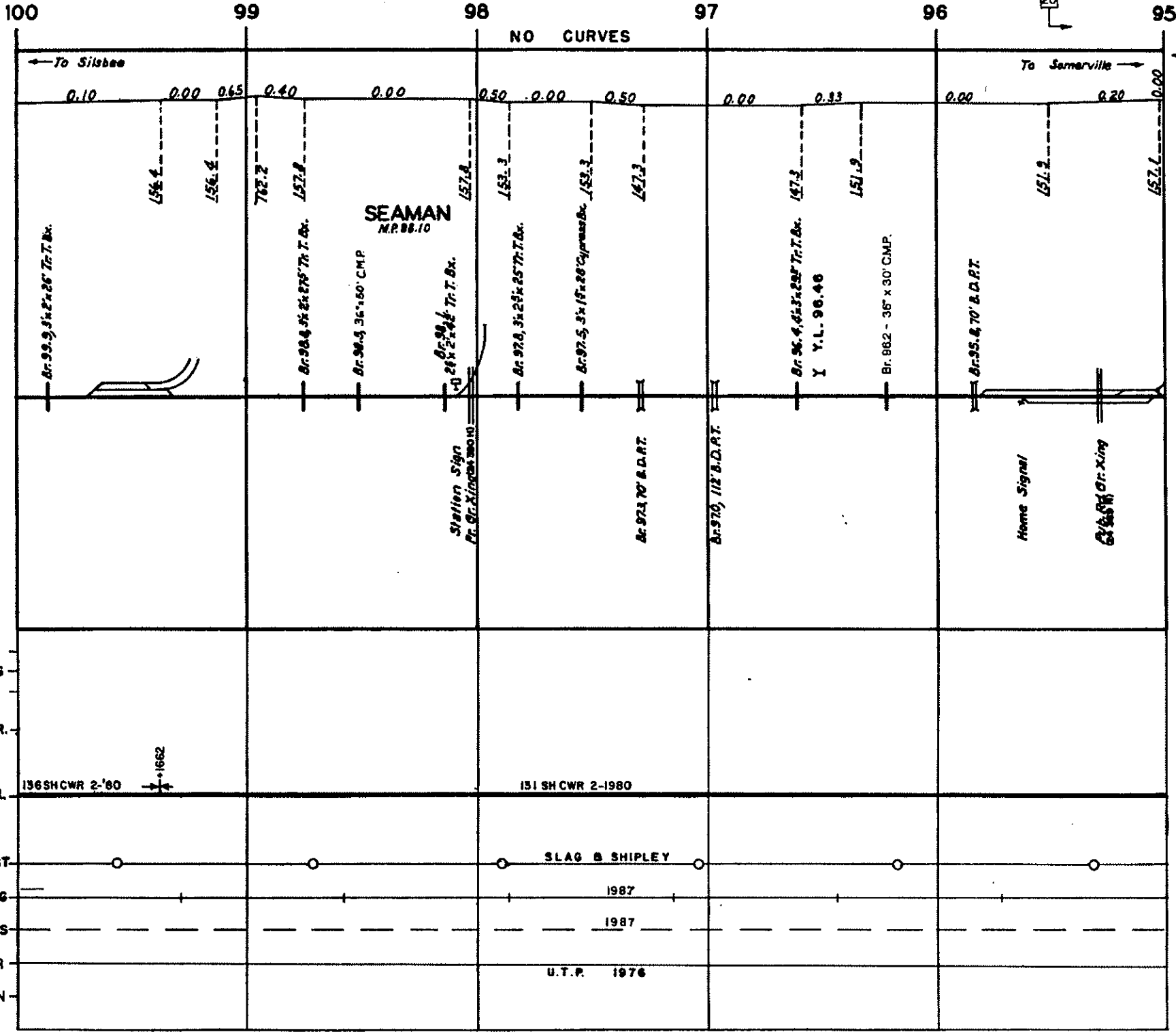






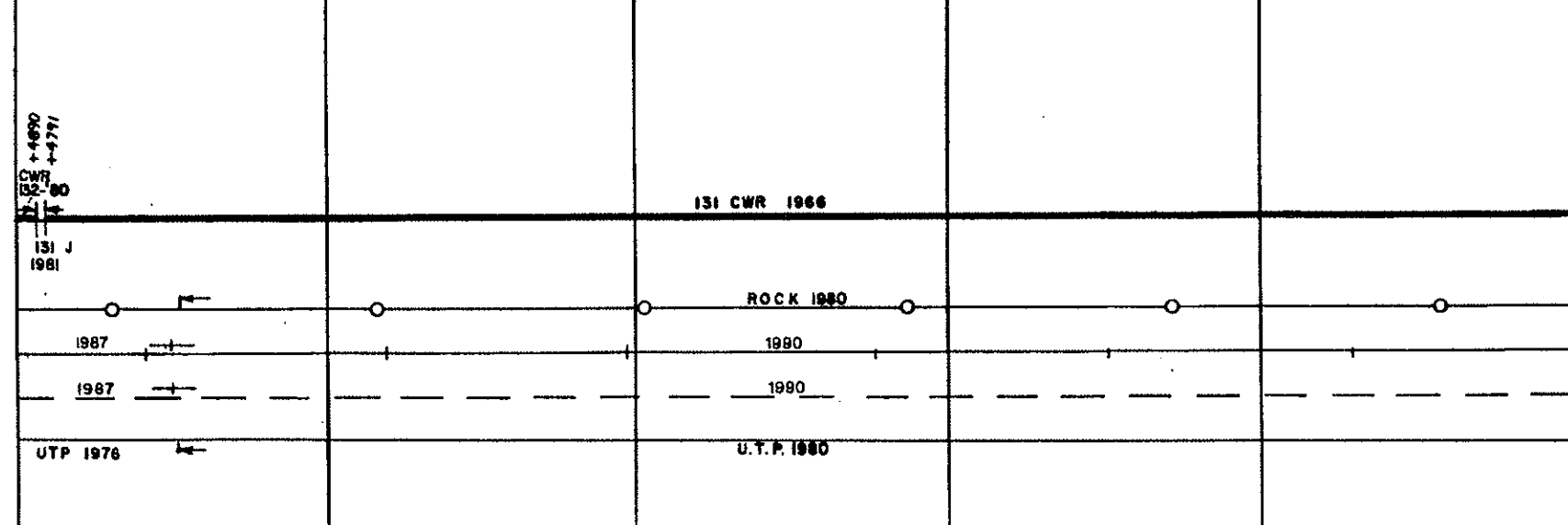
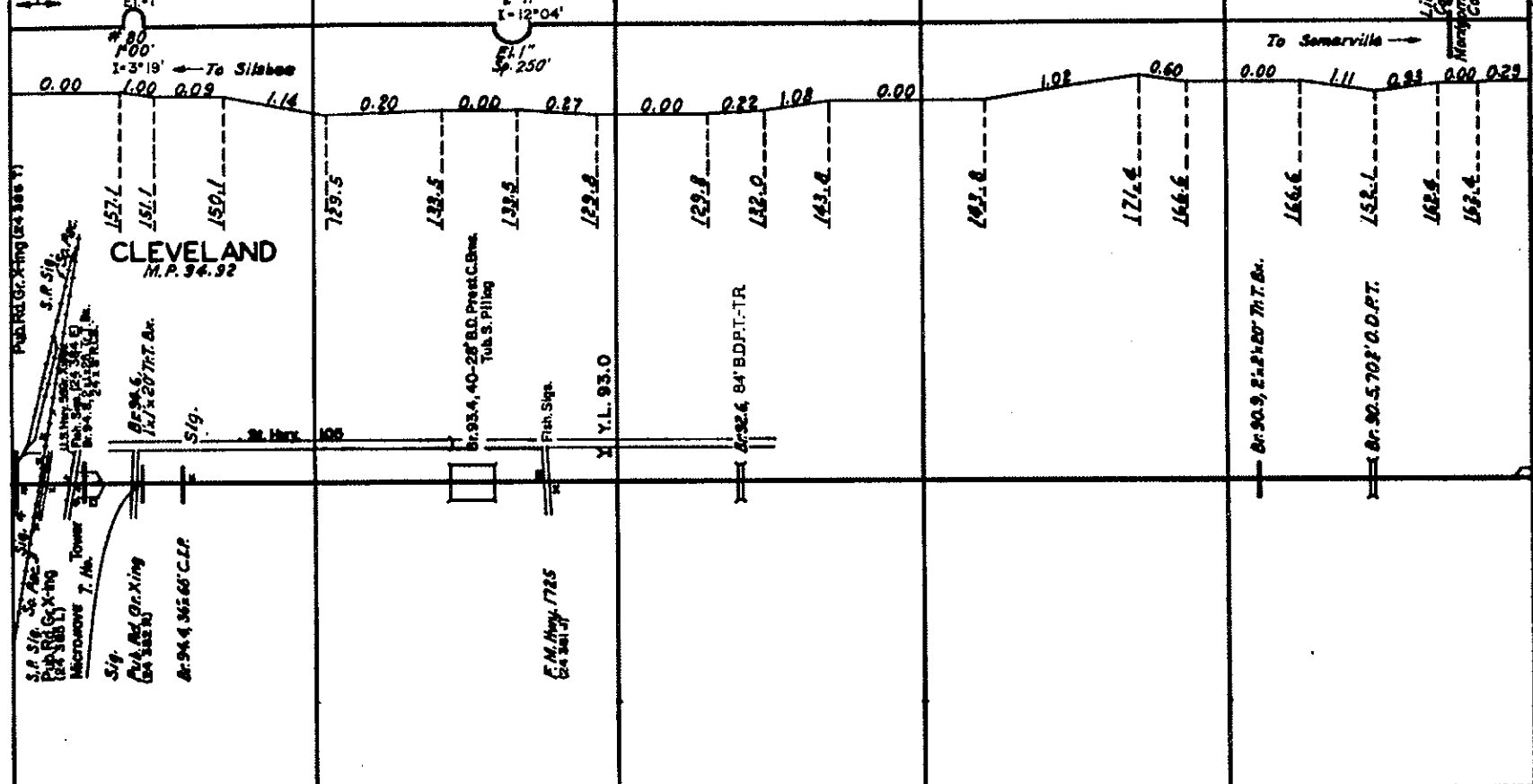




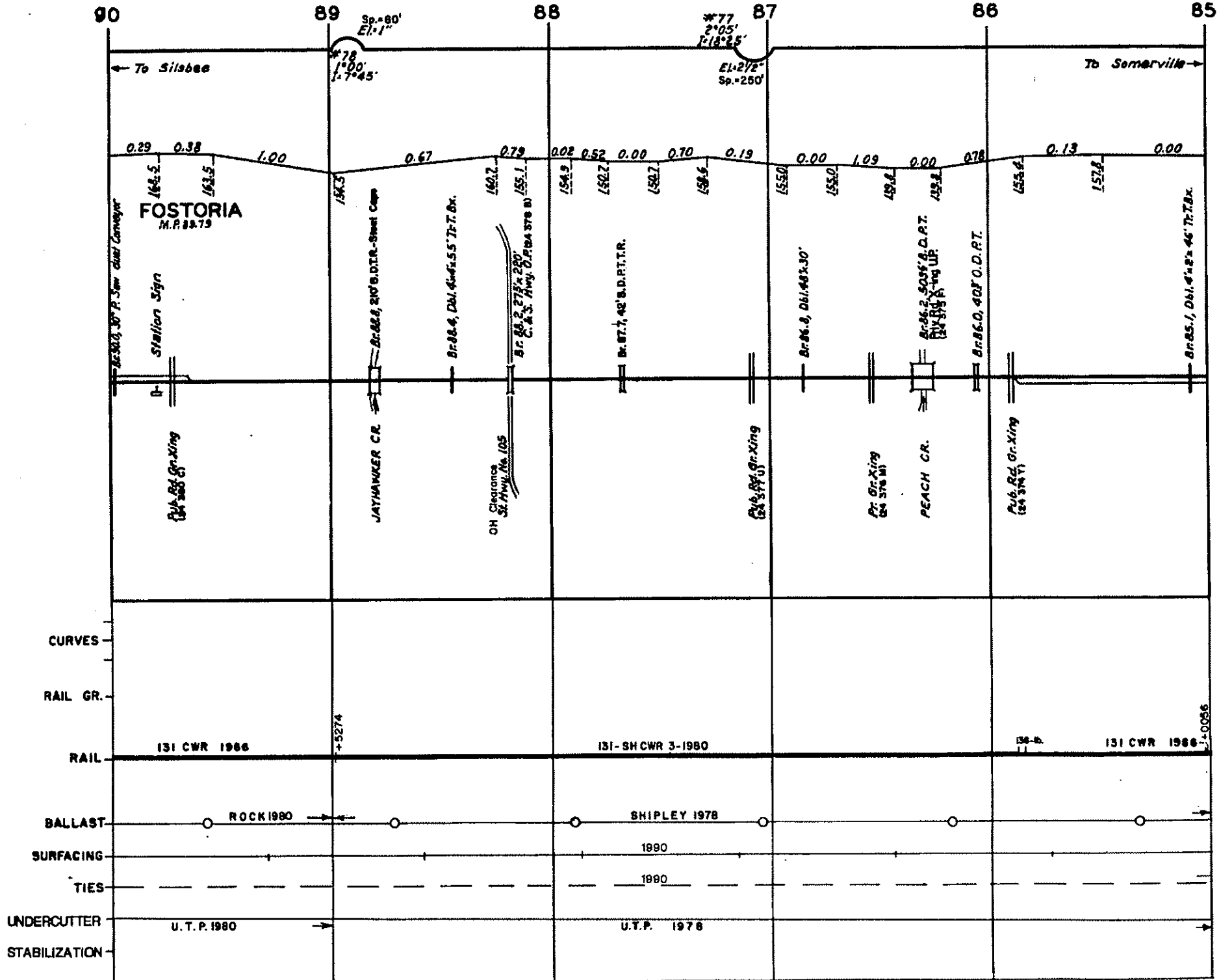


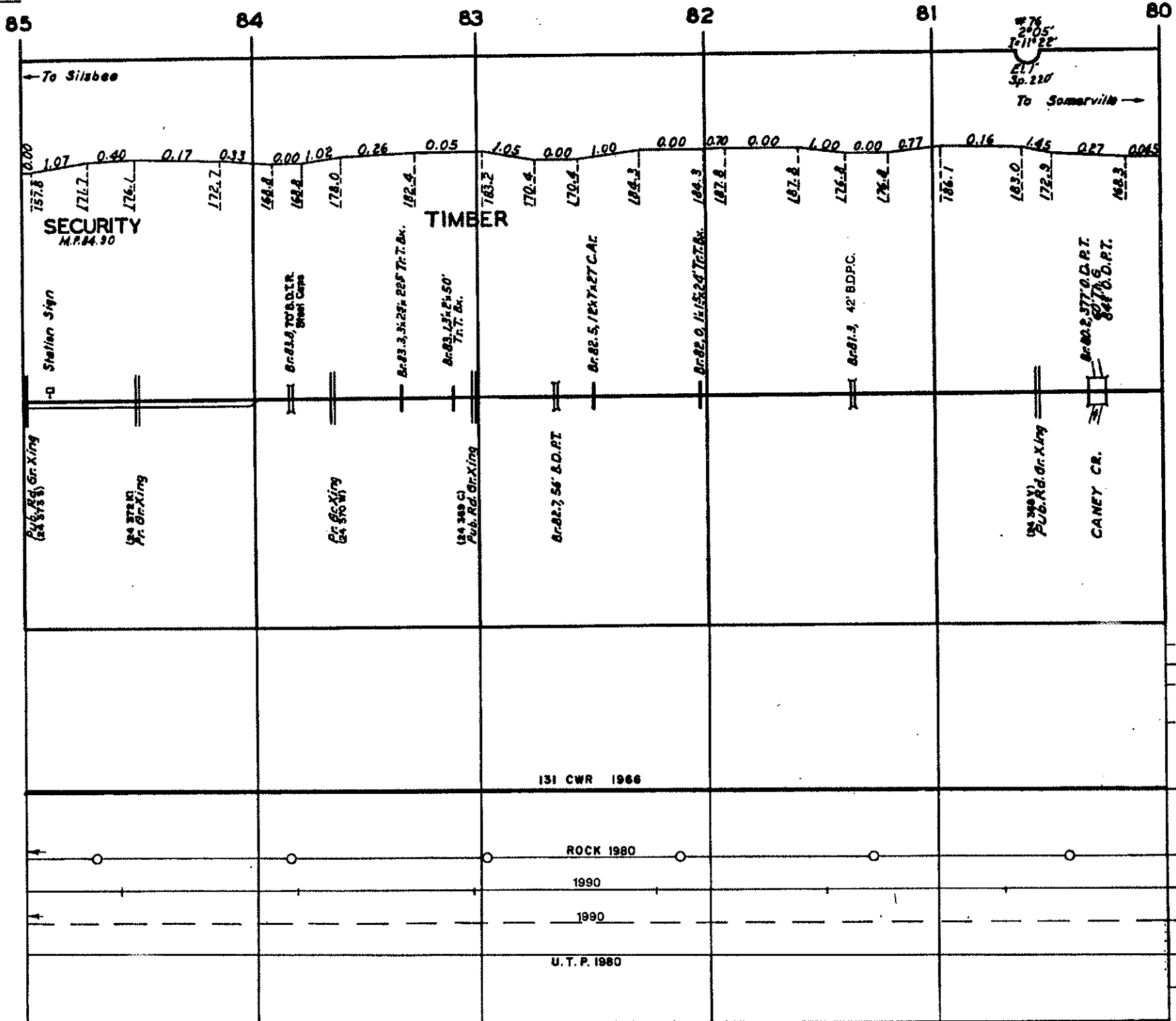
CURVES  
 RAIL GR.  
 RAIL  
 BALLAST  
 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION

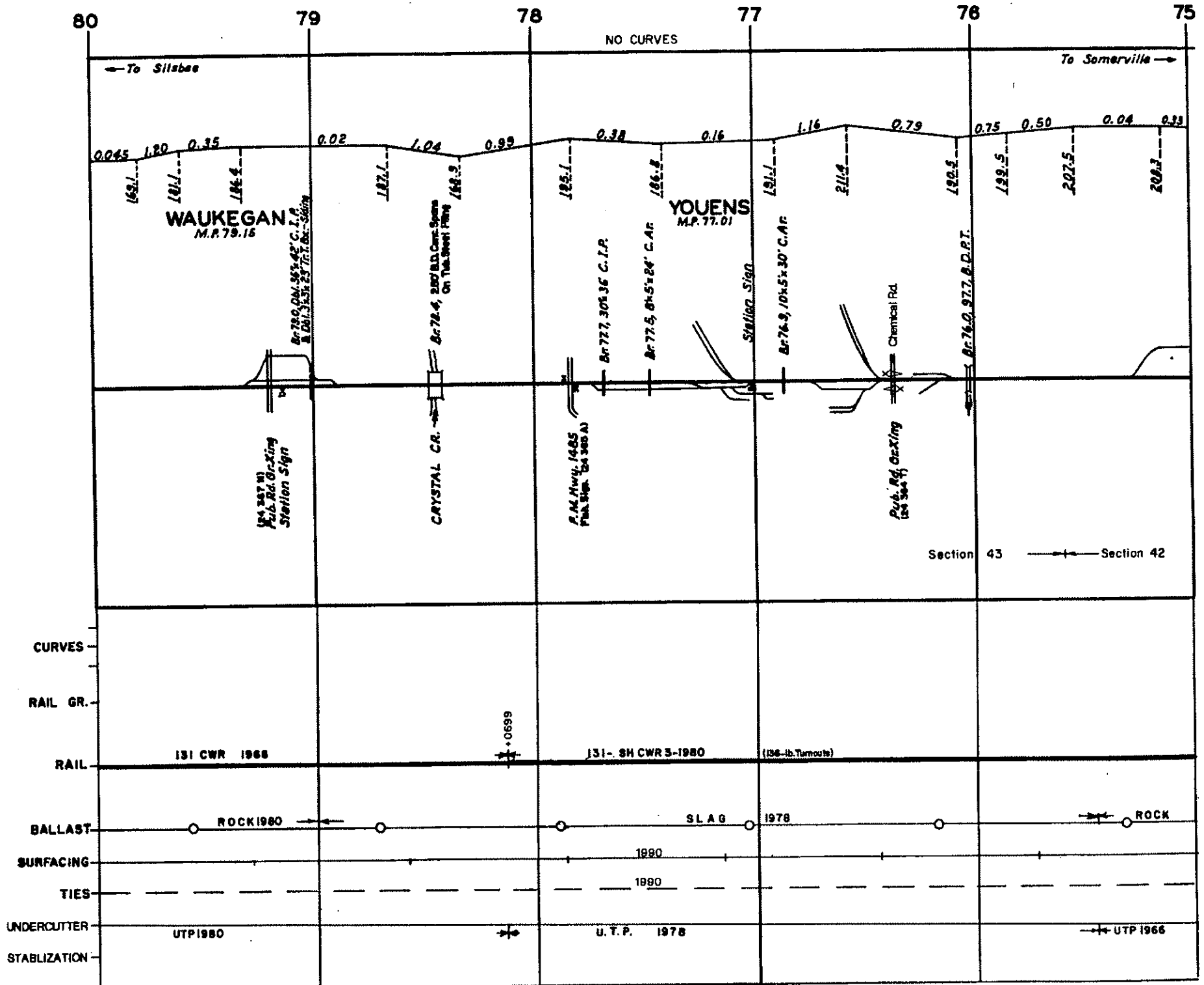
95 94 93 92 91 90

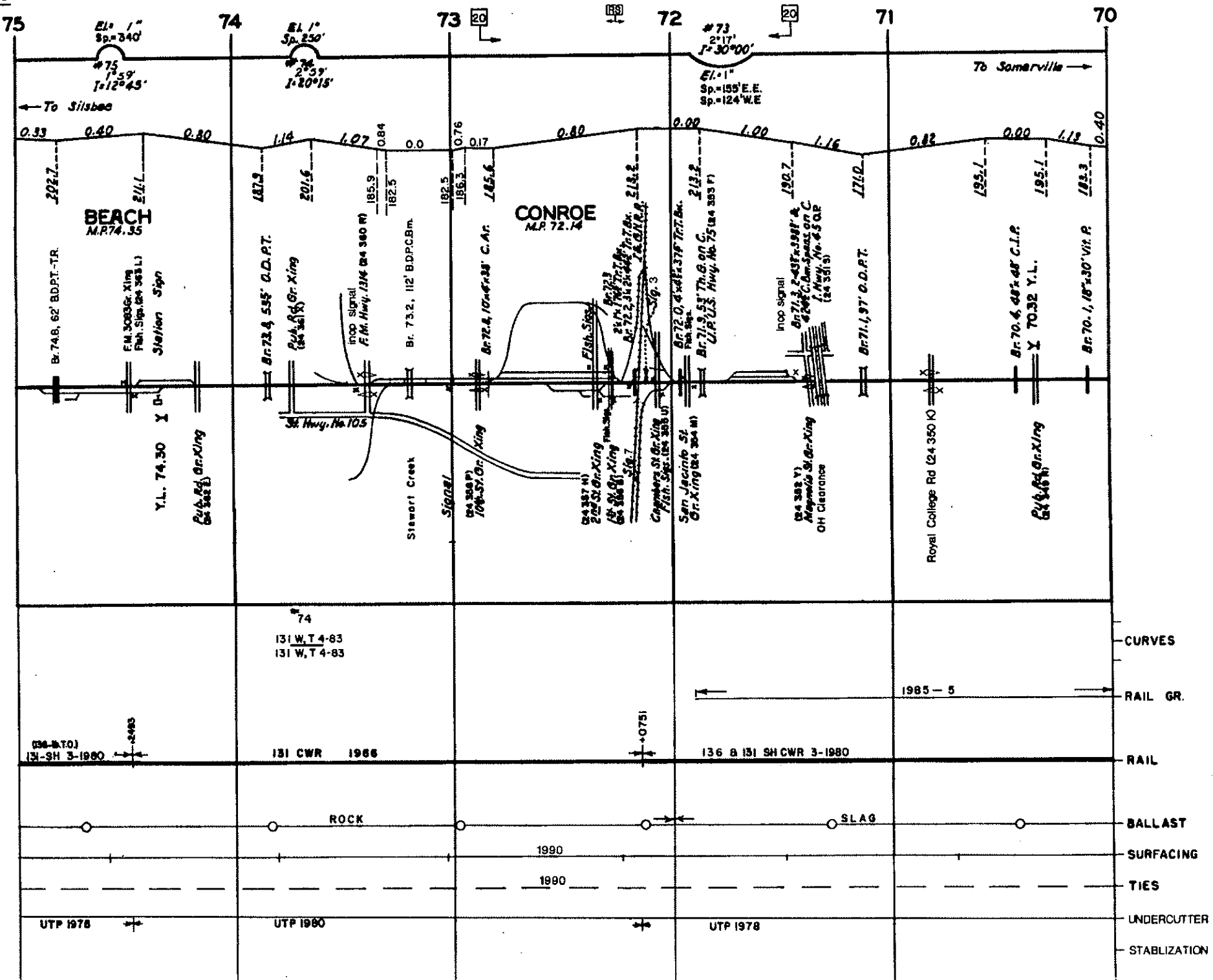


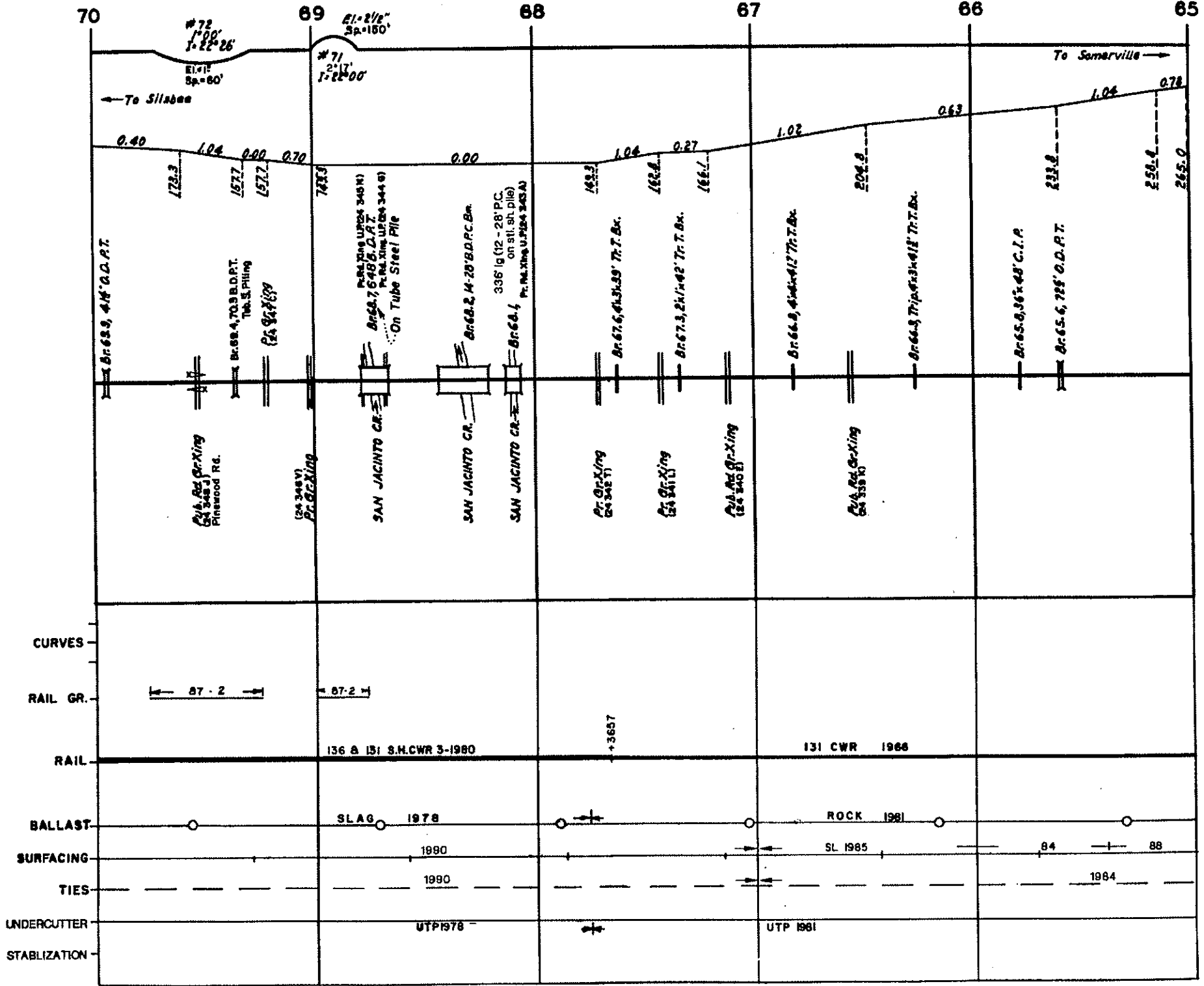


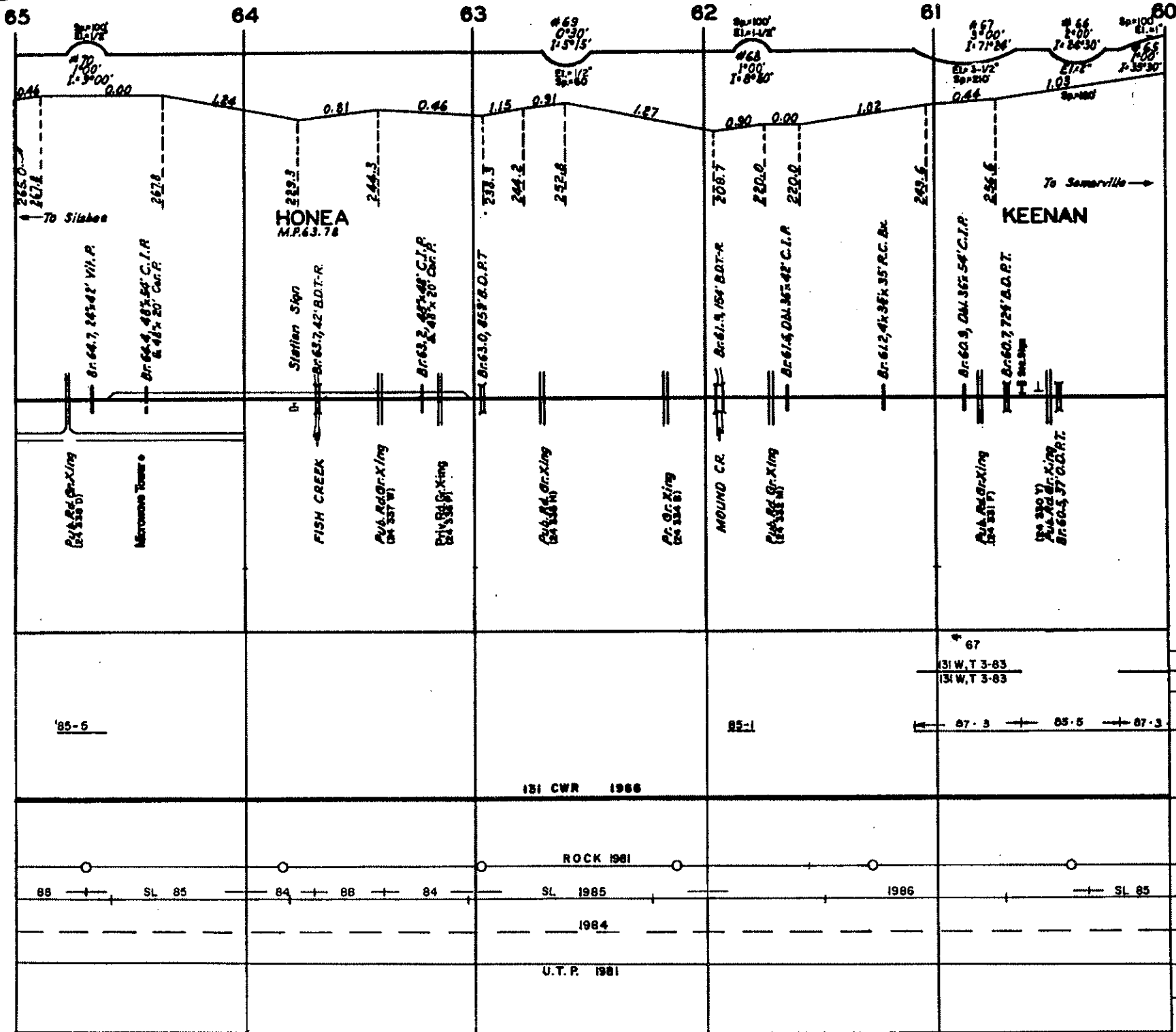


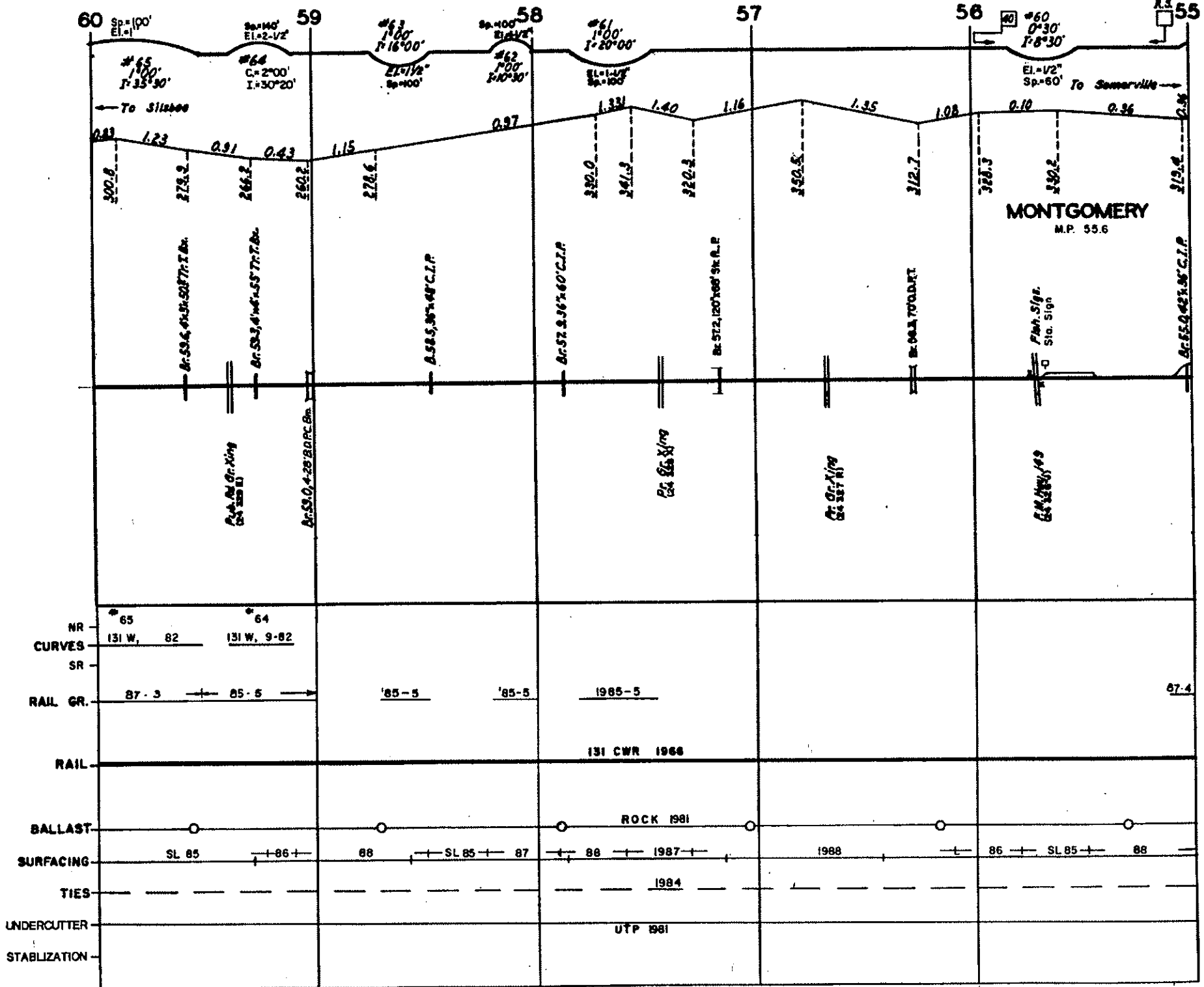


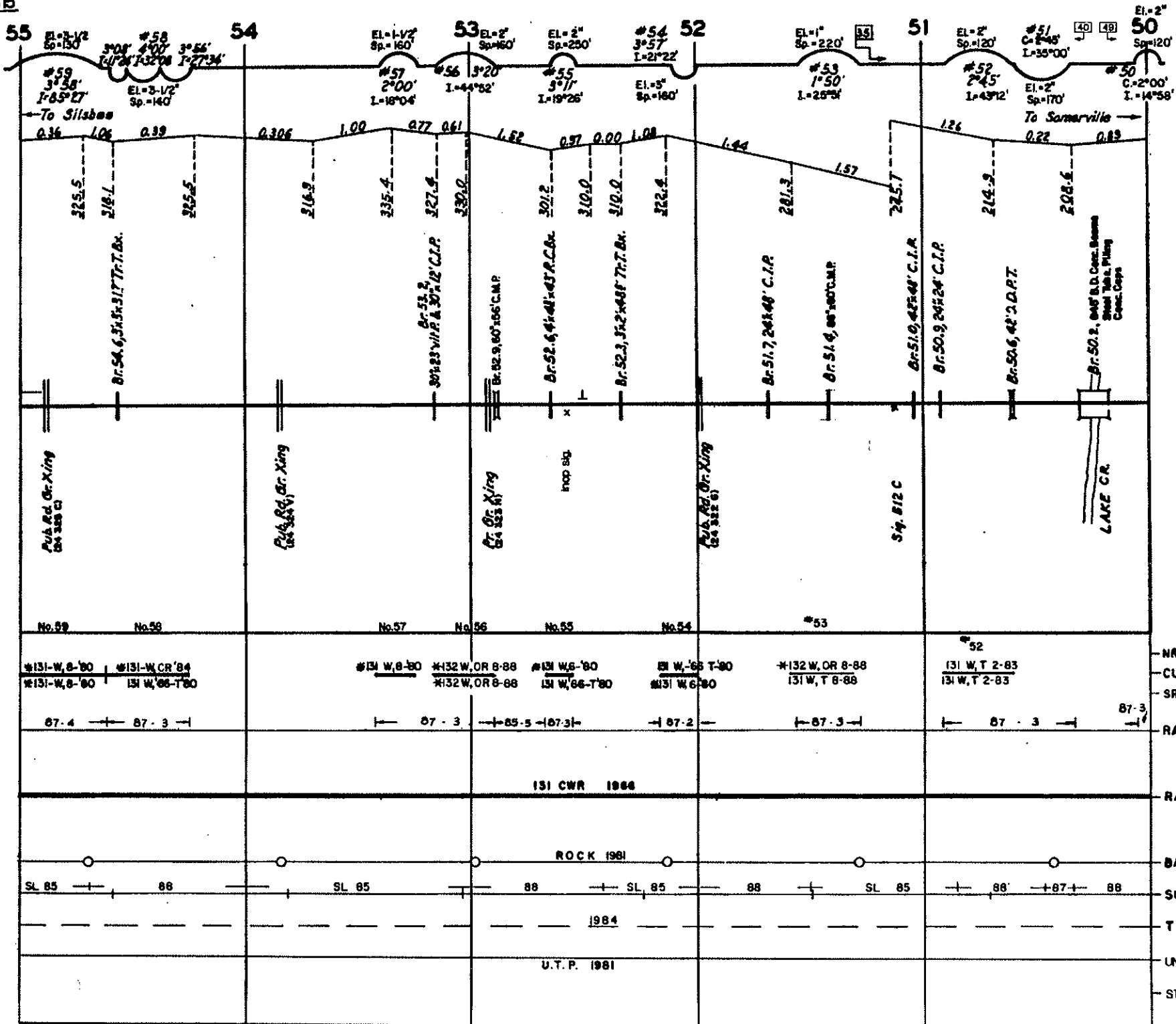




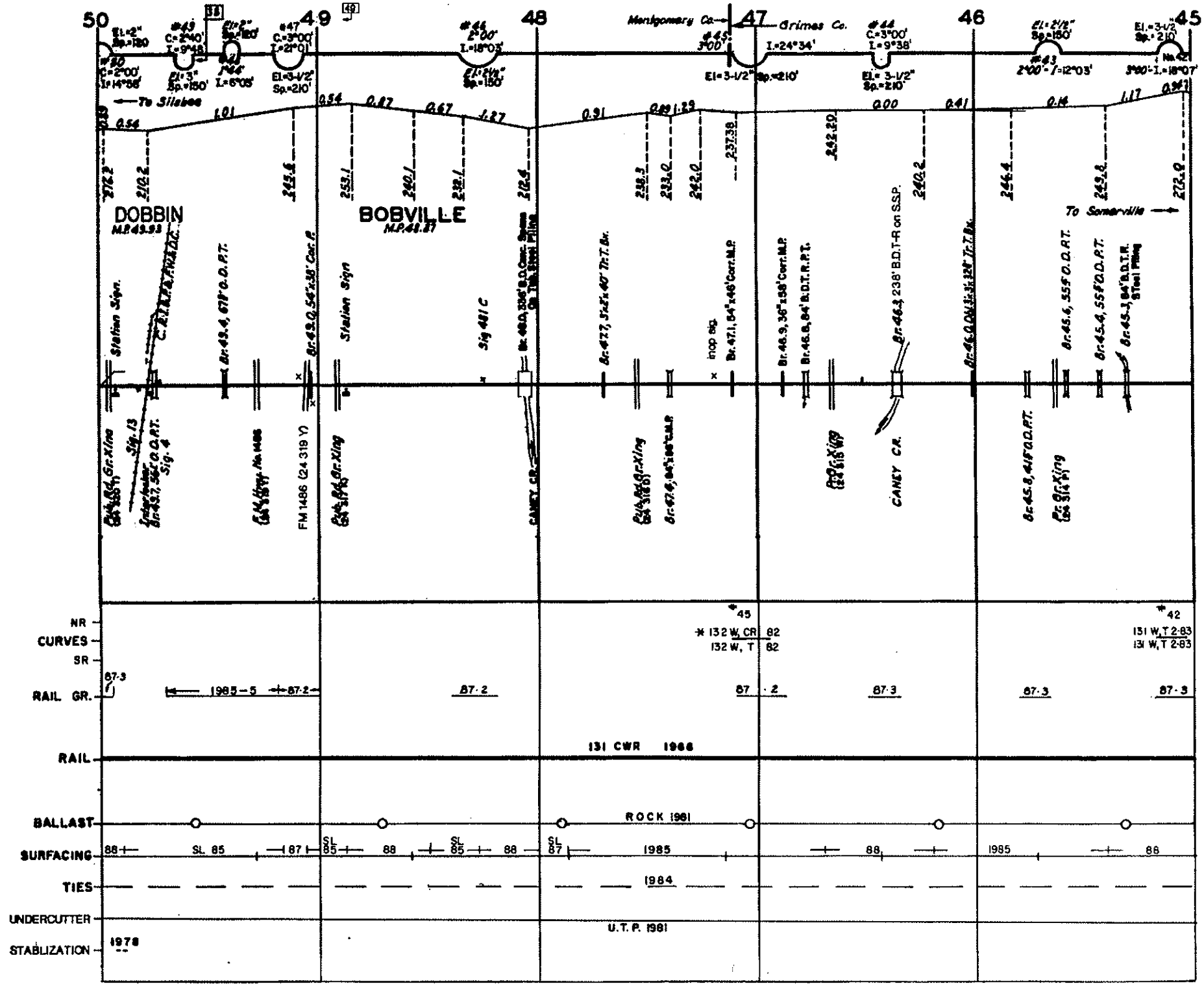


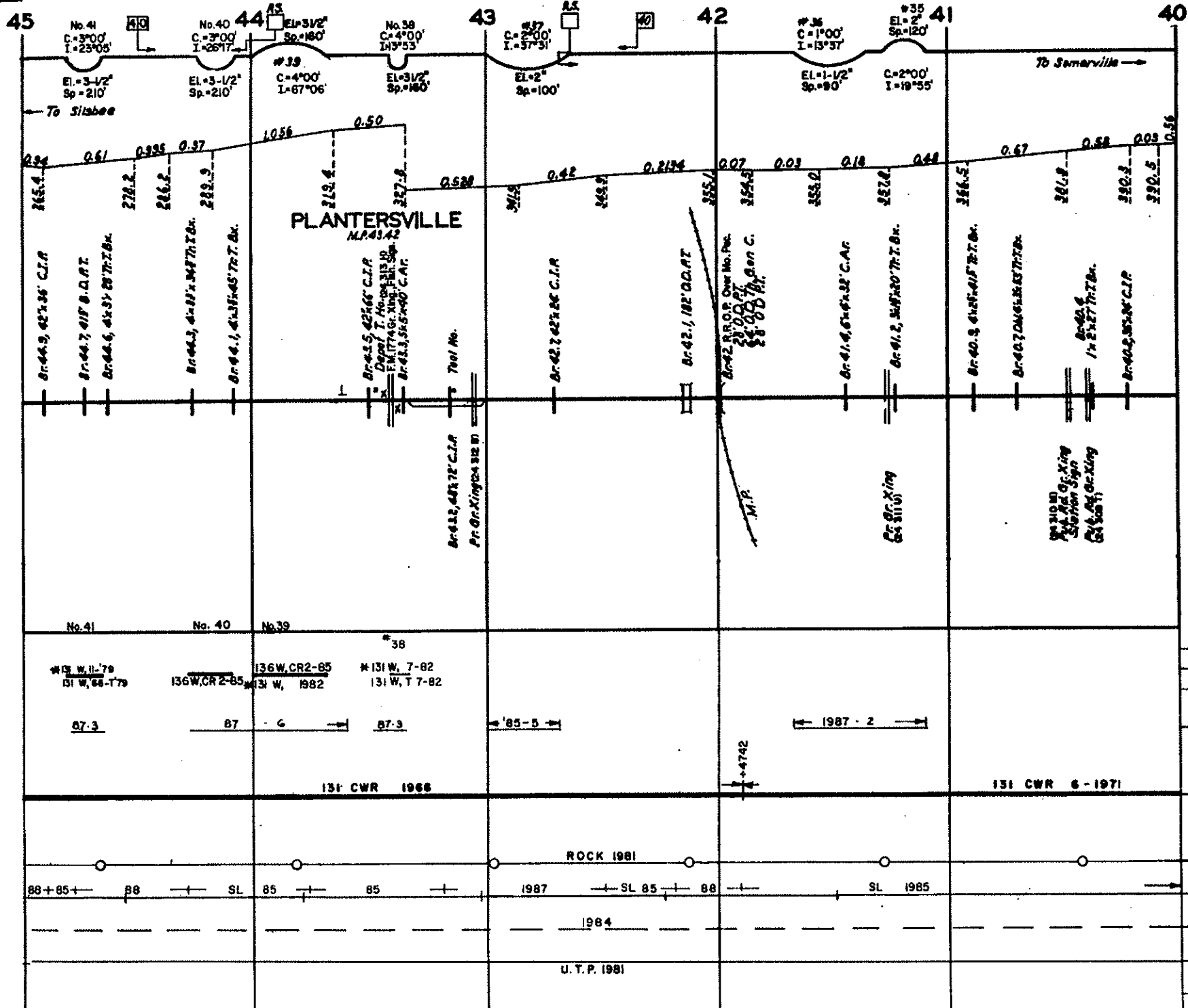




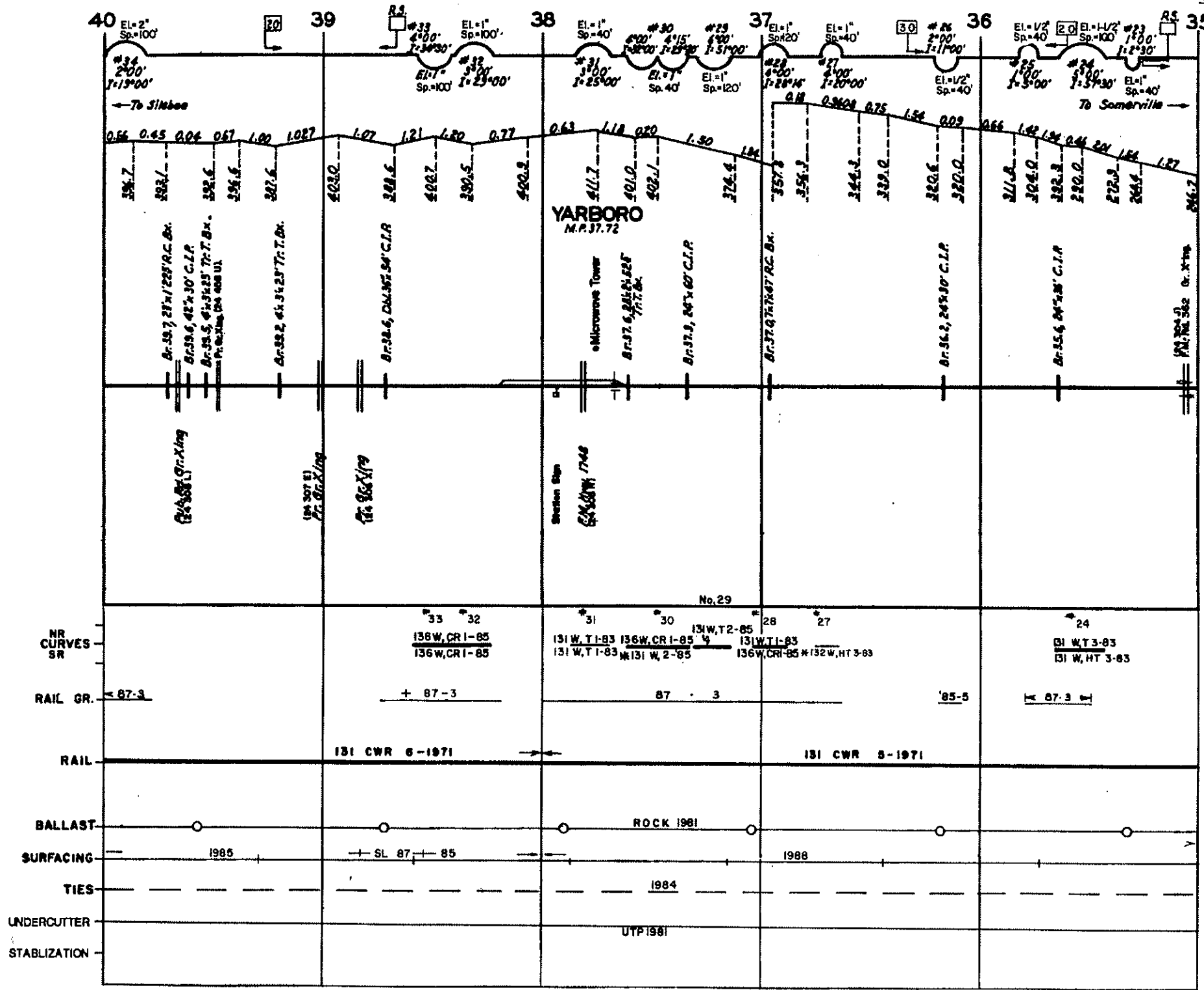




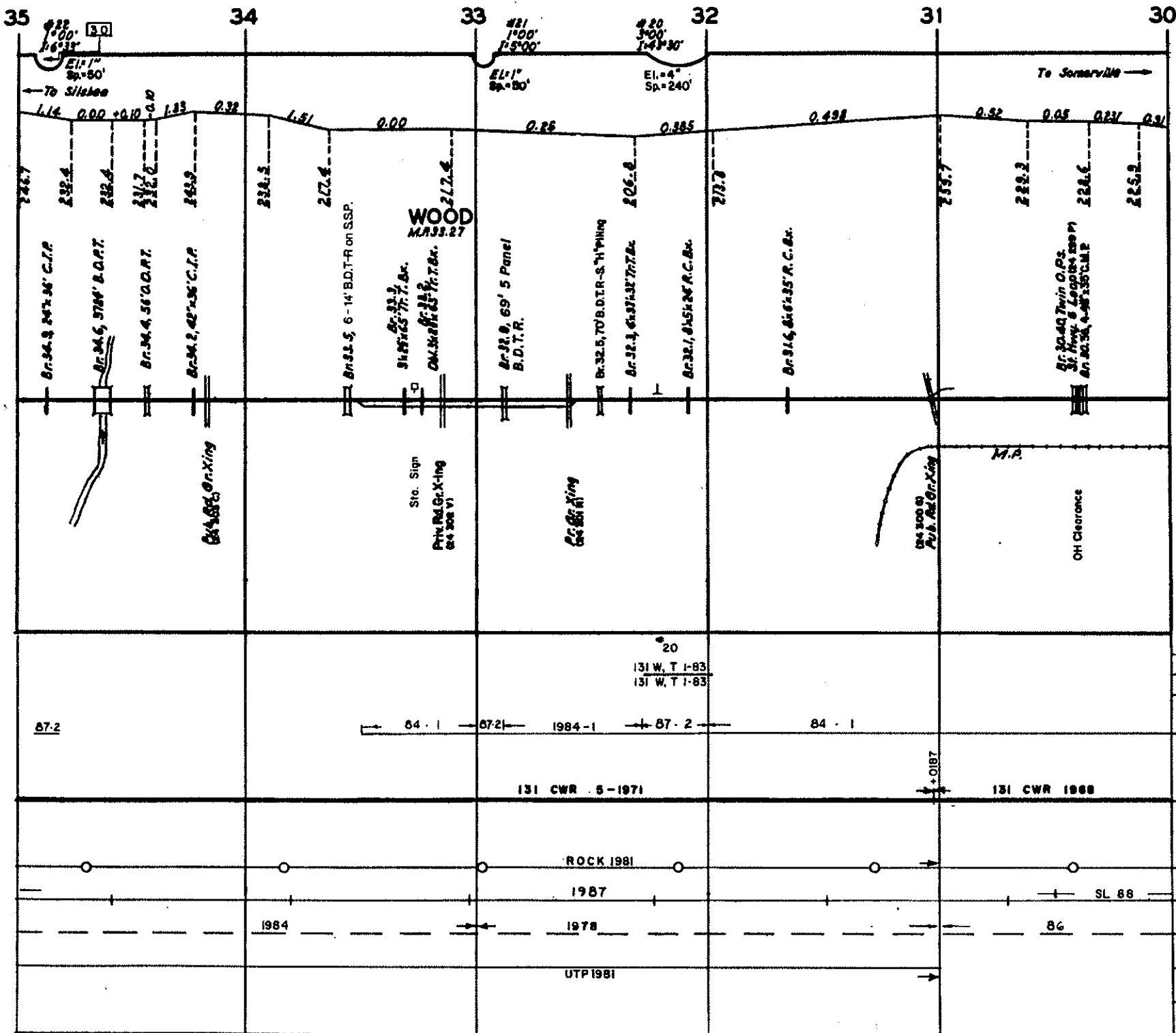


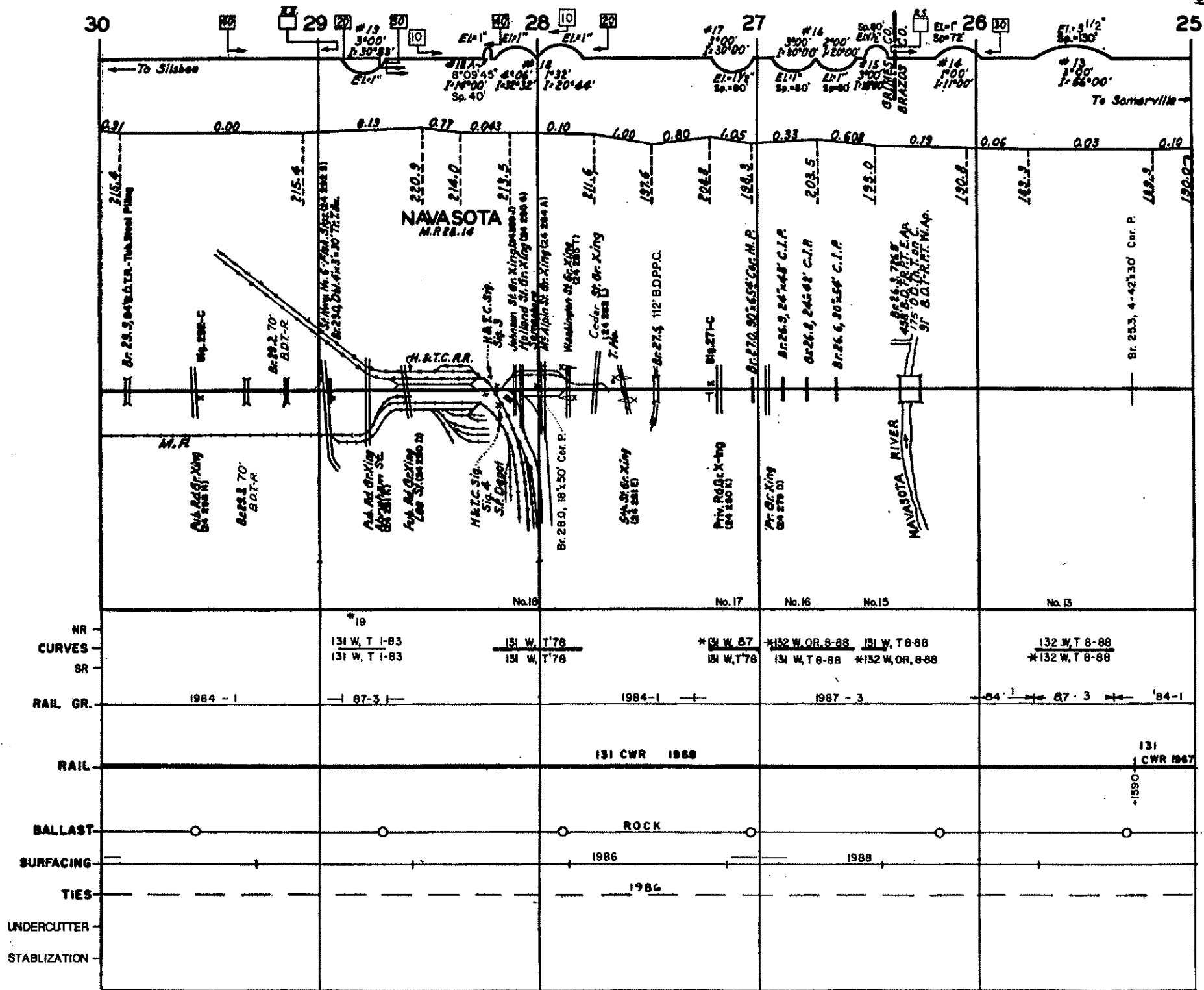


NR  
 CURVES  
 SR  
 RAIL GR.  
 RAIL  
 BALLAST  
 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION



**YARBORO**  
M.P. 37.72





25

24

23

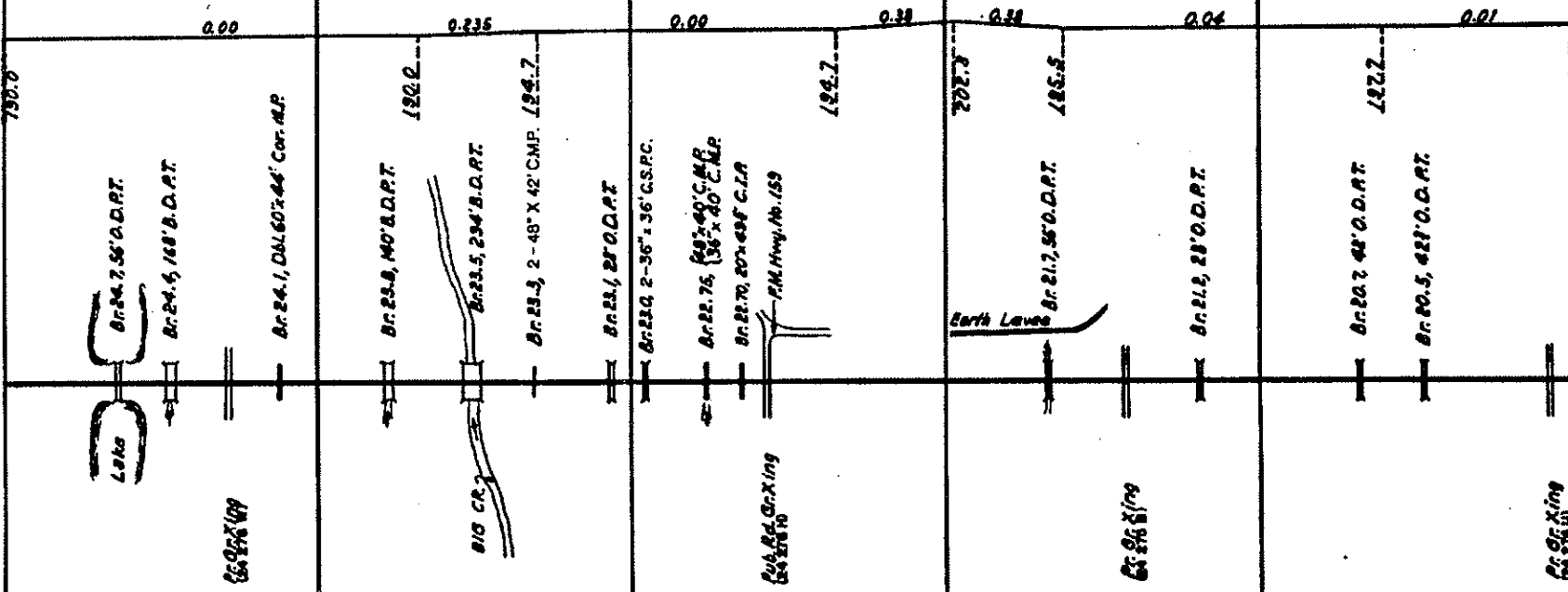
22

21

20

← To Silsbee

To Somerville →



- CURVES
- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION

1984-1

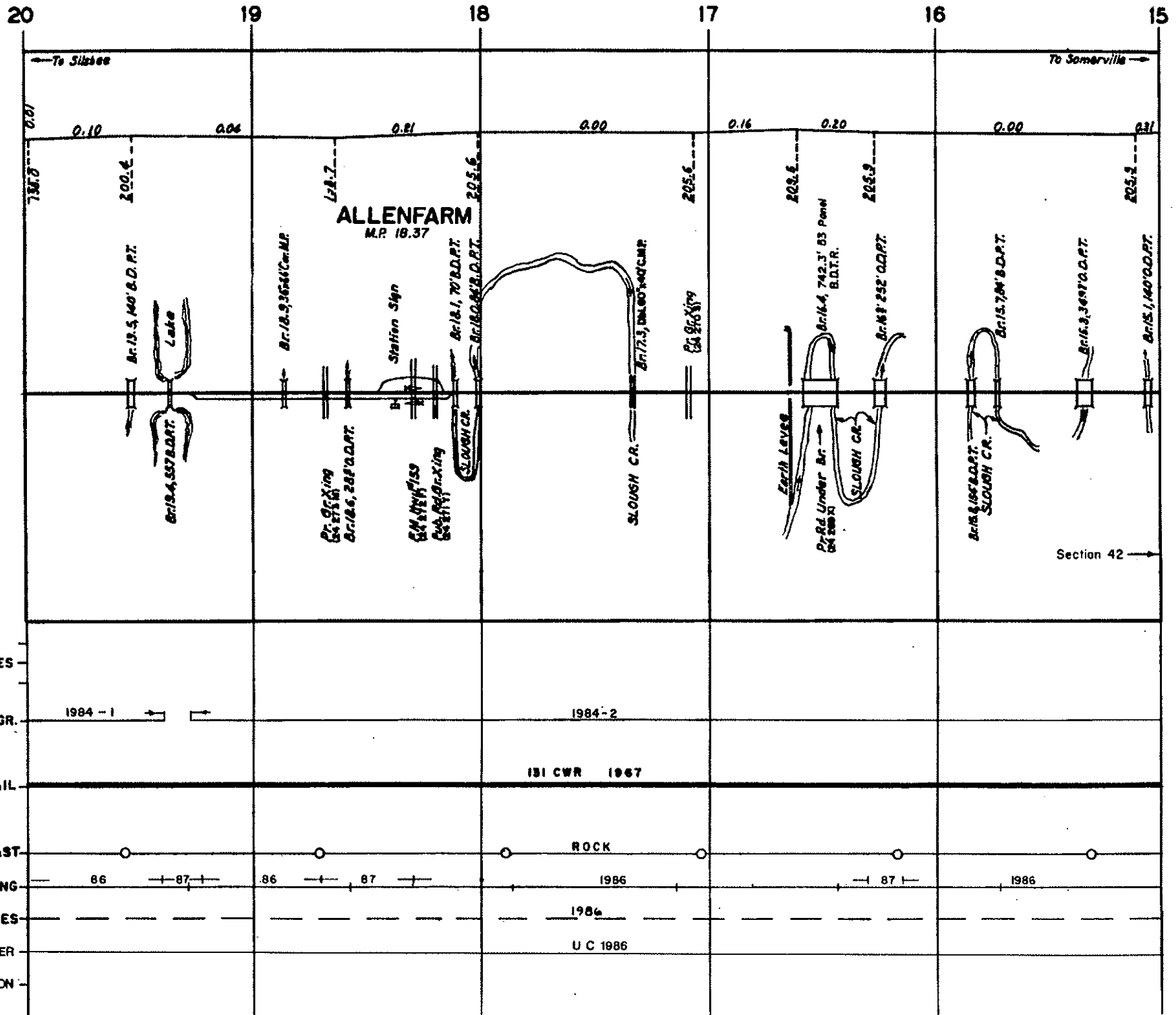
131 CWR 1967

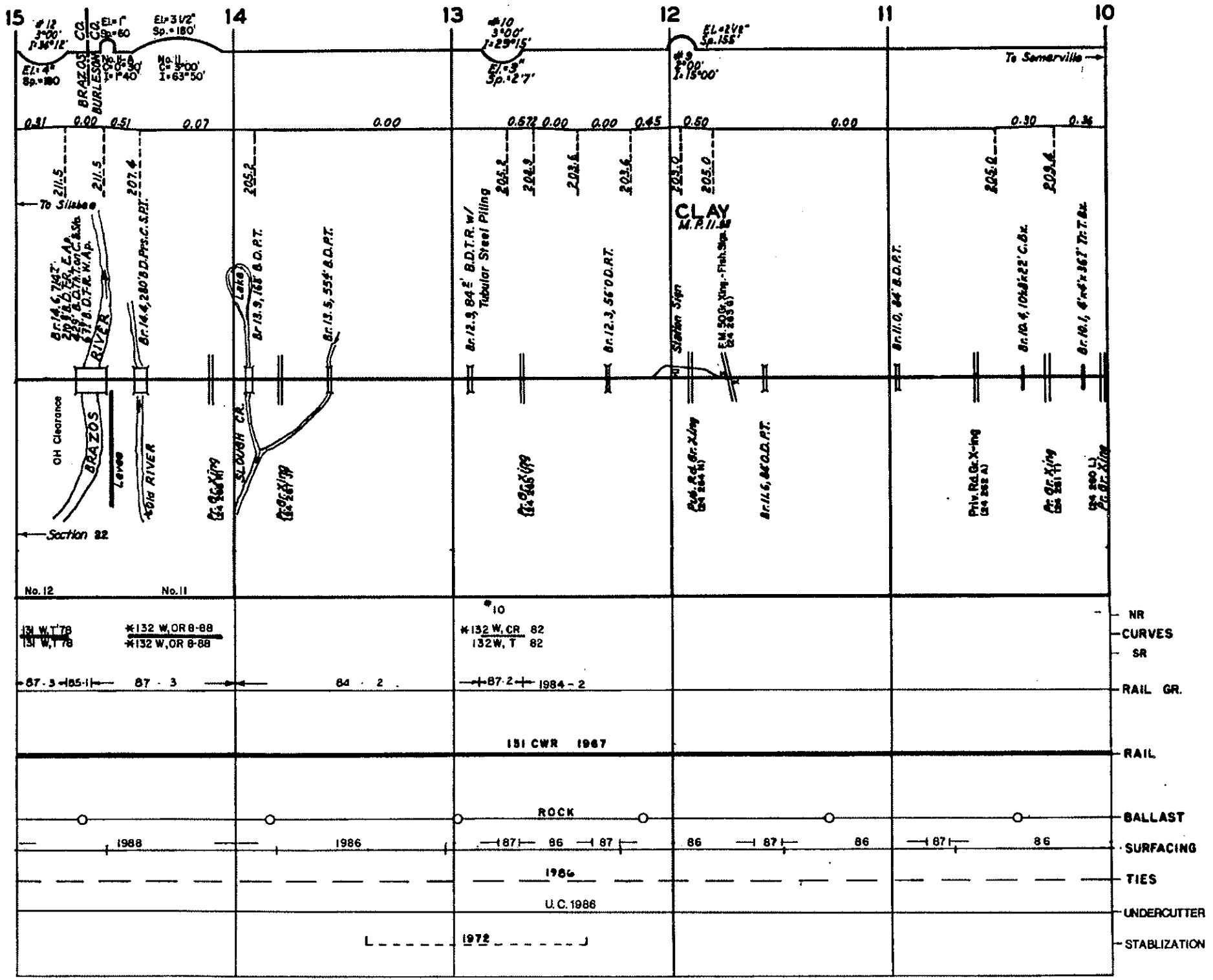
ROCK

1988

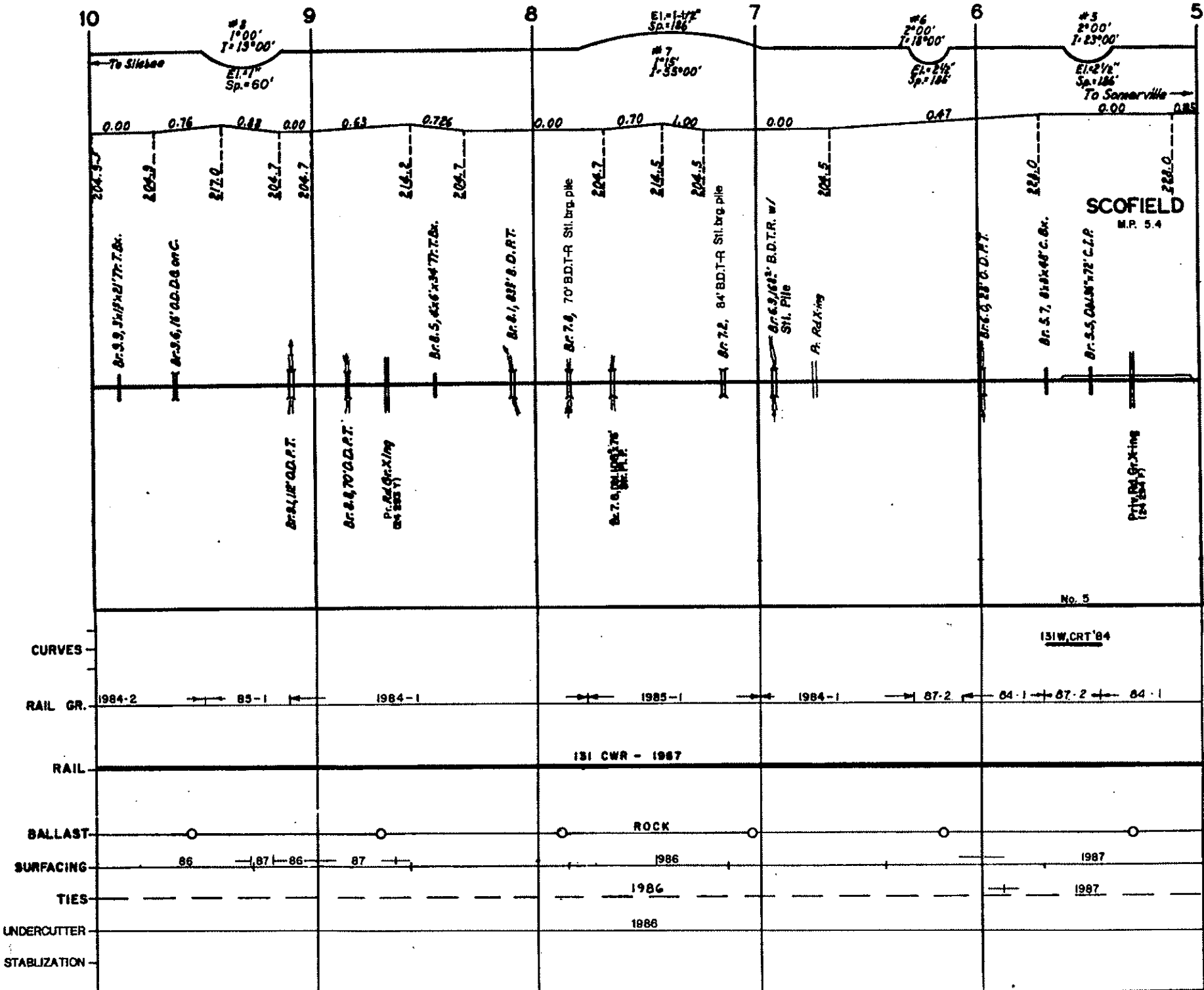
1986

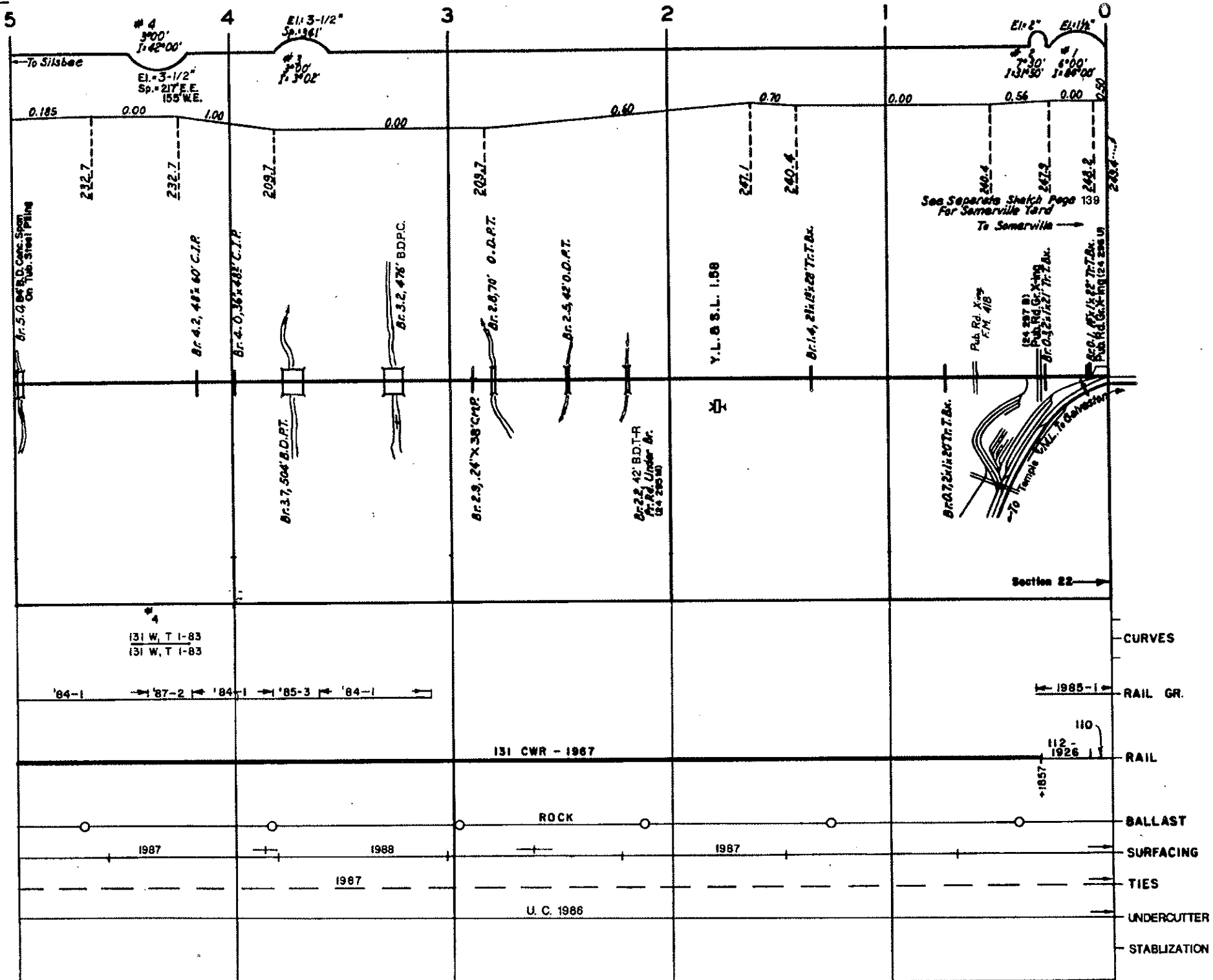
U C 1986









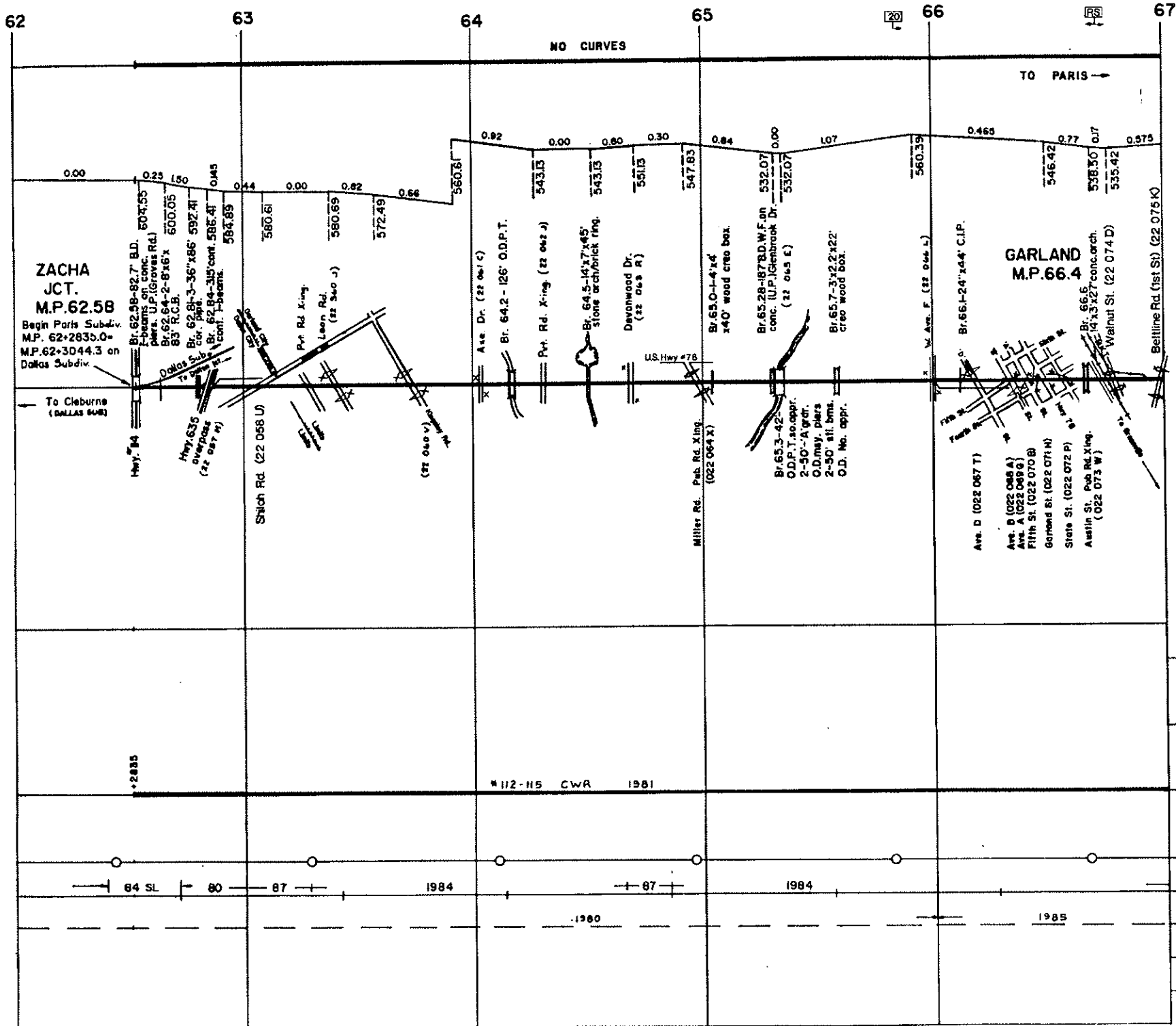


# Conroe Subdivision

Somerville (M.P. 0.0) to Silsbee (M.P. 152.6)

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*Intentionally Blank*



NO CURVES

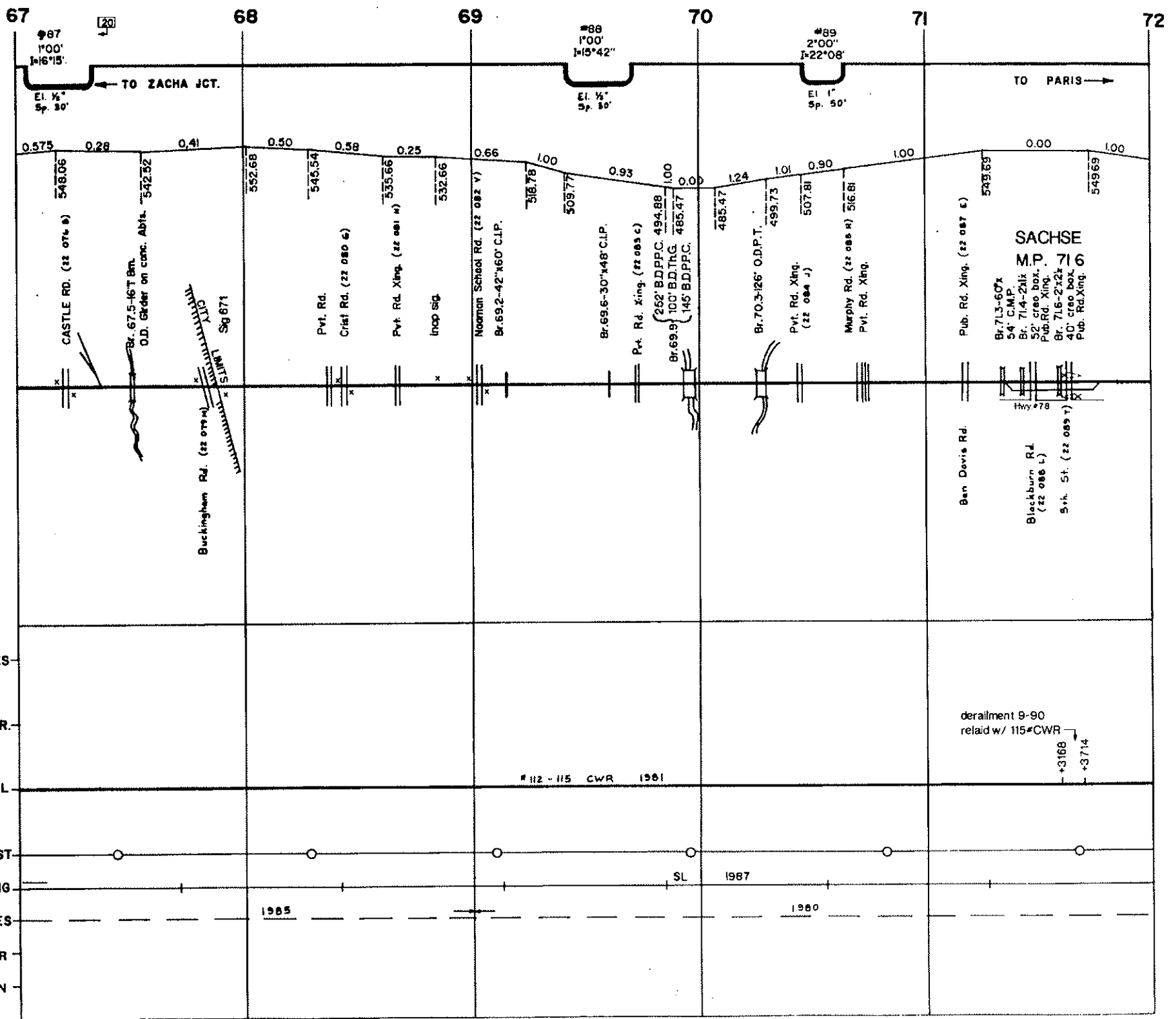
TO PARIS →

**ZACHA  
JCT.  
M.P. 62.58**

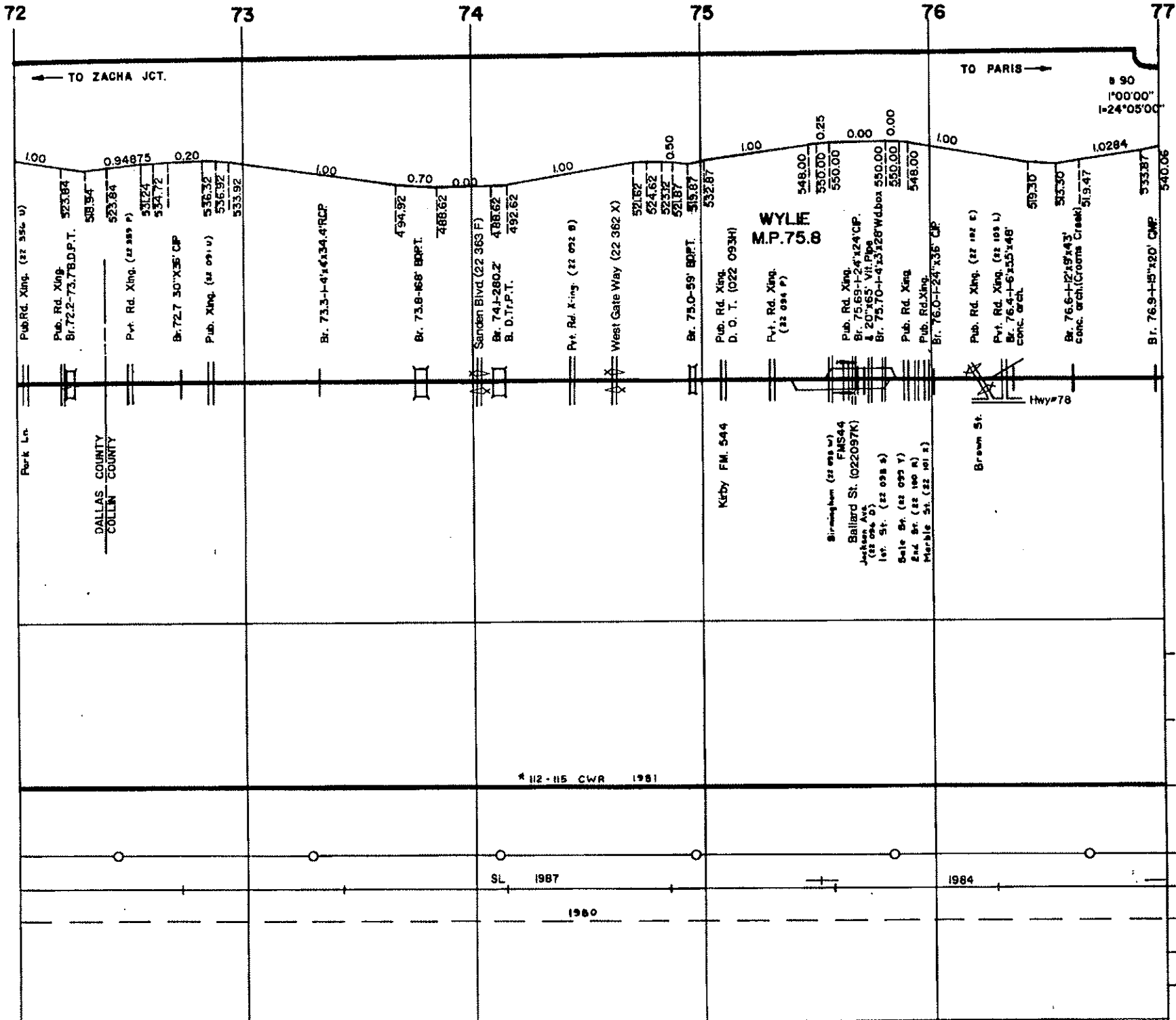
Begin Paris Subdiv.  
M.P. 62+2835.0 =  
M.P. 62+3044.3 on  
Dallas Subdiv.

**GARLAND  
M.P. 66.4**

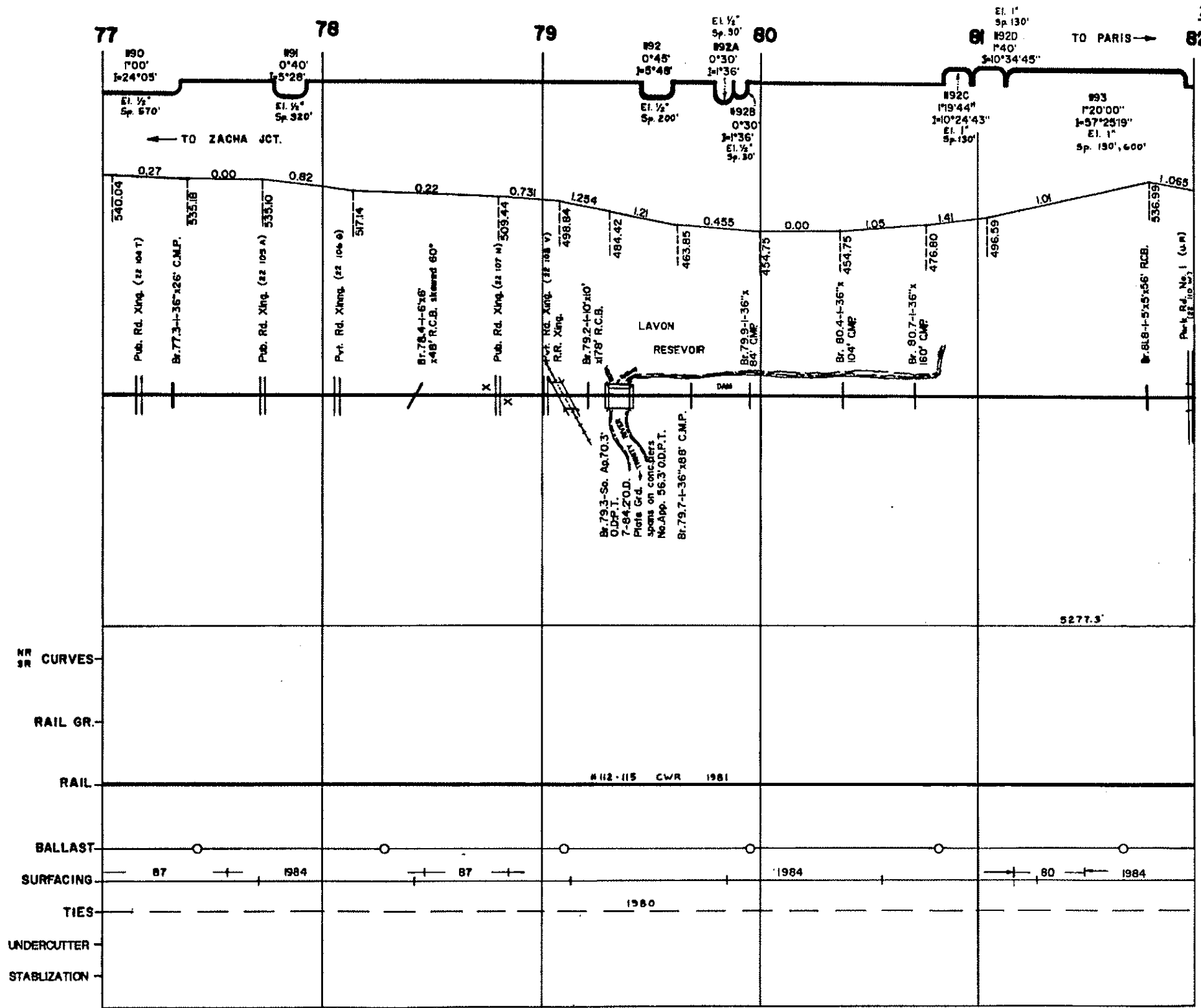
- CURVES NR SR
- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION

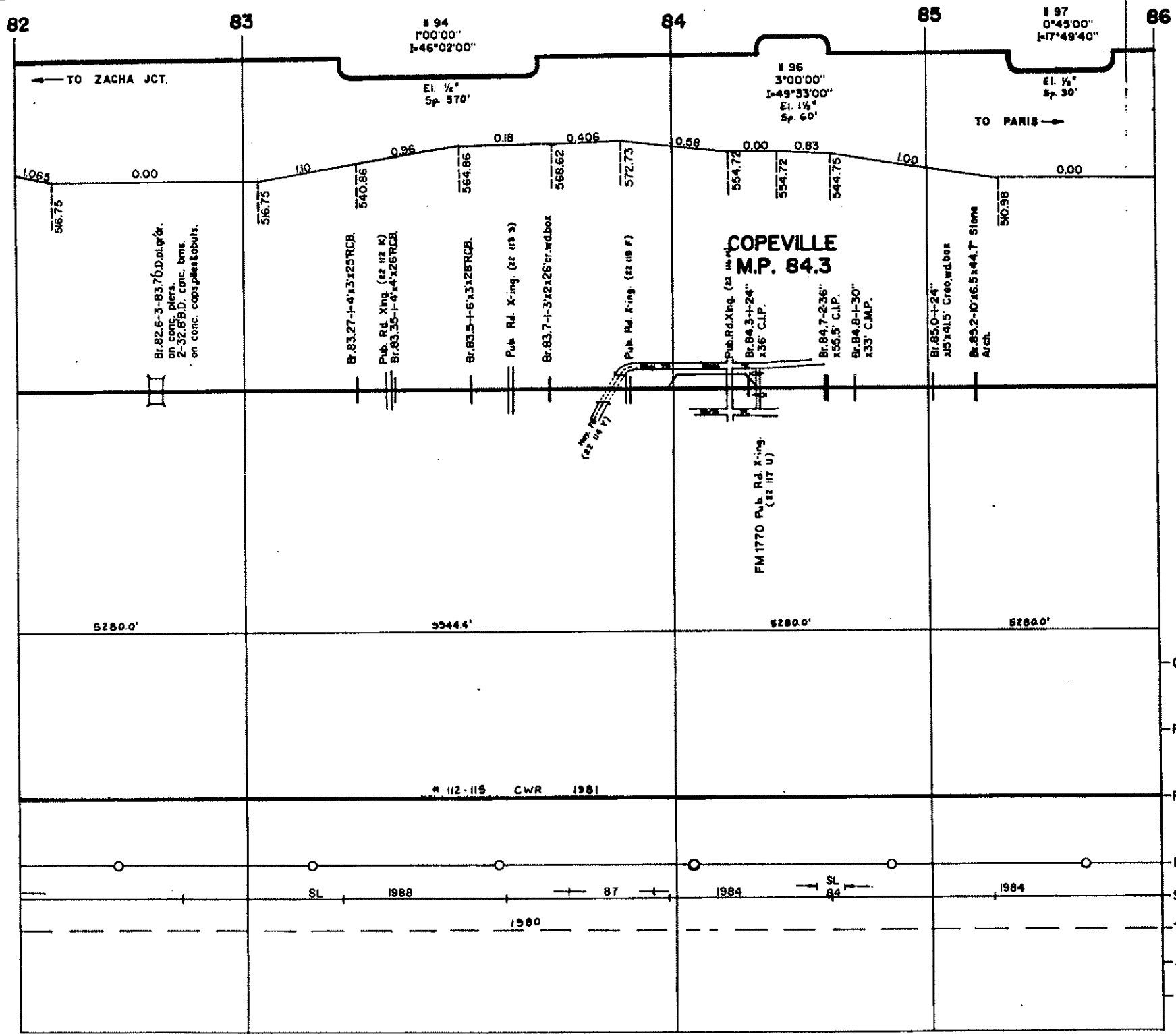


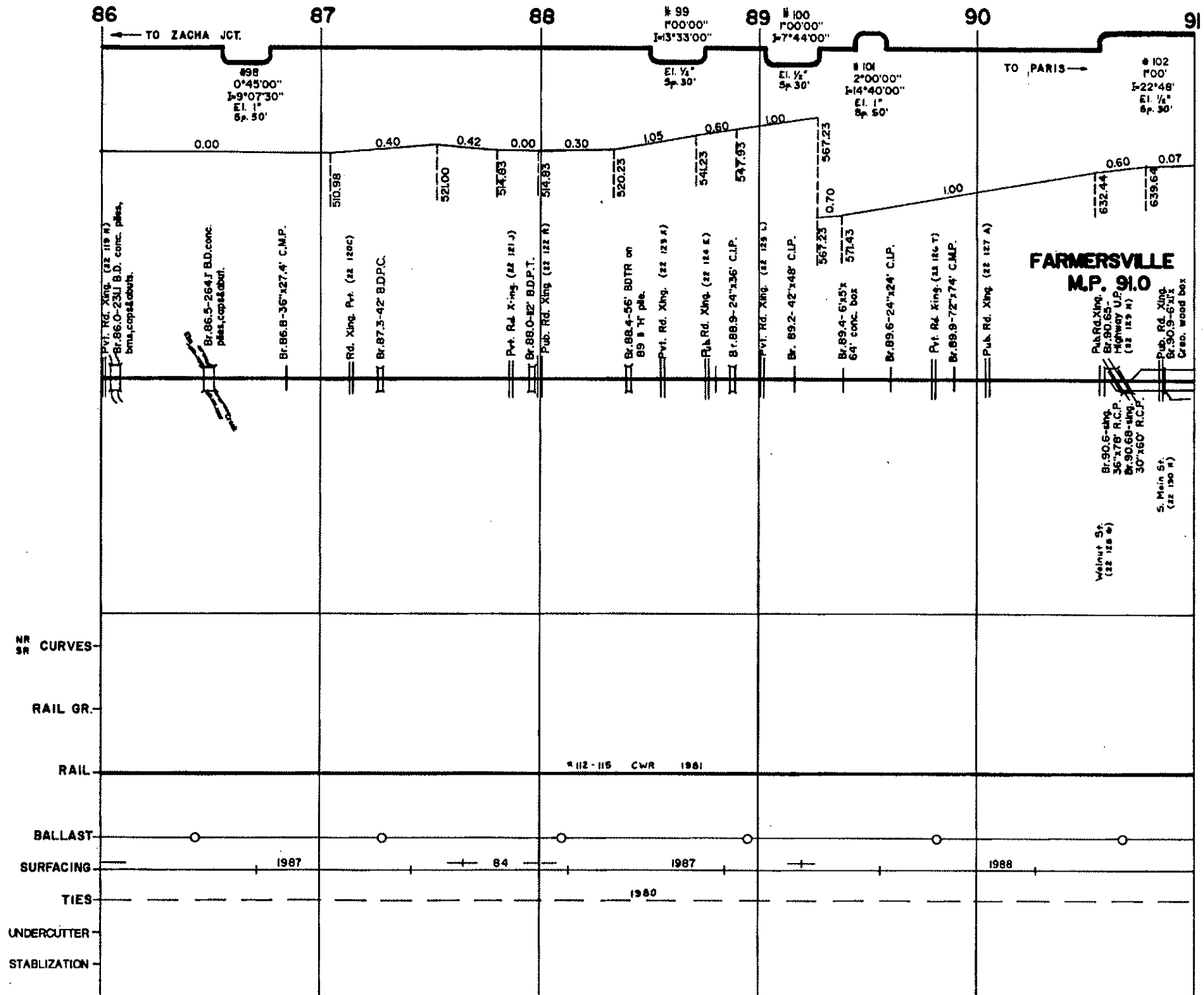
REVISED 3-1991





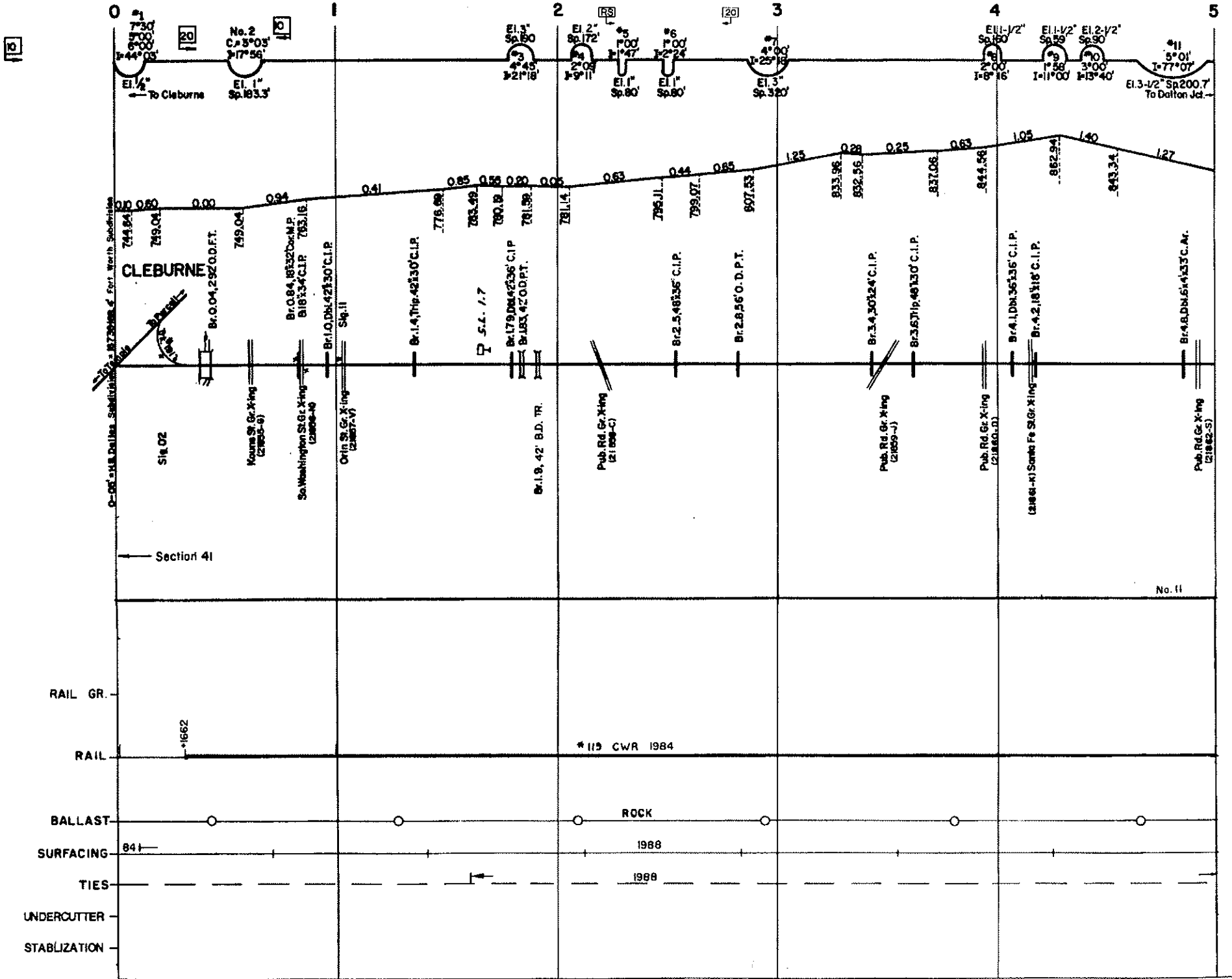


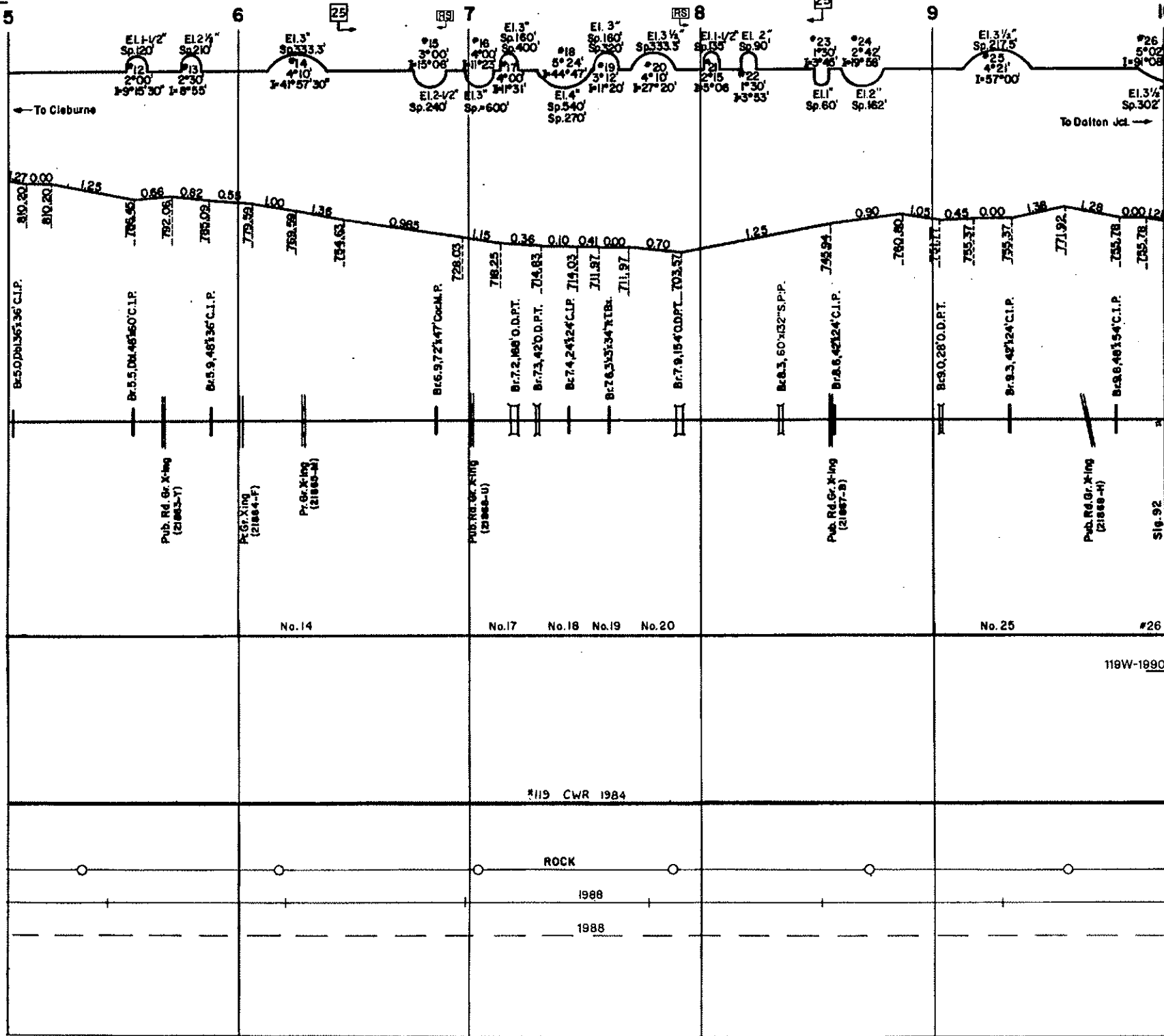




# Paris Subdivision

Farmersville (M.P. 91.0) to Zacha Jct. (M.P. 62.6)





119W-1990 CURVES NR SR

RAIL GR.

RAIL

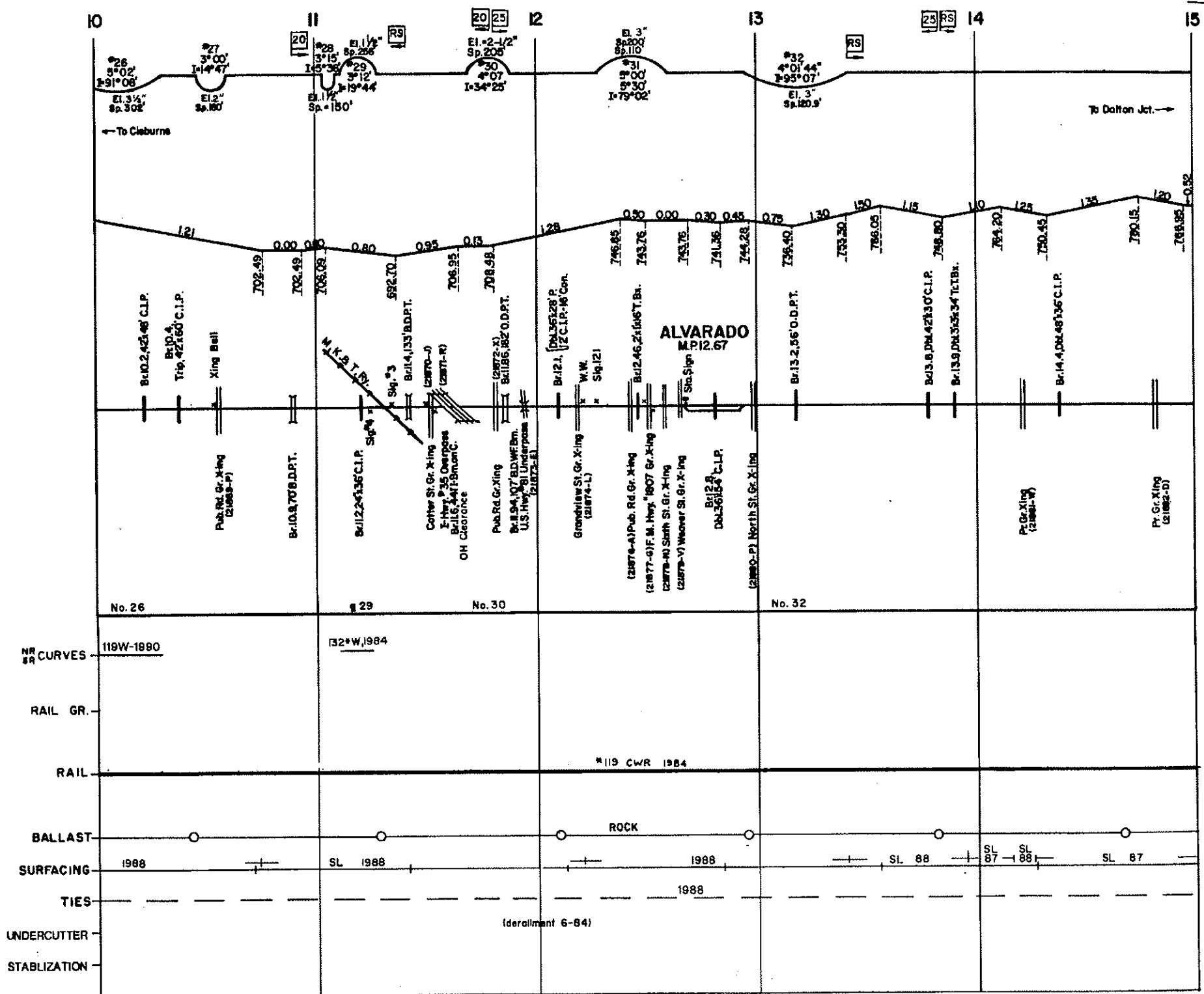
BALLAST

SURFACING

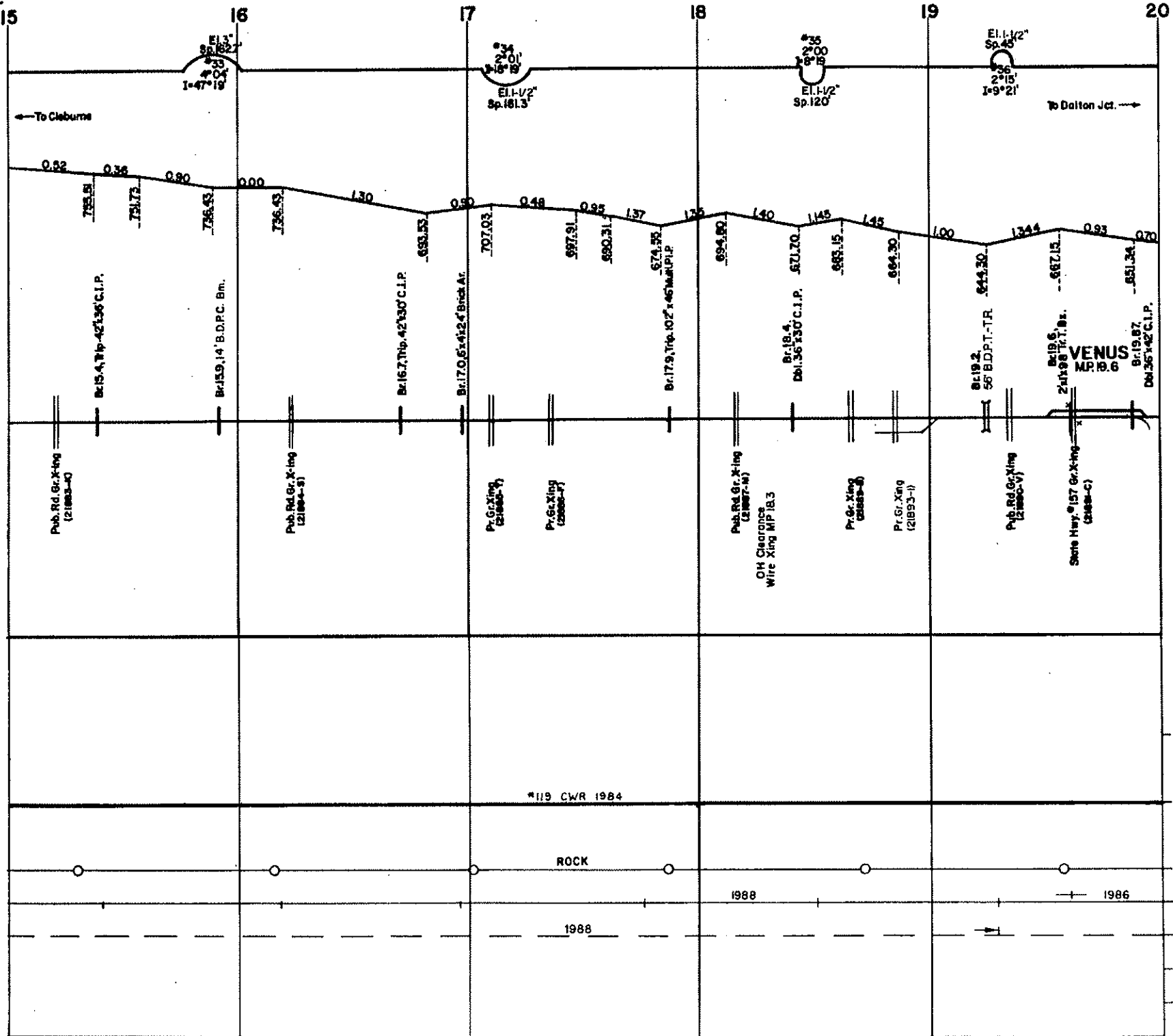
TIES

UNDERCUTTER

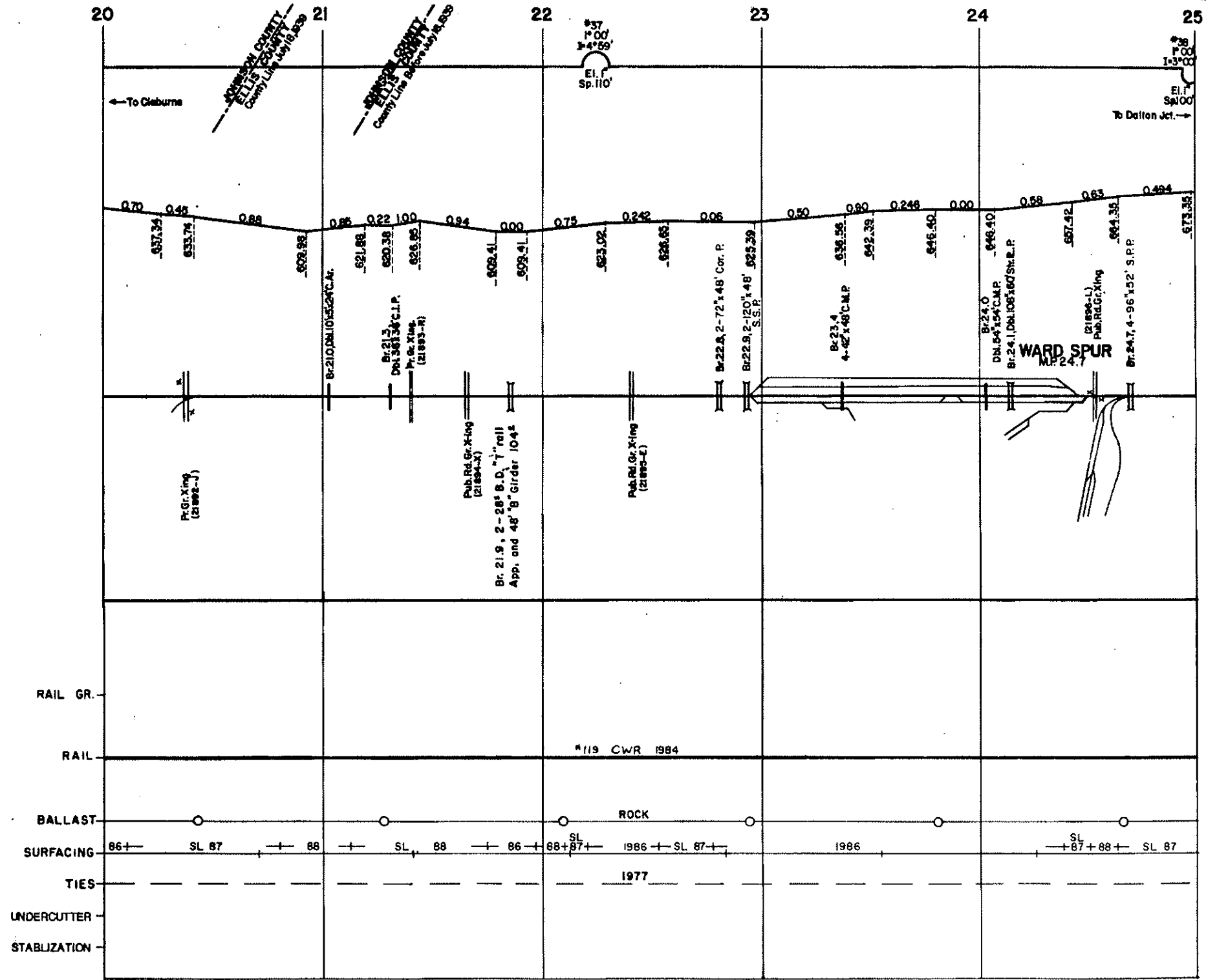
STABILIZATION

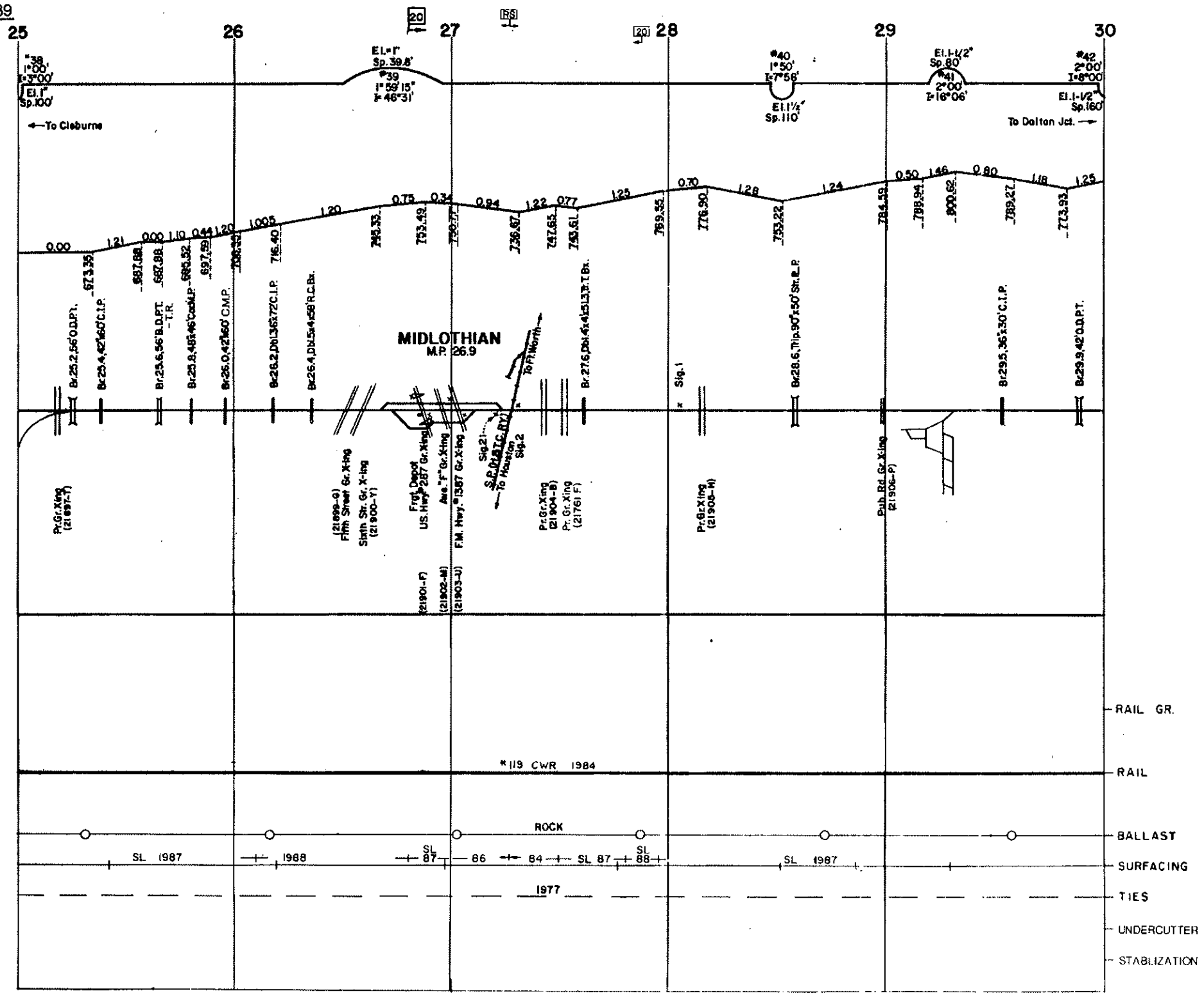


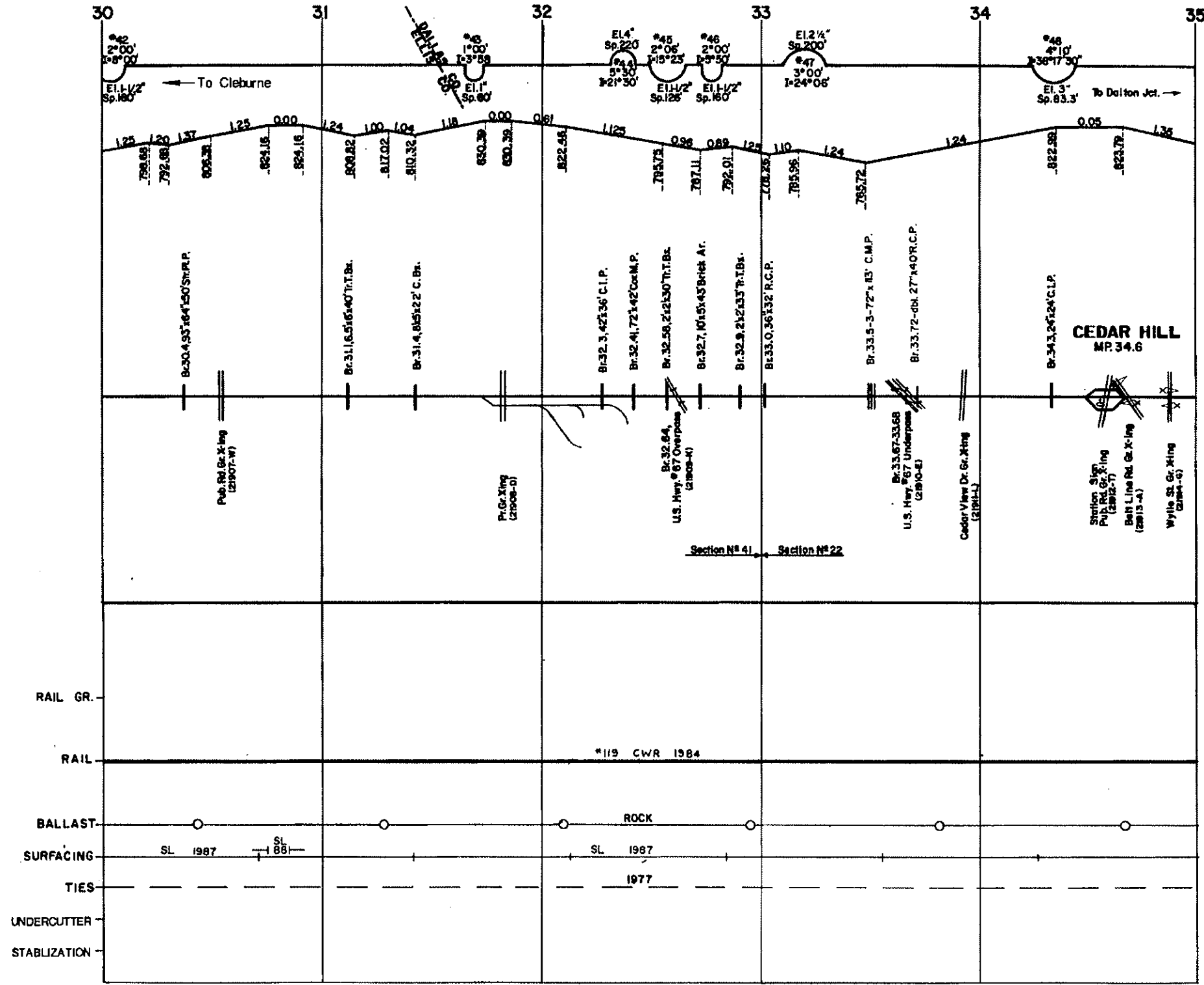
REVISED 3-1991

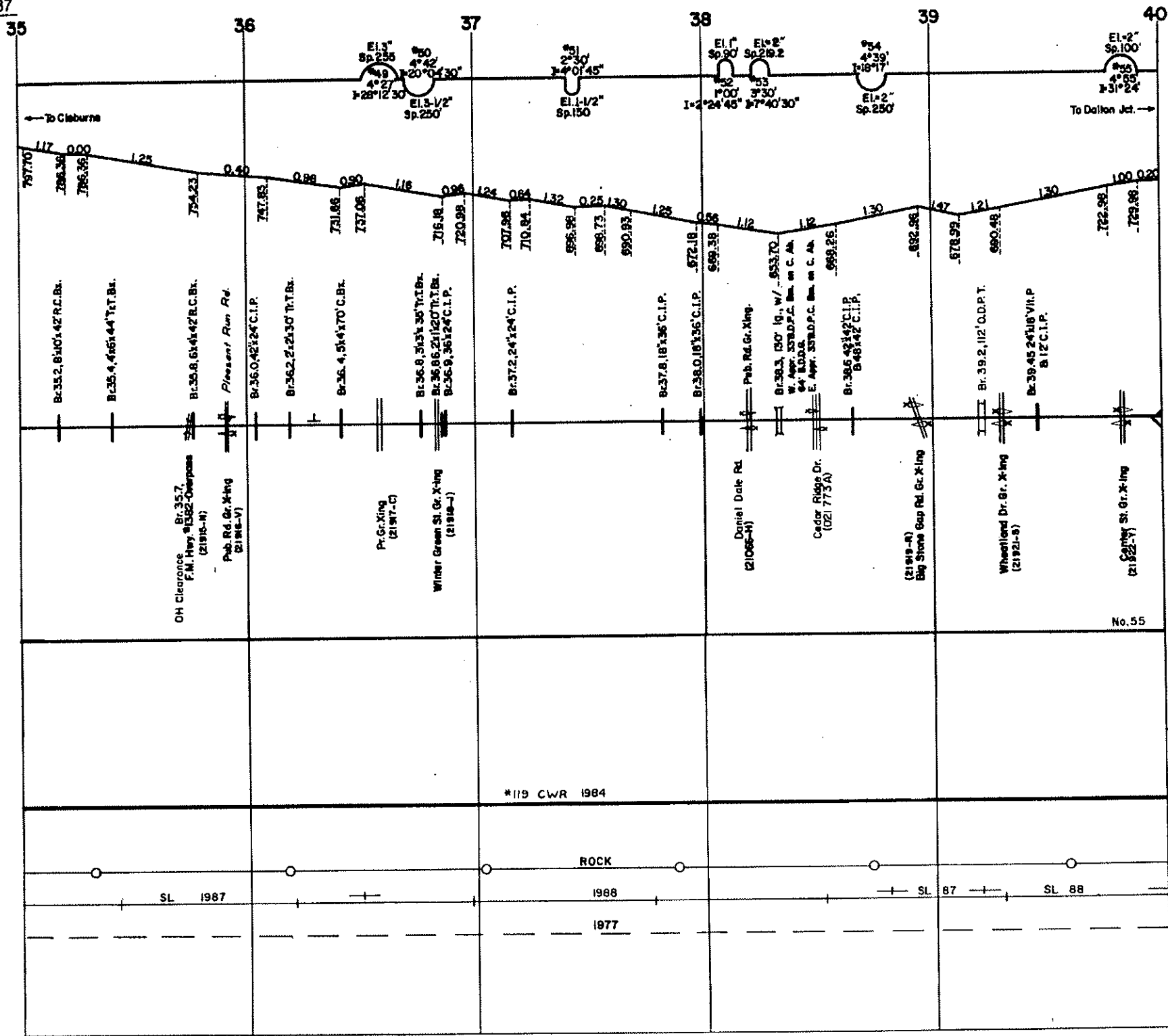






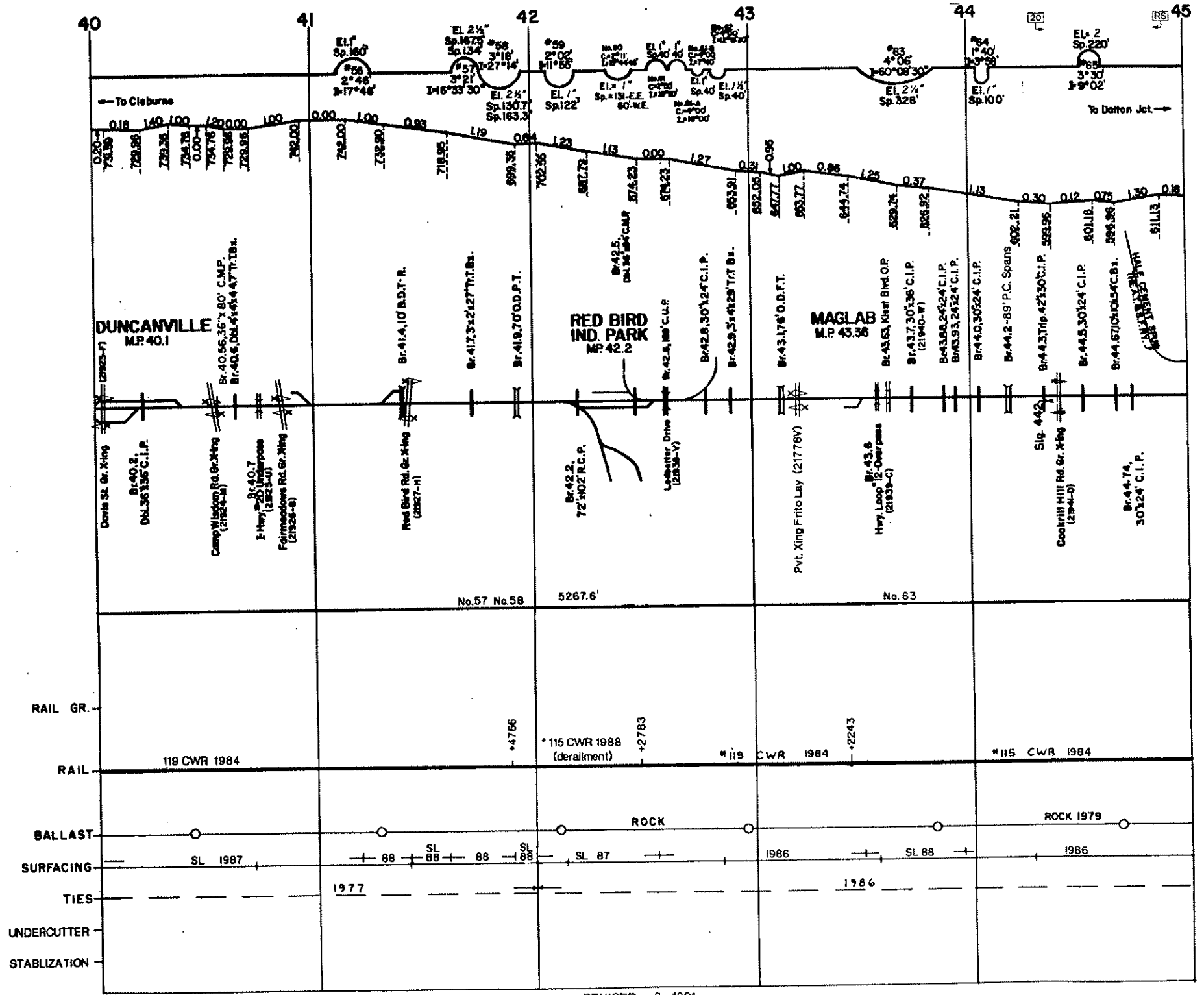


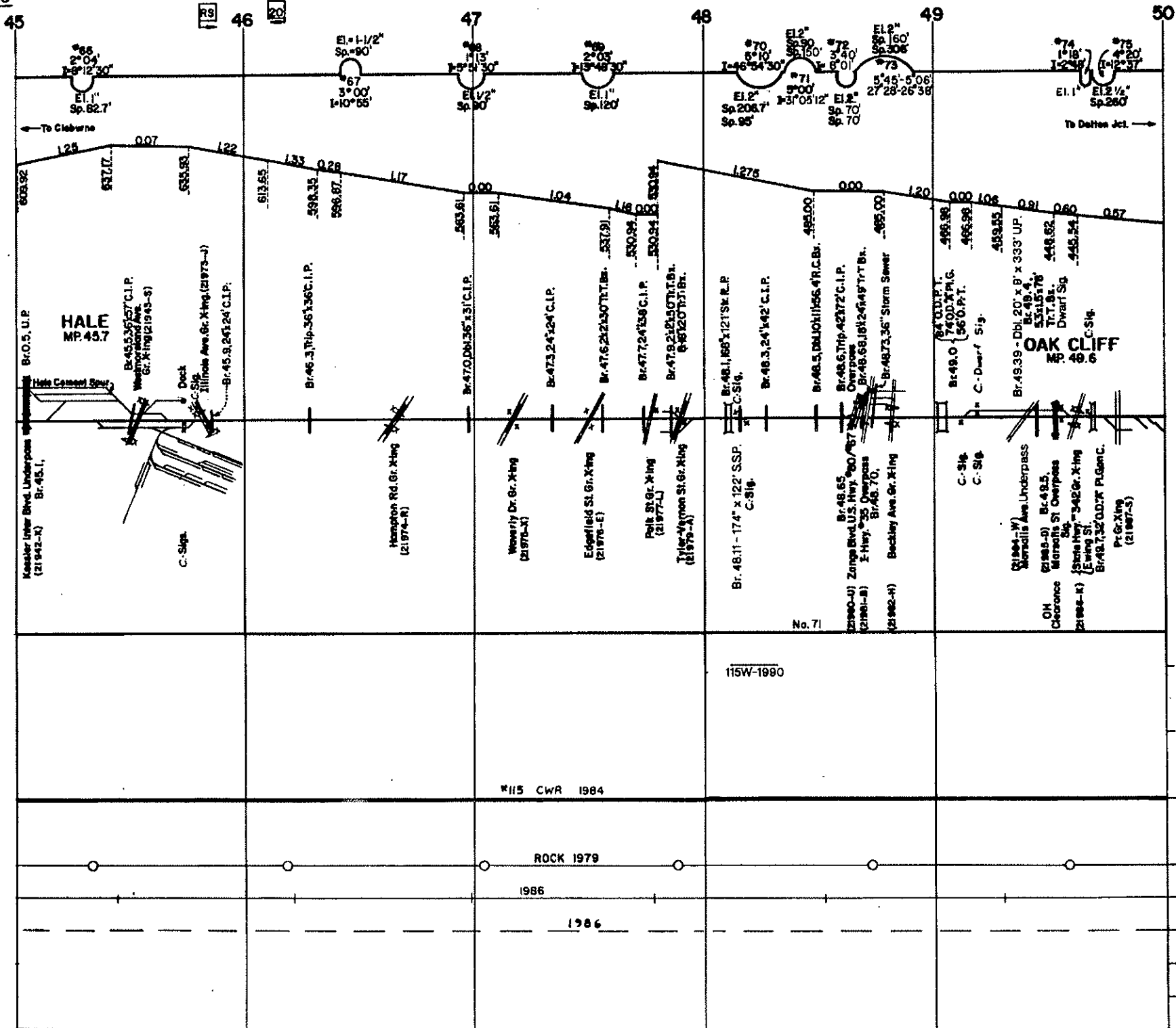




- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION

\*119 CWR 1984





CURVES RR

RAIL GR.

RAIL

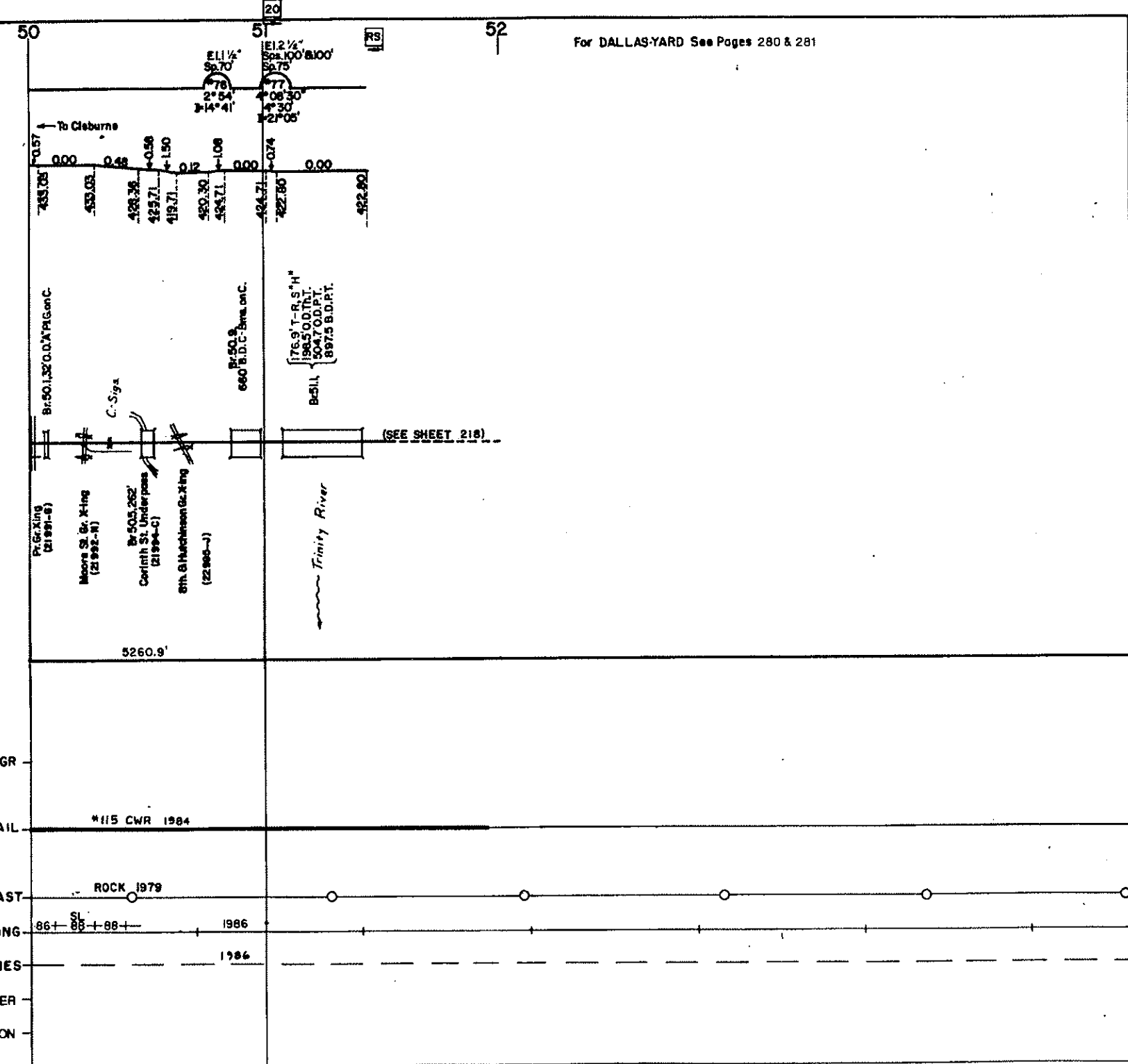
BALLAST

SURFACING

TIES

UNDERCUTTER

STABILIZATION



For DALLAS-YARD See Pages 280 & 281

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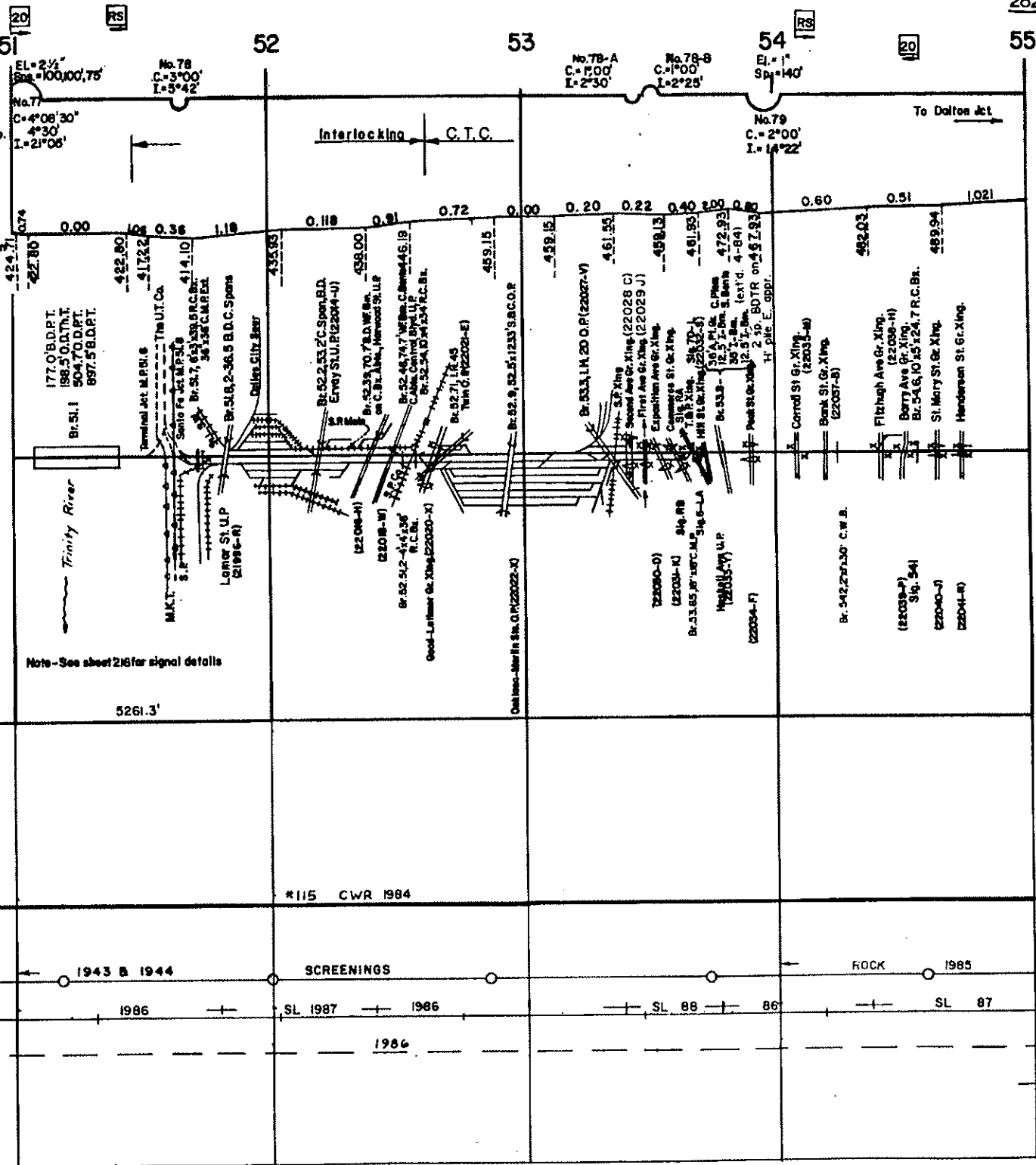
To Cleburne

To Dallas Jct

Contract 22291-A,B, dated 8-23-1937 covers joint use of A.T.&S.F. terminals by L.B.A. Ry. Co.

Contract 22090-A, dated 8-23-1937 covers joint use of A.T.&S.F. Main track and passing tracks from Frisco joint zone to Farmersville.

Contract 27777, dated 4-30-1947, joint with A.T.&S.F., S.P. and City of Dallas covers the construction of S.P. Main track and the operation and maintenance of certain tracks under the Dallas Master Plan.



RAIL GR.  
 RAIL  
 BALLAST  
 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION

Note - See sheet 218 for signal details

5261.3'

\*115 CWR 1984

1943 & 1944

SCREENINGS

ROCK 1985

1986

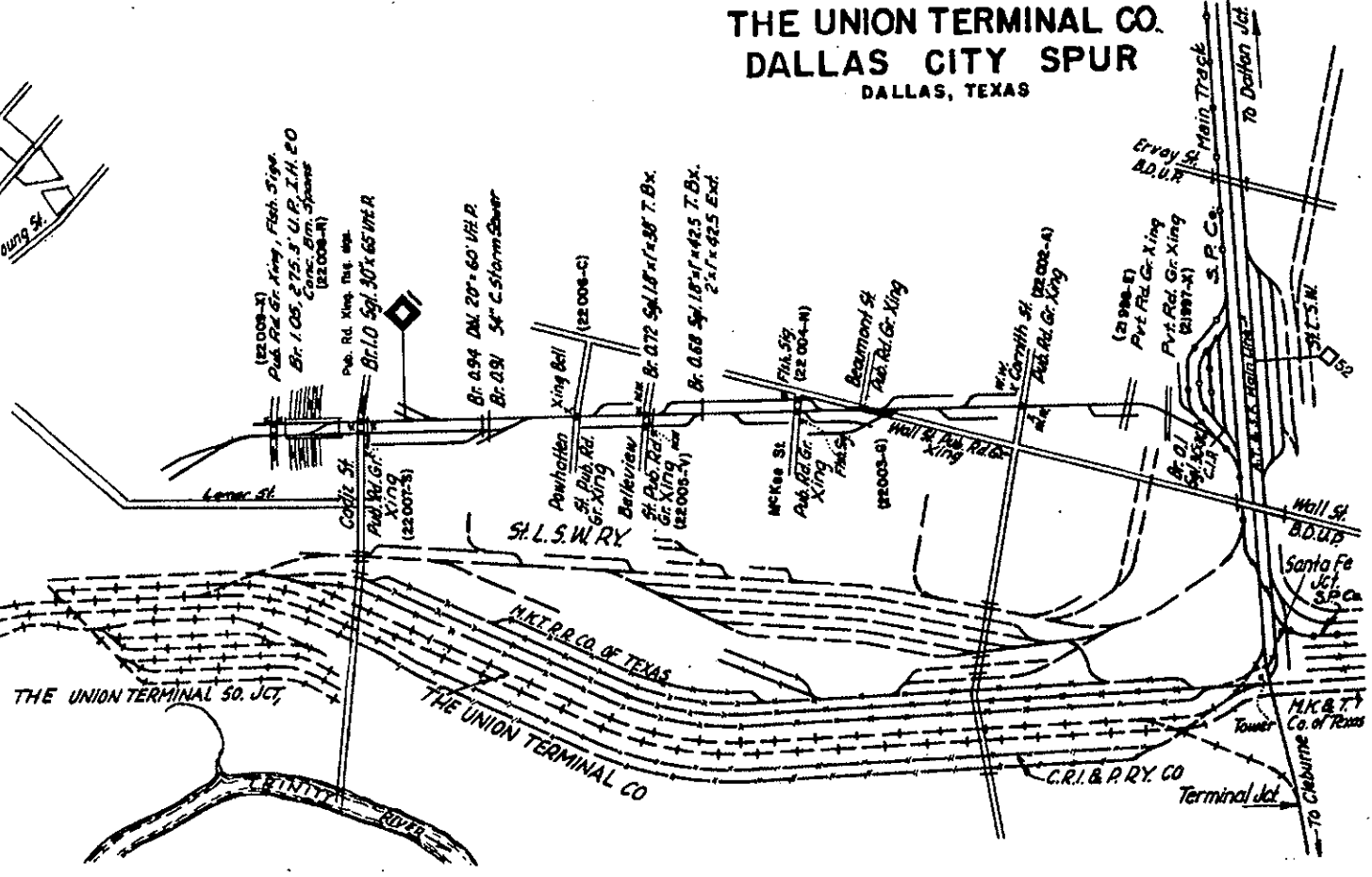
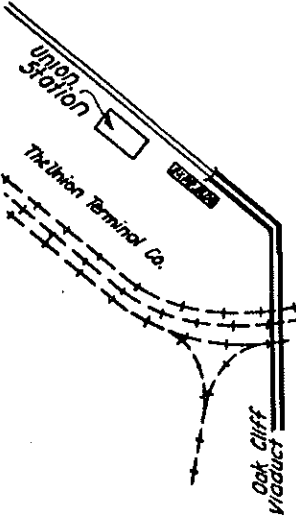
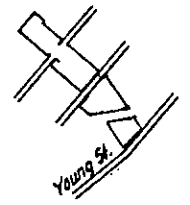
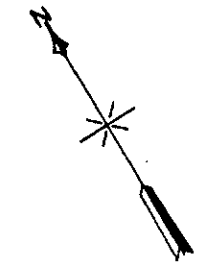
SL 1987 1986

SL 88 86

SL 87

1986

# THE UNION TERMINAL CO. DALLAS CITY SPUR DALLAS, TEXAS



(22008-X)  
Pub. Rd. Gr. Xing, Flash Sign  
Br. 1.05, 275' E. U.P. I.H. 20  
C.C. Co. Brm. Spans  
(22008-X)

Pub. Rd. Xing, This side  
Br. 1.0 Spl. 30' x 65' V.H.R.

Br. 0.94 DL 20' x 60' V.H.R.  
Br. 0.91 54' C. Storm Sewer

Xing Bell  
(22008-C)

Br. 0.72 Spl. 18' x 18' x 30' T. Bx.  
Br. 0.68 Spl. 18' x 18' x 42.5' T. Bx.  
2' x 1' x 42.5' Ext.

McKee St.  
Pub. Rd. Gr. Xing  
(22004-A)

Beaman St.  
Pub. Rd. Gr. Xing  
(22003-A)

W.W. Cornith St.  
Pub. Rd. Gr. Xing  
(22002-A)

(22001-E)  
Pvt. Rd. Gr. Xing  
Pvt. Rd. Gr. Xing  
(21991-X)

S. P. Co.

S.L.S.W.R.Y.

S.L.S.W.R.Y.

M.K.T. & Co. of Texas

THE UNION TERMINAL CO.

THE UNION TERMINAL SO. JCT.

C.R.I. & P.R.Y. CO.

M.K.T. & Co. of Texas

Terminal Jct.

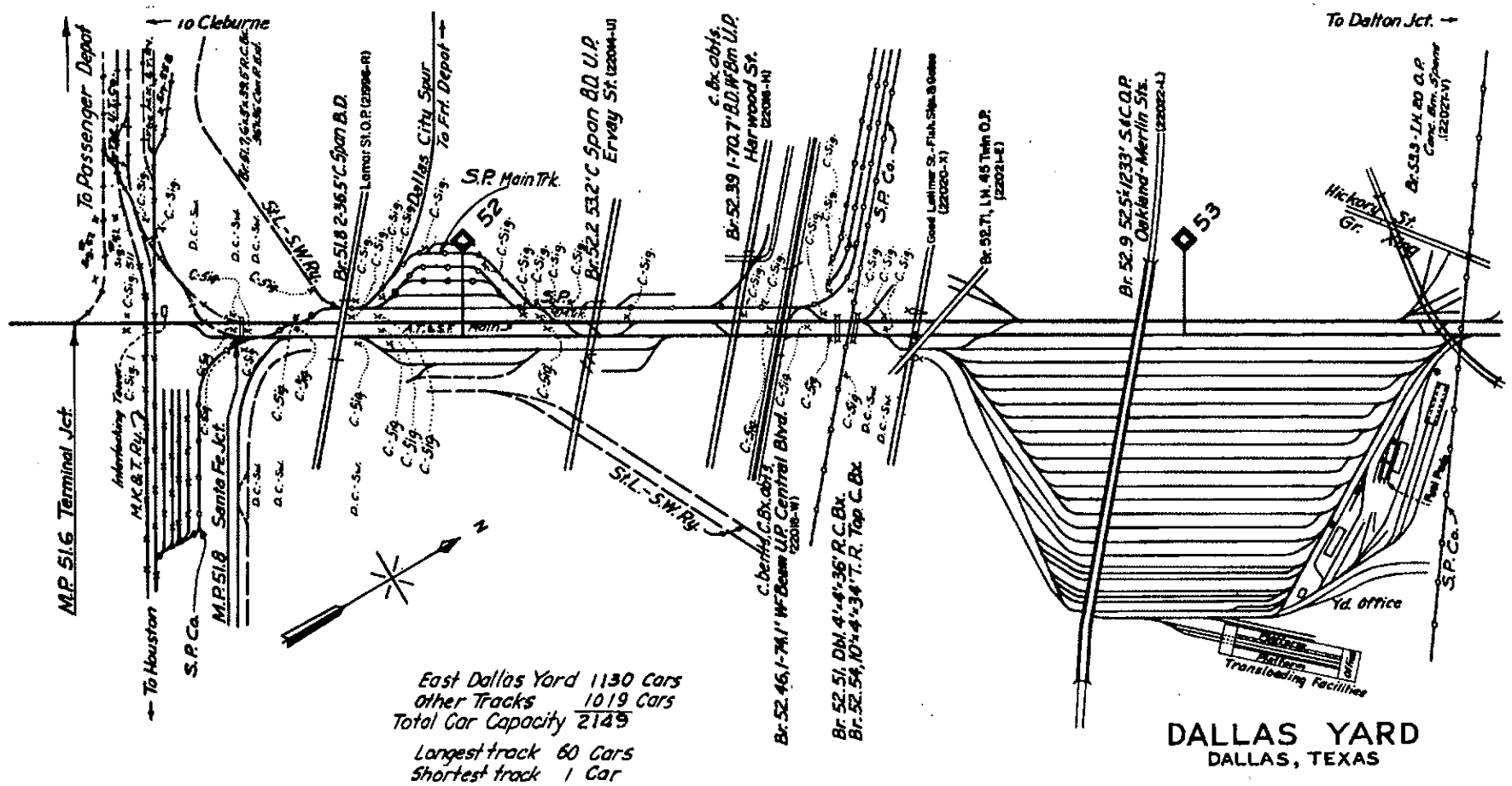
To Carburn

Wall St.  
B.D.U.P.  
Santa Fe Jct.  
S.P. Co.

Ervey St.  
A.D.U.R.

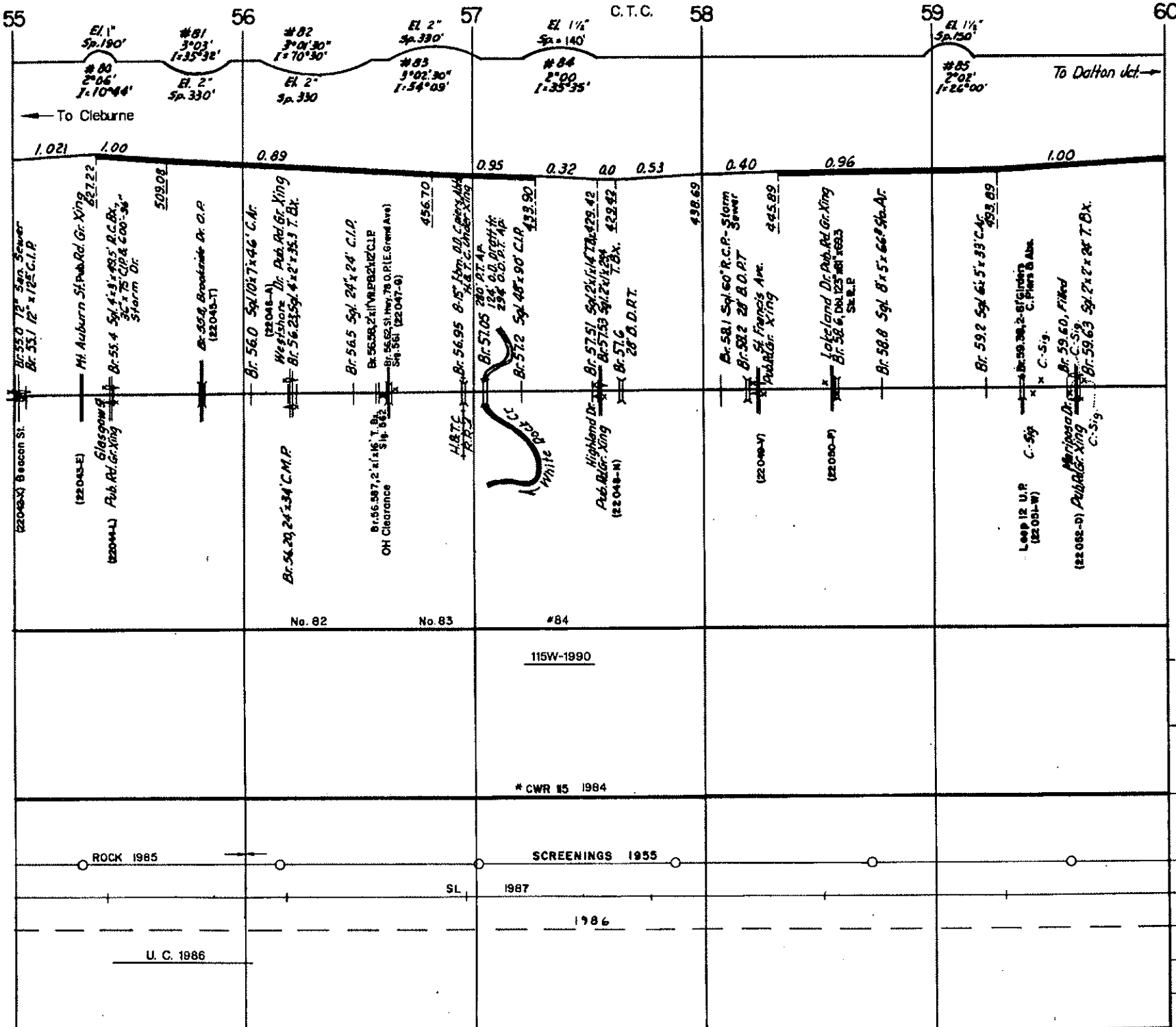
Main Trackage

To Dalton Jct.



East Dallas Yard 1130 Cars  
 other Tracks 1019 Cars  
 Total Car Capacity 2149  
 Longest track 60 Cars  
 Shortest track 1 Car

**DALLAS YARD**  
 DALLAS, TEXAS



C.T.C.

To Cleburne

To Dalton det.

1.021 1.00 0.89 0.95 0.32 0.0 0.53 0.40 0.96 1.00

(22048-X) Beacon St. Br. 53.0 72' 5.00' Sewer  
 Br. 53.1 12' x 12' C.I.P.  
 (22043-E) MI Auburn St. Pub. Rd. Gr. Xing 427.22  
 (22044-L) Glasgow St. Pub. Rd. Gr. Xing 427.22  
 Br. 53.4 54' 4.3' x 49.5' R.C.B.  
 36' x 75' C.I.P. & 600'-36"  
 Storm Dr.  
 509.08  
 Br. 50.2, Brookside Dr. O.P.  
 (22049-T)  
 Br. 56.0 54' 10.7' x 46' C.A.  
 (22048-A) Westshore Dr. Pub. Rd. Gr. Xing  
 Br. 56.23 54' 4.2' x 35.3' T. Bx.  
 Br. 56.5 54' 24' x 24' C.I.P.  
 Br. 56.58 24' 11' x 18' 2' C.I.P.  
 OH Clearance 3' 11' 0"  
 Br. 56.53 St. Hwy. 78 O.P.I.E. (Green Ave)  
 Sht. 561 (22047-6) 456.70  
 H.B.T.C. Br. 56.95 6' 57' 1.57' O.D. Catch Basin  
 4.0' x 7.0' C. Under Xing  
 Br. 57.05 20' 0' T.A.P.  
 Br. 57.12 54' 40' x 90' C.I.P. 433.90  
 Highland Dr. Pub. Rd. Gr. Xing 429.42  
 Br. 57.51 54' 21' x 14' T.B. 429.42  
 Pub. Rd. Gr. Xing Br. 57.59 54' 21' x 14' T.B. 429.42  
 (22048-N) Br. 57.6 20' 0' D.R.T.  
 Br. 58.1 54' 60' R.C.P. Storm  
 Sewer  
 Br. 58.2 20' 0' D.R.T.  
 (22048-W) St. Francis Ave. 445.89  
 Pub. Rd. Gr. Xing  
 (22048-P) Lakeland Dr. Pub. Rd. Gr. Xing  
 Br. 58.6 104' 125' x 81' x 69.3  
 S.H.E.P.  
 Br. 58.8 54' 5' x 66' 5/8" A.C.  
 Br. 59.2 54' 61' 5' x 83' C.A. 493.89  
 Loop 12 U.P. (22048-W) C-5/9  
 Br. 59.38 2'-0" 2'-0" 2'-0" 2'-0"  
 C. Piers & Abs.  
 C-5/9  
 Br. 59.60, Filled  
 (22048-D) Pub. Rd. Gr. Xing C-5/9  
 Br. 59.63 54' 2' x 28' T. Bx.  
 C-5/9

No. 82

No. 83

#84

115W-1990

\* CWR 15 1984

ROCK 1985

SCREENINGS 1955

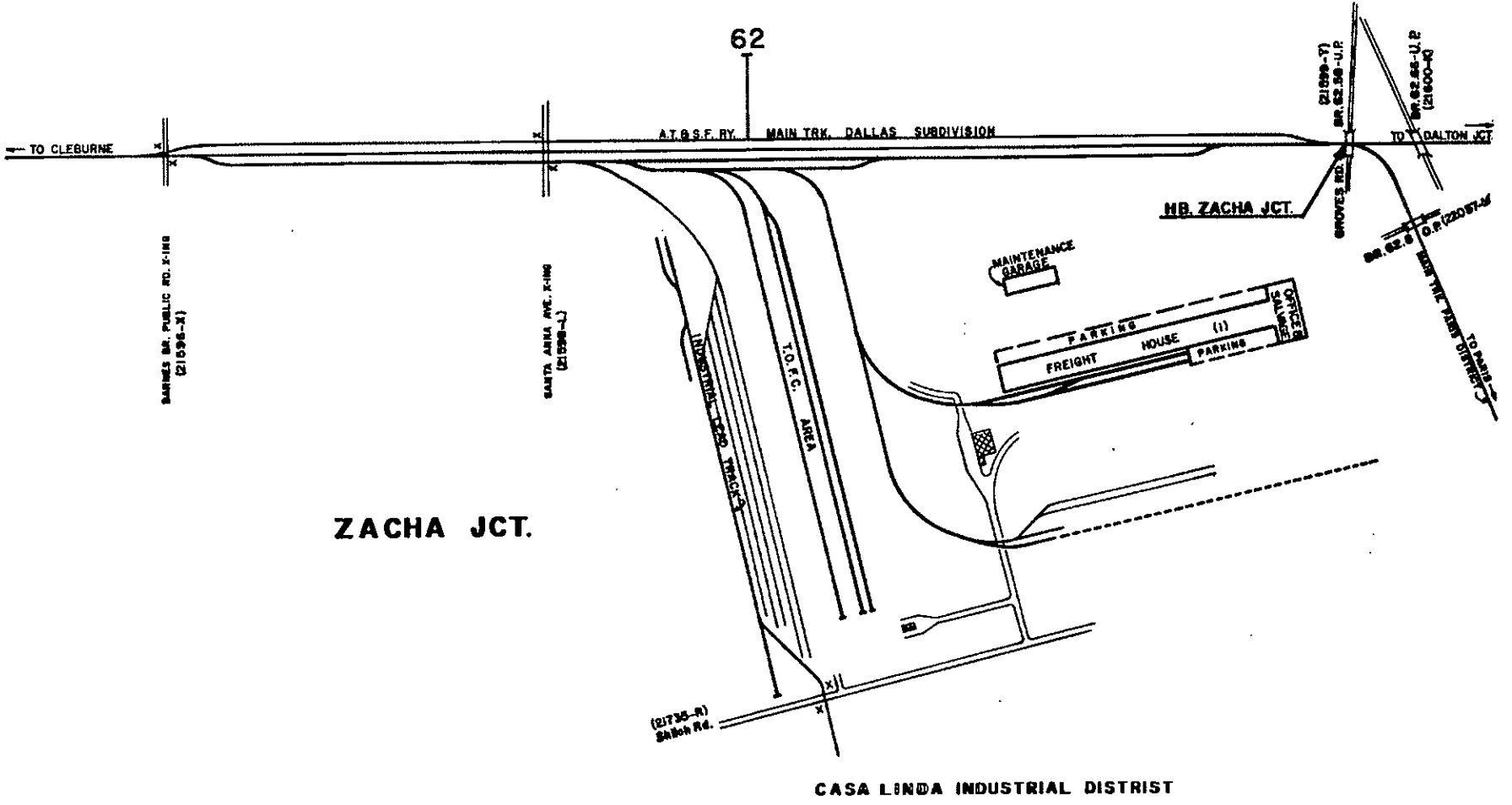
SL 1987

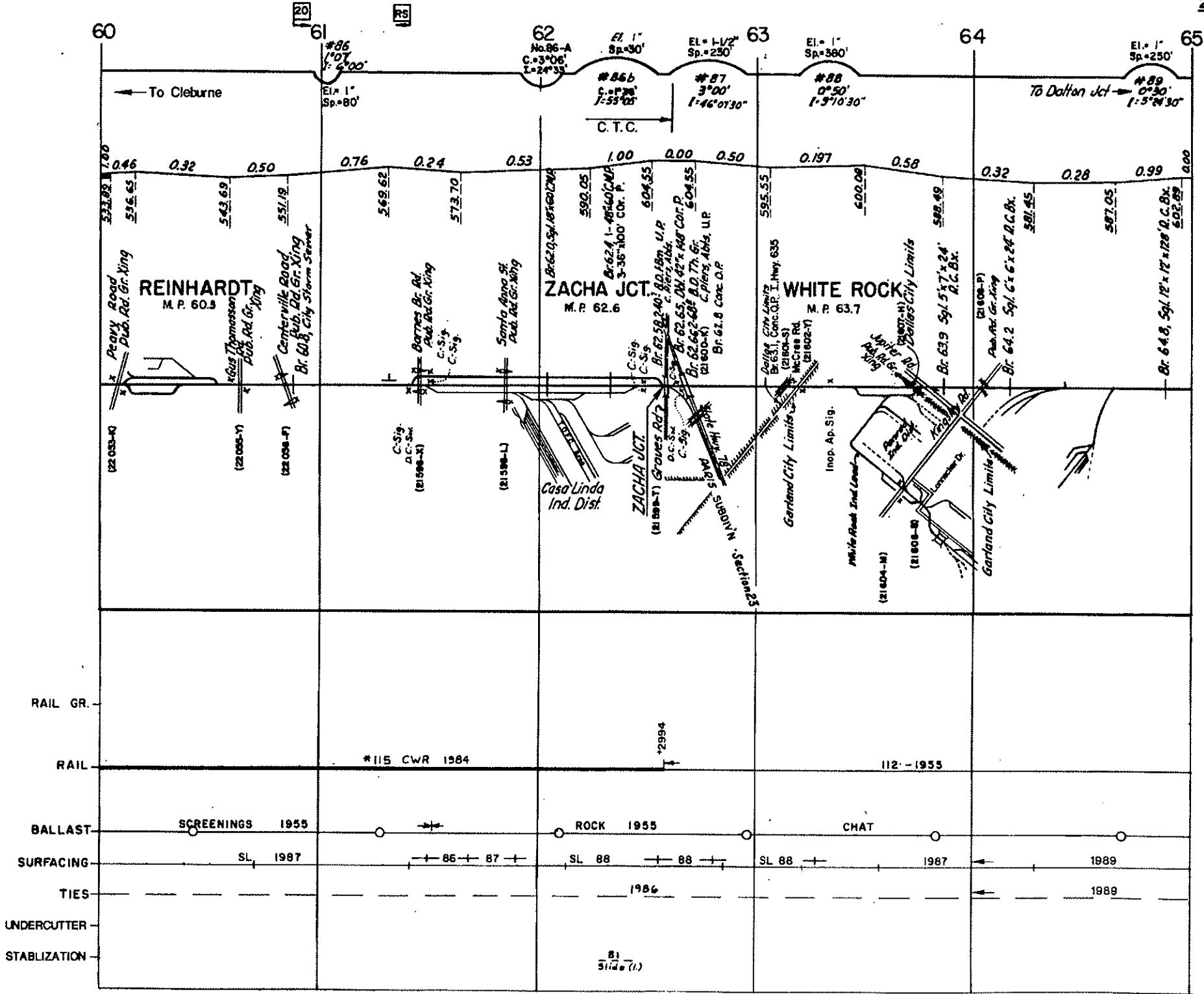
1986

U. C. 1986

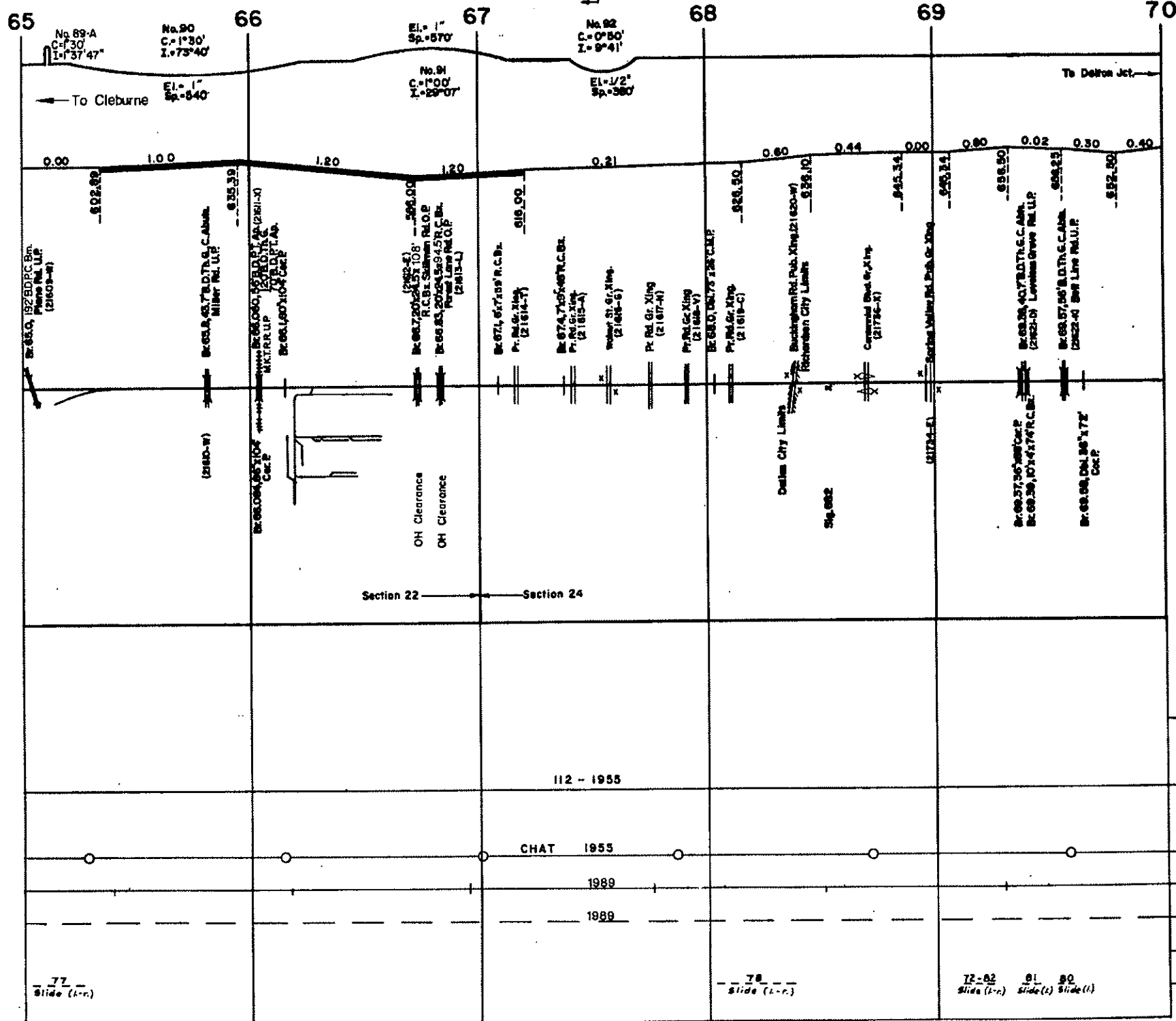
CURVES 95  
 RAIL GR.  
 RAIL  
 BALLAST  
 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION

*Intentionally Blank*

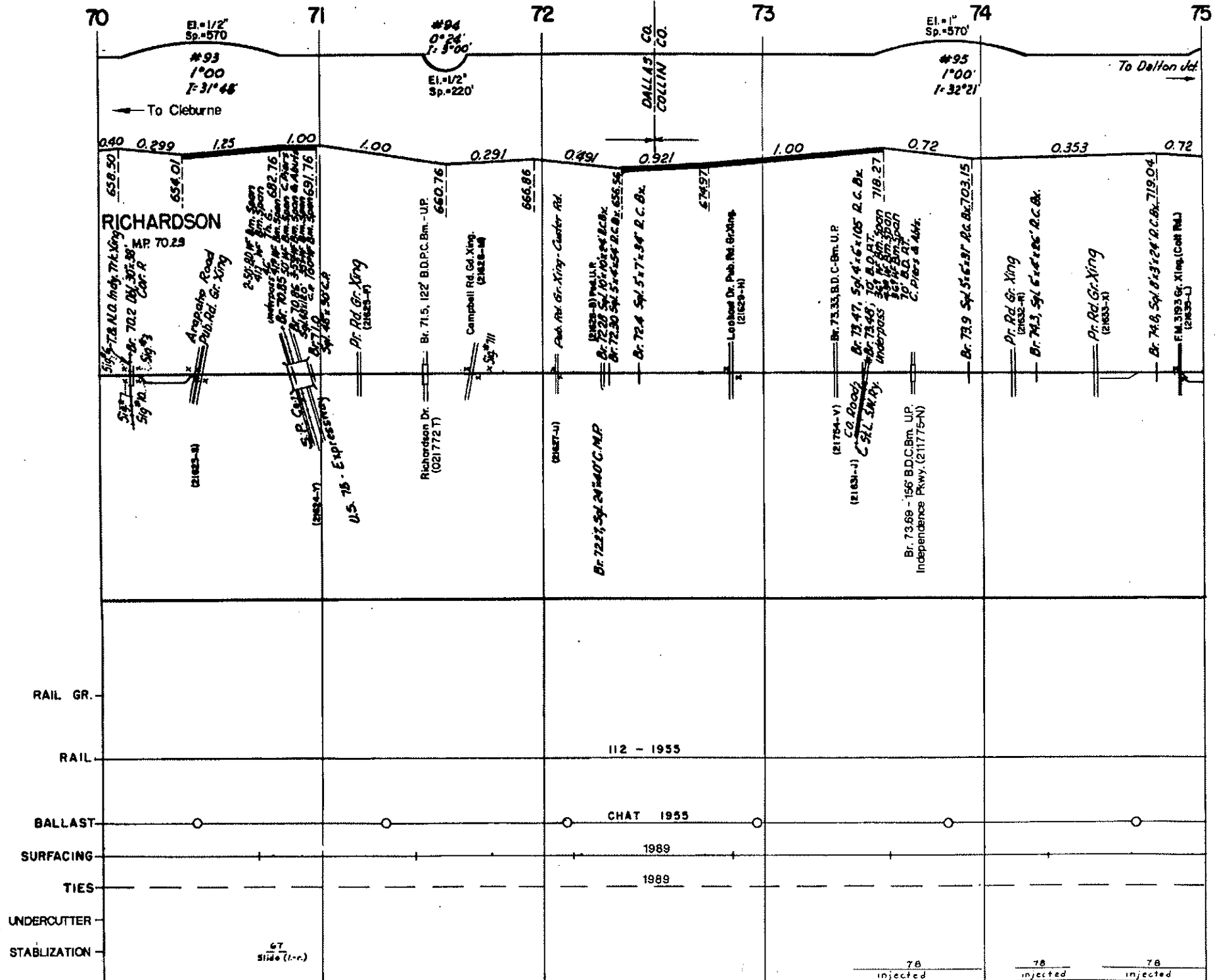




REVISED 3-1991







REVISED 3-1991

67  
5174 (1-C)

76  
injected

76  
injected

76  
injected

112 - 1955

CHAT 1955

1989

1989

RICHARDSON

MP 70.29

Apache Road  
Pub. Rd. Gr. Xing

(21833-4)

(21834-7)

U.S. 75 - Expressway

Richardson D.C. Bm. - U.P.  
(021772 T)

(21837-1)

Dr. 72.27, Spl. 24' x 40' C.M.P.

Leobard Dr. Pub. Rd. Grading  
(21829-N)

(21794-V)

Co. Road  
C.H.L. S.W. Ry.

Br. 73.69 - 156 B.D.C. Bm. U.P.  
Independence Pkwy. (211775-N)

Br. 73.9 Spl. 56' x 81' R.C. Bx. 710.15

Pr. Rd. Gr. Xing  
(21832-M)

Br. 74.3 Spl. 64' x 86' R.C. Bx.

Pr. Rd. Gr. Xing  
(21833-N)

Br. 74.8 Spl. 83' x 28' R.C. Bx. 719.04

F.M. 3193 Gr. Xing (Cont. Pkwy.)  
(21839-L)

0.40 0.299 1.25 1.00 1.00 0.291 0.491 0.921 1.00 0.72 0.353 0.72

El.=1/2'  
Sp.=570  
#93  
1°00'  
1-31°48'

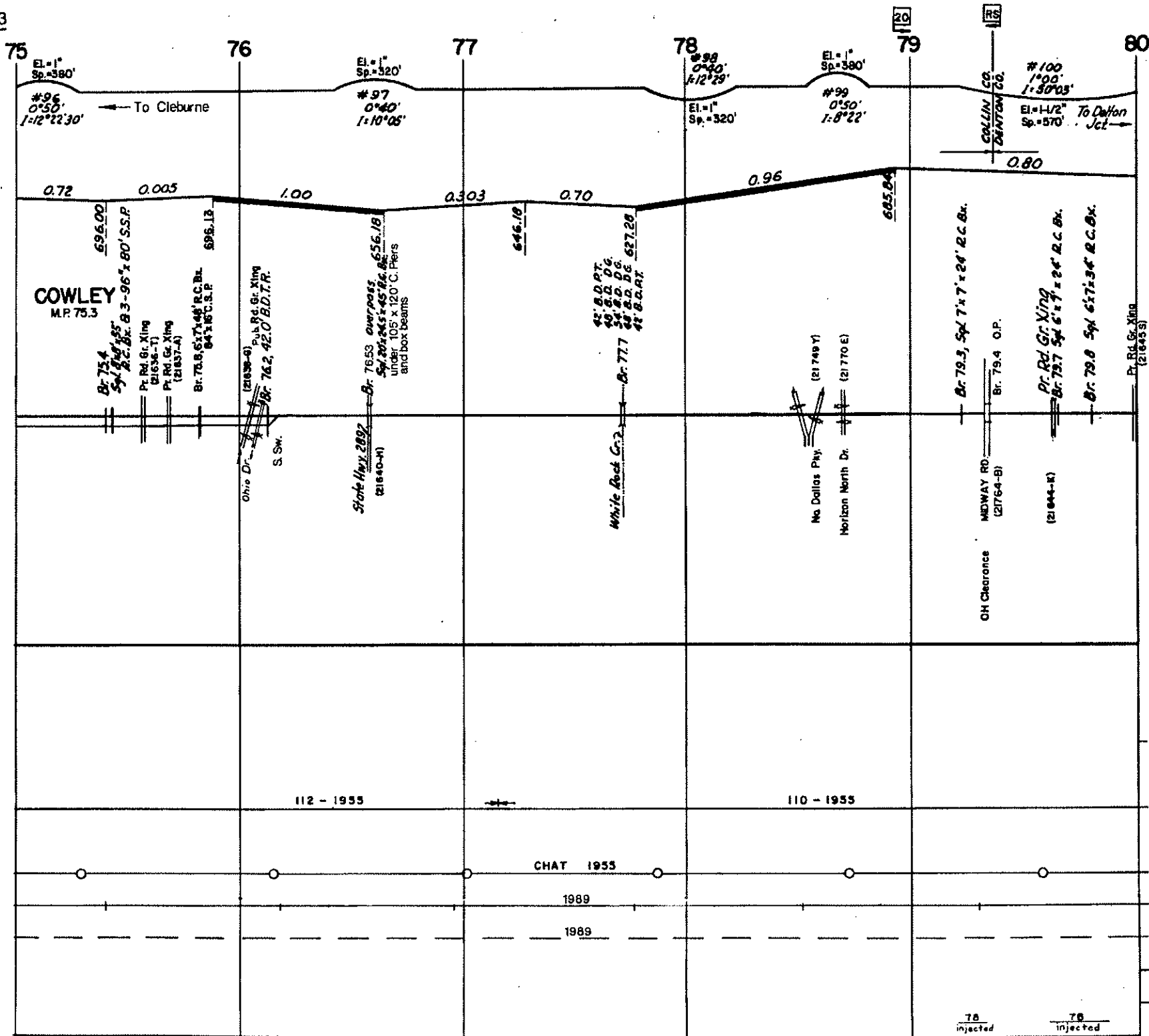
#94  
0°24'  
1-5°00'  
El.=1/2'  
Sp.=220'

El.=1'  
Sp.=570  
#95  
1°00'  
1-32°21'

DALLAS CO.  
COLLIN CO.

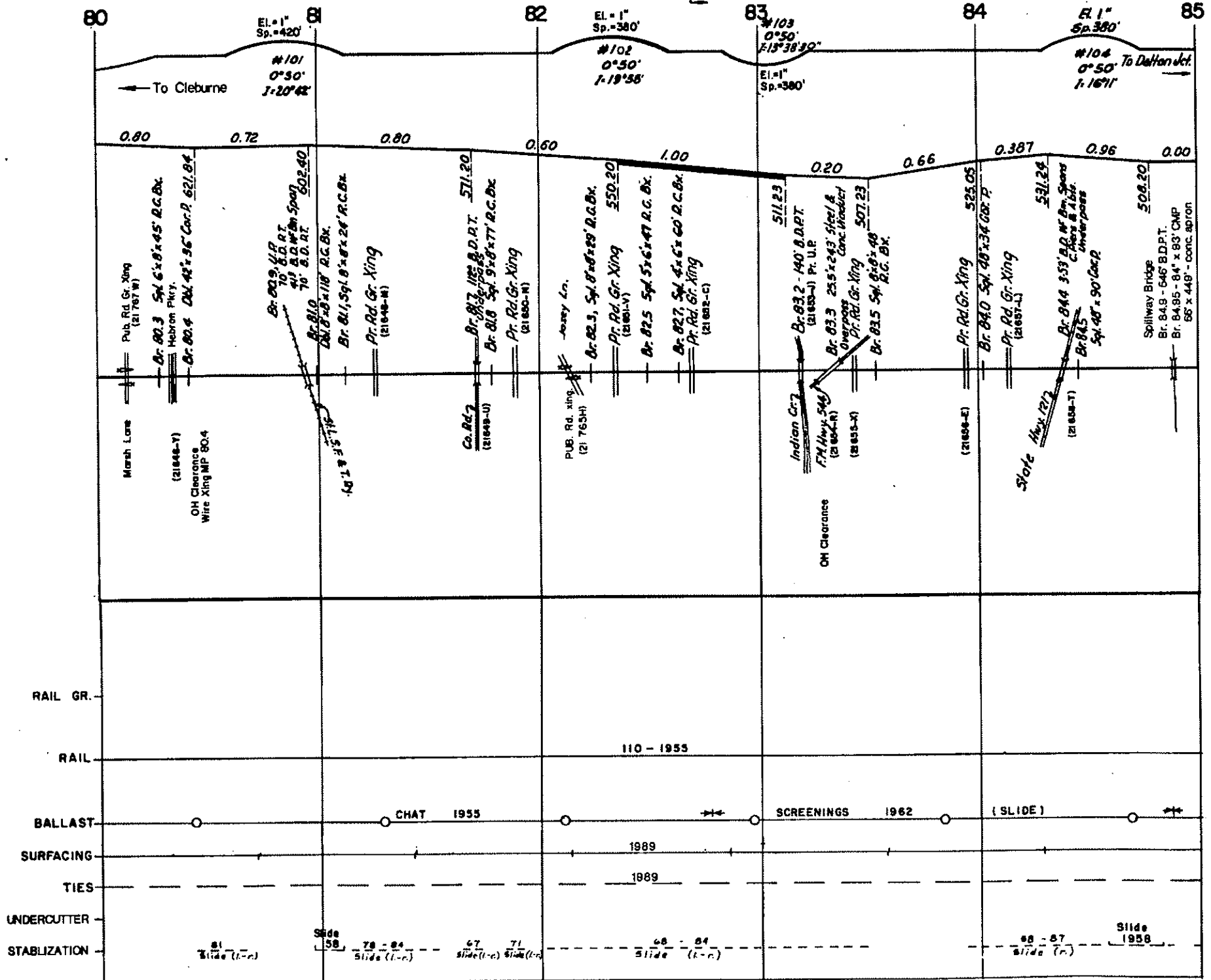
To Cleburne

To Dalton Jct

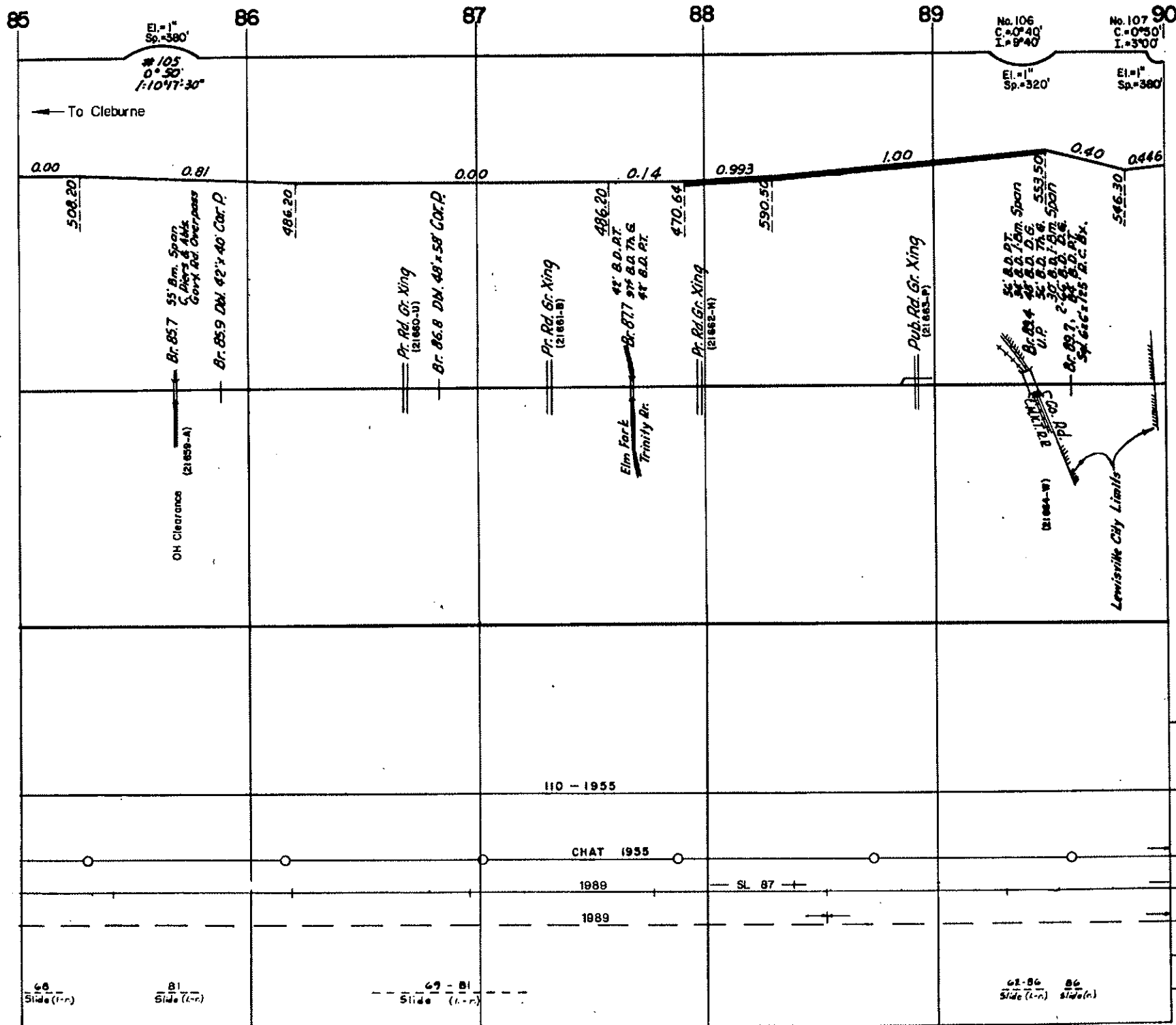


RAIL GR.  
 RAIL  
 BALLAST  
 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION

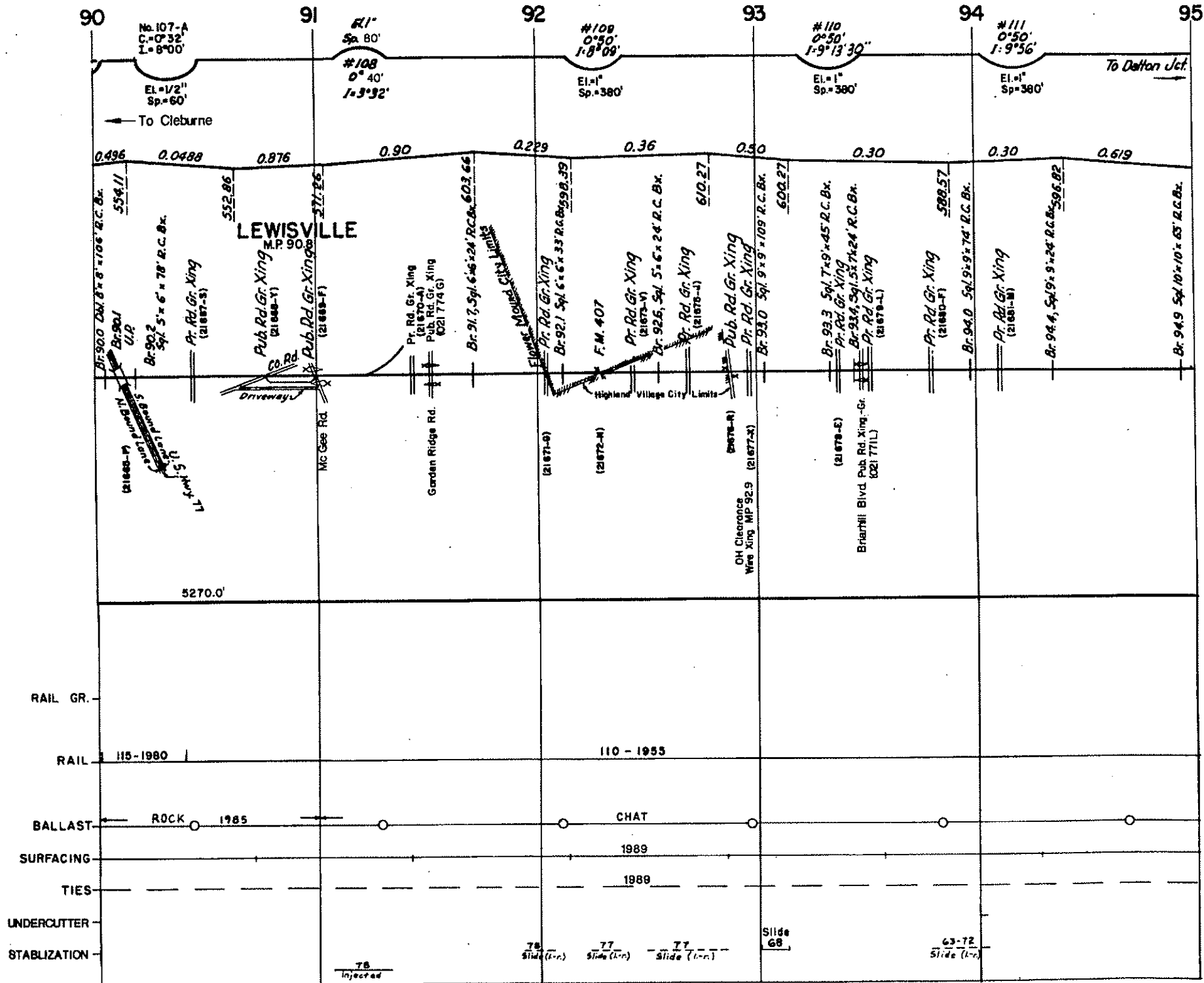
78 injected      79 injected

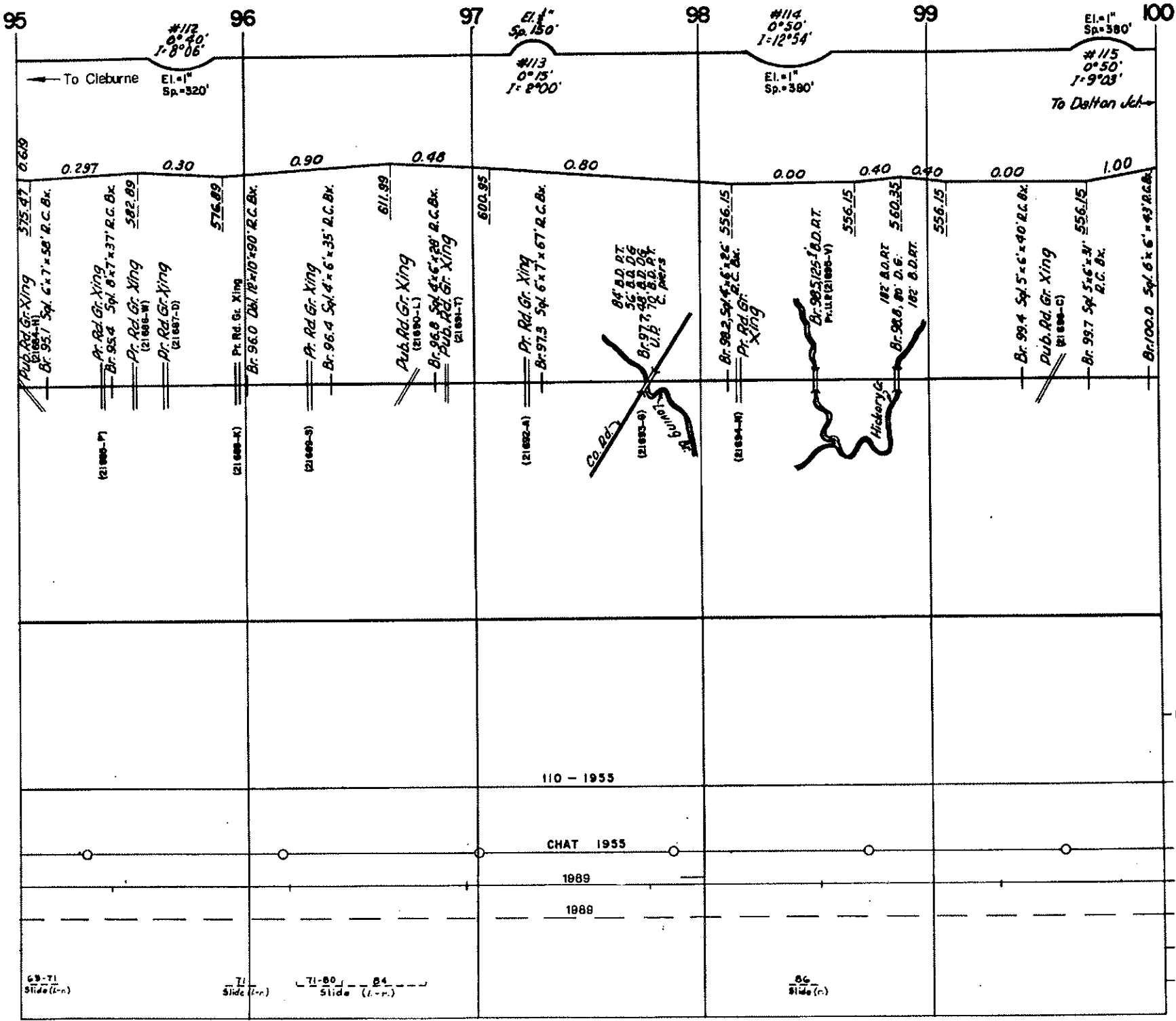


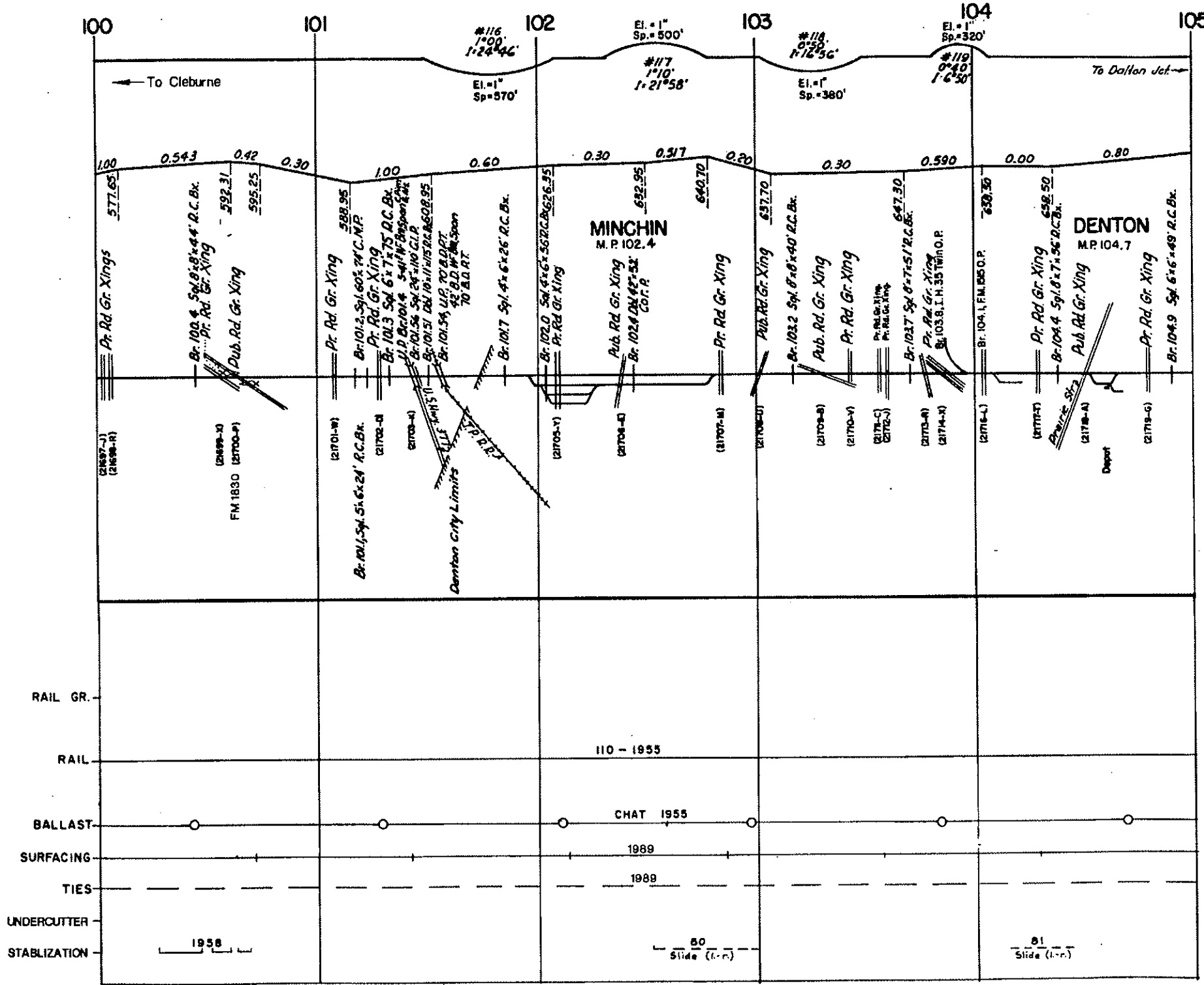
REVISED 3-1991



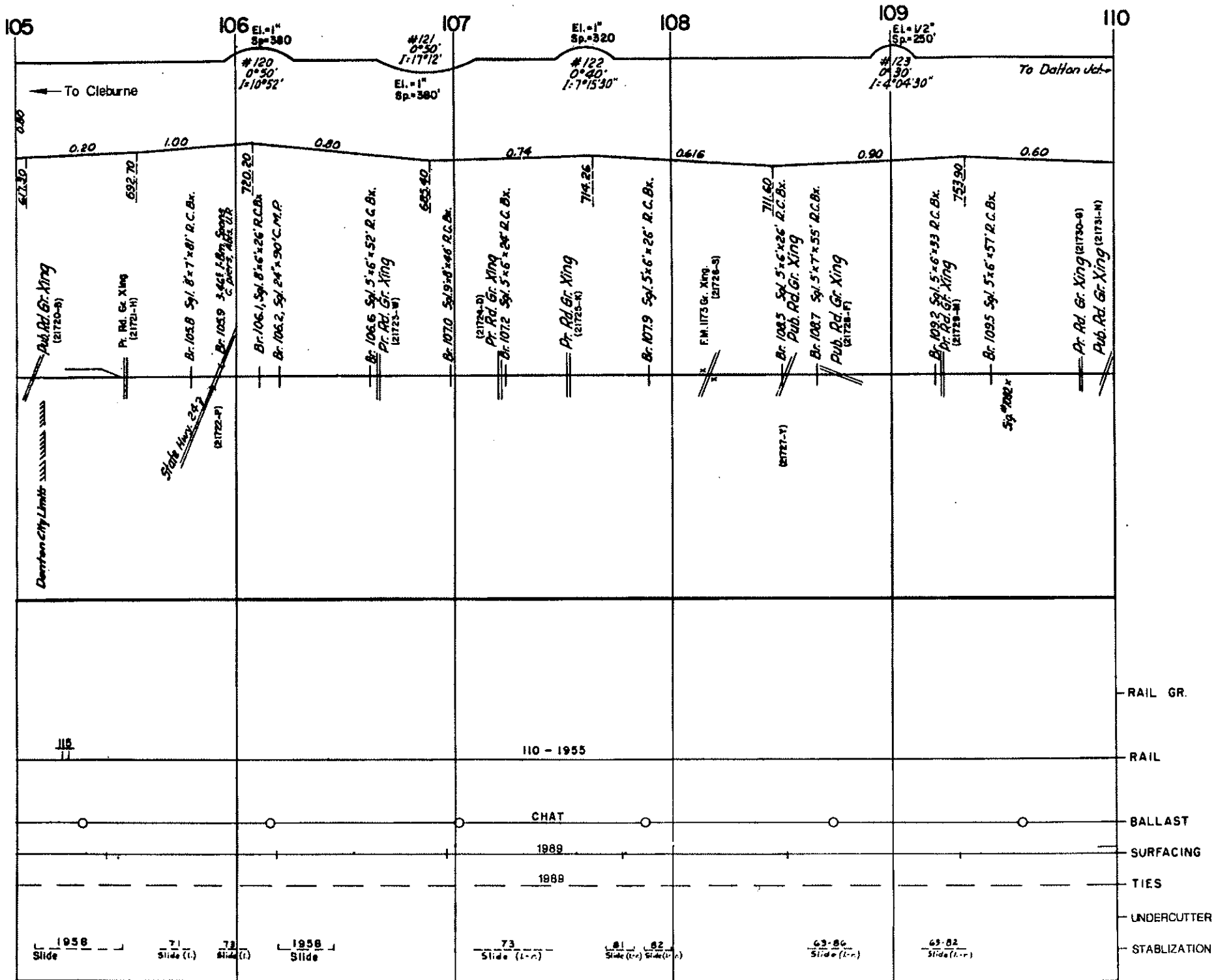
RAIL GR.  
 RAIL  
 BALLAST  
 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION



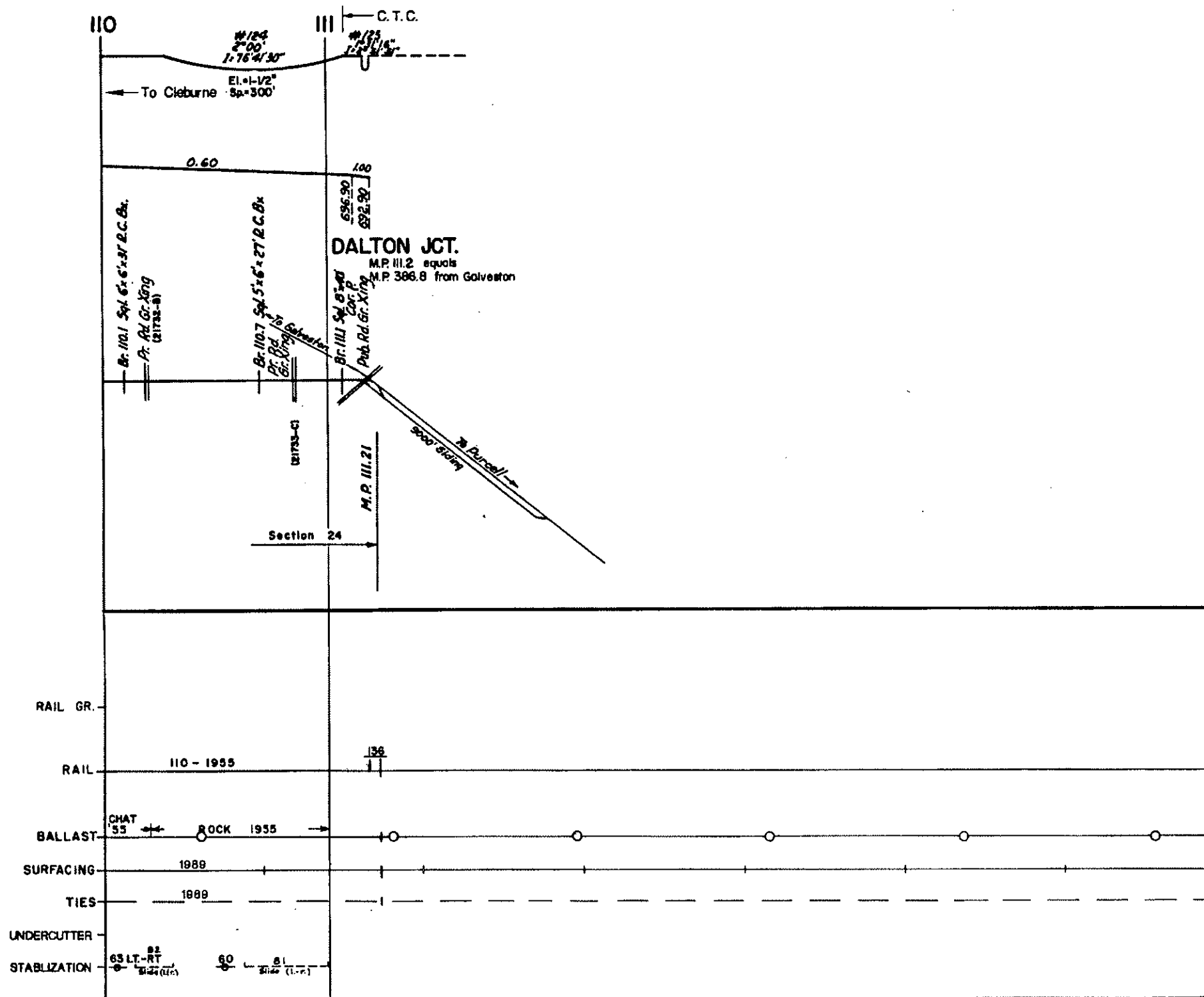




REVISED 3-1991

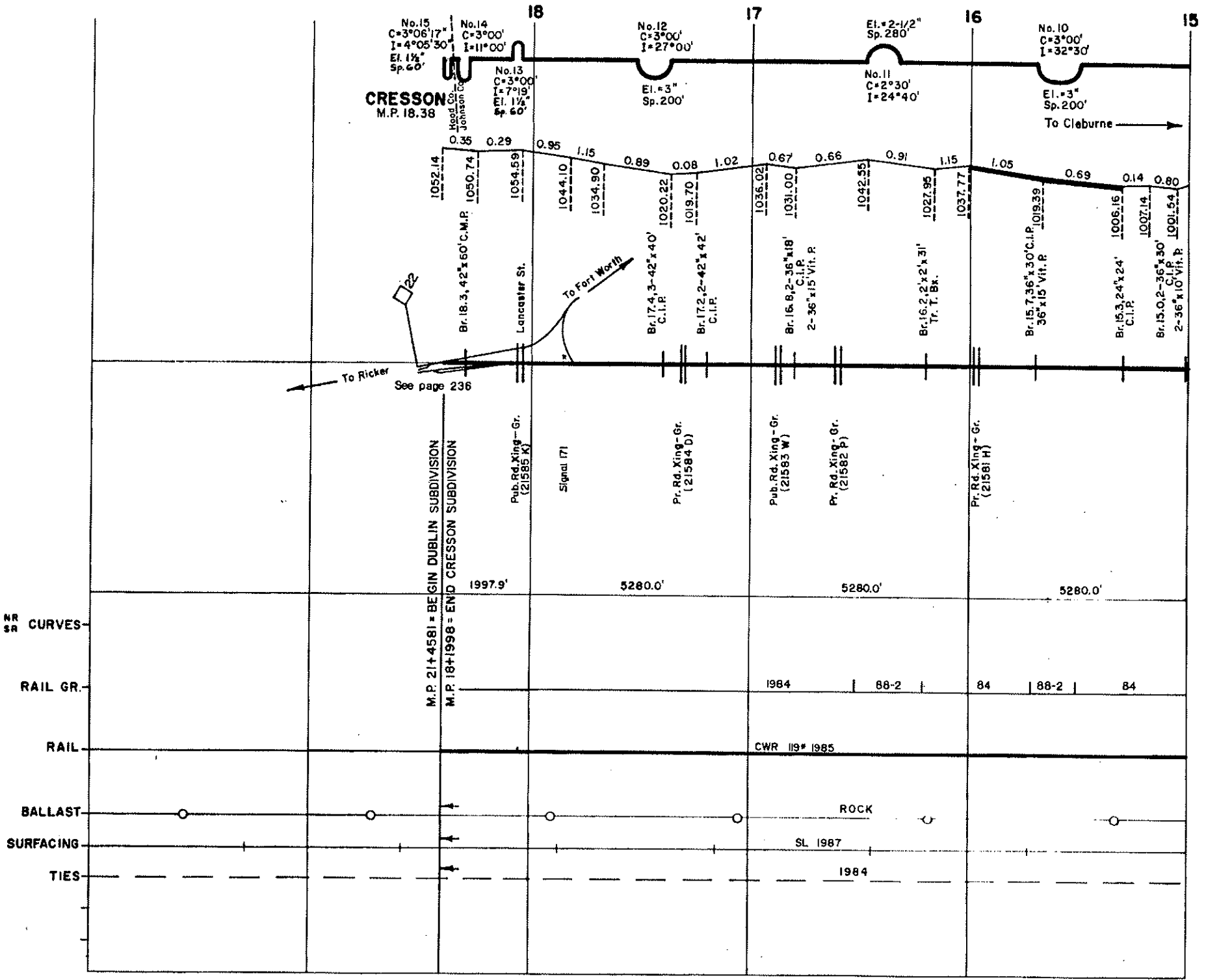




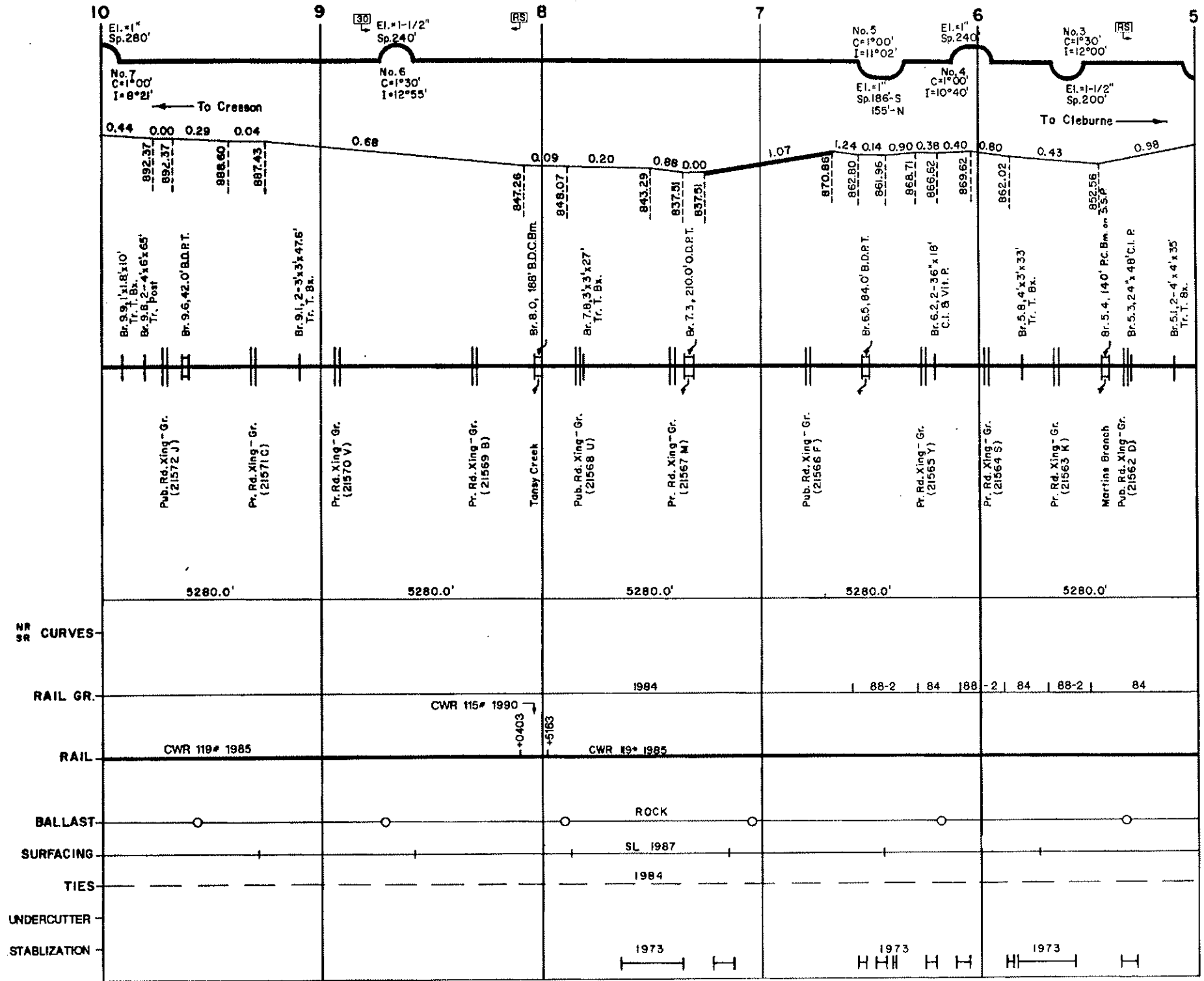


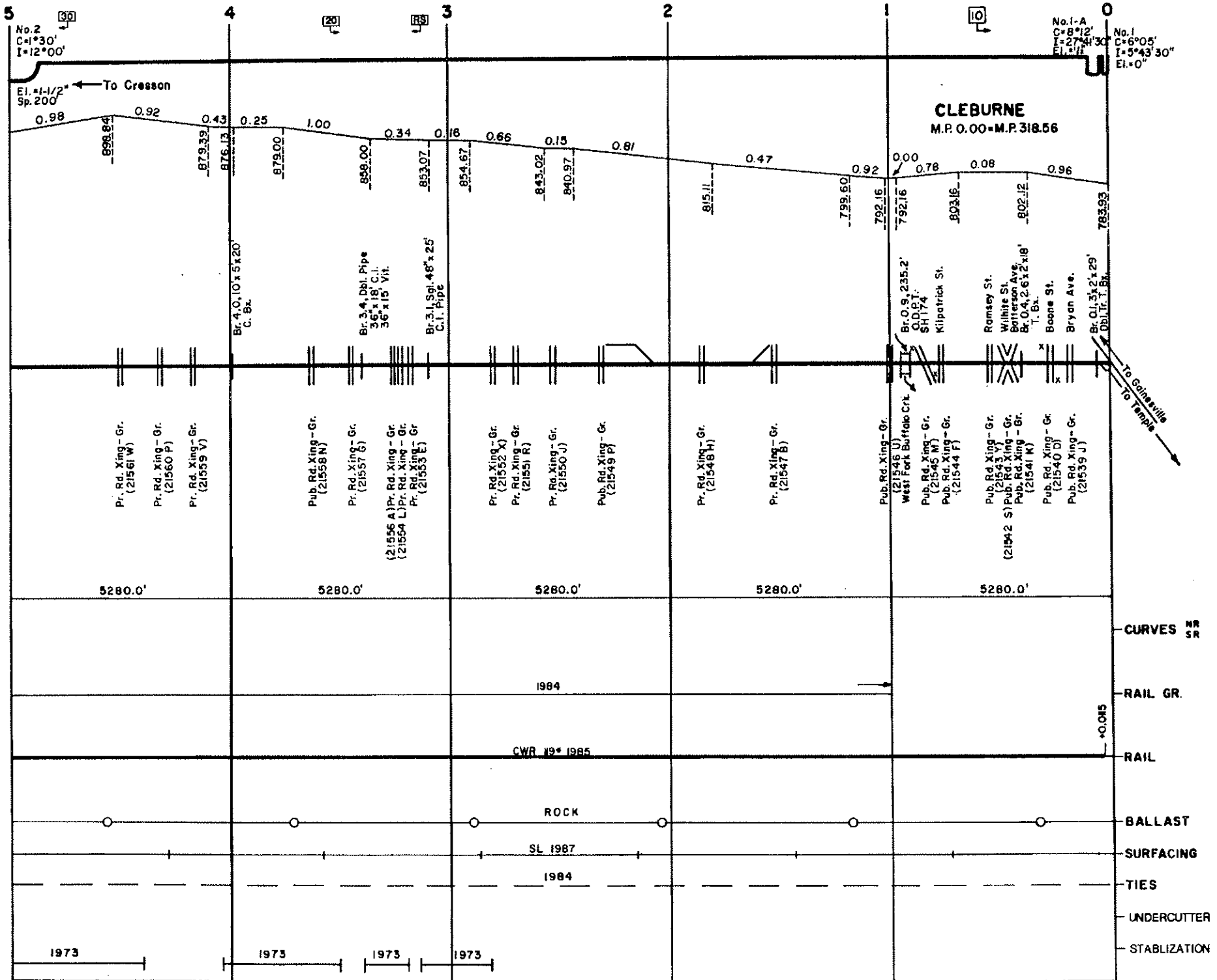
# Dallas Subdivision

Cleburne (M.P. 0.0) to Dallas (M.P. 111.2)







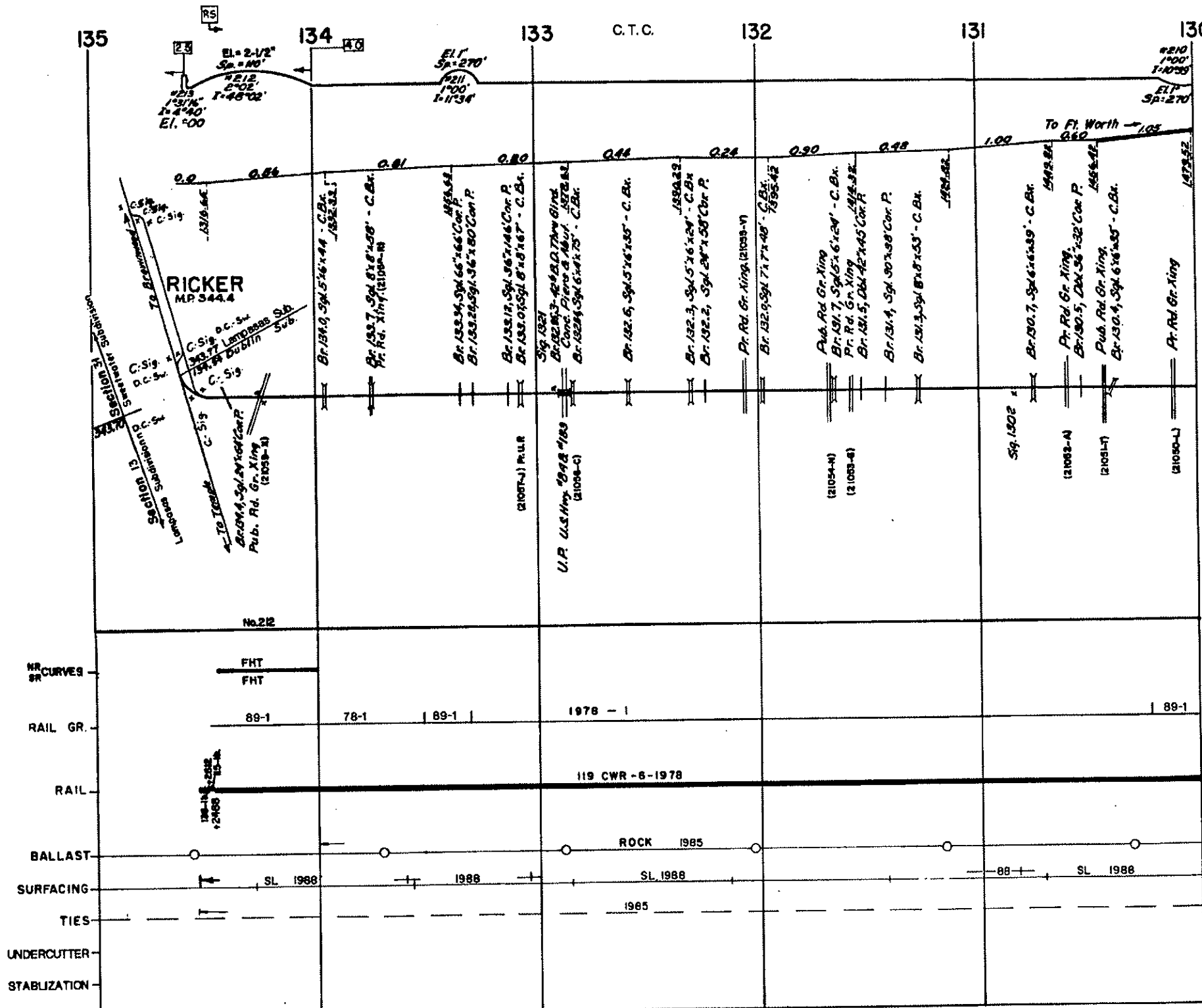


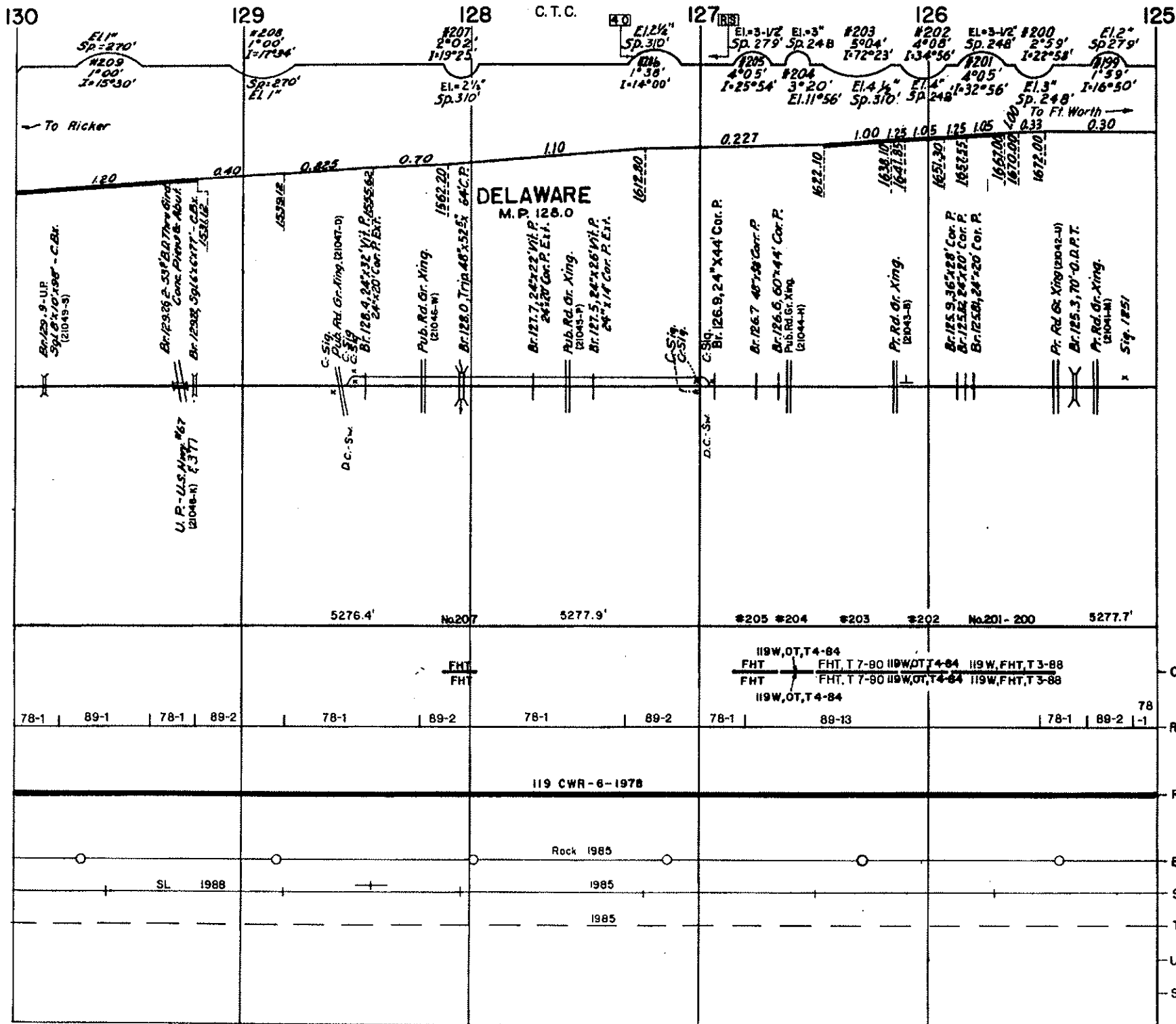
# Cresson Subdivision

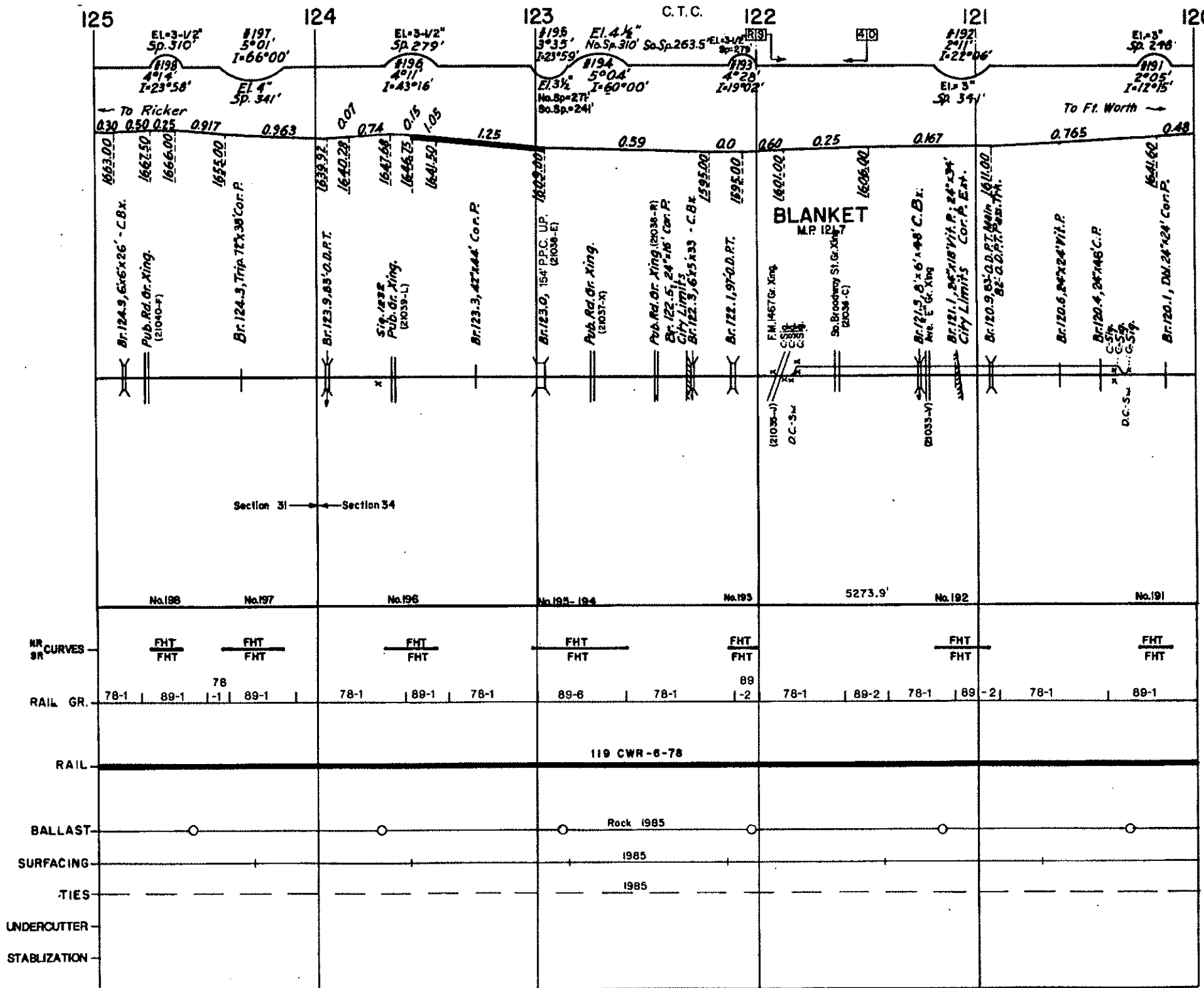
Cleburne (M.P. 0.0) to Cresson (M.P. 18.4)

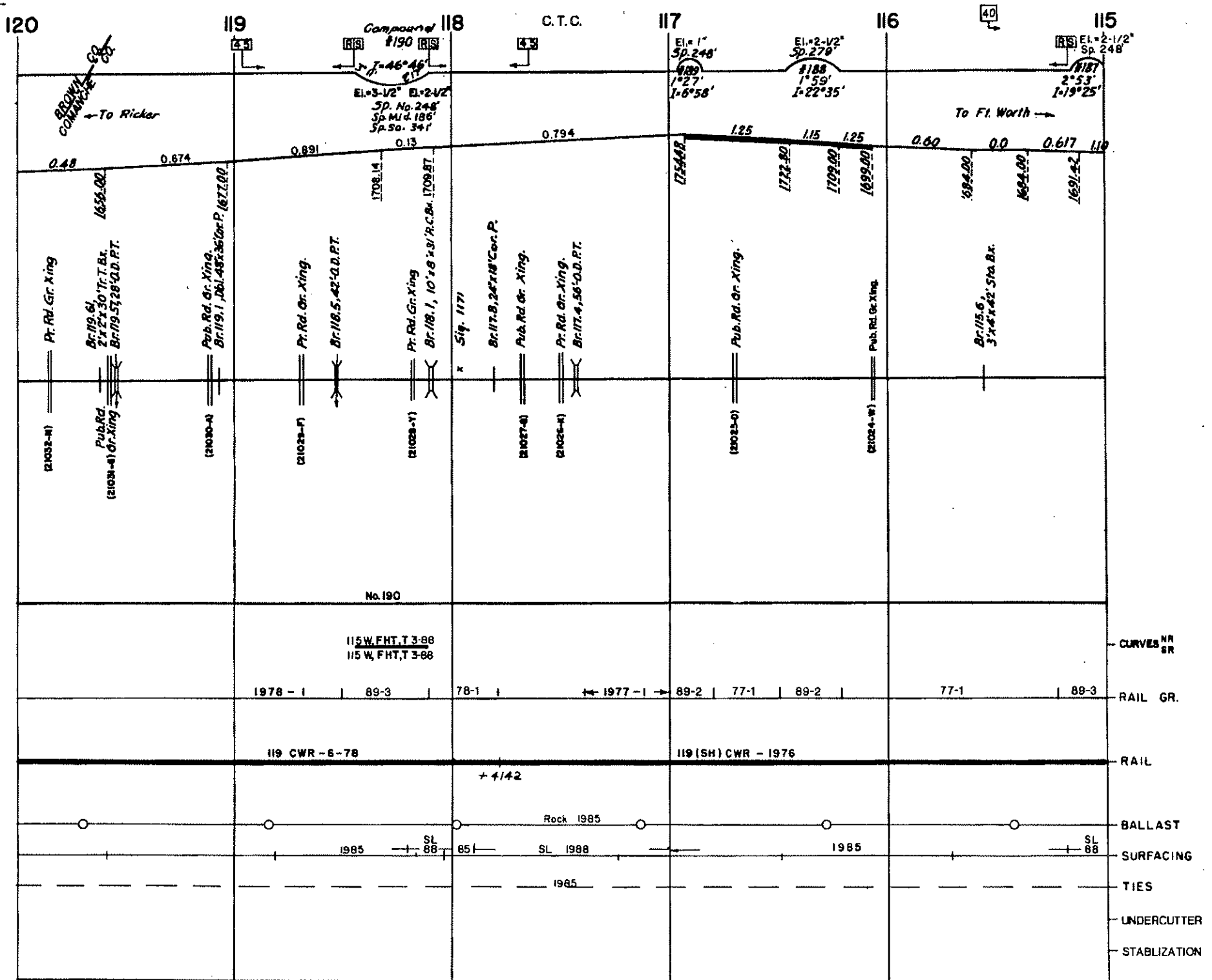
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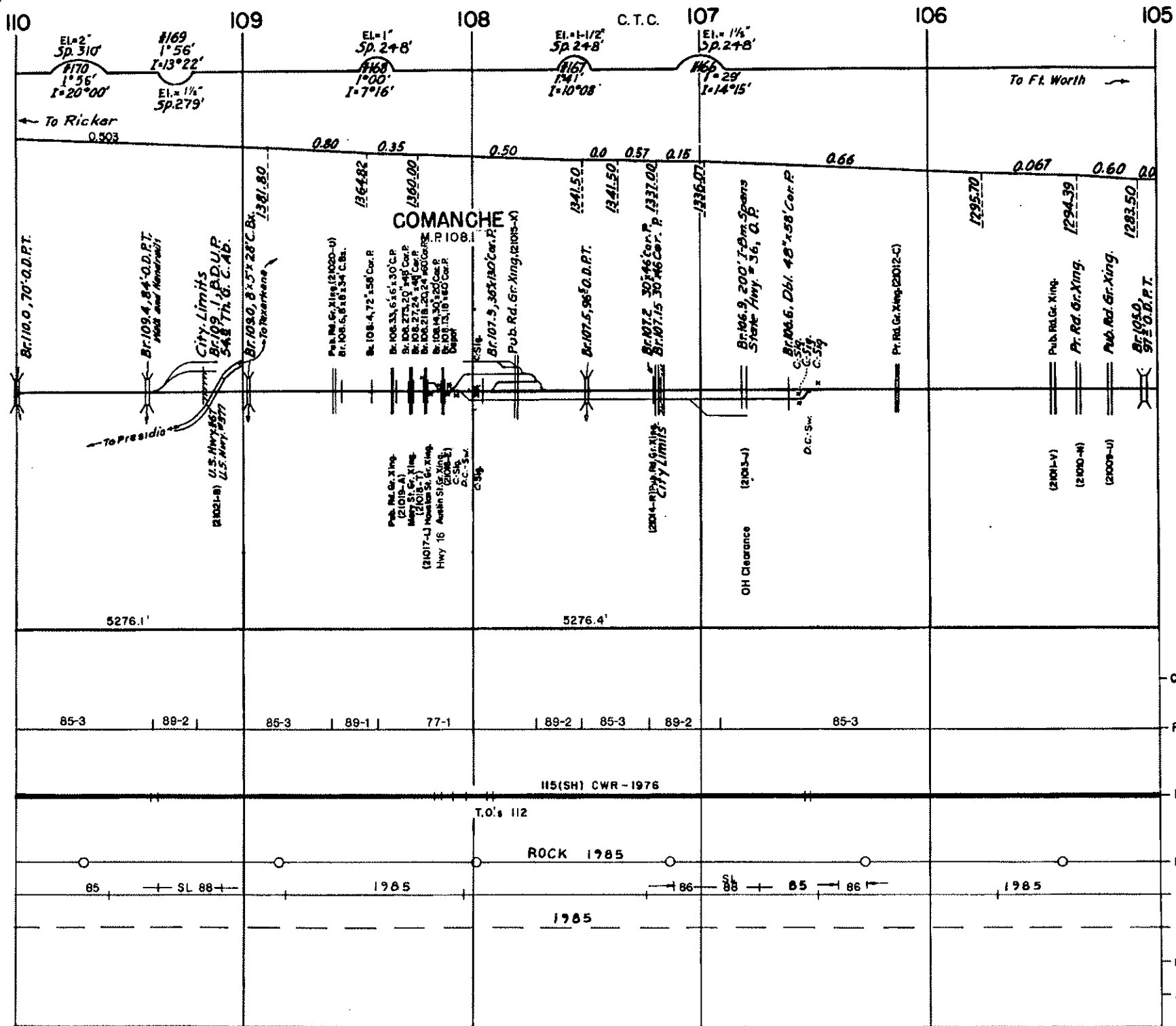


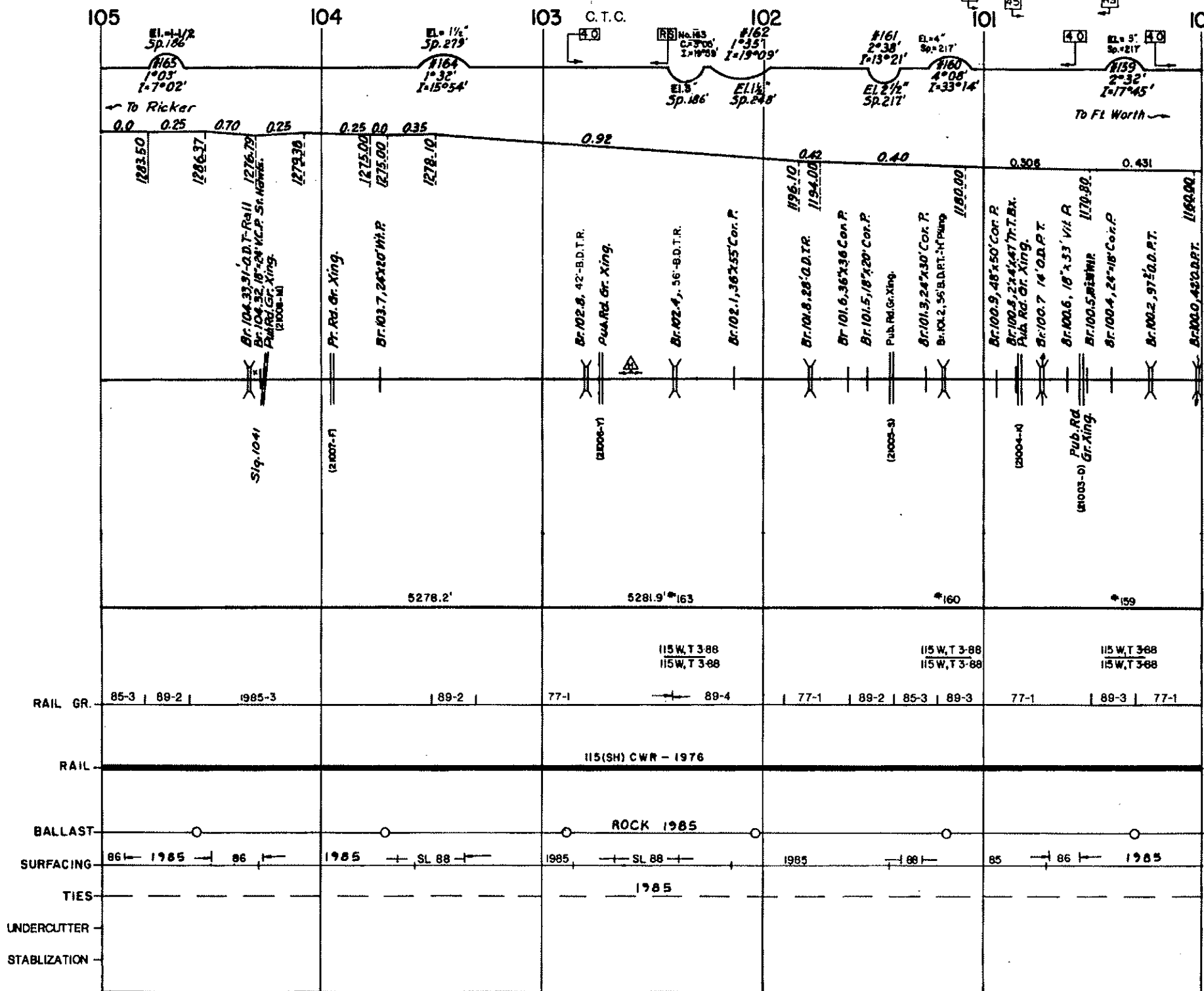


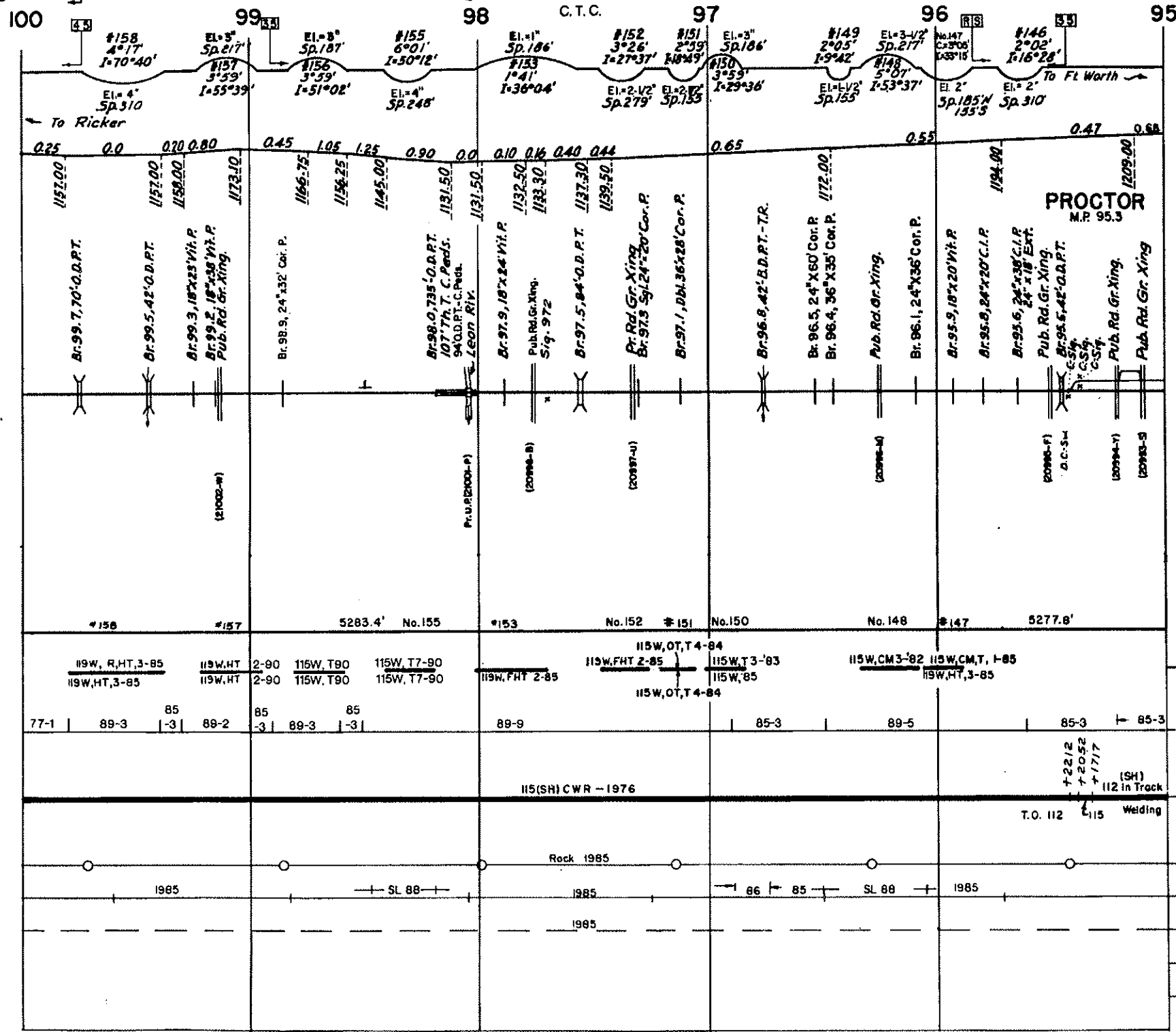










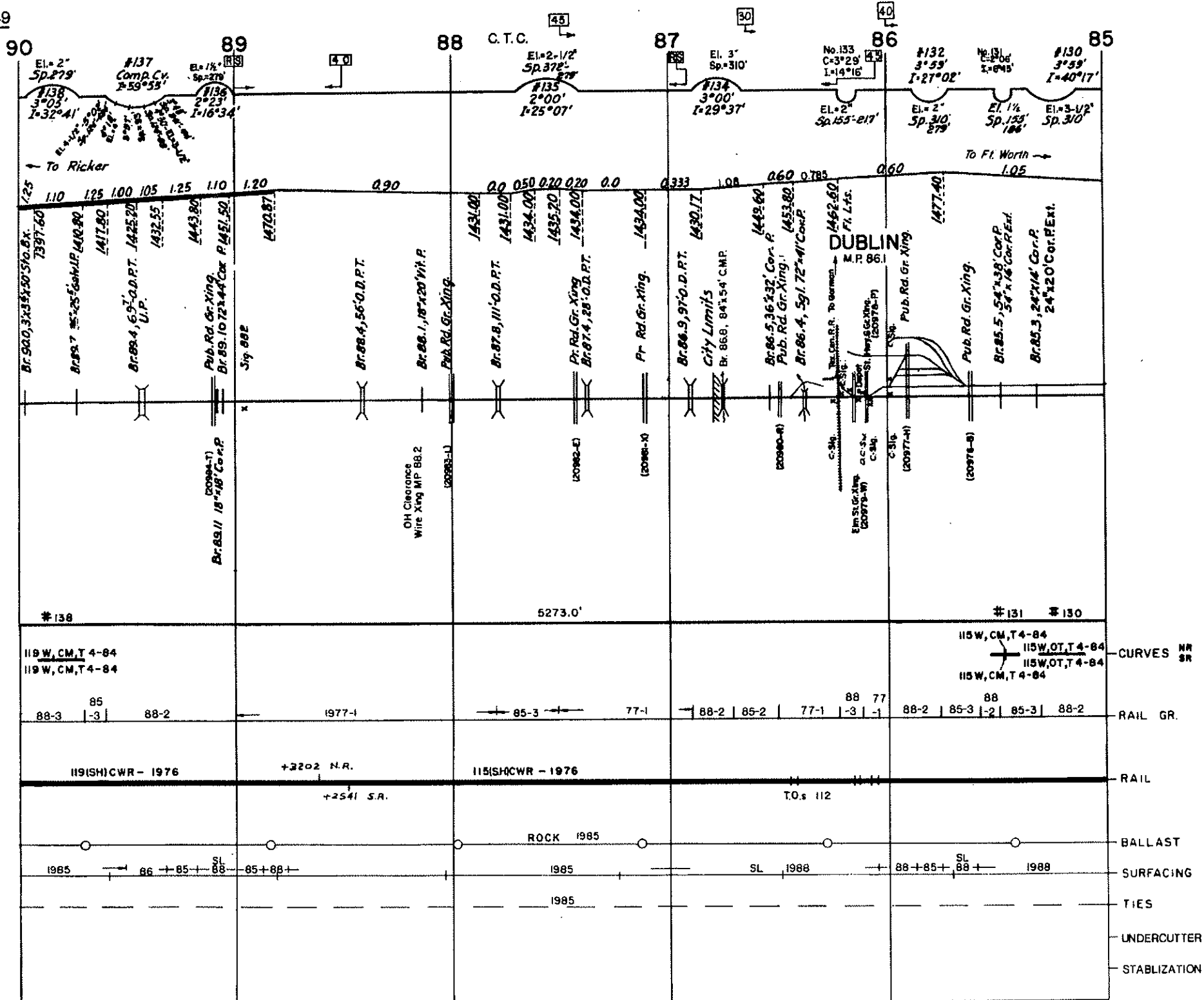


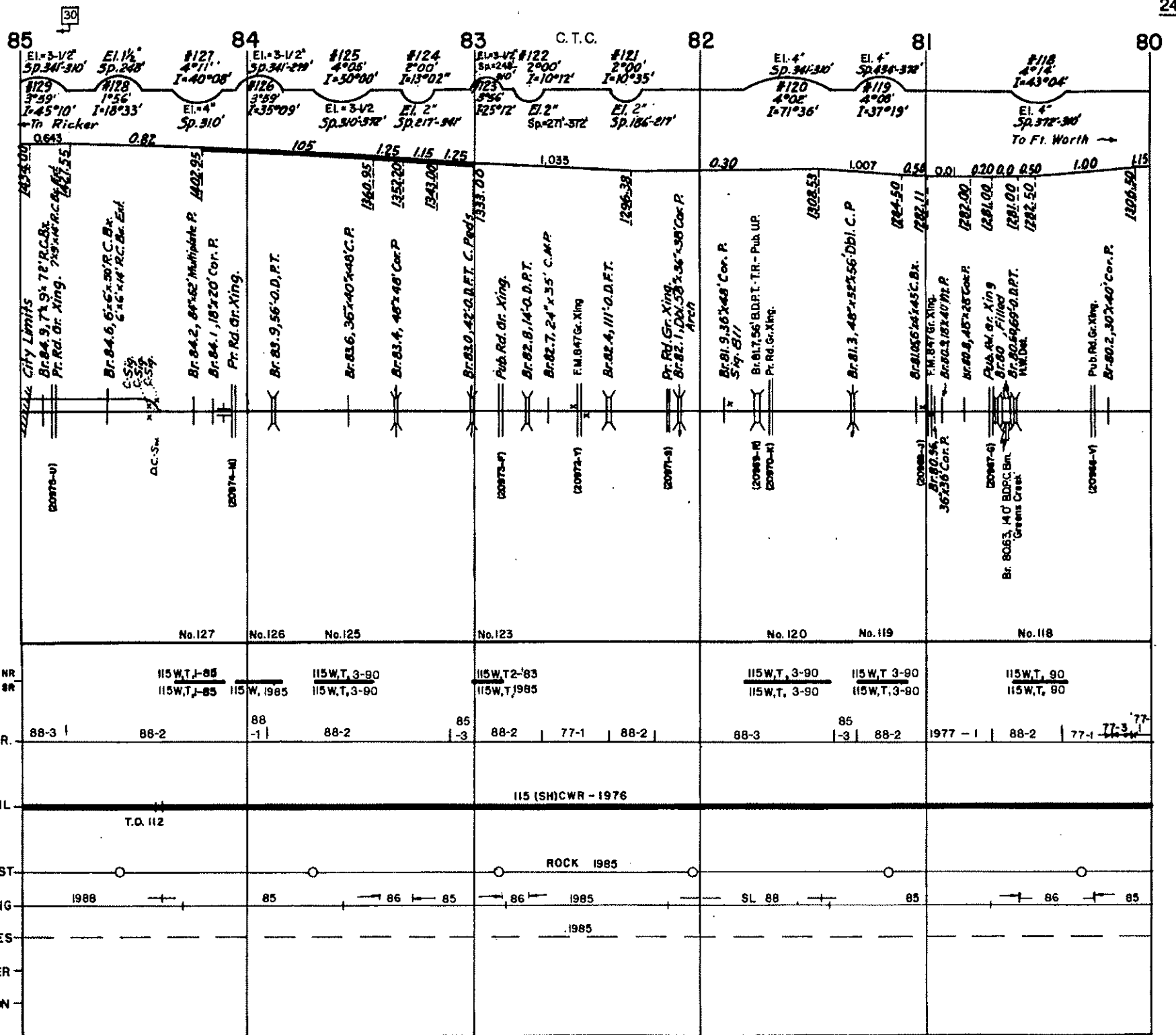
**PROCTOR**  
M.P. 95.3

CURVES NR SR  
RAIL GR.  
RAIL  
BALLAST  
SURFACING  
TIES  
UNDERCUTTER  
STABILIZATION

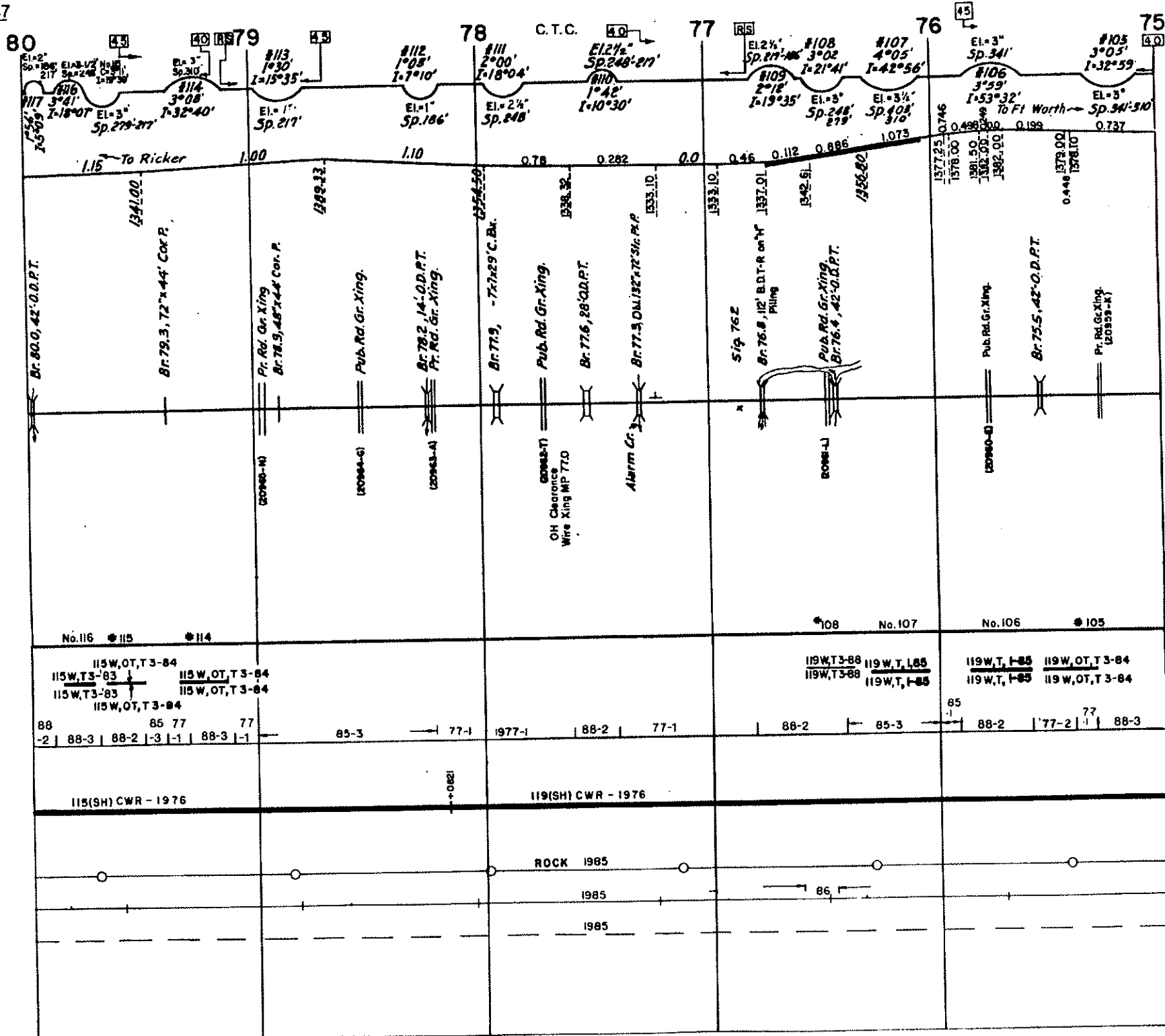




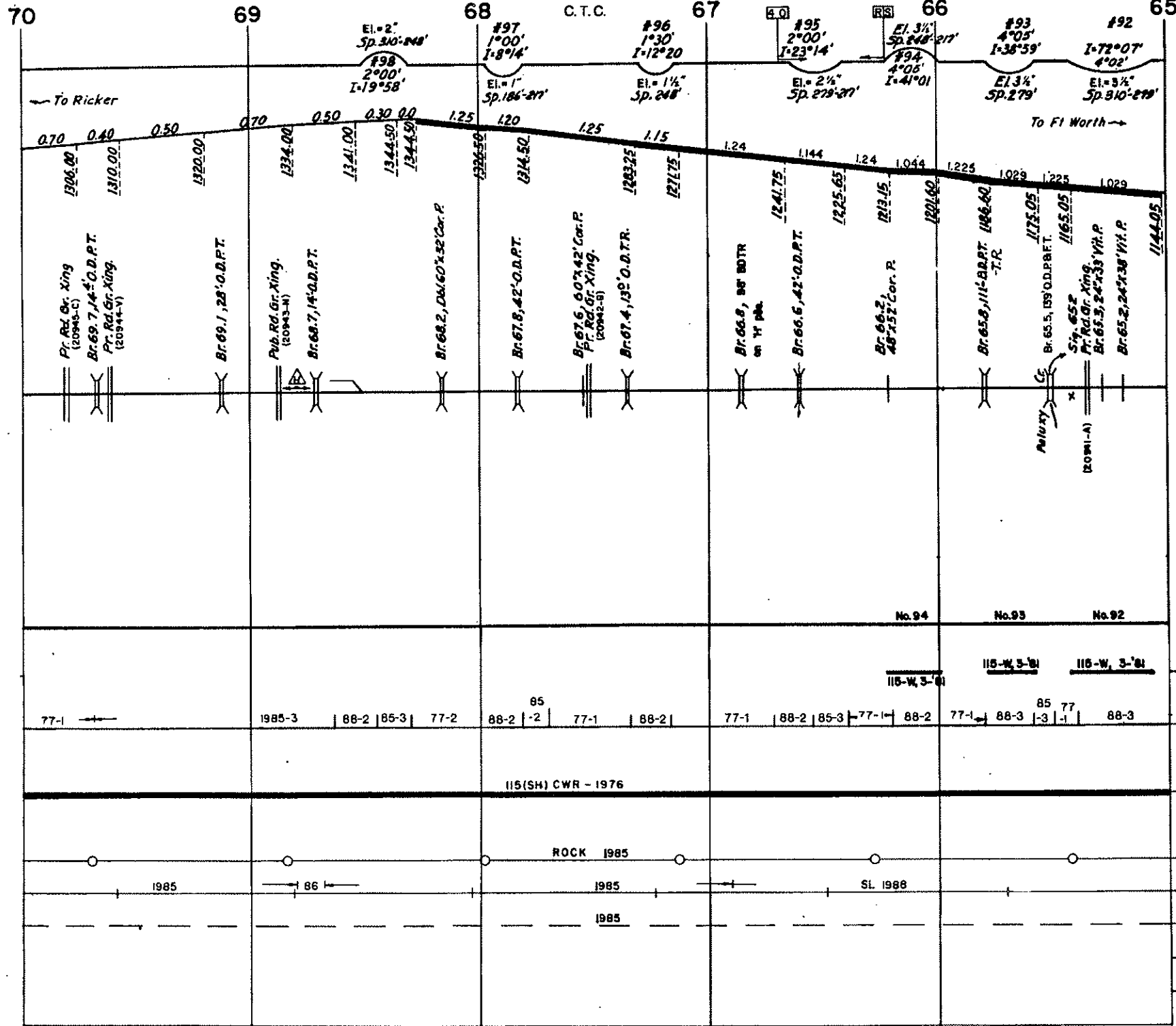


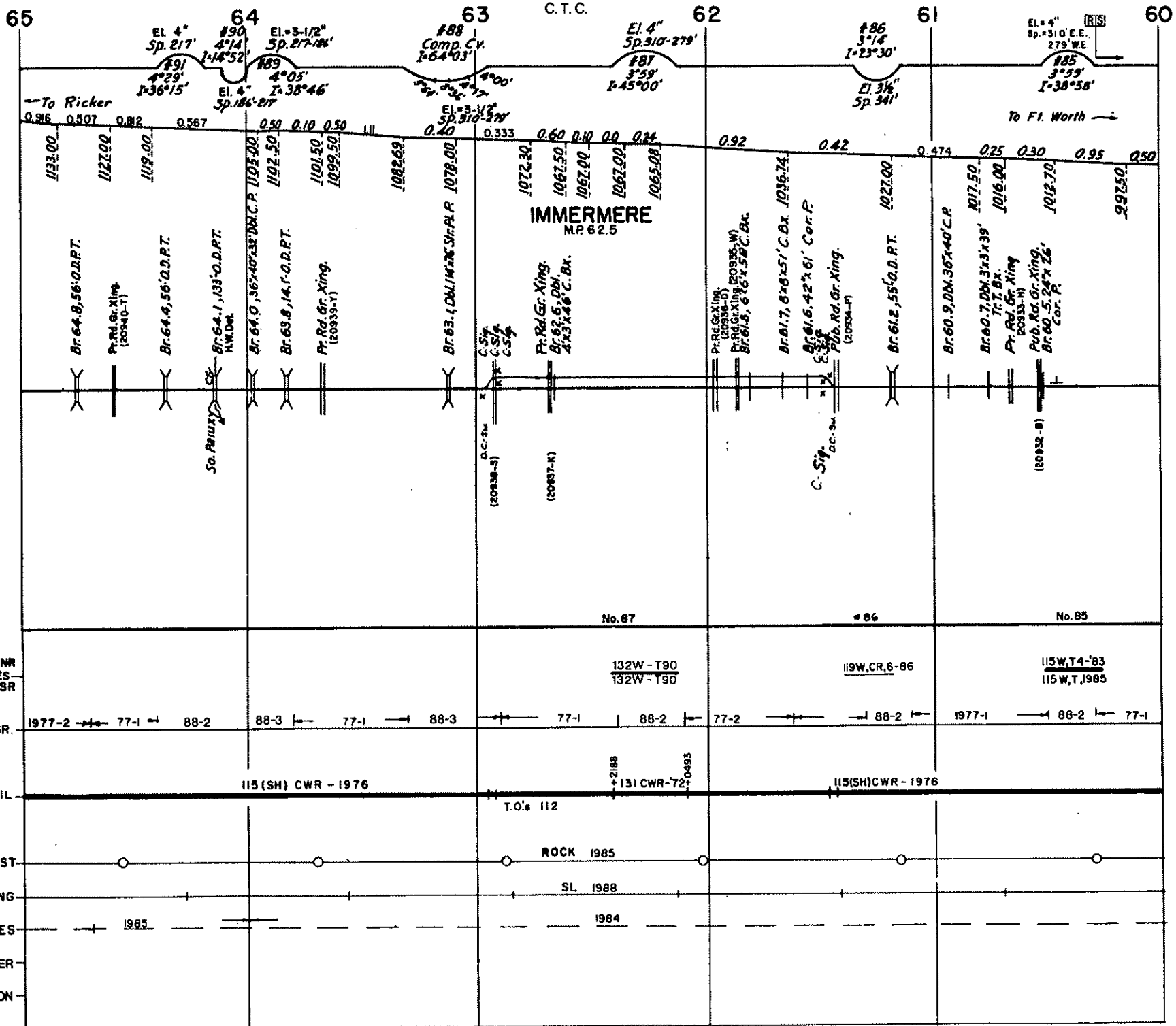


REVISED 3-1991









65

64

63

C.T.C.

62

61

60

--To Ricker

To Ft. Worth --

**IMMERMERE**  
R.R. 62.5

NR  
CURVES  
SR

RAIL  
GR.

RAIL

BALLAST

SURFACING

TIES

UNDERCUTTER

STABILIZATION

No. 87

No. 86

No. 85

132W - T90  
132W - T90

119W, CR, 6-86

115W, T4-'83  
115W, T, 1985

115(SH) CWR - 1976

T.O.'s 112

+2188  
+131 CWR-72  
+0493

115(SH) CWR - 1976

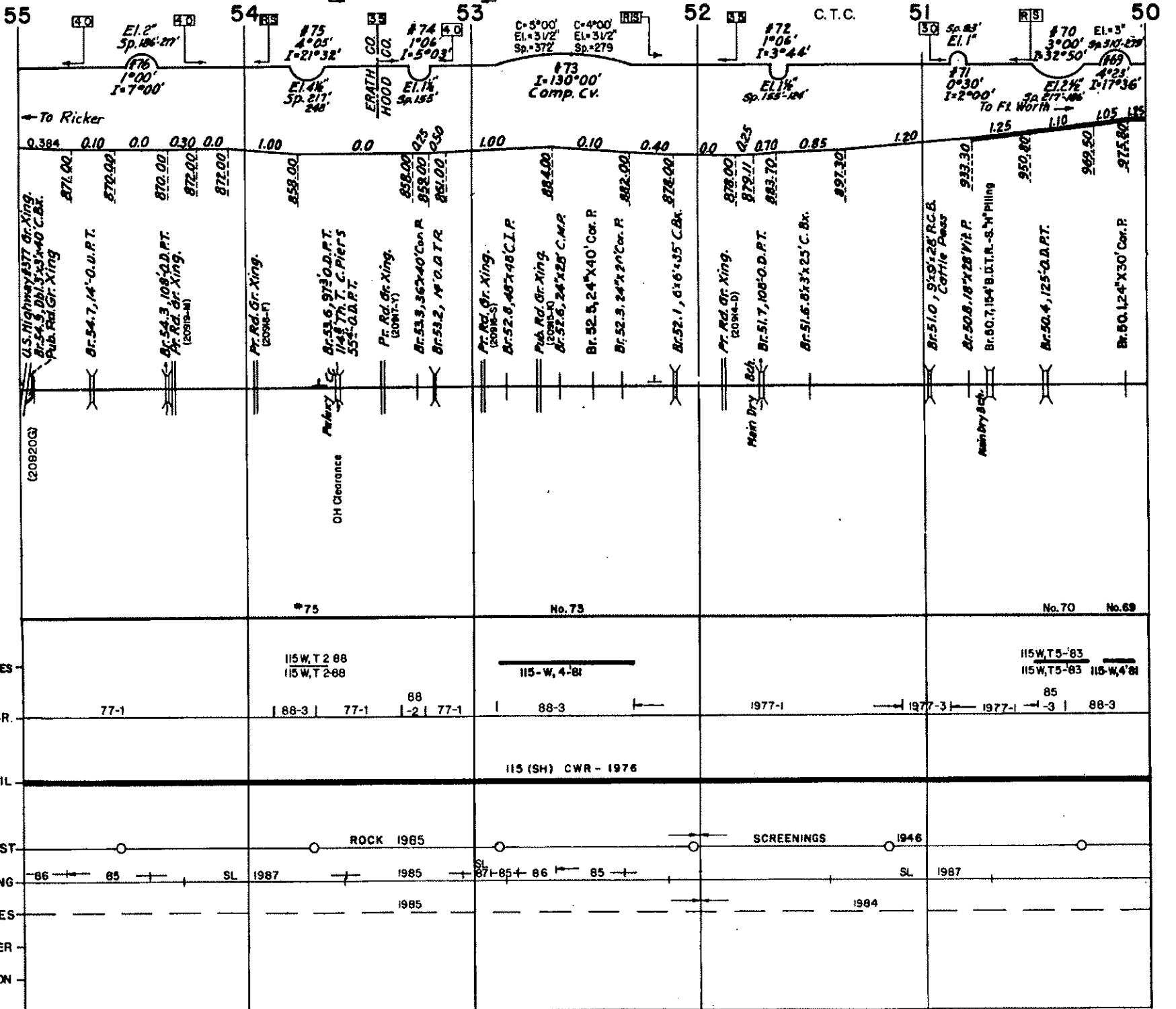
ROCK 1985

SL 1988

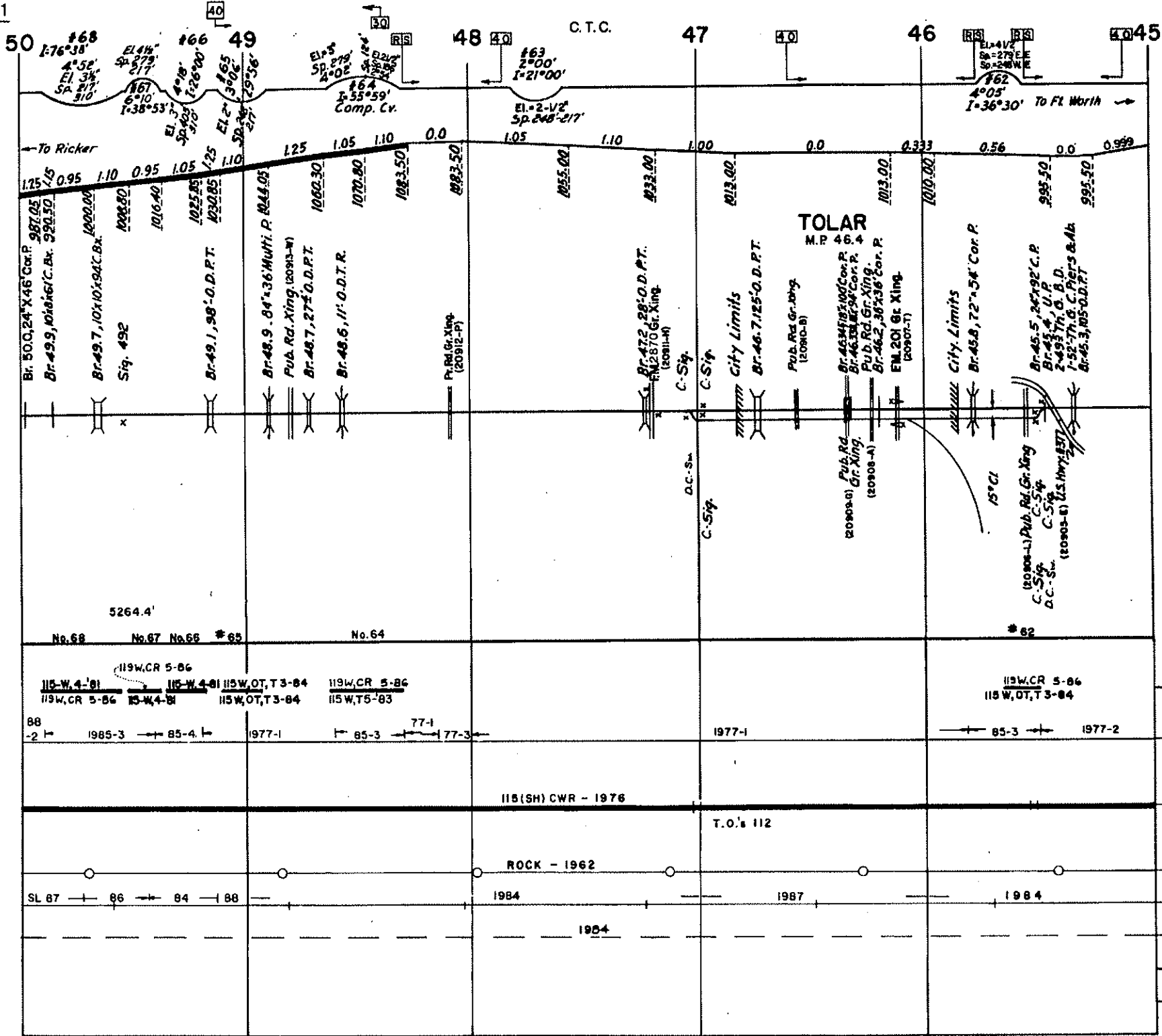
1984



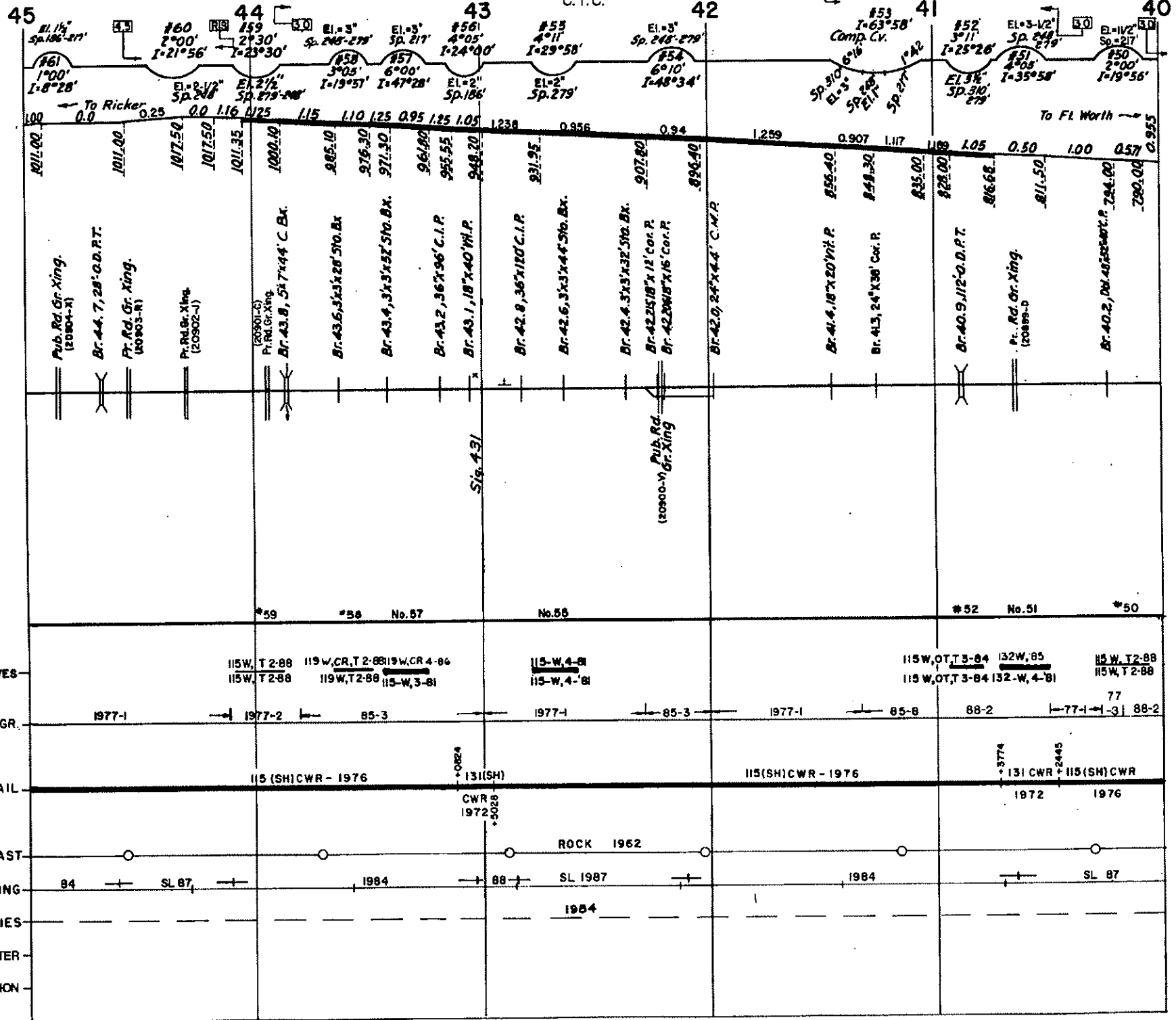




REVISED 3-1991



C.T.C.



NR CURVES SR

RAIL GR.

RAIL

BALLAST

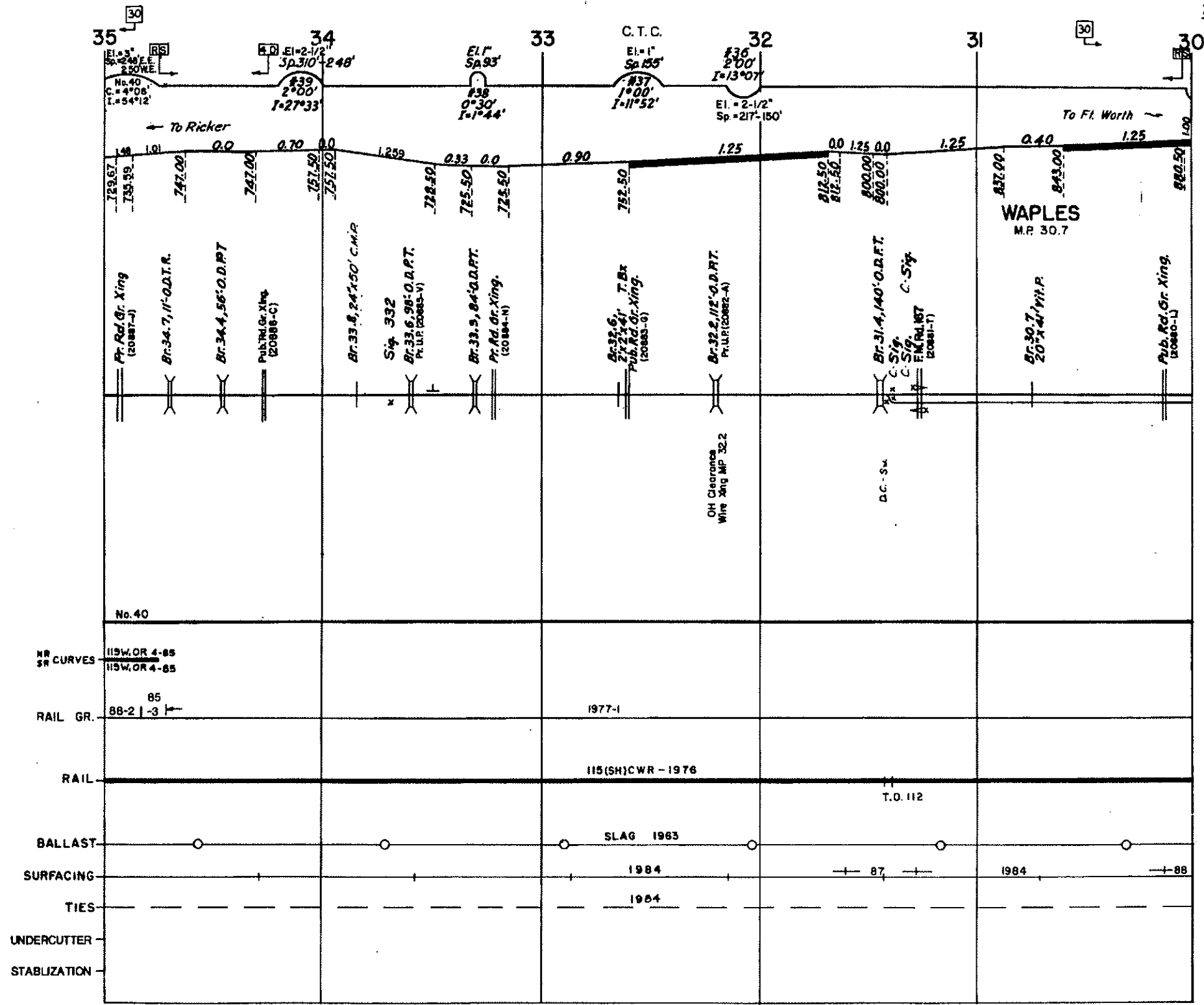
SURFACING

TIES

UNDERCUTTER

STABILIZATION





35

34

33

C.T.C.

32

31

30

El. = 3.3  
 Sp. = 248' E  
 2.50' W  
 No. 40  
 C. = 4°08'  
 I. = 54°12'

El. = 2-1/2  
 Sp. = 310-248'  
 #39  
 2°00'  
 I = 27°33'

El. = 5A93'  
 #38  
 0°30'  
 I = 1°44'

El. = 1'  
 Sp. = 155'  
 #37  
 1°00'  
 I = 11°52'

#36  
 2°00'  
 I = 13°07'  
 El. = 2-1/2'  
 Sp. = 217-150'

To Ricker

To Ft. Worth

1.00 0.0 0.70 0.0 1.259 0.33 0.0 0.90 1.25 0.0 1.25 0.0 1.25 0.40 1.25 1.00

729.67  
 735.99  
 747.00  
 747.00  
 757.50  
 757.50  
 728.50  
 725.50  
 725.50  
 752.50  
 812.50  
 812.50  
 800.00  
 800.00  
 827.00  
 843.00  
 880.50

WAPLES M.P. 50.7

Pr. Rd. Gr. Xing (20887-J)  
 Br. 34.7, 11'-O.D.T.R.  
 Br. 34.4, 56'-O.D.P.T.  
 Pub. Rd. Gr. Xing (20888-C)  
 Br. 33.8, 24'-x-50' C.M.R.  
 Sig. 332  
 Br. 33.6, 98'-O.D.P.T.  
 Pt. U.P. (20885-N)  
 Br. 33.9, 84'-O.D.P.T.  
 Pr. Rd. Gr. Xing (20884-N)  
 Br. 32.6, 2'-x-2'-x-41' T. Bx  
 Pub. Rd. Gr. Xing (20883-O)  
 Br. 32.2, 112'-O.D.P.T.  
 Pt. U.P. (20882-A)  
 Br. 31.4, 140'-O.D.P.T.  
 C. Sig. (20881-T)  
 C. Sig. (20881-T)  
 Pub. Rd. Gr. Xing (20880-L)  
 Pub. Rd. Gr. Xing (20880-L)

No. 40  
 NR SR CURVES 115W, OR 4-85  
 115W, OR 4-85  
 85  
 RAIL GR. 88-2 | -3  
 1977-1

RAIL 115(SH)CWR - 1976 T.O. 112

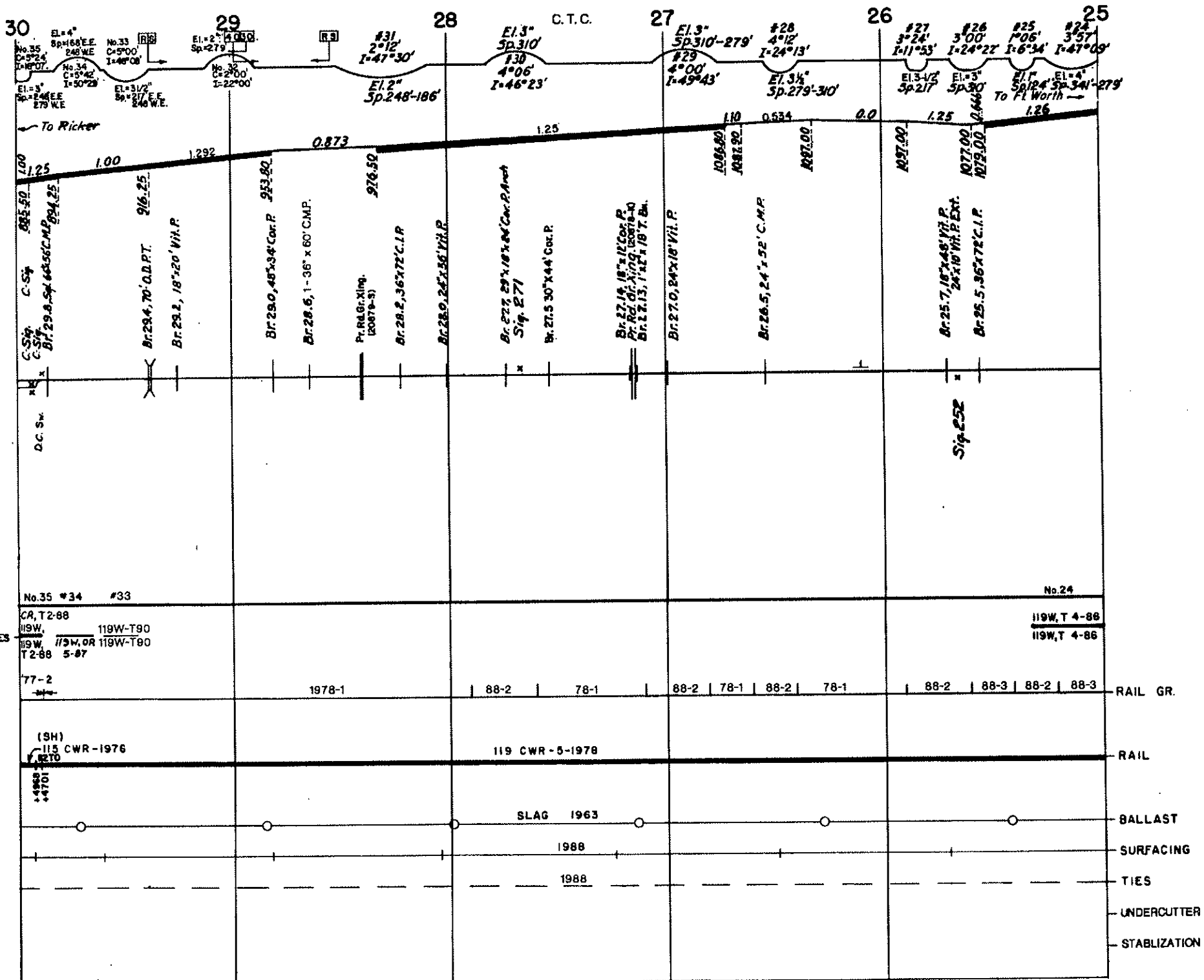
BALLAST SLAG 1963

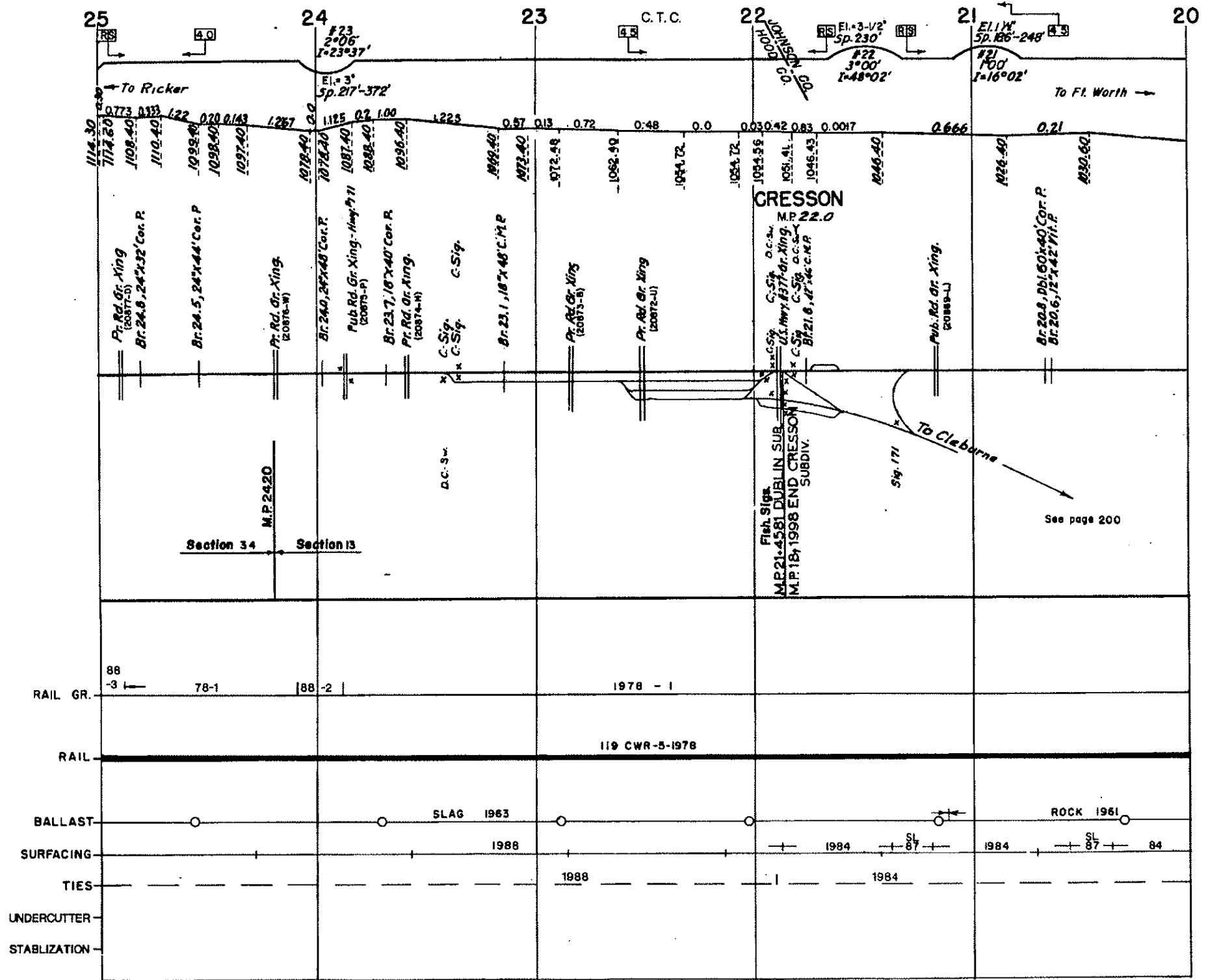
SURFACING 1984 87 1984 88

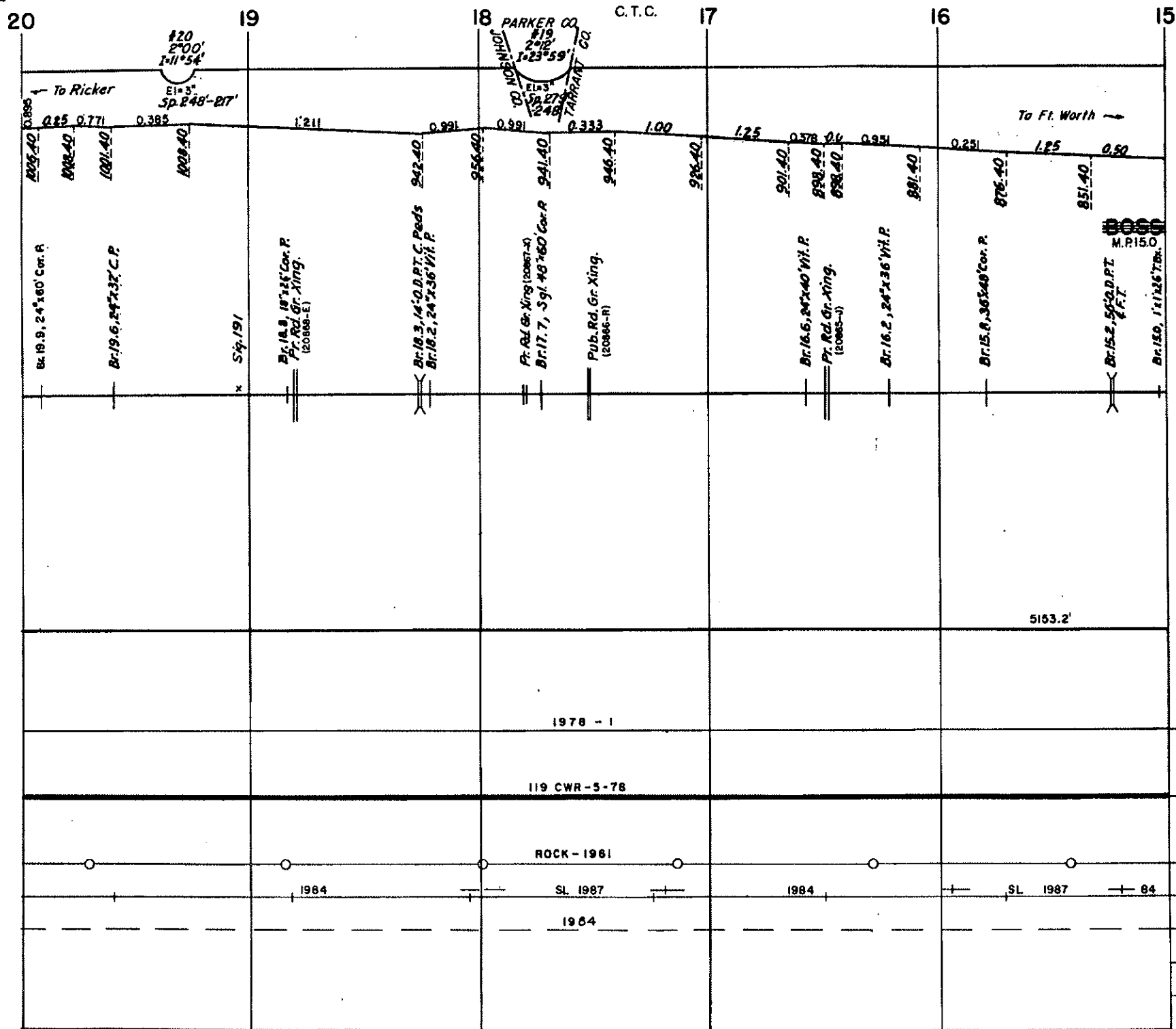
TIES 1984

UNDERCUTTER

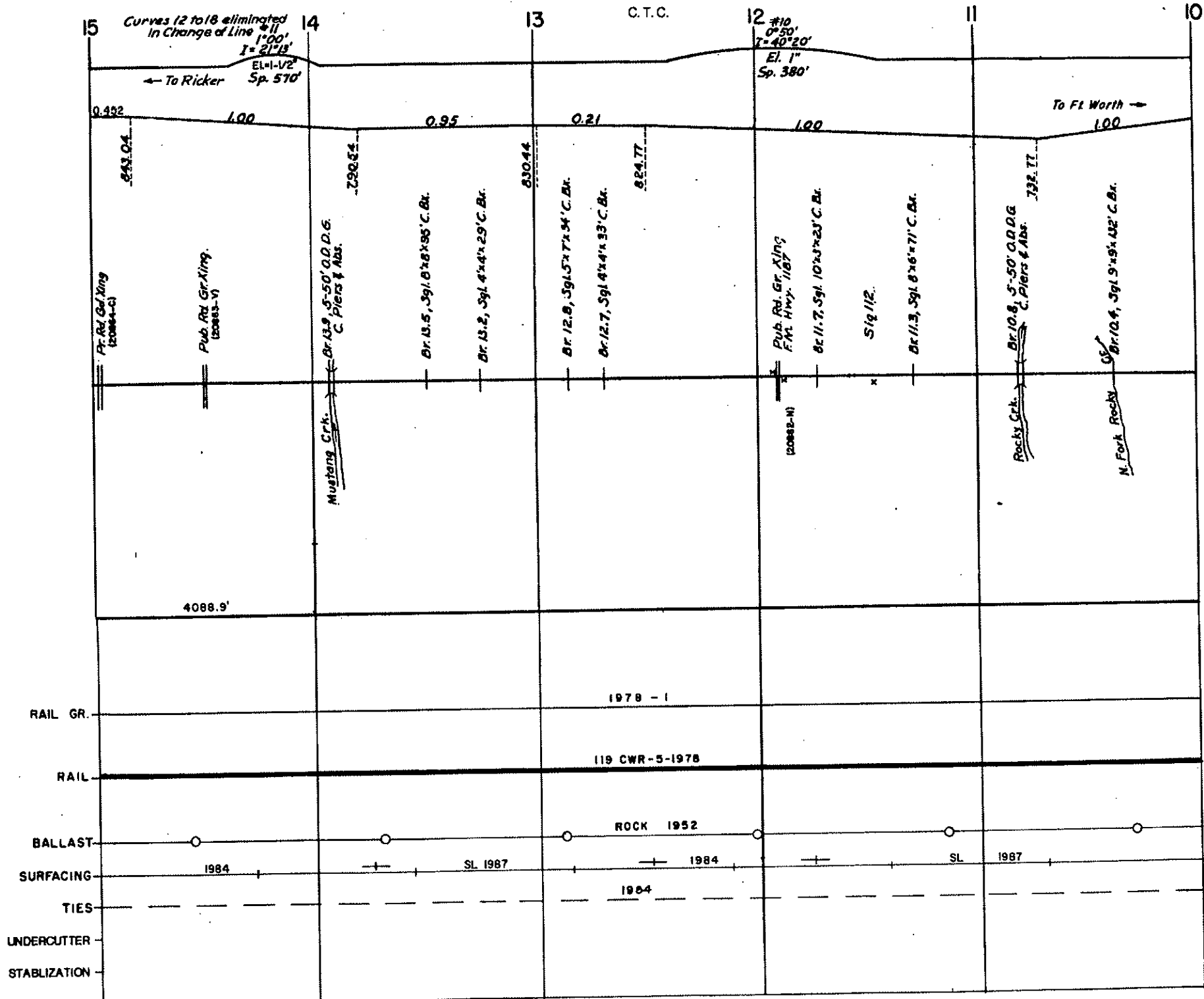
STABILIZATION











Curves 12 to 16 eliminated in Change of Line #11

C.T.C.

← To Ricker  
El. 1-1/2  
Sp. 570'  
I = 21'19"

#10  
0°50'  
7-40°20'  
El. 1"  
Sp. 380'

To Ft. Worth →

0.452 1.00 0.95 0.21 1.00 1.00

853.04 790.54 830.44 824.77

Pr. Rd. Gr. Xing (20864-C)  
Pub. Rd. Gr. Xing (20863-V)  
Mustang Crk. Br. 13.3, 5-50' O.D.D.G. C. Piers & Abs.  
Br. 13.5, Sgl. 8'x8'x85' C. Br.  
Br. 13.2, Sgl. 4'x4'x29' C. Br.  
Br. 12.8, Sgl. 5'x7'x94' C. Br.  
Br. 12.7, Sgl. 4'x4'x33' C. Br.  
Pub. Rd. Gr. Xing F.M. Hwy. 1187  
Br. 11.7, Sgl. 10'x3'x23' C. Br.  
Sig 112.  
Br. 11.3, Sgl. 8'x6'x71' C. Br.  
Rocky Crk. Br. 10.3, 5-50' O.D.D.G. C. Piers & Abs.  
N. Fork Rocky Br. 10.4, Sgl. 9'x9'x102' C. Br.

4088.9'

1978 - 1

119 CWR-5-1978

ROCK 1952

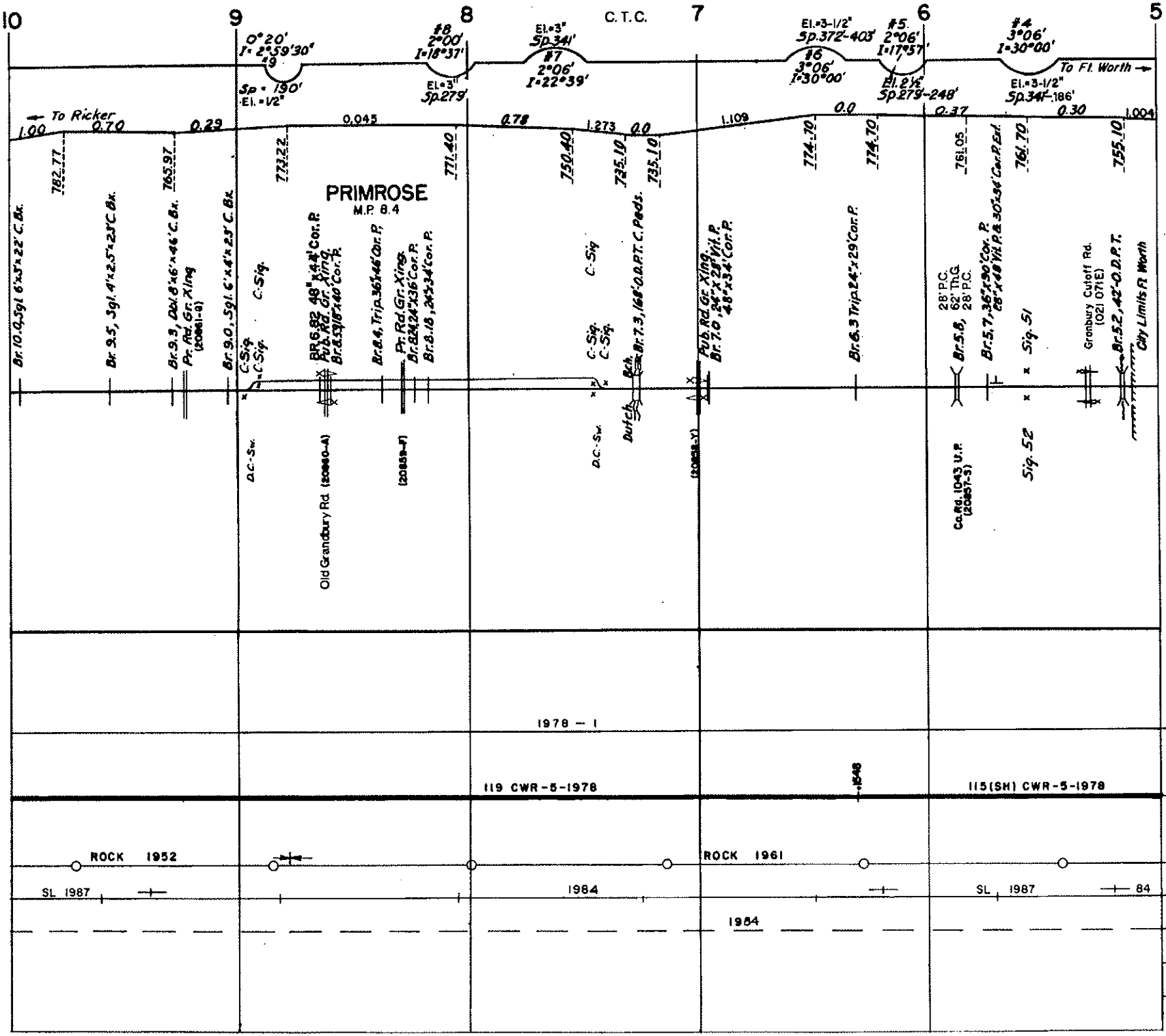
1984

SL 1987

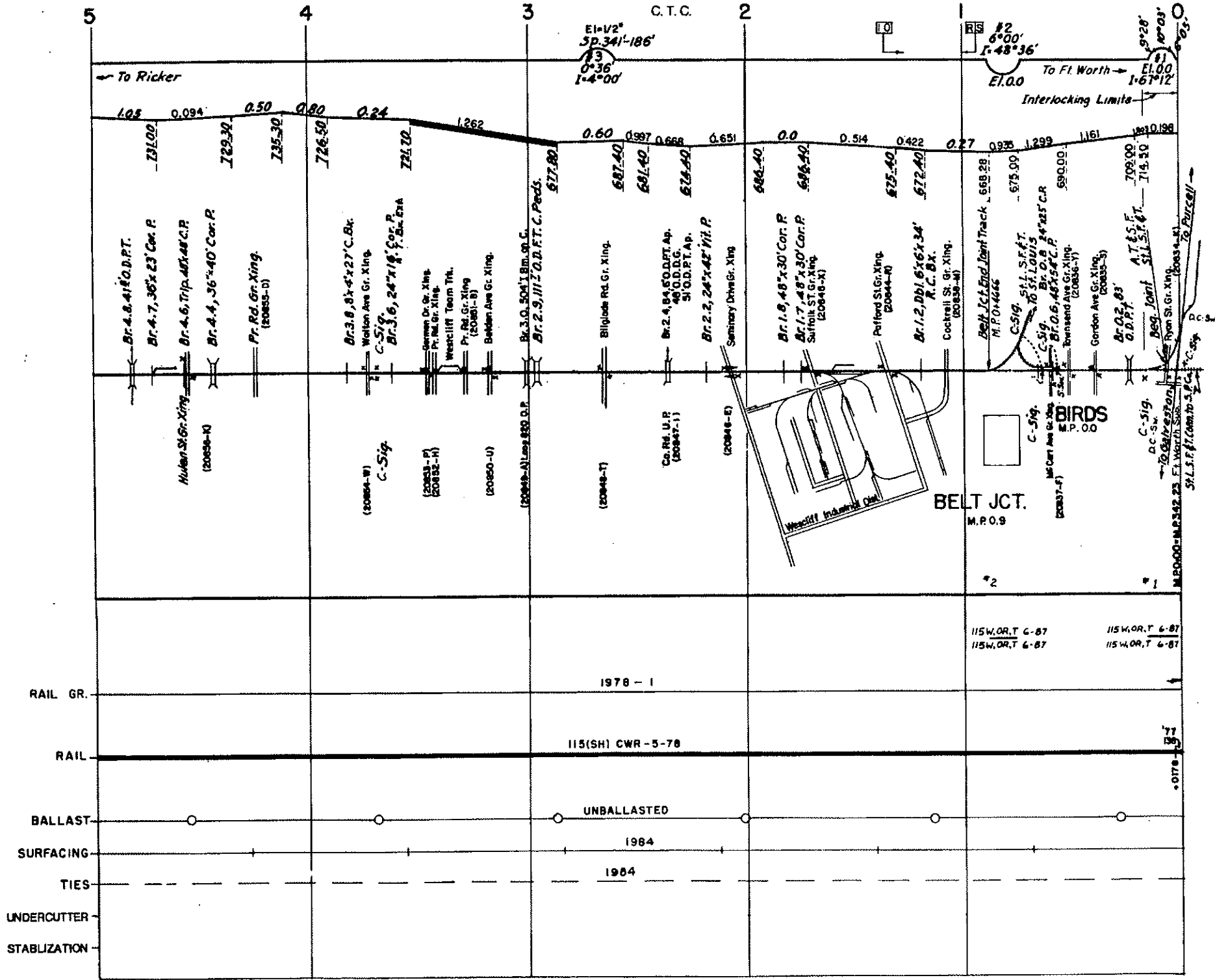
1984

SL 1987

1984



RAIL GR.  
 RAIL  
 BALLAST  
 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION



REVISED 3-1991

# Dublin Subdivision

Birds (M.P. 0.0) to Ricker (M.P. 134.5)

218

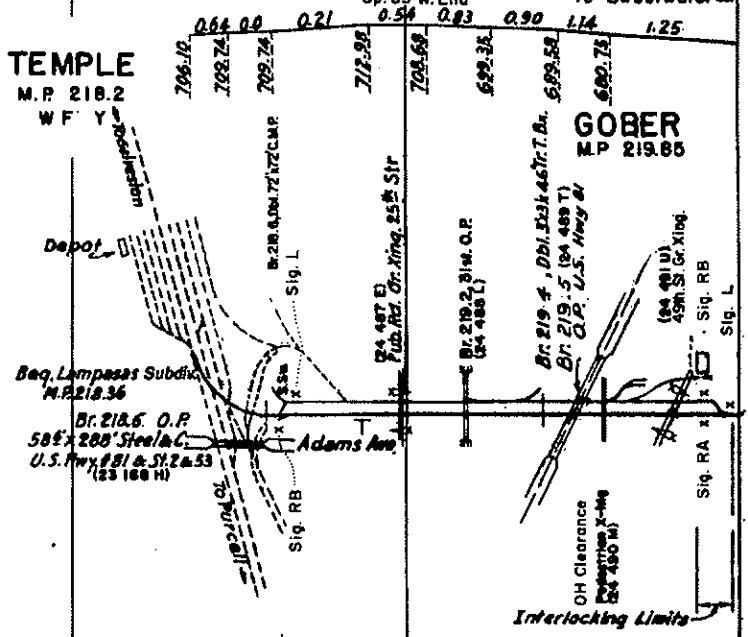
3°06', 3°32', 3°25', 1°58'50"  
 I=82°18'  
 219  
 EL=1'  
 Sp. 59' W. End  
 C.T.C. → A.B.S.

220

See Page 122

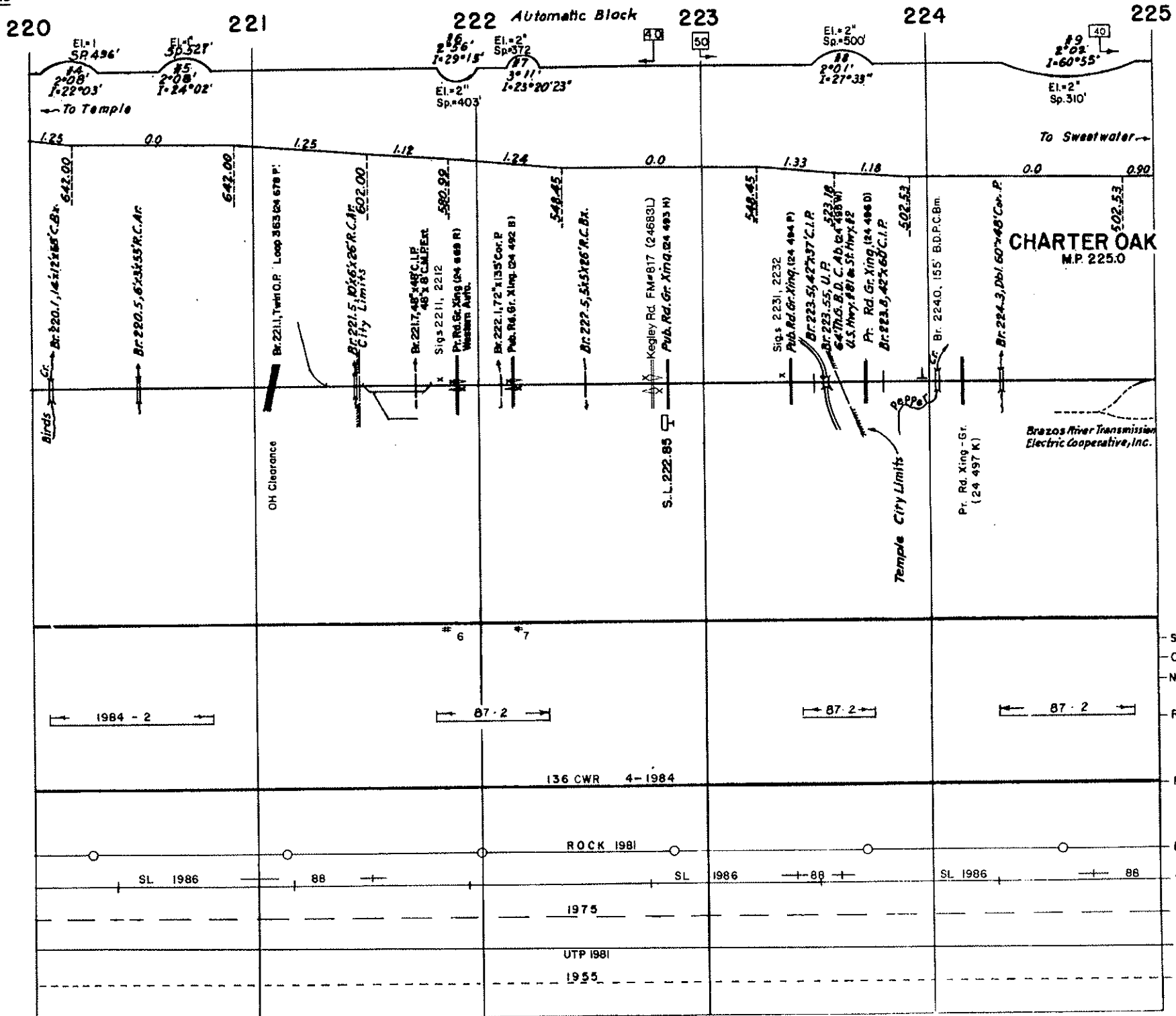
TEMPLE  
 M.P. 218.2  
 W F Y

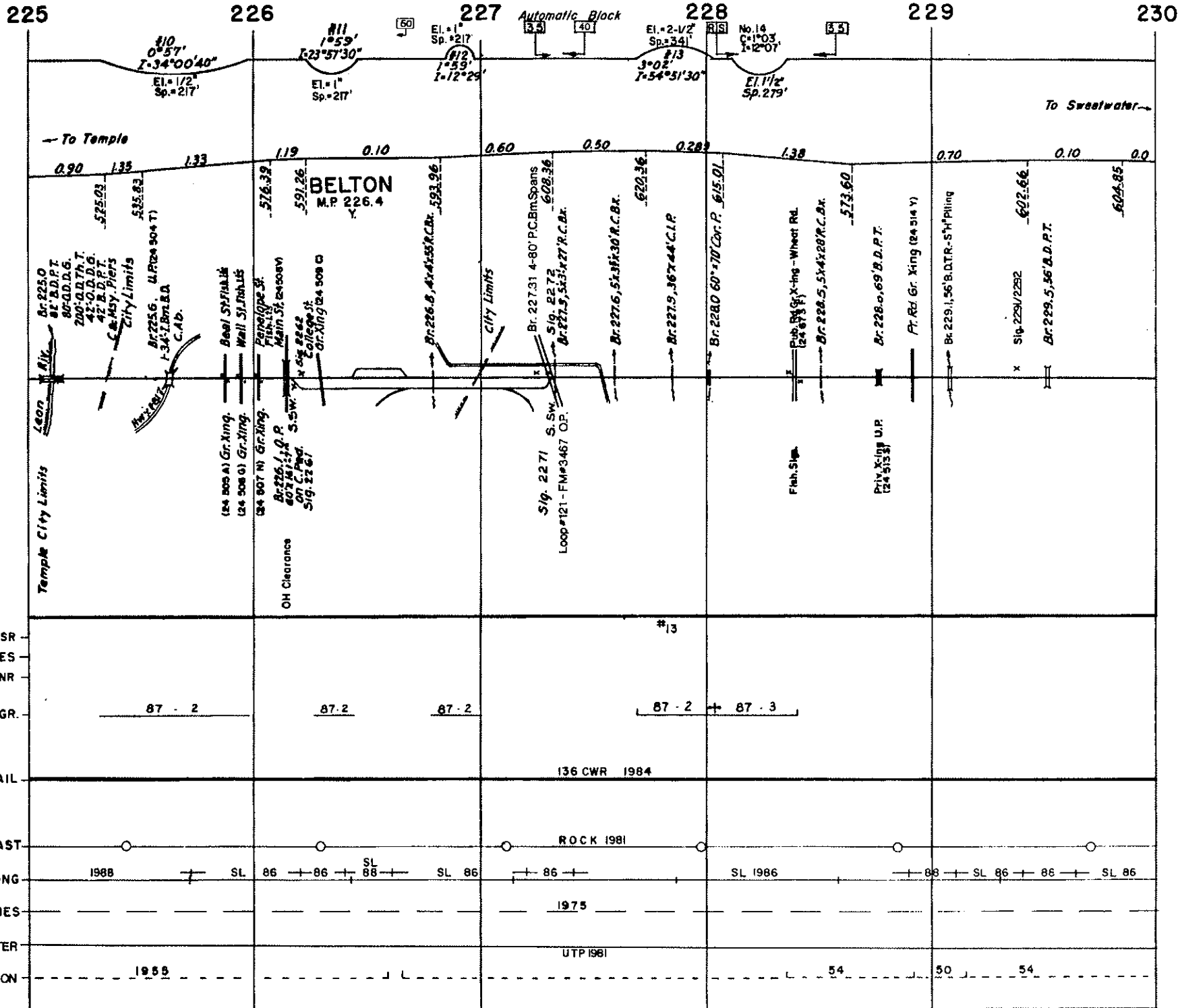
GOBER  
 M.P. 219.85



Section 21

CURVES		* 115 J, 12-83 * 115 J, 12-83
RAIL GR.		89-26 N 89-17 S
RAIL	136, 119, 115 CWR 1976, 136 CWR 1976	
BALLAST	+2230, GRAVEL	+5256, 1981 ROCK, +4198
SURFACING		SL 1986
TIES		1975
UNDERCUTTER		UTP 1981
STABILIZATION		L 45, L 55





230

231

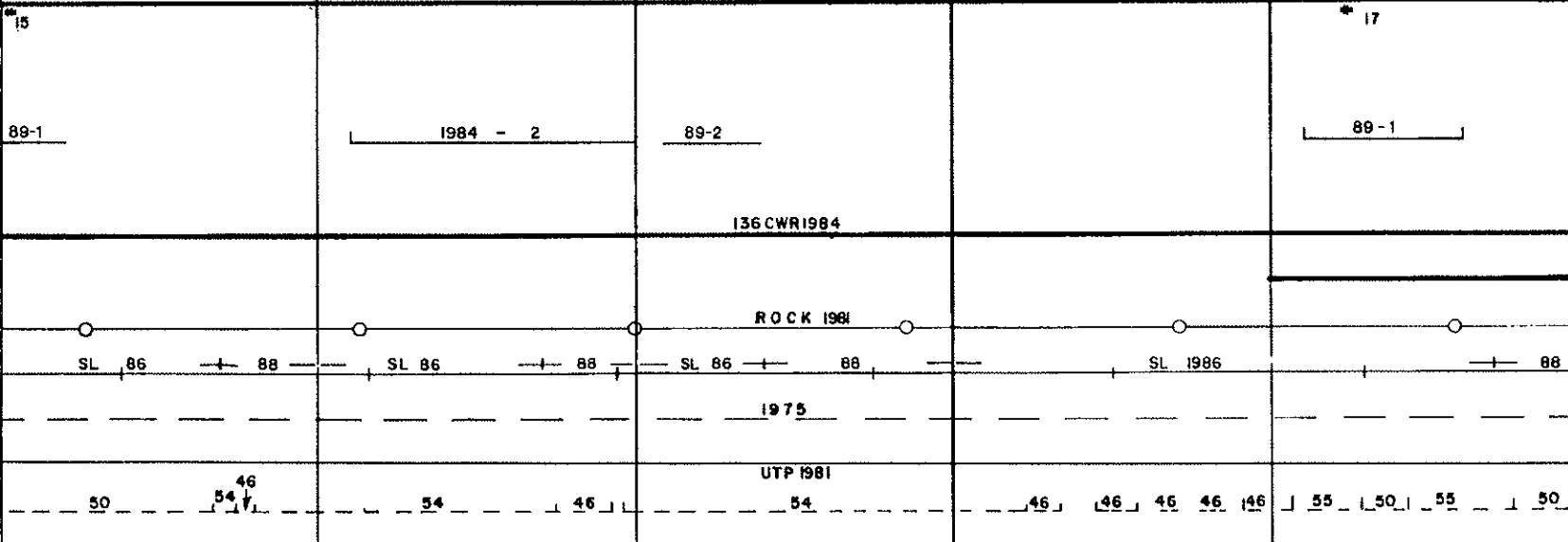
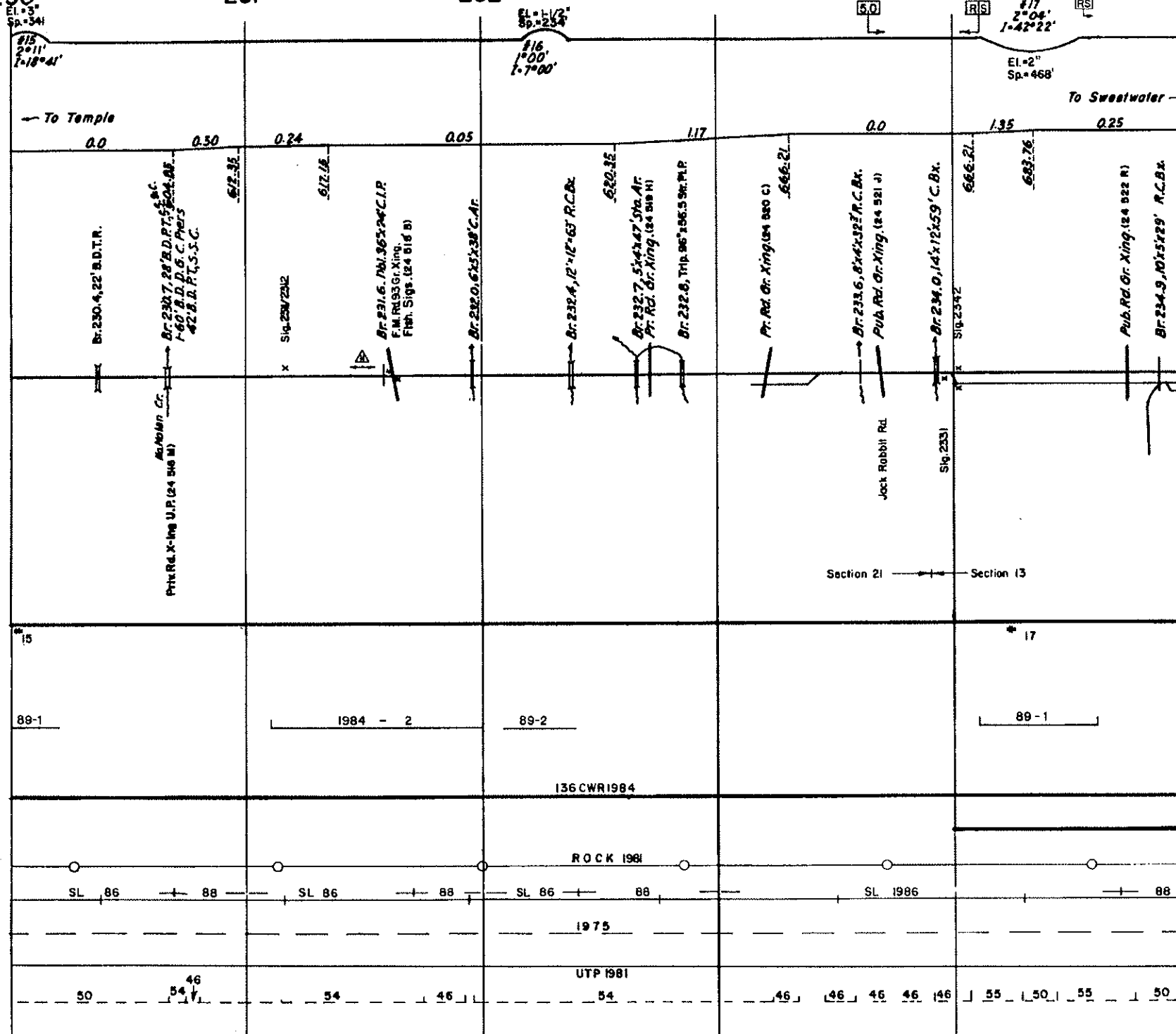
232

Automatic Block

233

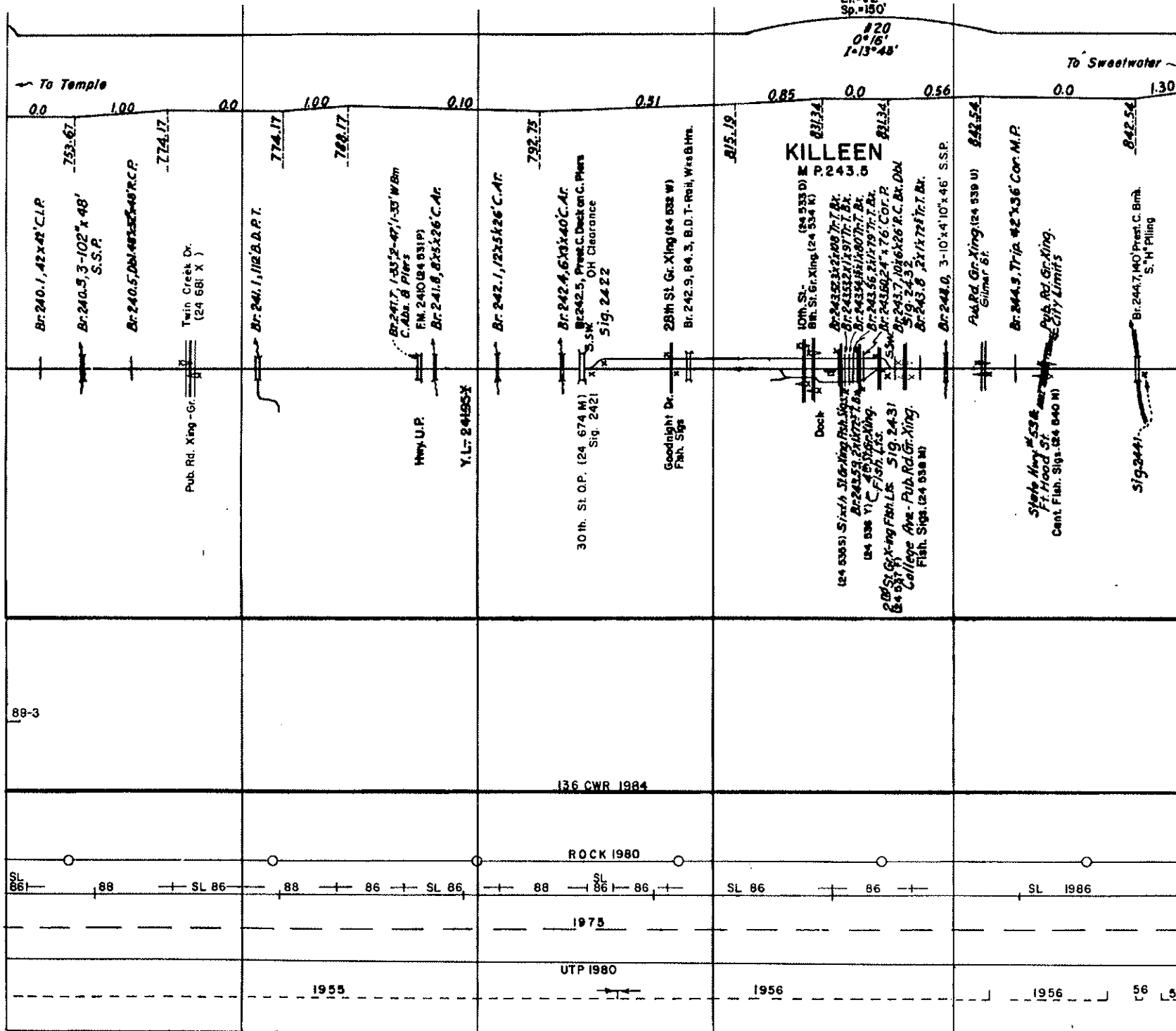
234

235









EI. = VZ  
 Sp. = 150'  
 #20  
 0° 16'  
 I = 13° 48'

88-3

SL 86

1975

UTP 1980

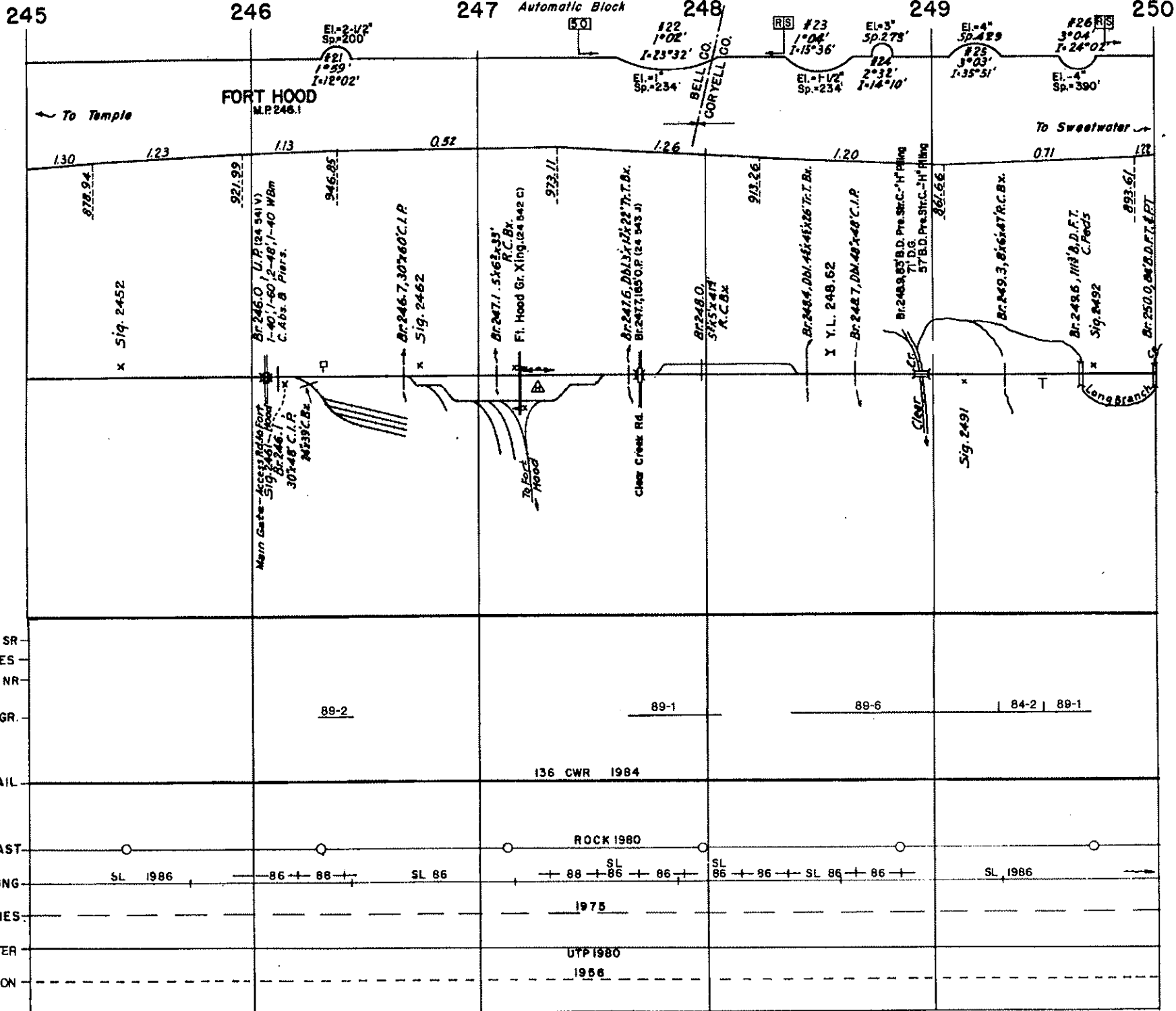
1955

1956

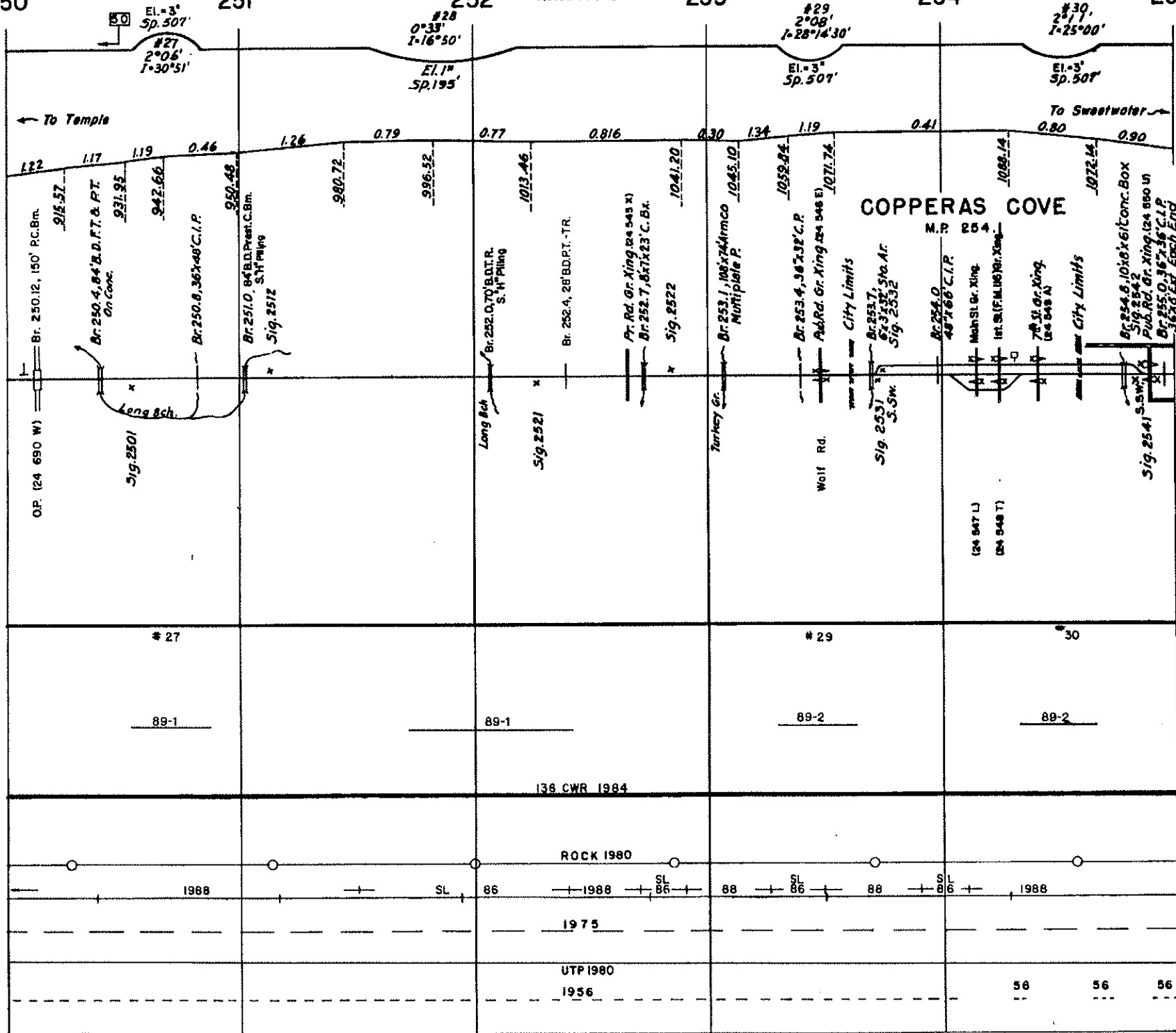
1956

56

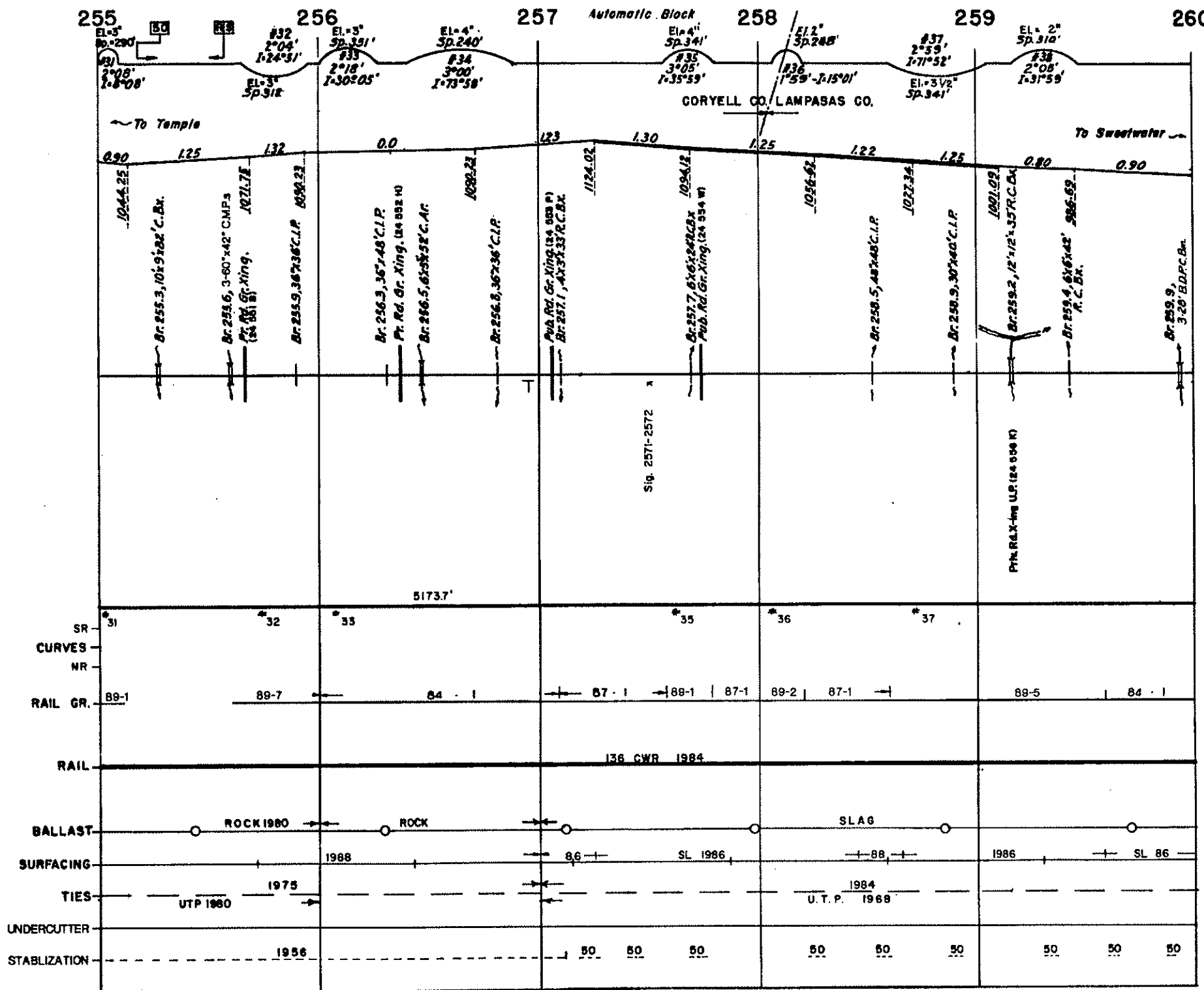
CURVES  
 RAIL GR  
 RAIL  
 BALLAST  
 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION



250 Automatic Block 253 254 255



SR  
CURVES  
NR  
RAIL GR.  
RAIL  
BALLAST  
SURFACING  
TIES  
UNDERCUTTER  
STABILIZATION



260

261

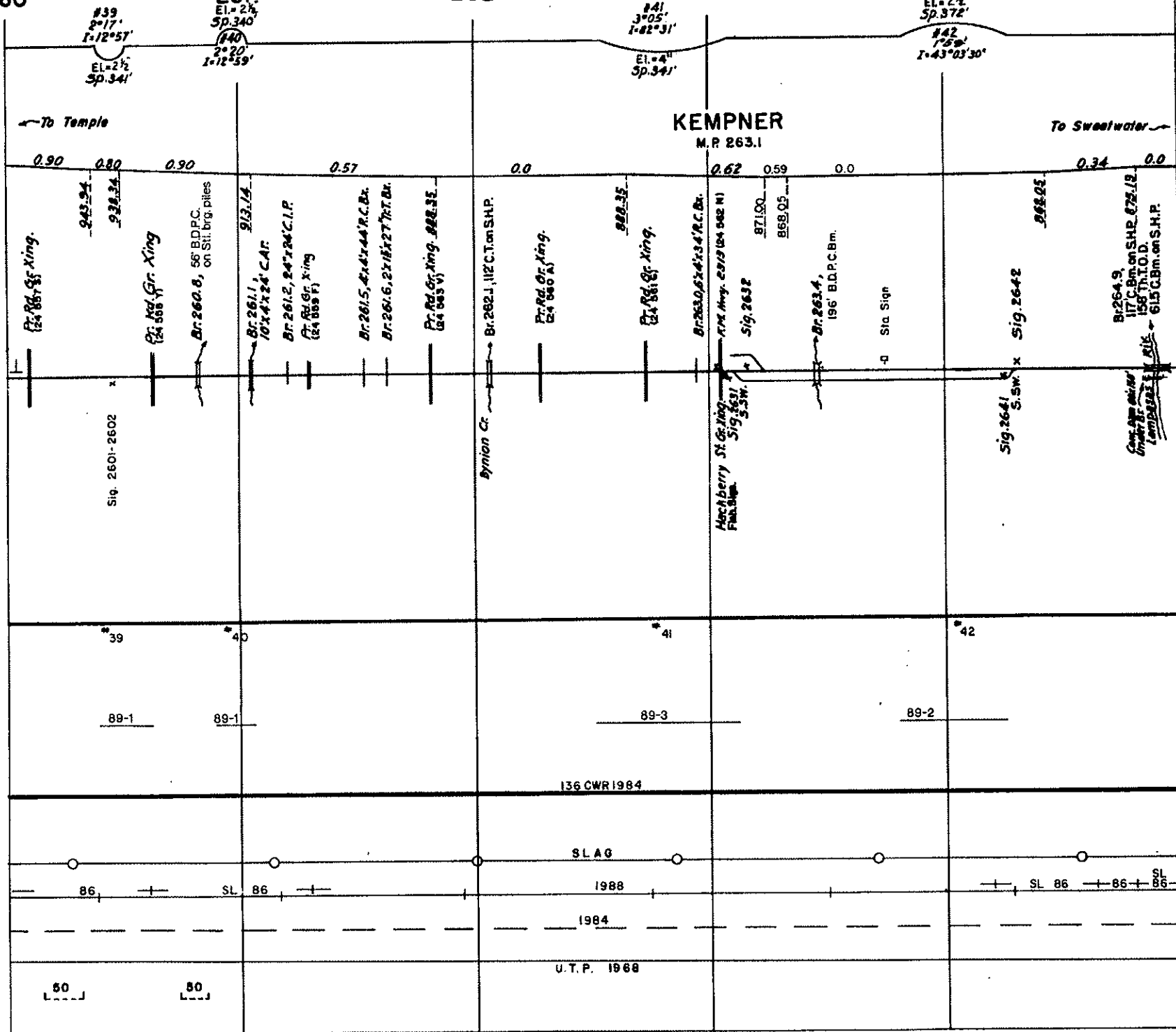
Automatic Block

262

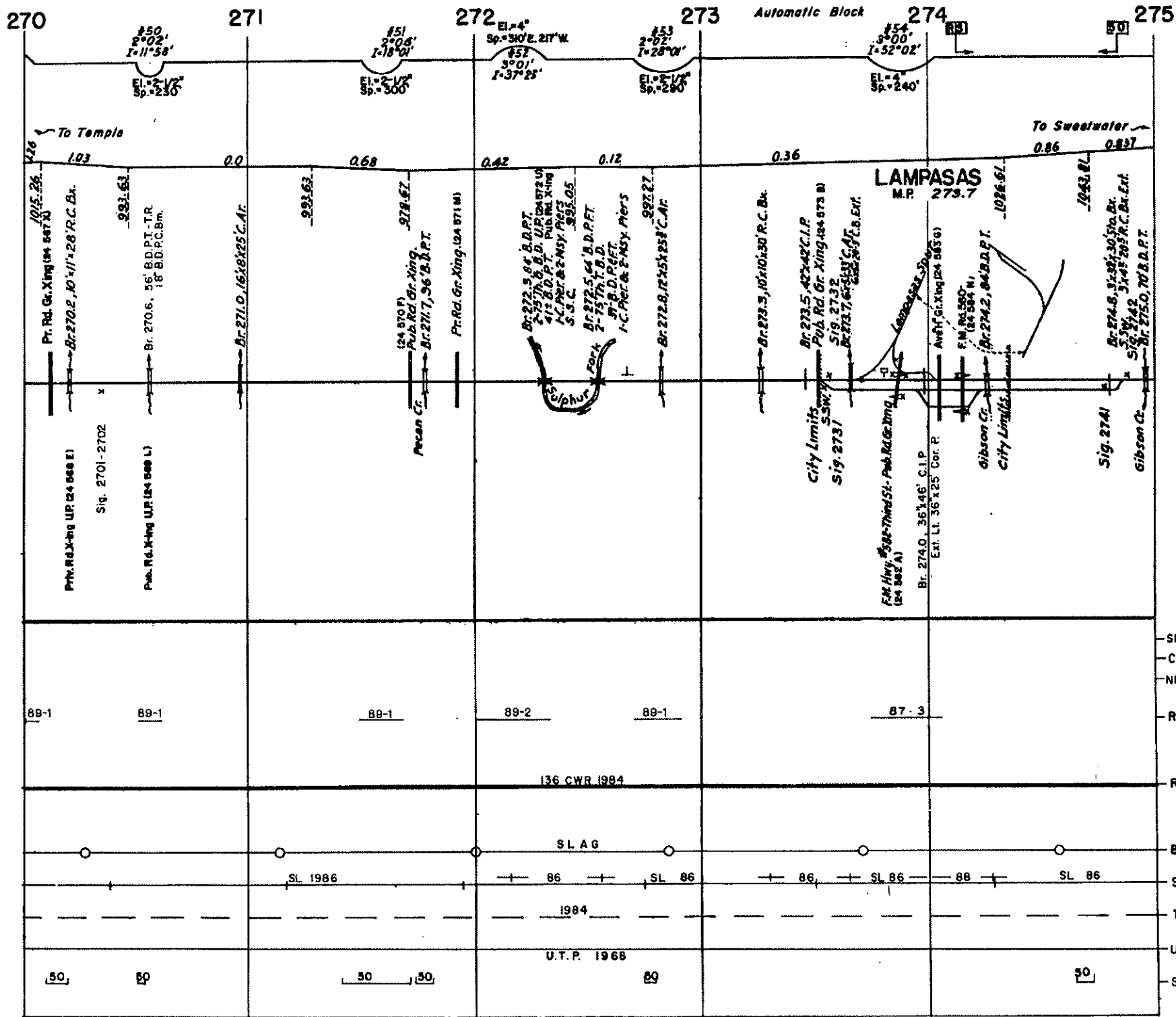
263

264

265









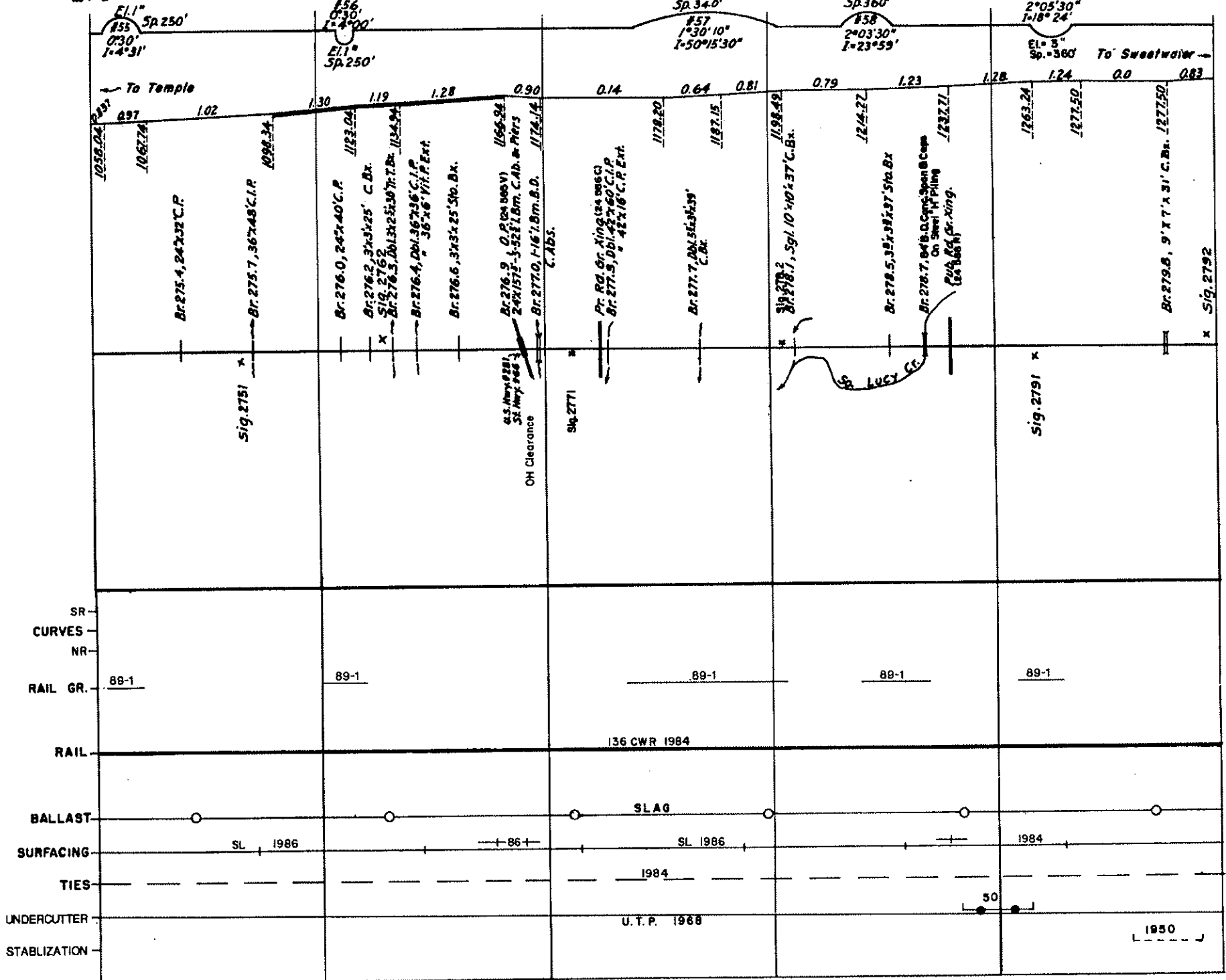
275

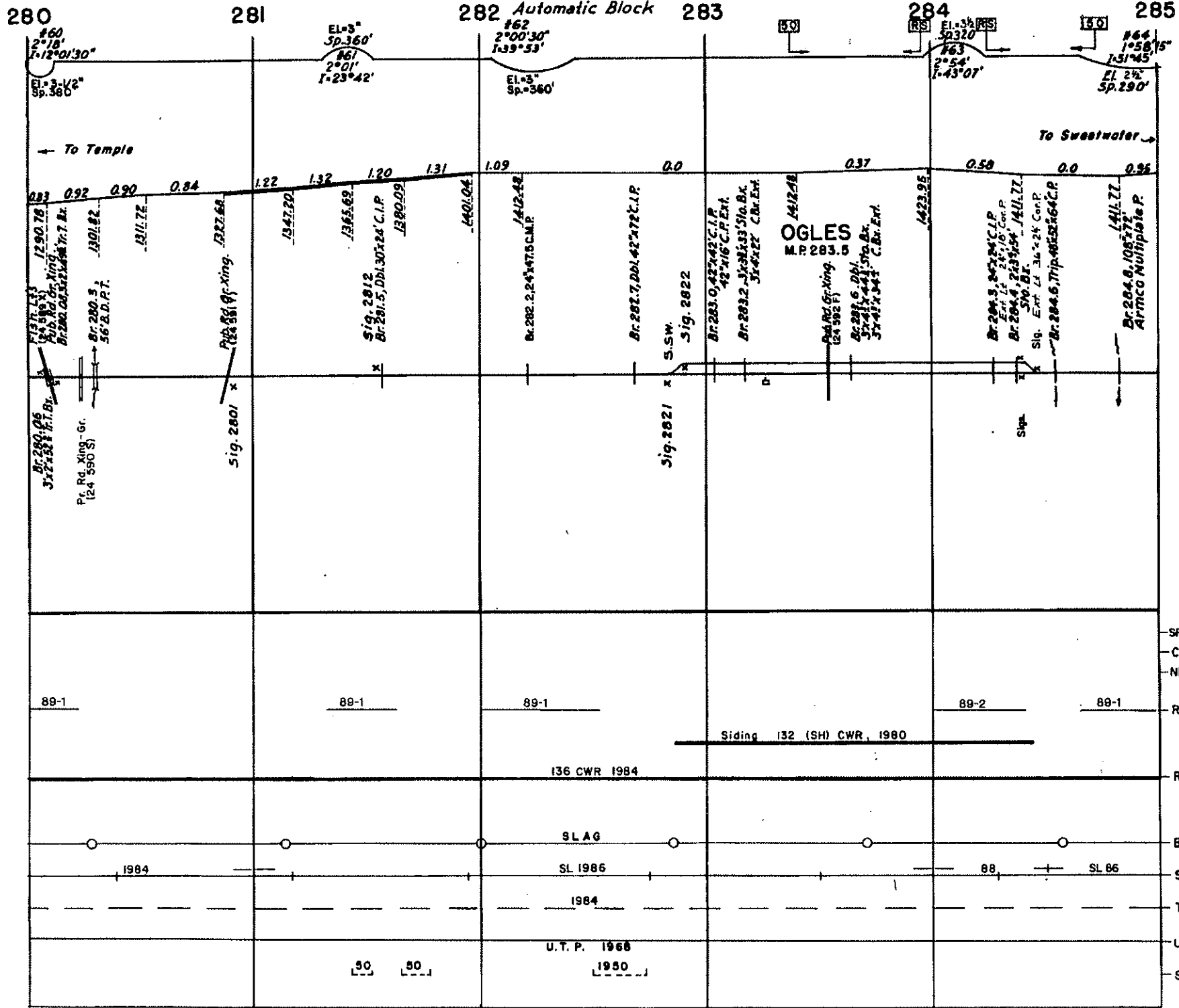
276

277 Automatic Block 278

279

280





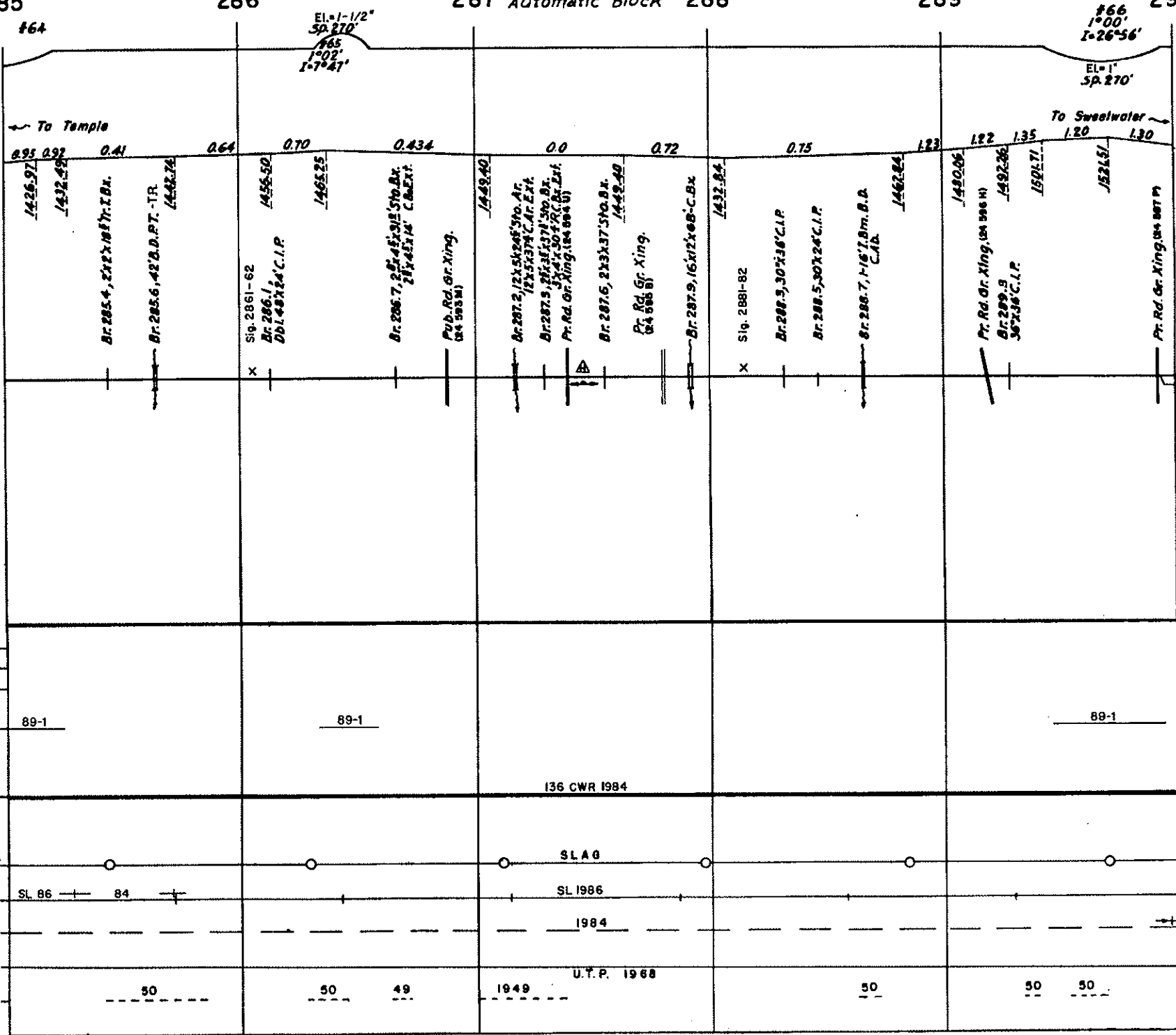
285

286

287 Automatic Block 288

289

290



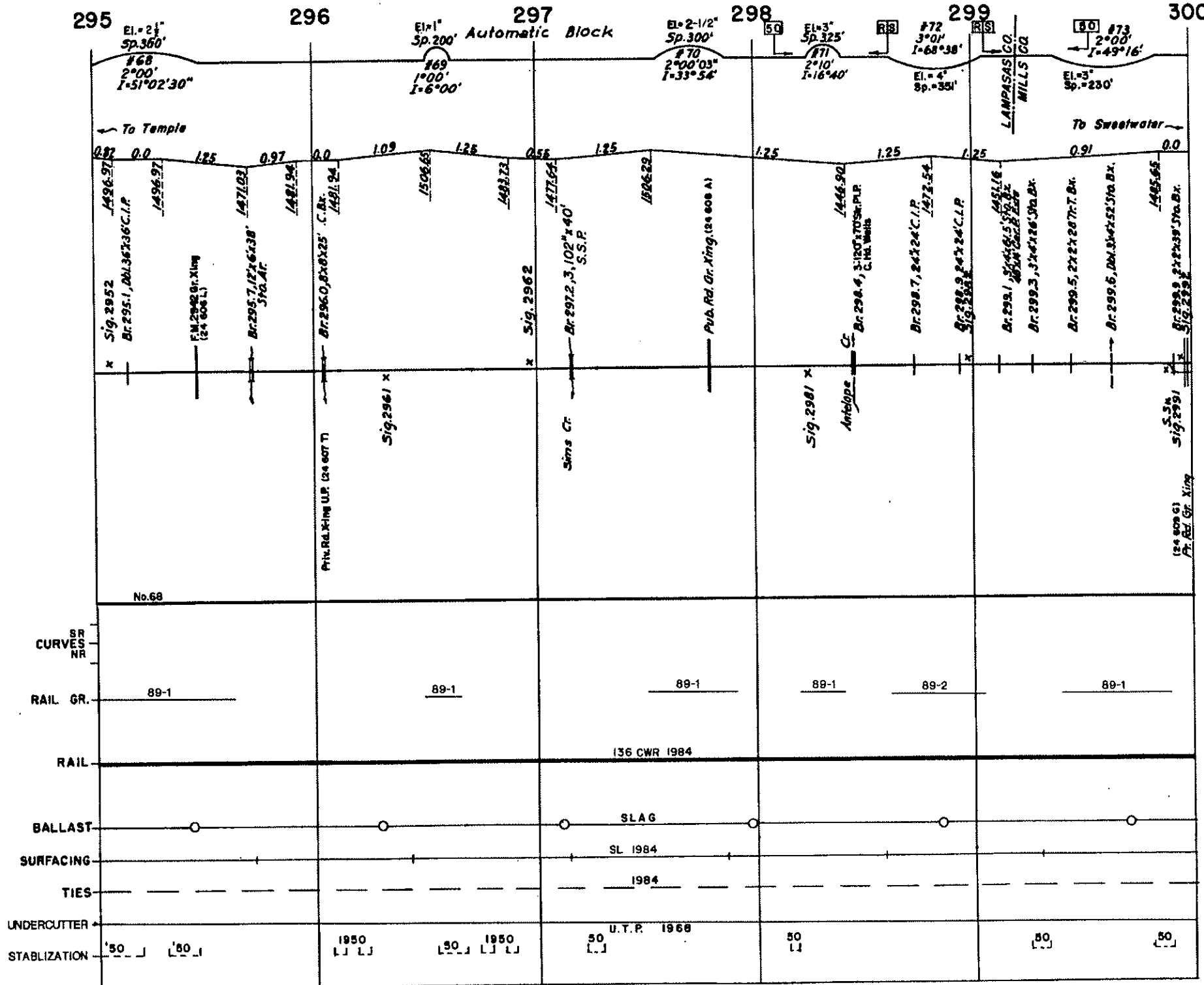
EL = 1-1/2"  
SP. 270'  
#65  
1°02'  
1-7°41'

#66  
1°00'  
1-26°56'  
EL = 1"  
SP. 270'

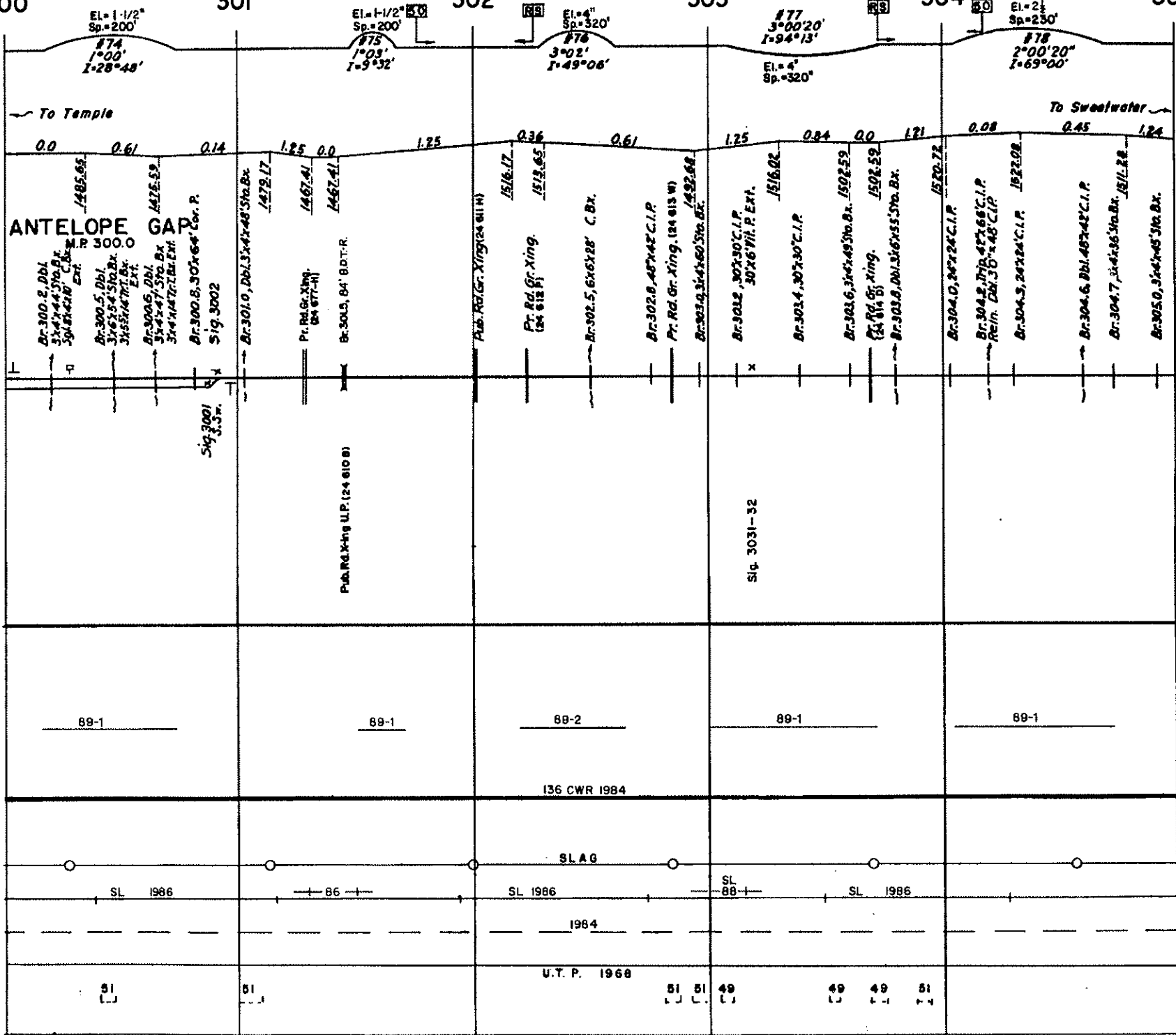
To Temple

To Sweetwater

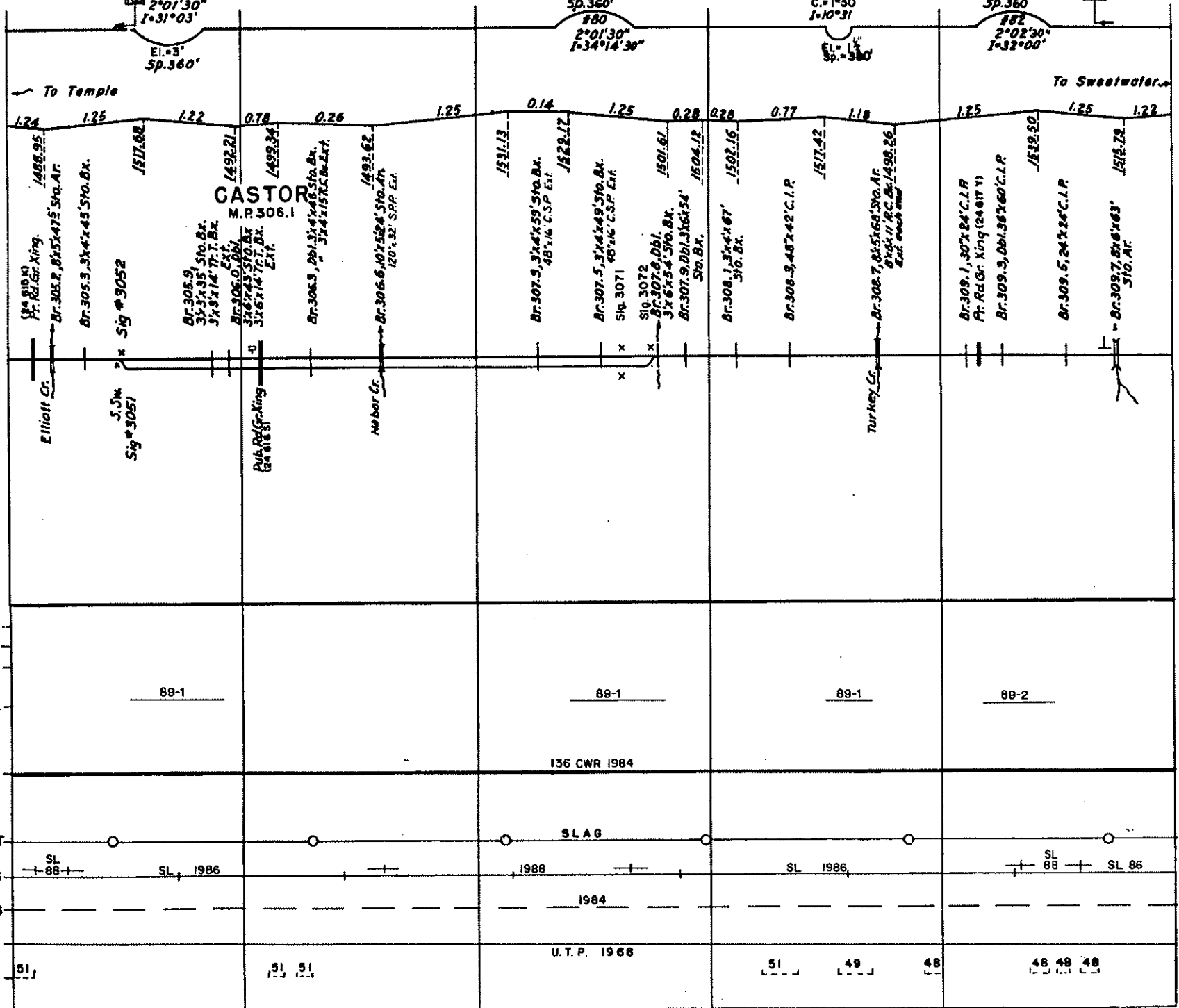




300 301 302 Automatic Block 303 304 305



305 306 307 Automatic Block 308 309 310



SR  
CURVES  
NR  
RAIL GR.  
RAIL  
BALLAST  
SURFACING  
TIES  
UNDERCUTTER  
STABILIZATION

89-1

89-1

89-1

89-2

136 CWR 1984

SLAG

SL 88

SL 1986

1988

SL 1986

SL 88

SL 86

1984

U.T.P. 1968

51

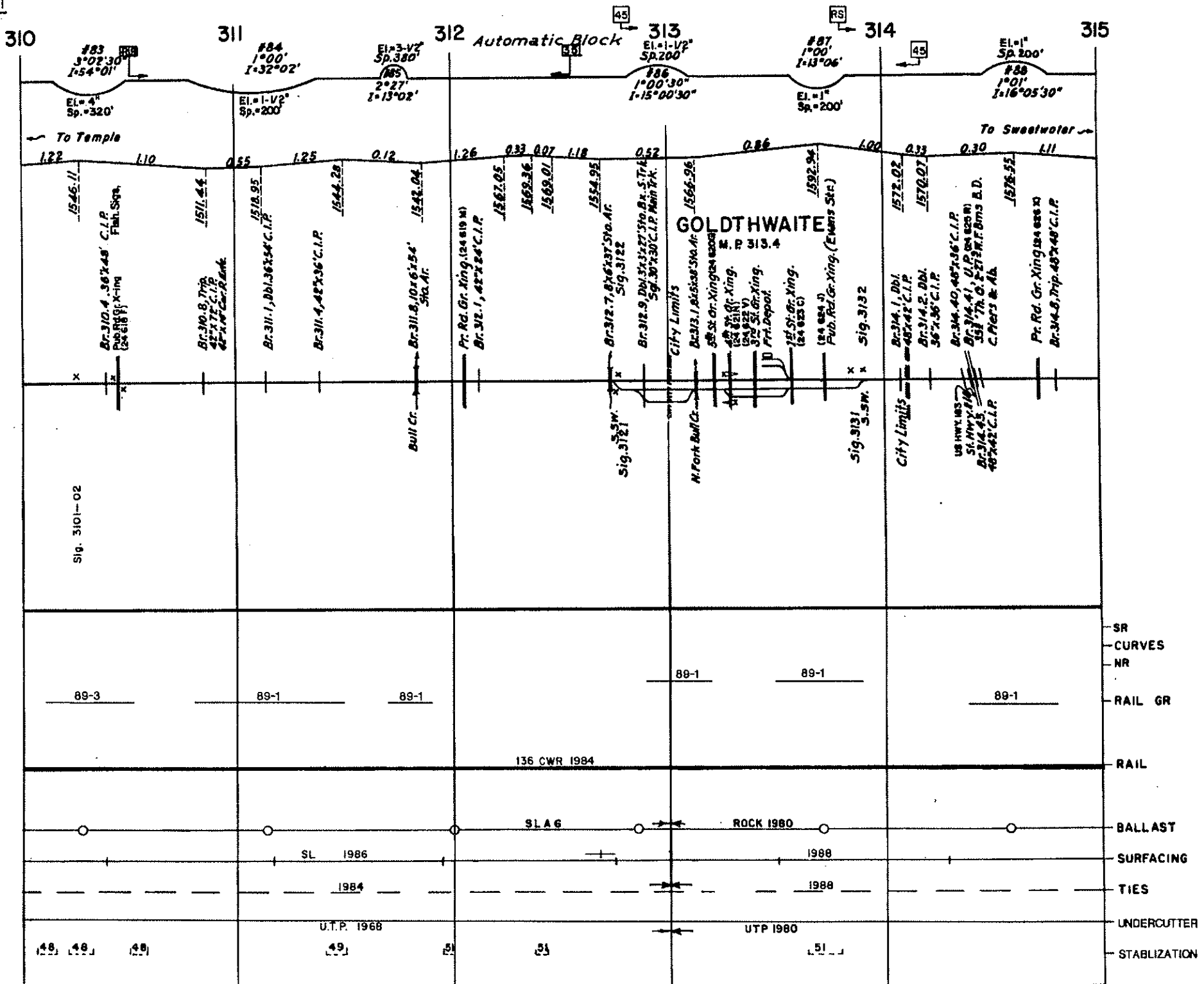
51 51

51

49

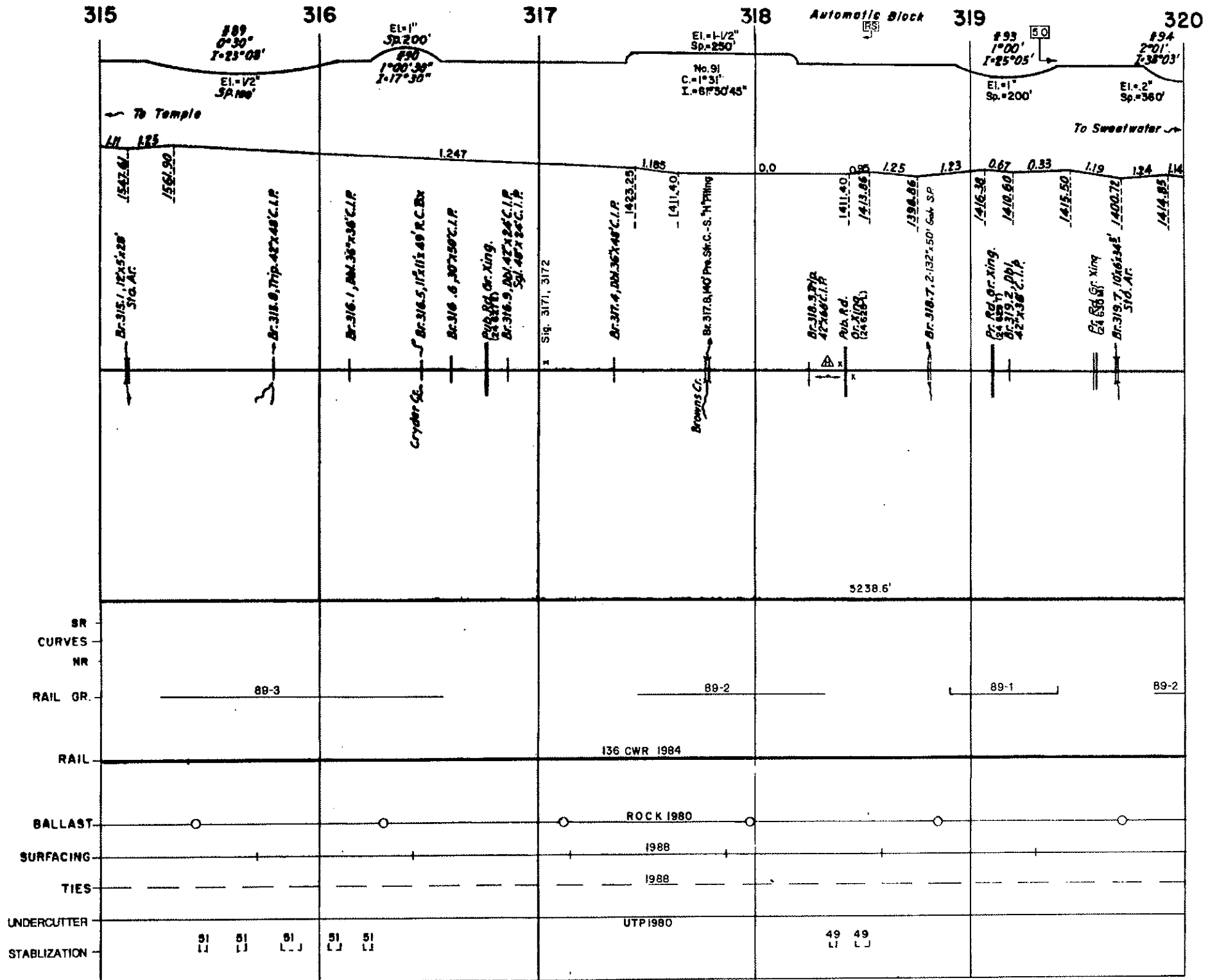
48

48 48 48

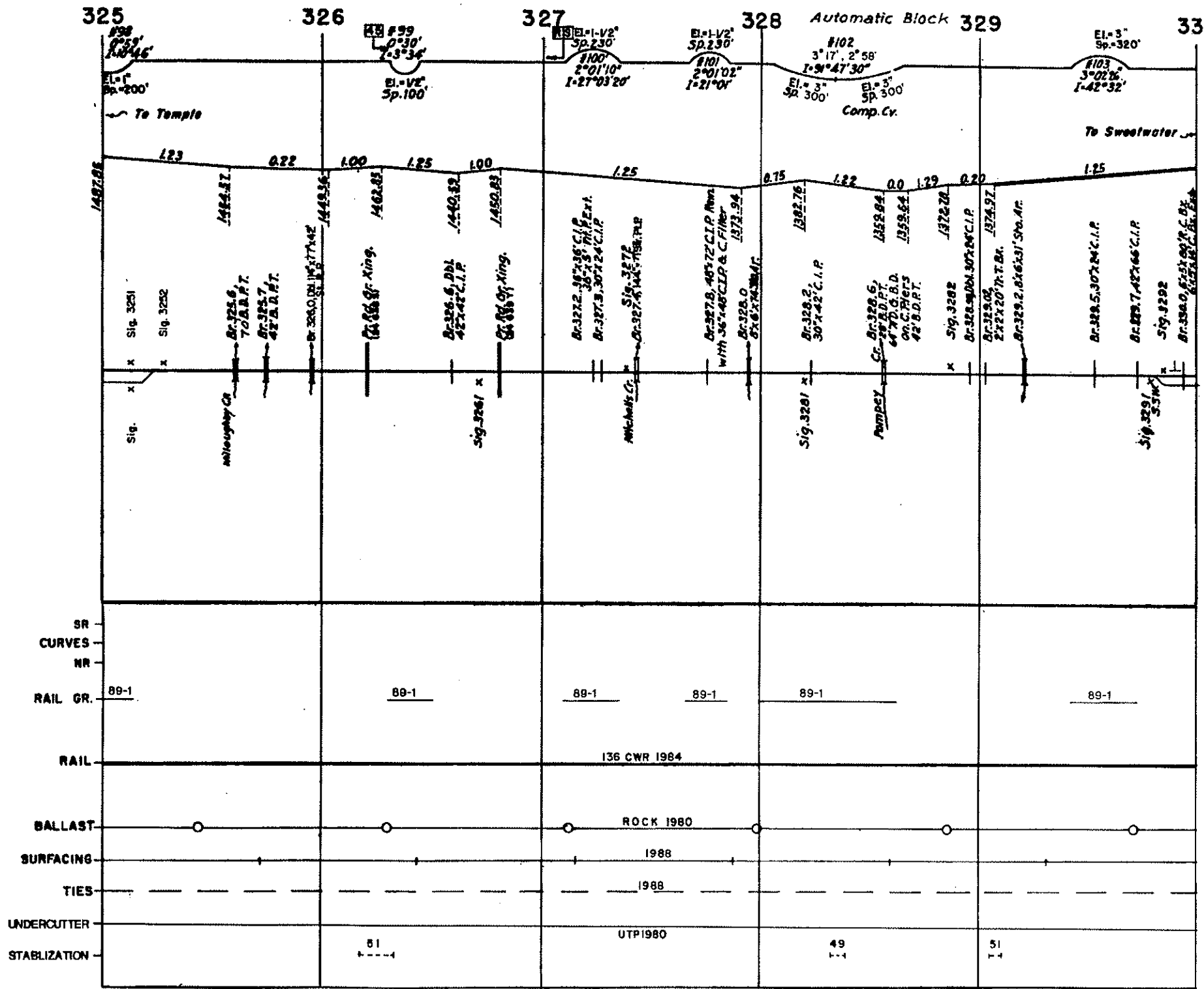


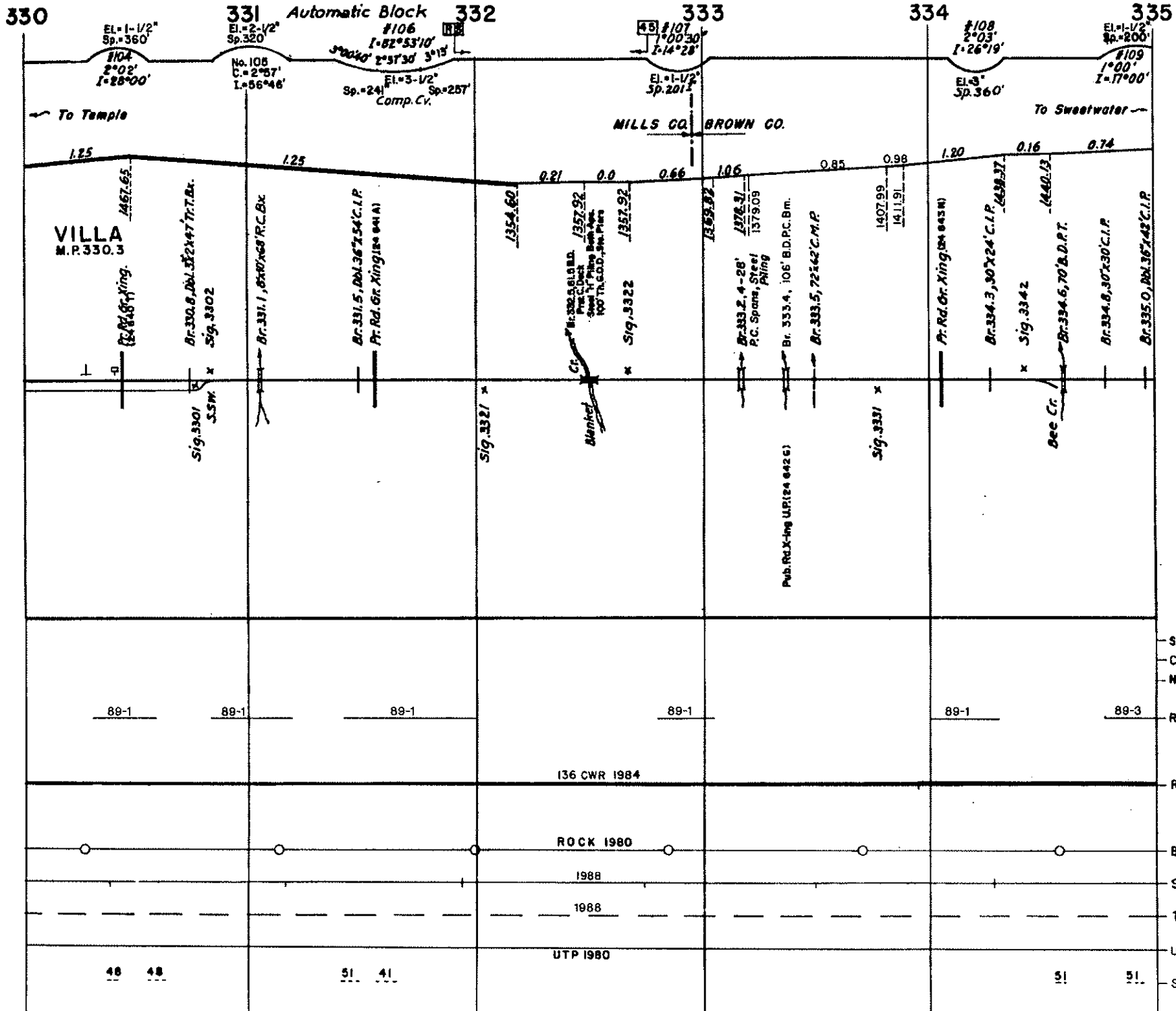
- SR
- CURVES
- NR
- RAIL GR
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION

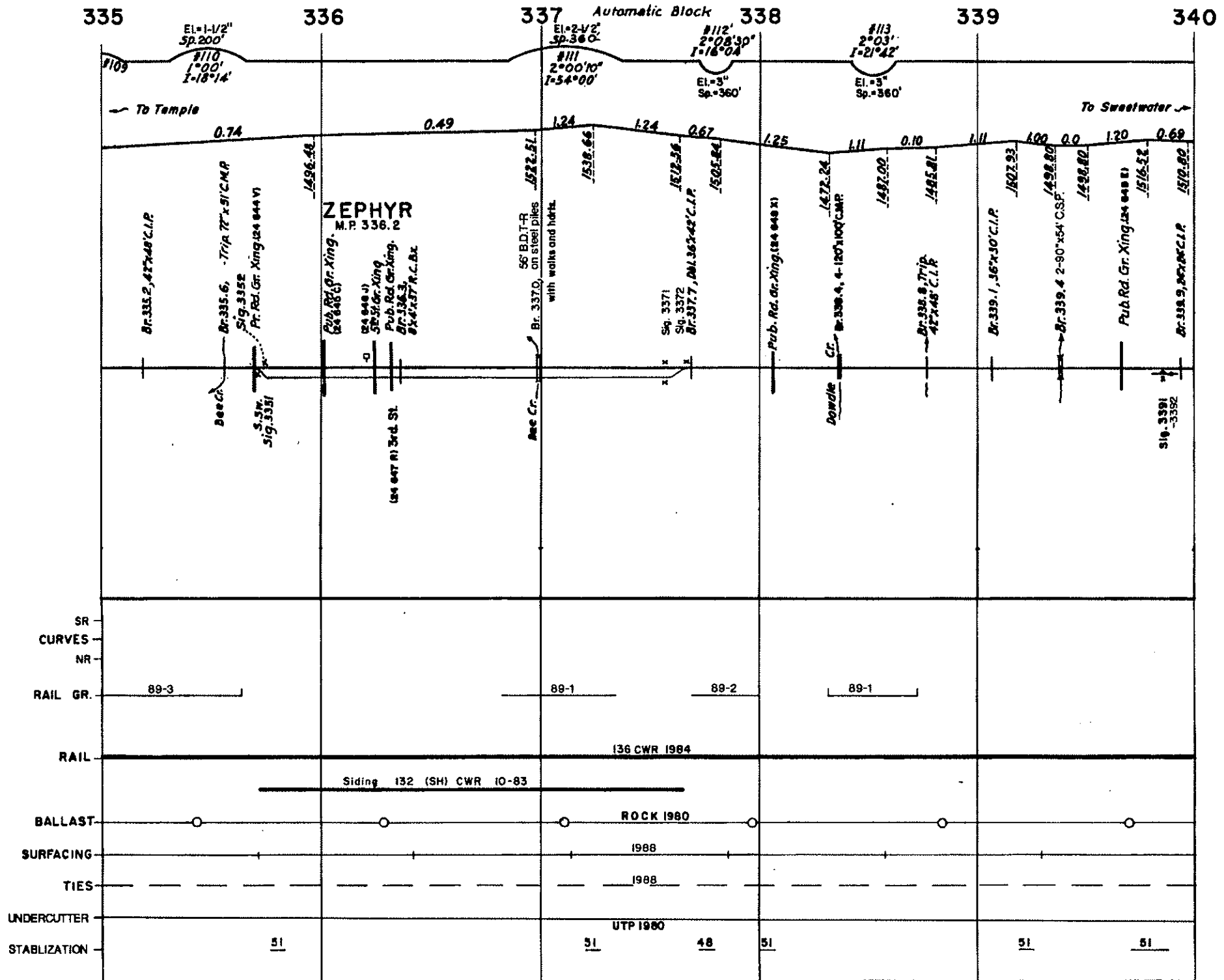


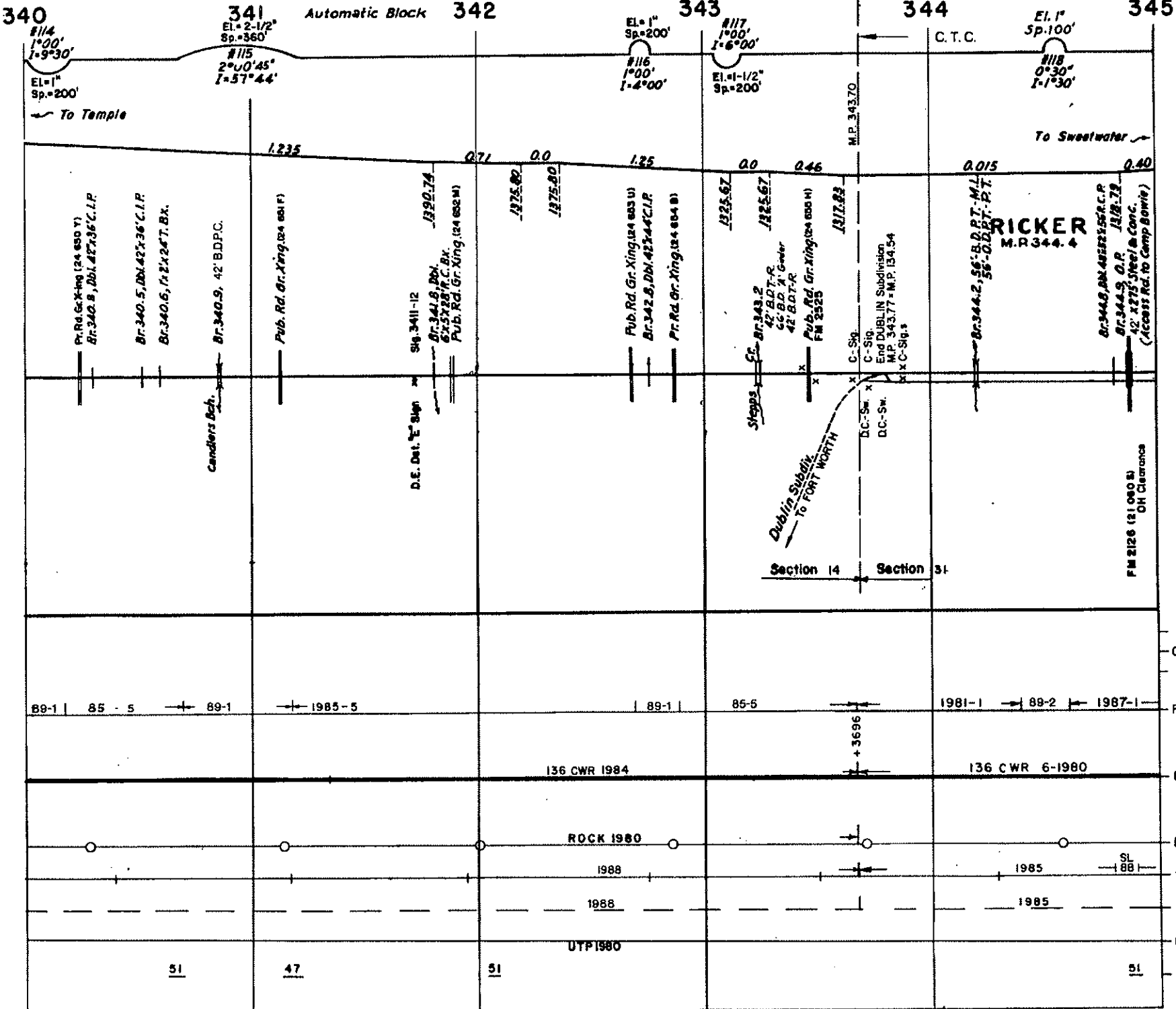




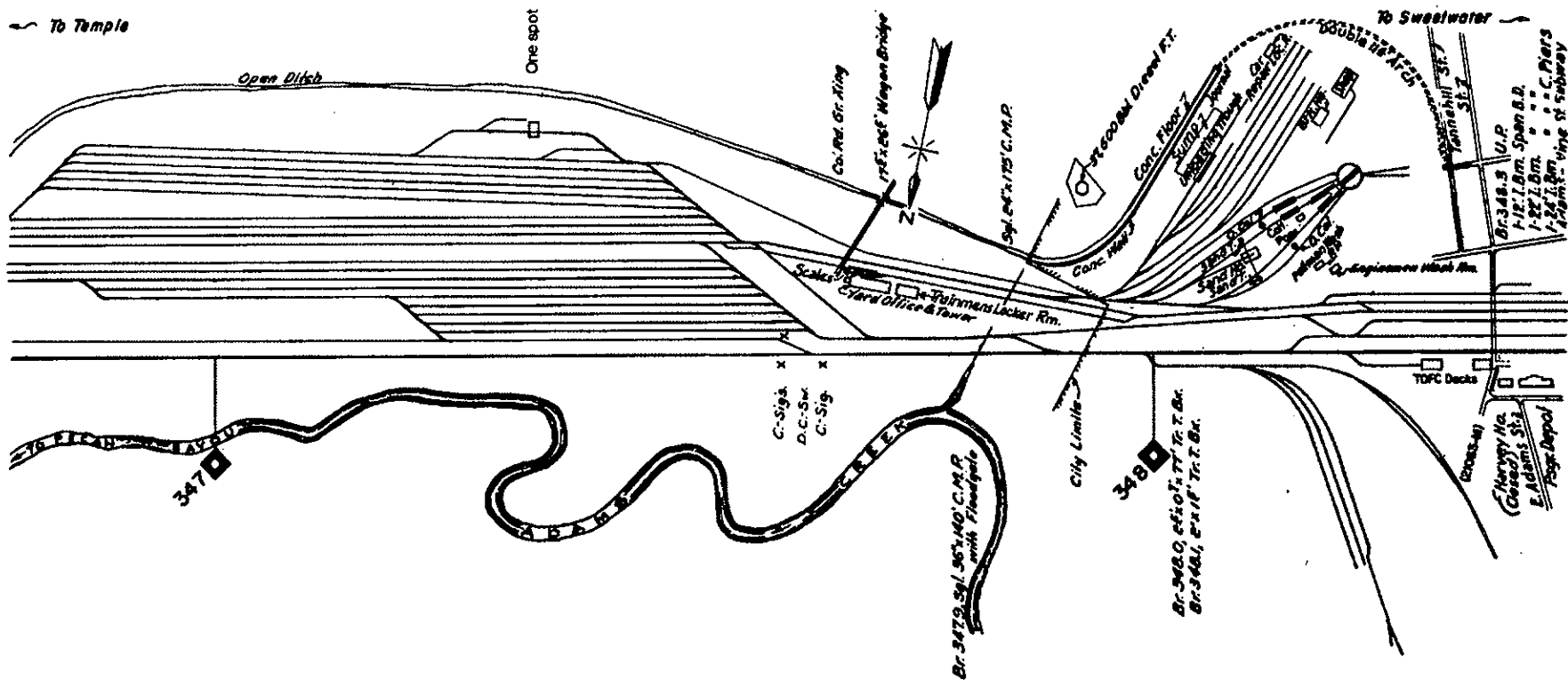








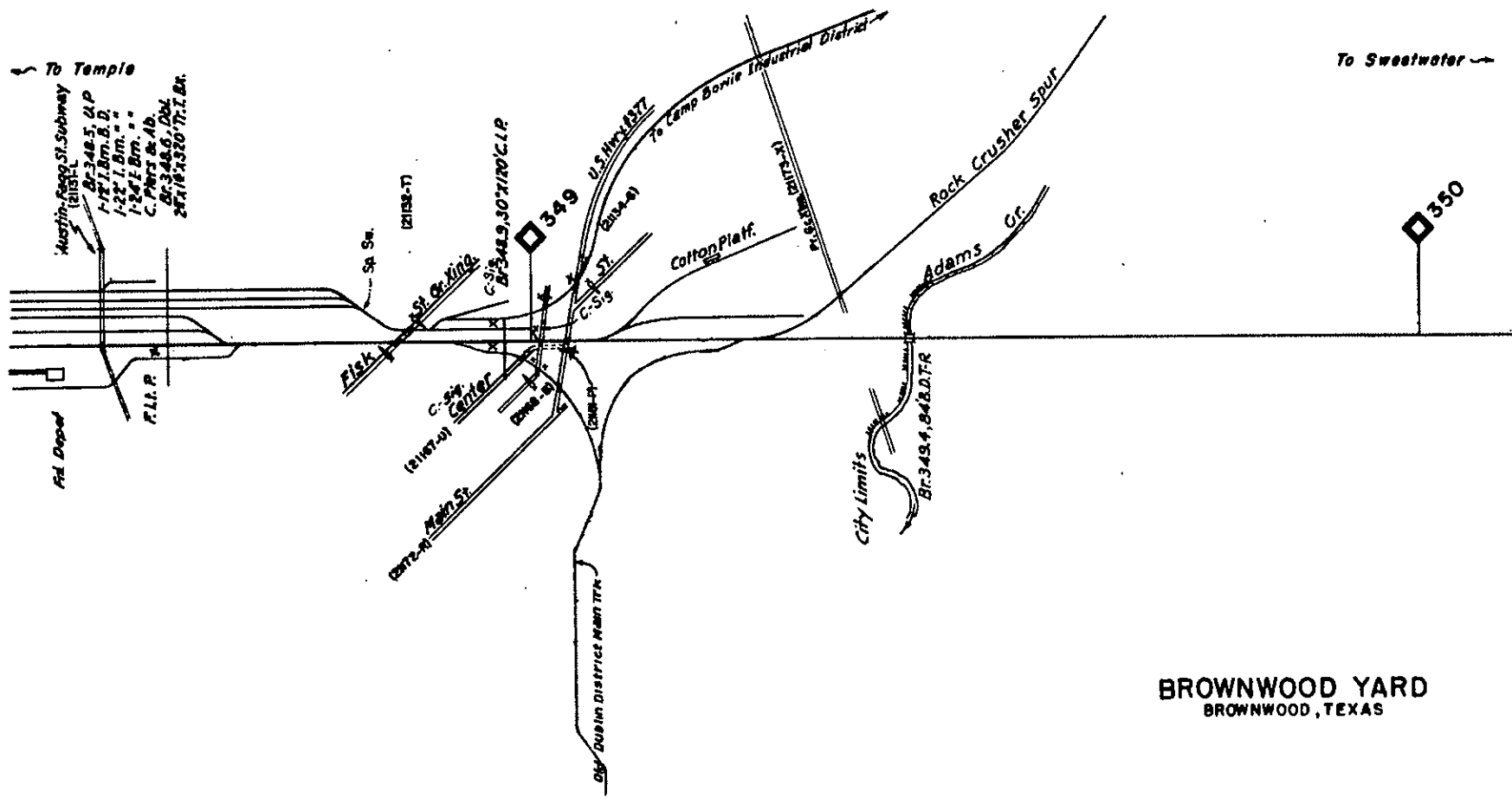




**BROWNWOOD YARD**  
BROWNWOOD, TEXAS

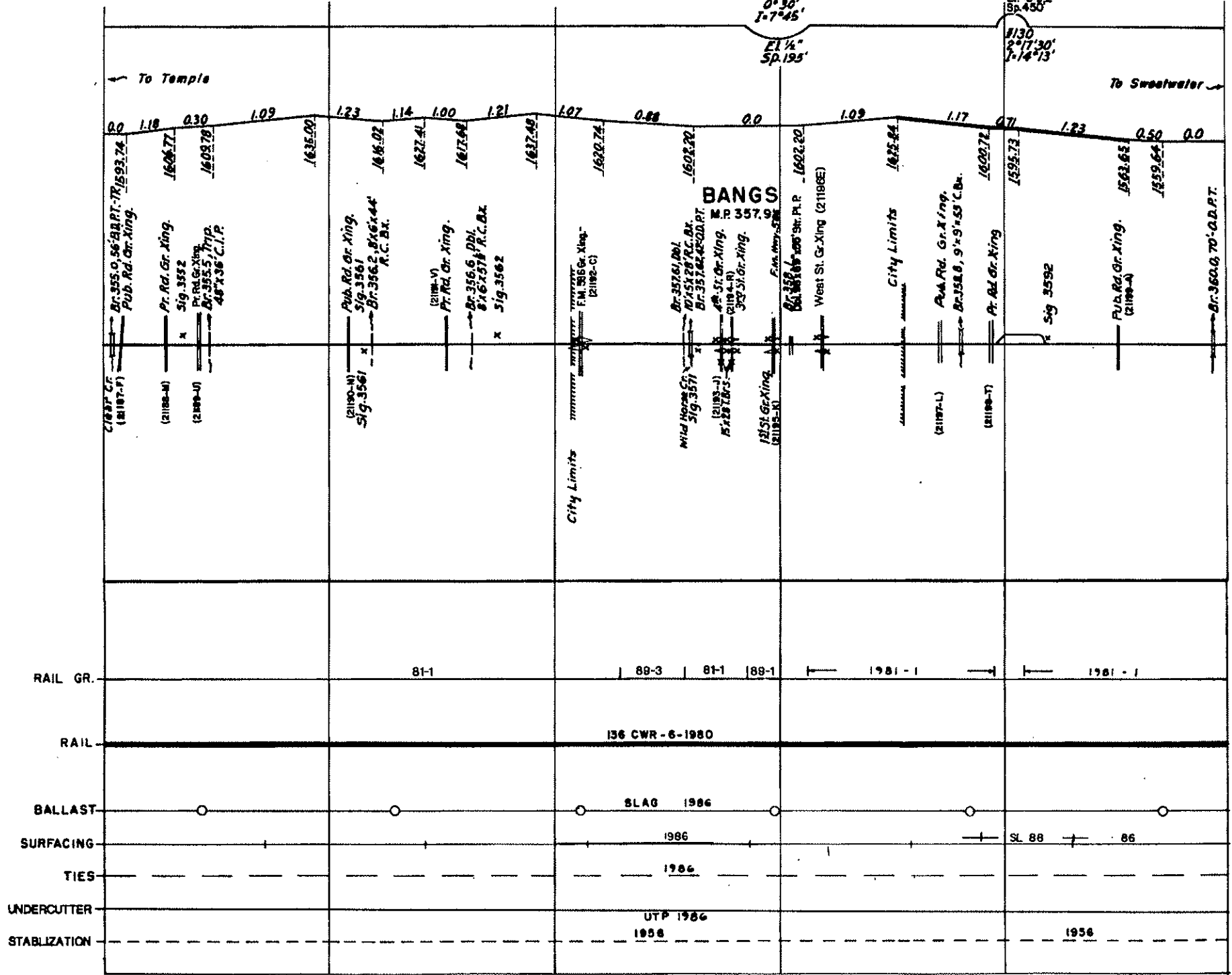
Longest Track 150 Cars  
 Shortest Track 2 Cars  
 Total Car Capacity Cars





**BROWNWOOD YARD**  
BROWNWOOD, TEXAS





360

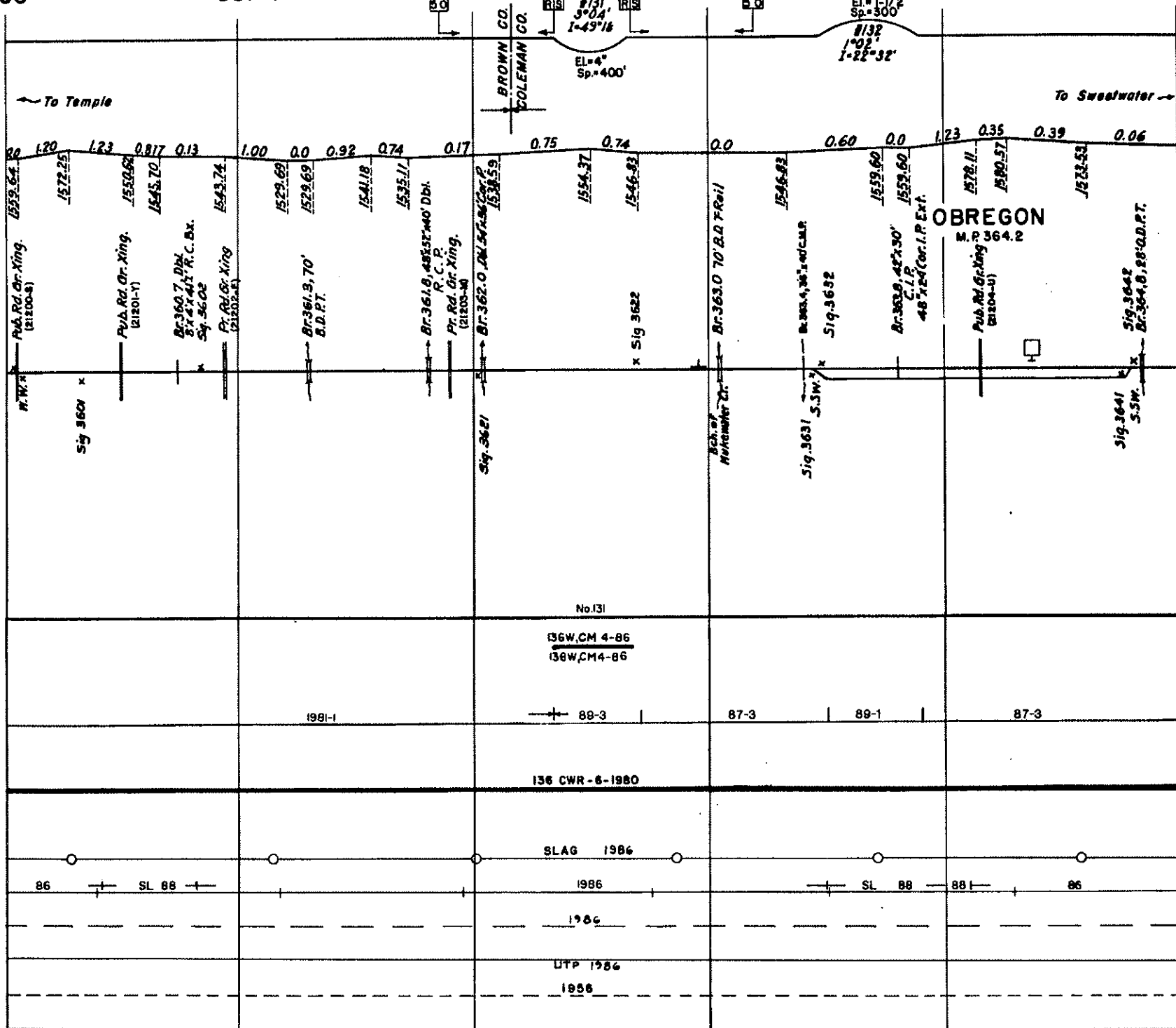
361 Automatic Block

362

363

364

365



365

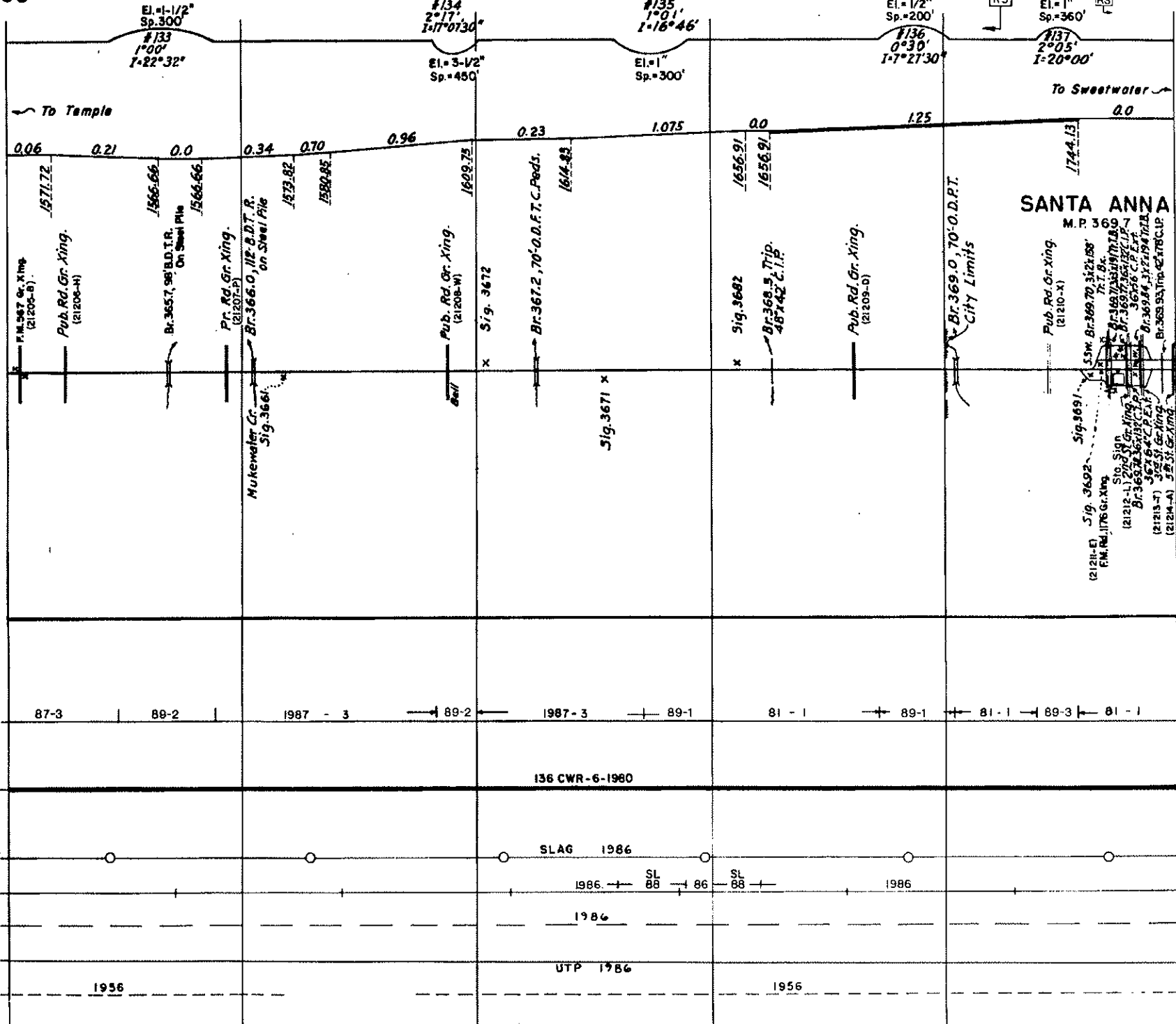
366

367 Automatic Block

368

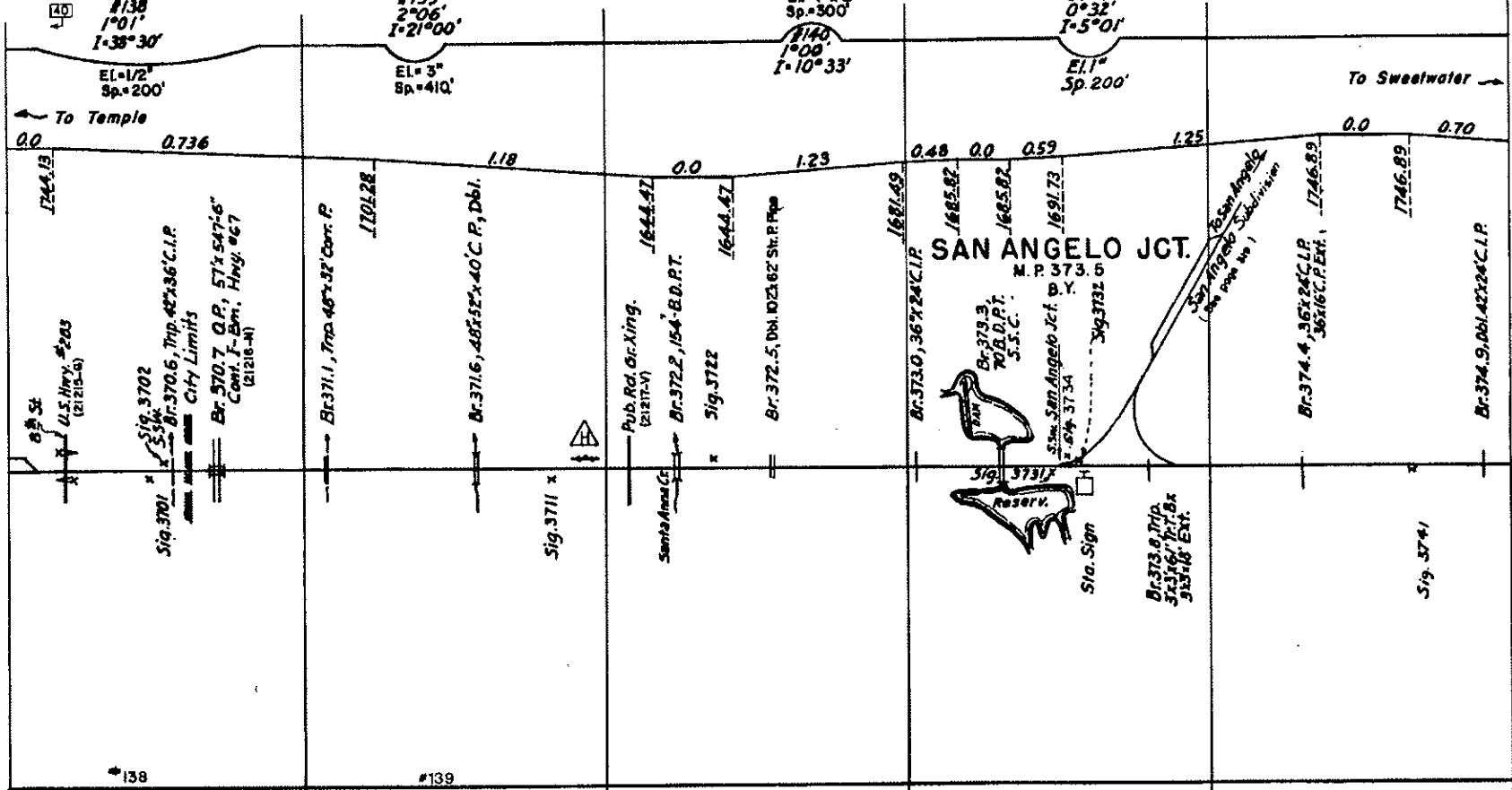
369

370



RAIL GR.	87-3	89-2	1987 - 3	89-2	1987 - 3	89-1	81 - 1	89-1	81 - 1	89-3	81 - 1
RAIL					136 CWR-6-1980						
BALLAST					SLAG 1986						
SURFACING					1986	SL 88	86	SL 88	1986		
TIES					1986						
UNDERCUTTER					UTP 1986						
STABILIZATION	1956								1956		

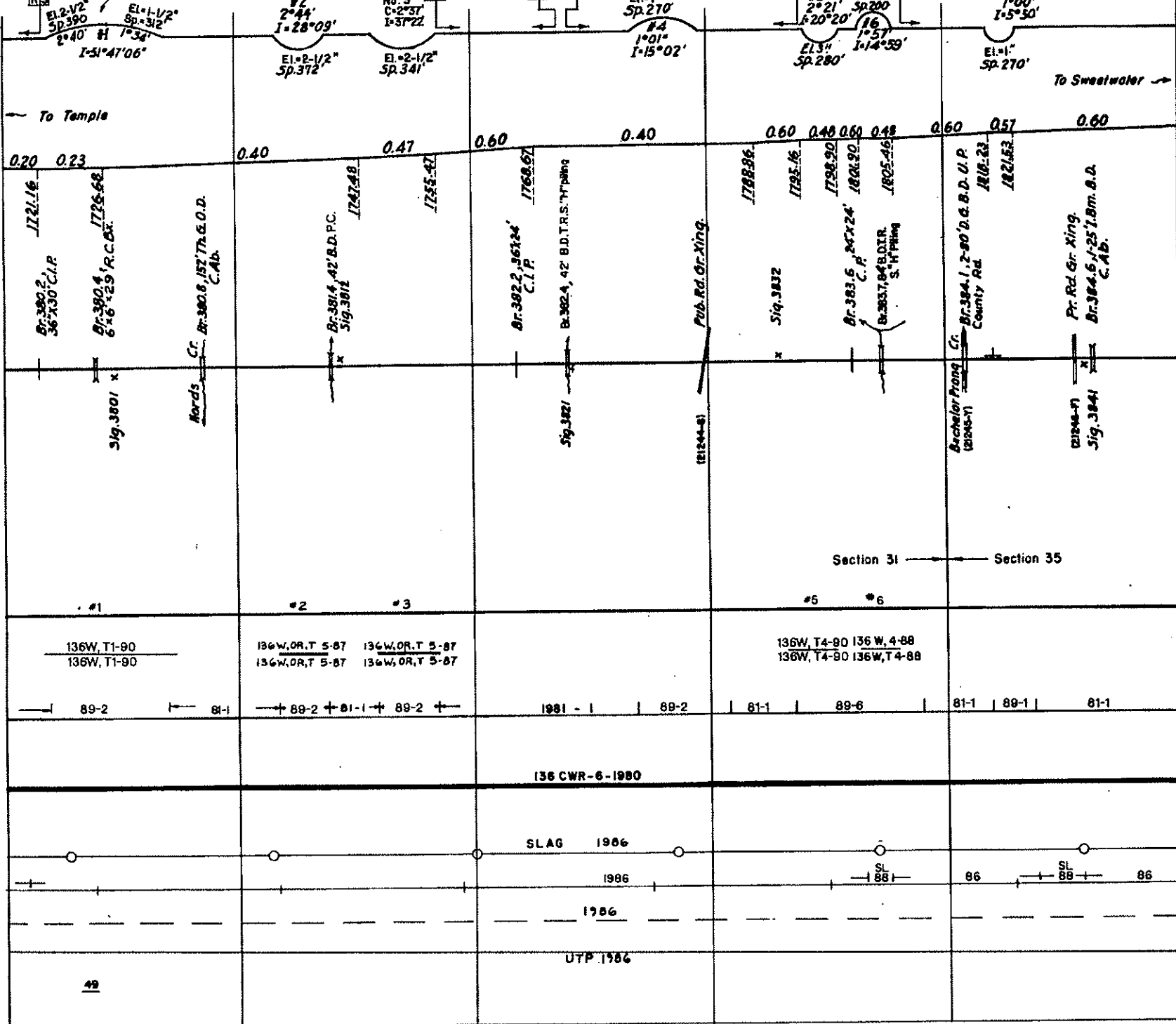
370 371 372 Automatic Block 373 374 375



#138		#139									
136 W, T 4-88 136 W, T 4-88		136 W, T 3-90 136 W, T 3-90								CURVES	
69-2	81-1	89-3	1981-1	89-1	81-1	89-2	81-1	1981-3	81-1	81-3	RAIL GR.
136 CWR-6-1980											
SLAG 1986											
1986											
SL 1988											
1986											
UTP 1986											
1936											
1956											
1936											
RAIL											
BALLAST											
SURFACING											
TIES											
UNDERCUTTER											
STABILIZATION											



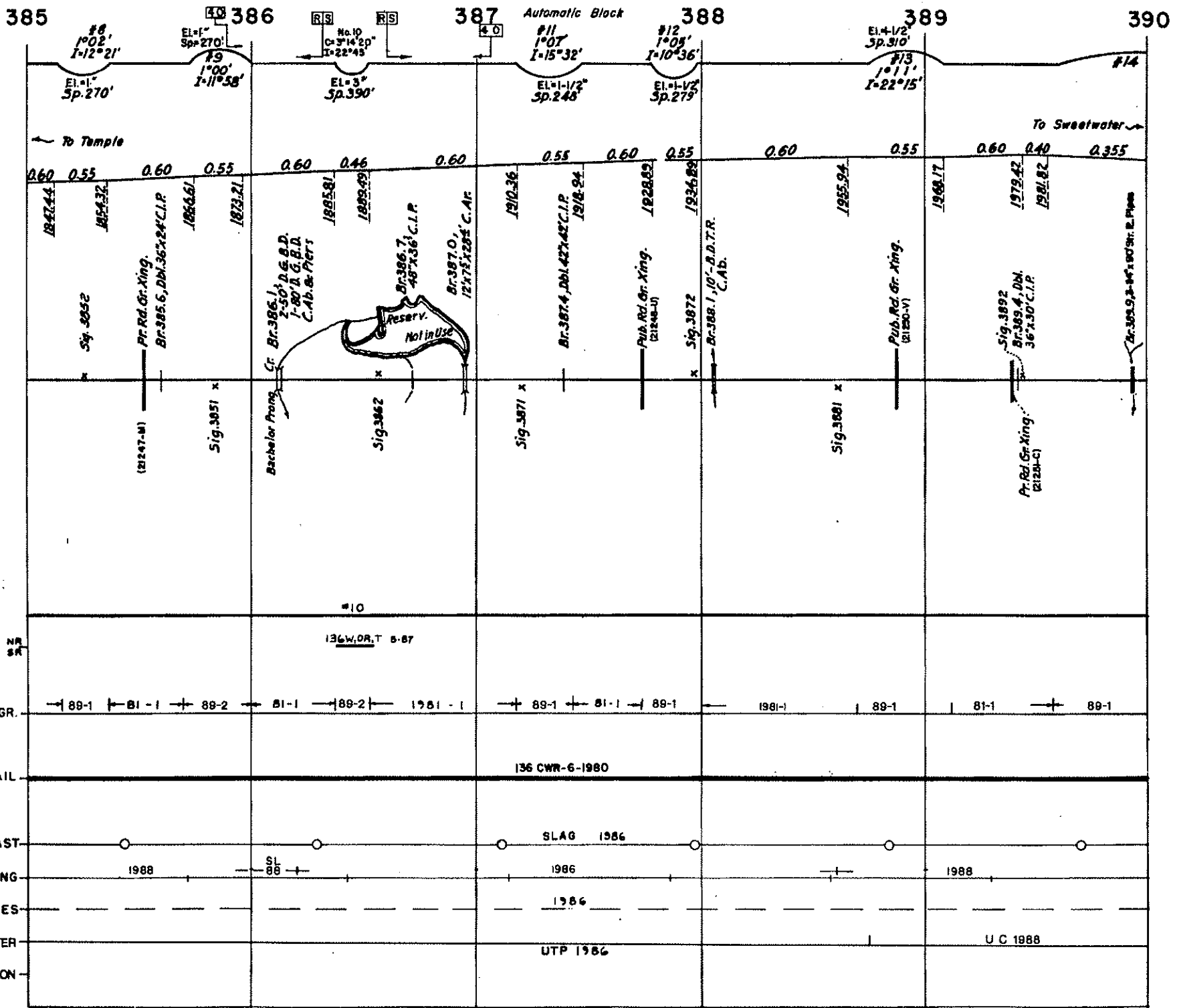
380 381 382 383 384 385

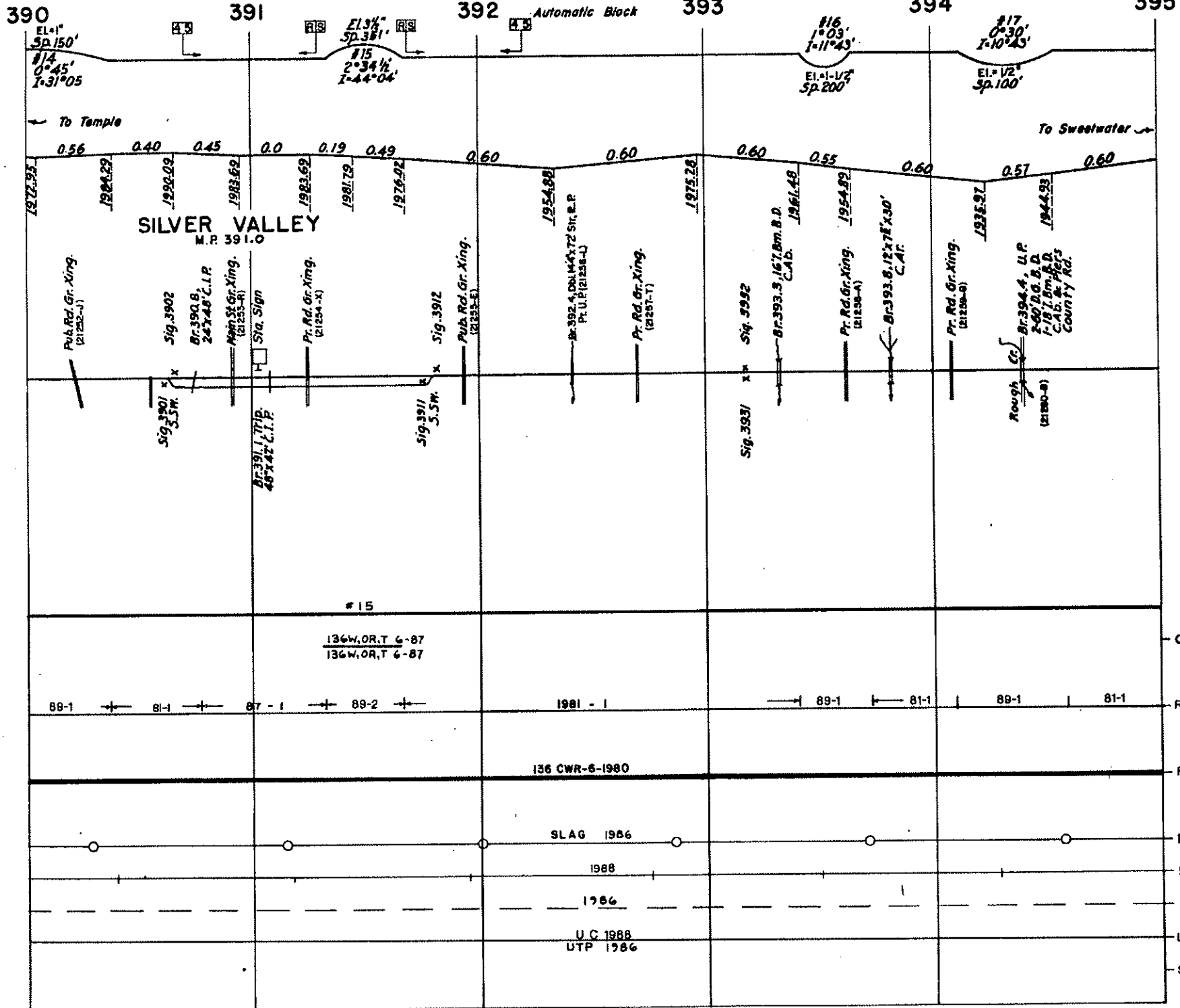


136W, T1-80 136W, T1-90		136W, OR, T 5-87 136W, OR, T 5-87	136W, OR, T 5-87 136W, OR, T 5-87	136W, T4-80 136W, 4-88 136W, T4-80 136W, T4-88	
89-2	81-1	89-2	81-1	89-2	81-1
136 CWR-6-1980					
SLAG 1986					
1986					
1986					
UTP 1986					

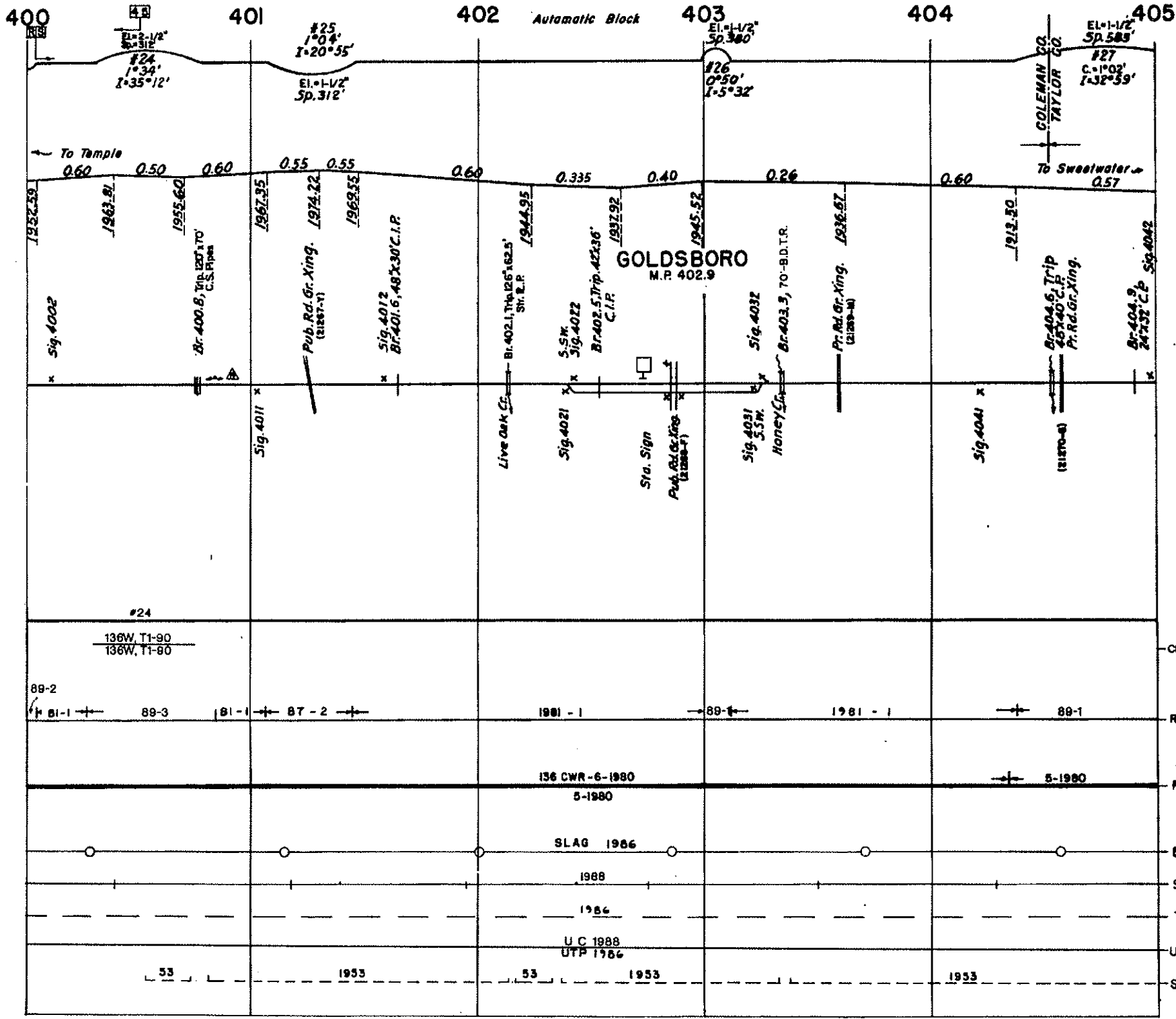
CURVES  
RAIL GR.  
RAIL  
BALLAST  
SURFACING  
TIES  
UNDERCUTTER  
STABILIZATION





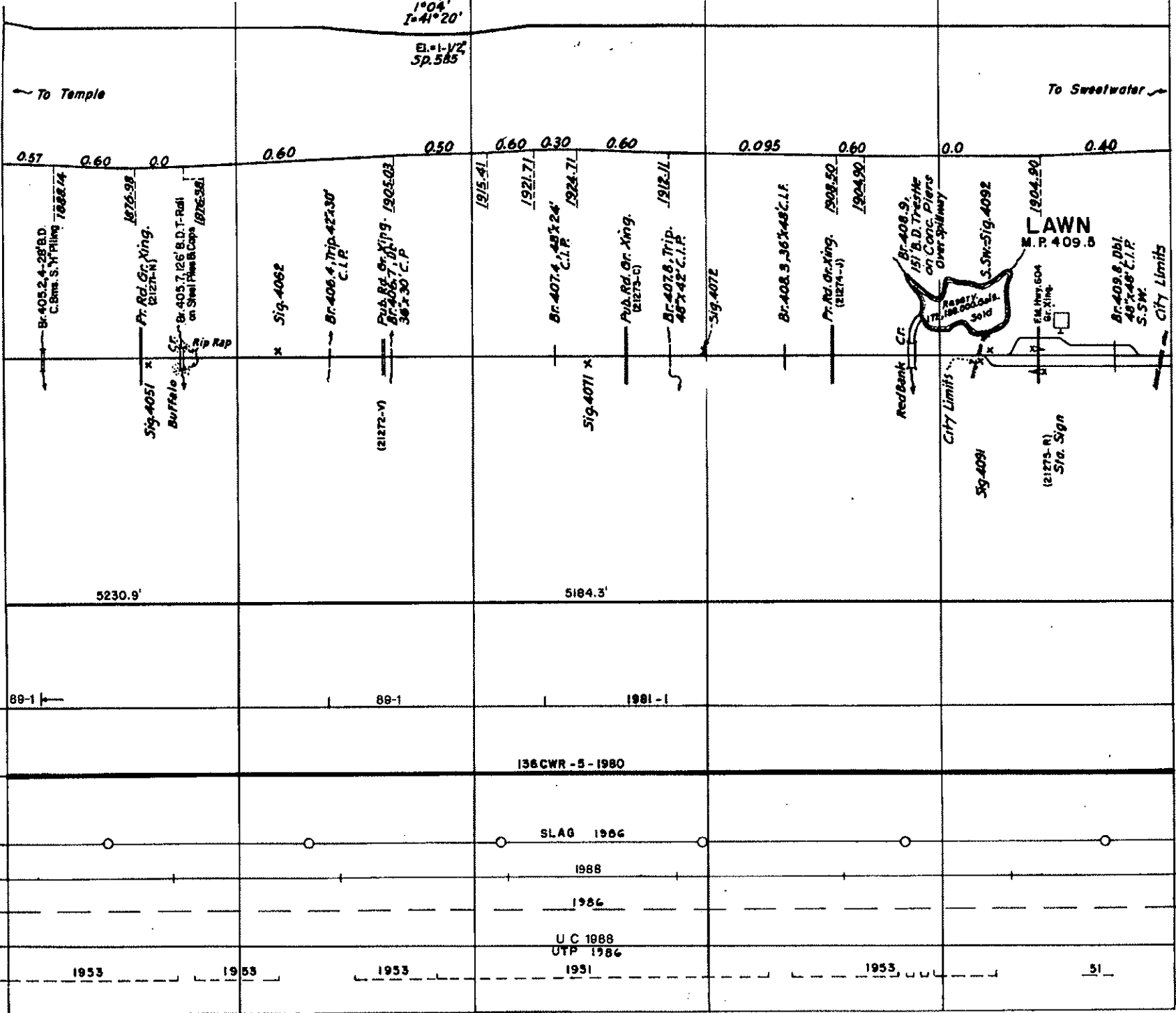


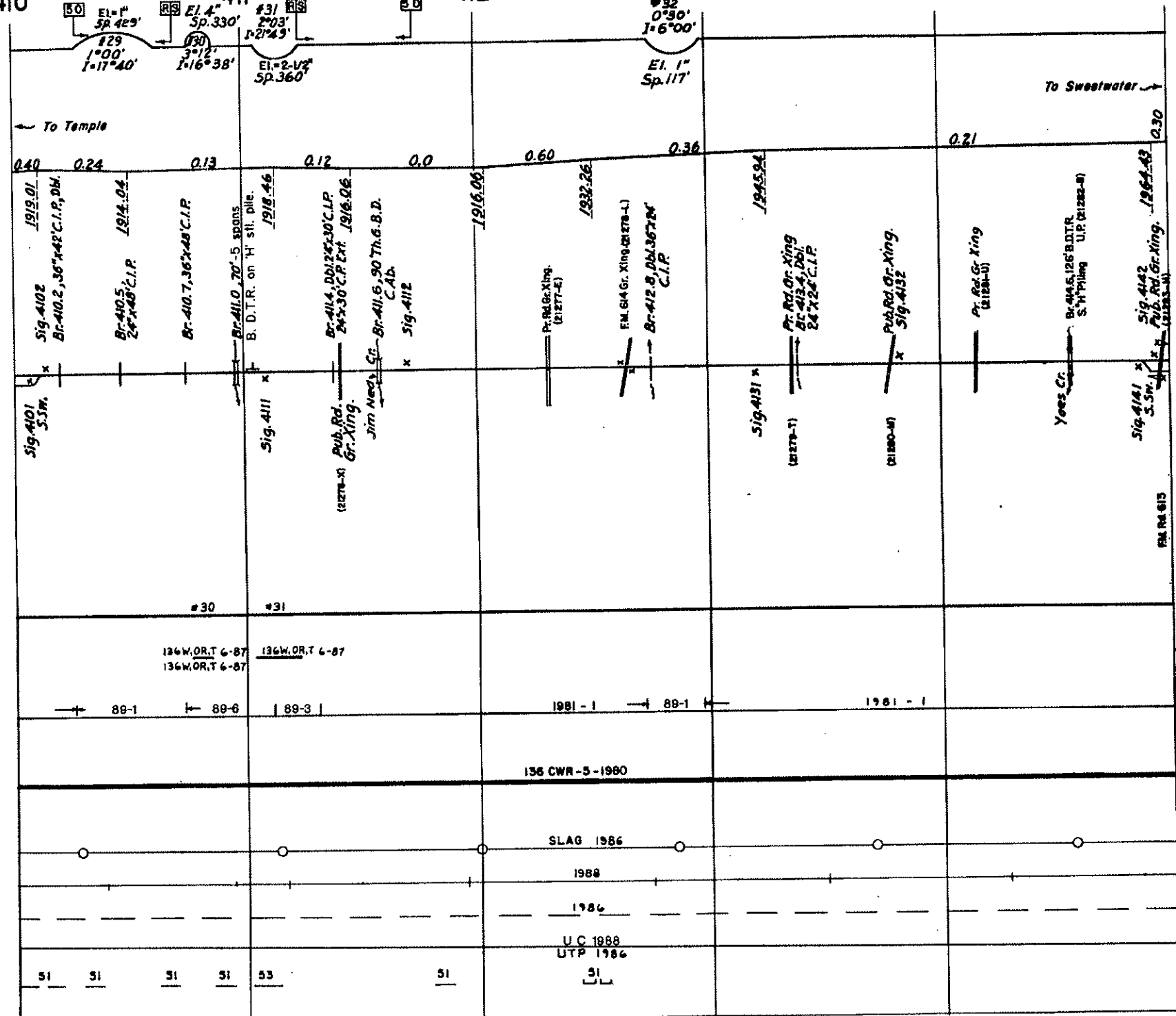




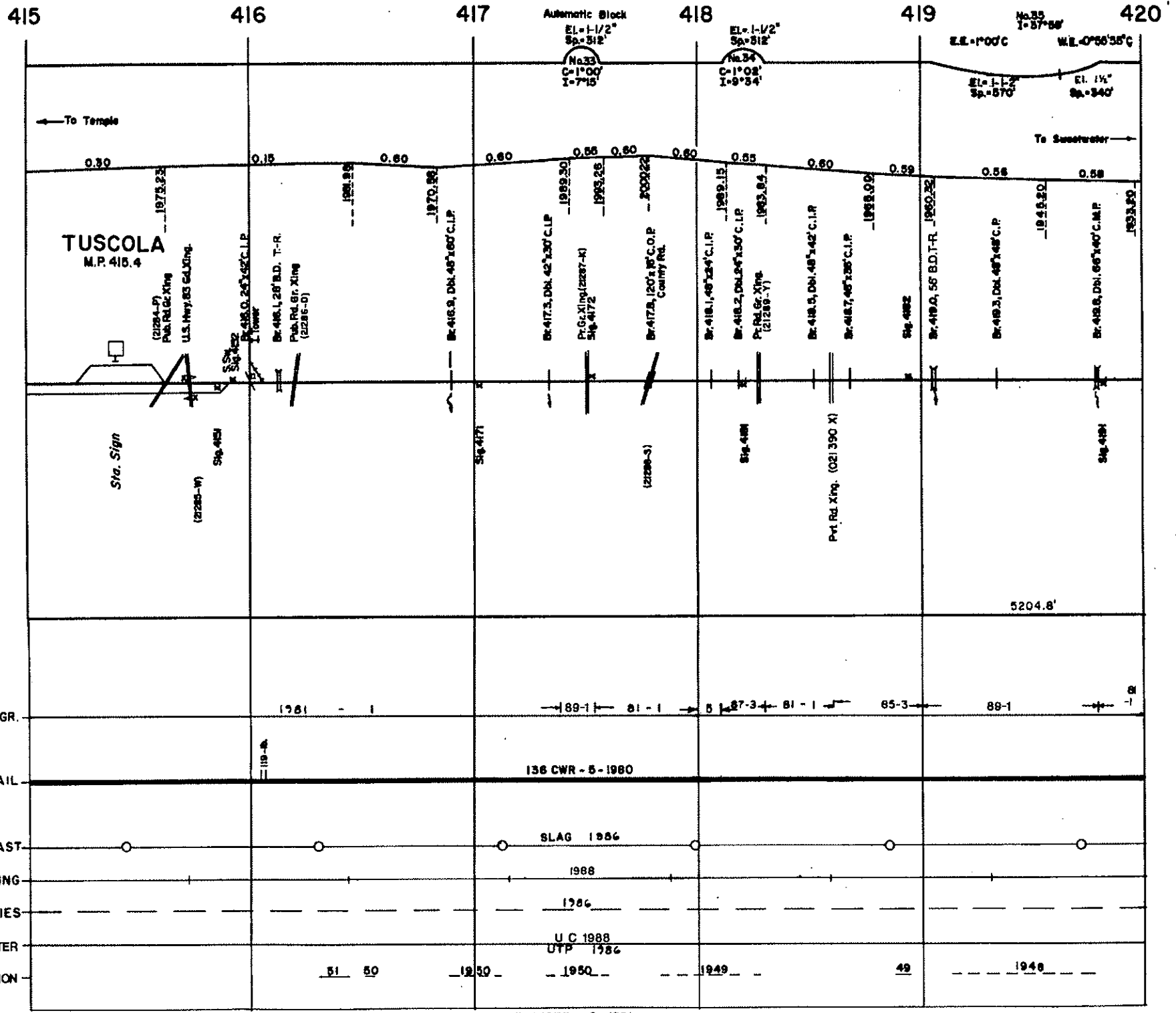
405 406 407 Automatic Block 408 409 410

#28  
1°04'  
I=41°20'  
El.=1-VZ  
Sp.585





CURVES NR BR  
 RAIL GR.  
 RAIL  
 BALLAST  
 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION



415

416

417

418

419

420

← To Temple

To Susquehanna →

**TUSCOLA**  
M.P. 415.4

Automatic Block  
EL. = 1-1/2"  
Sp. = 512"  
No. 33  
C = 1° 00'  
T = 7° 15'

EL. = 1-1/2"  
Sp. = 512"  
No. 34  
C = 1° 00'  
T = 9° 54'

EL. = 1° 00' C  
W.L. = 0° 56' 35" C  
EL. = 1-1/2"  
Sp. = 570"  
EL. = 1 1/4"  
Sp. = 340"

0.30

0.15

0.60

0.60

0.55

0.60

0.60

0.55

0.60

0.59

0.56

0.58

1973.23

1981.98

1970.92

1989.20

1983.28

2009.22

1989.15

1983.94

1989.09

1980.22

1948.90

1933.80

(21284-P)  
Pub. Rd. Xing.  
U.S. Hwy. 83 64' Xing.

Br. 416.0, 26' x 50' C.I.P.  
I. Tower

Br. 416.1, 26' B.D. T-R.  
Pub. Rd. Gr. Xing.  
(22286-6)

Br. 416.9, DN. 48' x 60' C.I.P.

Br. 417.3, DN. 42' x 30' C.I.P.  
Pt. G. Xing. (22287-4)  
Sig. 417.2

Br. 417.8, 120' x 10' C.O.P.  
Country Rd.

Br. 418.1, 48' x 24' C.I.P.  
Br. 418.2, DN. 34' x 30' C.I.P.  
Pvt. Rd. Gr. Xing.  
(21286-Y)

Br. 418.5, DN. 48' x 42' C.I.P.  
Br. 418.7, 48' x 28' C.I.P.

Br. 419.0, 55' B.D. T-R.  
Sig. 419.2

Br. 419.3, DN. 48' x 48' C.I.P.

Br. 419.8, DN. 66' x 40' C.I.P.  
Sig. 419.1

Sta. Sign

(22285-W)  
Sig. 418.1

Sig. 417.1

(22288-3)  
Sig. 418.1

Pvt. Rd. Xing. (021390 X)

5204.8'

RAIL GR.

RAIL

BALLAST

SURFACING

TIES

UNDERCUTTER

STABILIZATION

1981

189-1

81-1

87-3

81-1

85-3

88-1

81

136 CWR - 5 - 1980

SLAG 1986

1988

1986

U C 1988  
UTP 1986

51 50

1950

1950

1949

49

1948

420

421

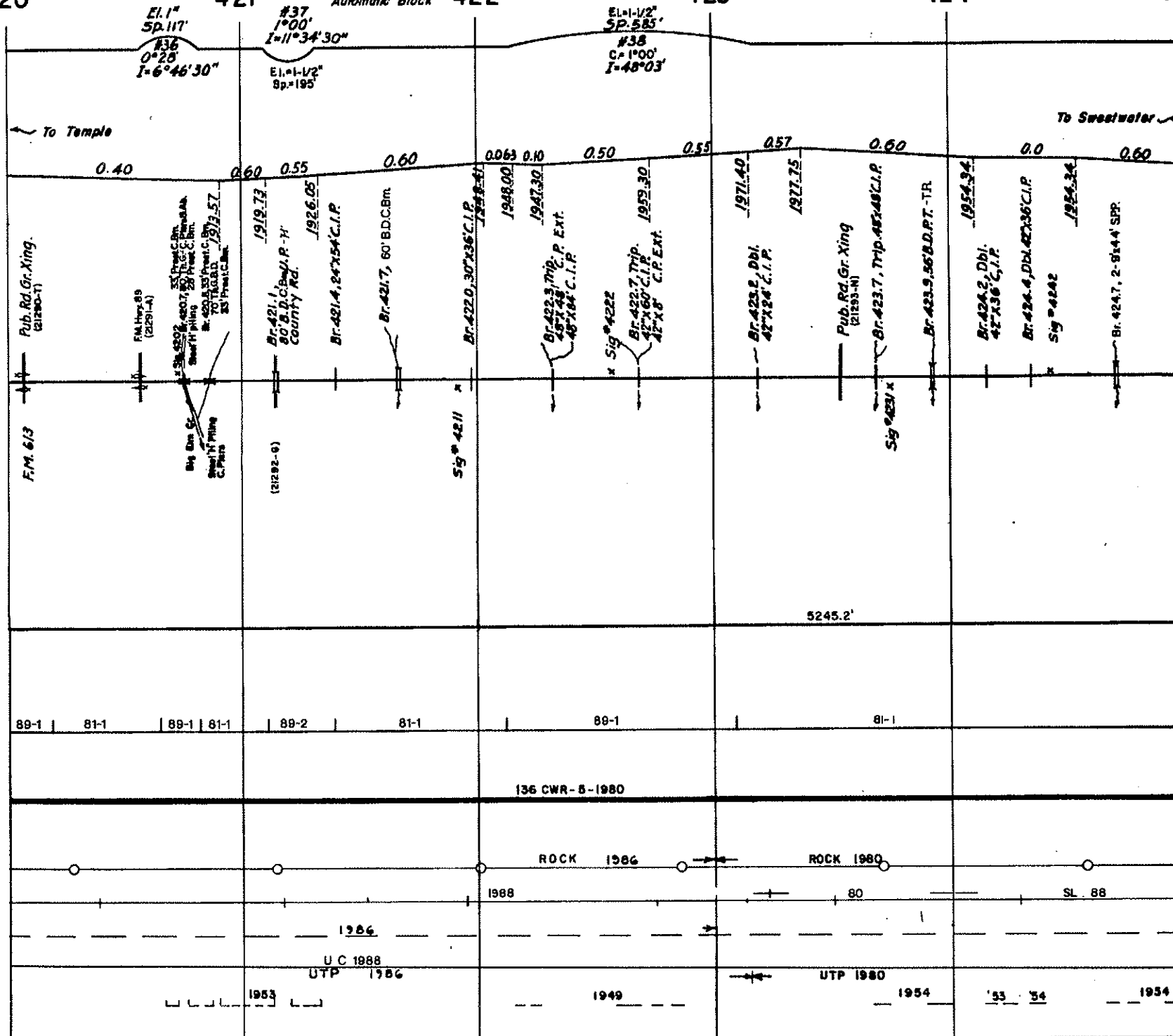
Automatic Block

422

423

424

425







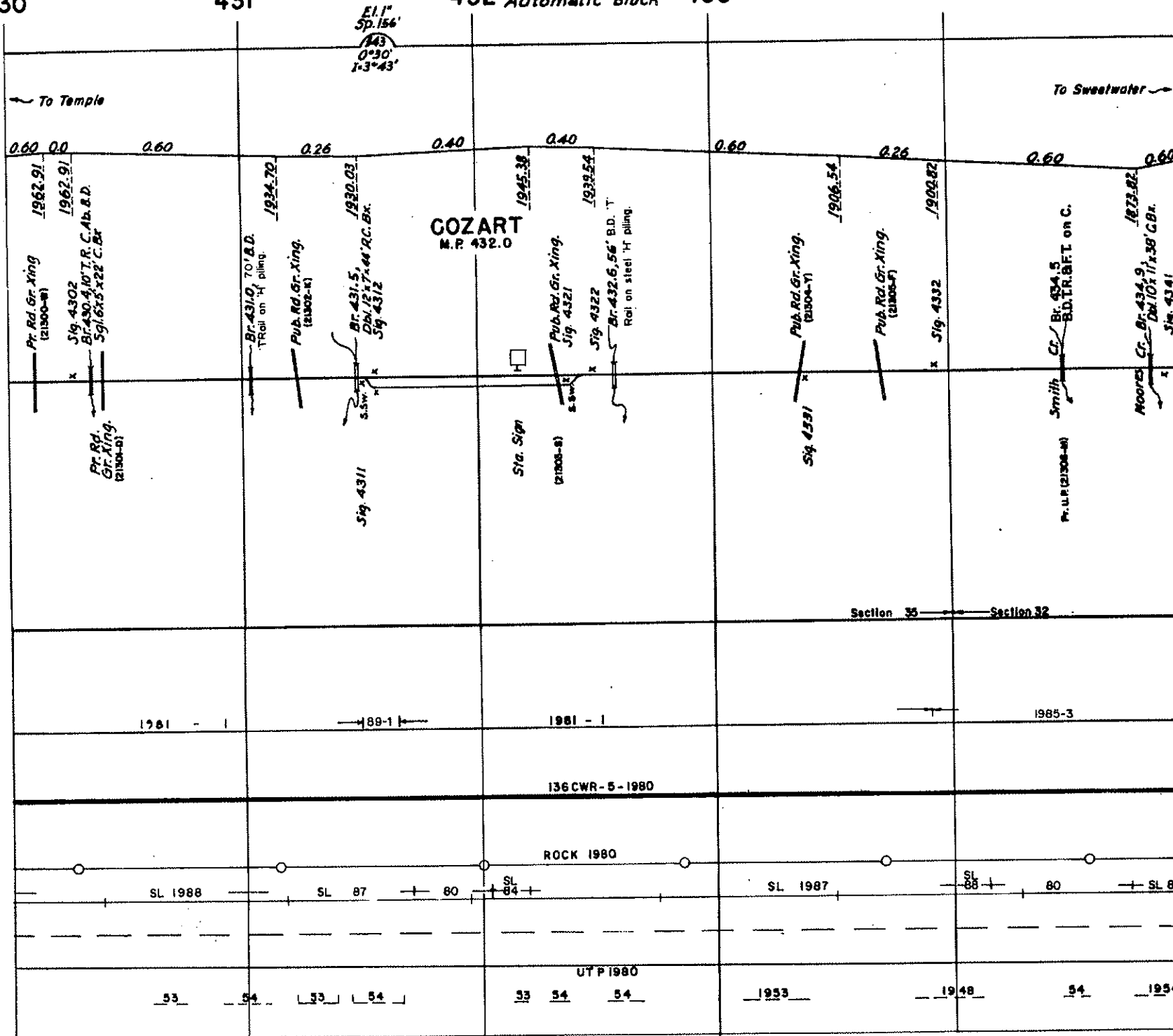
430

431

432 Automatic Block 433

434

435



435

436

437 Automatic Block

438

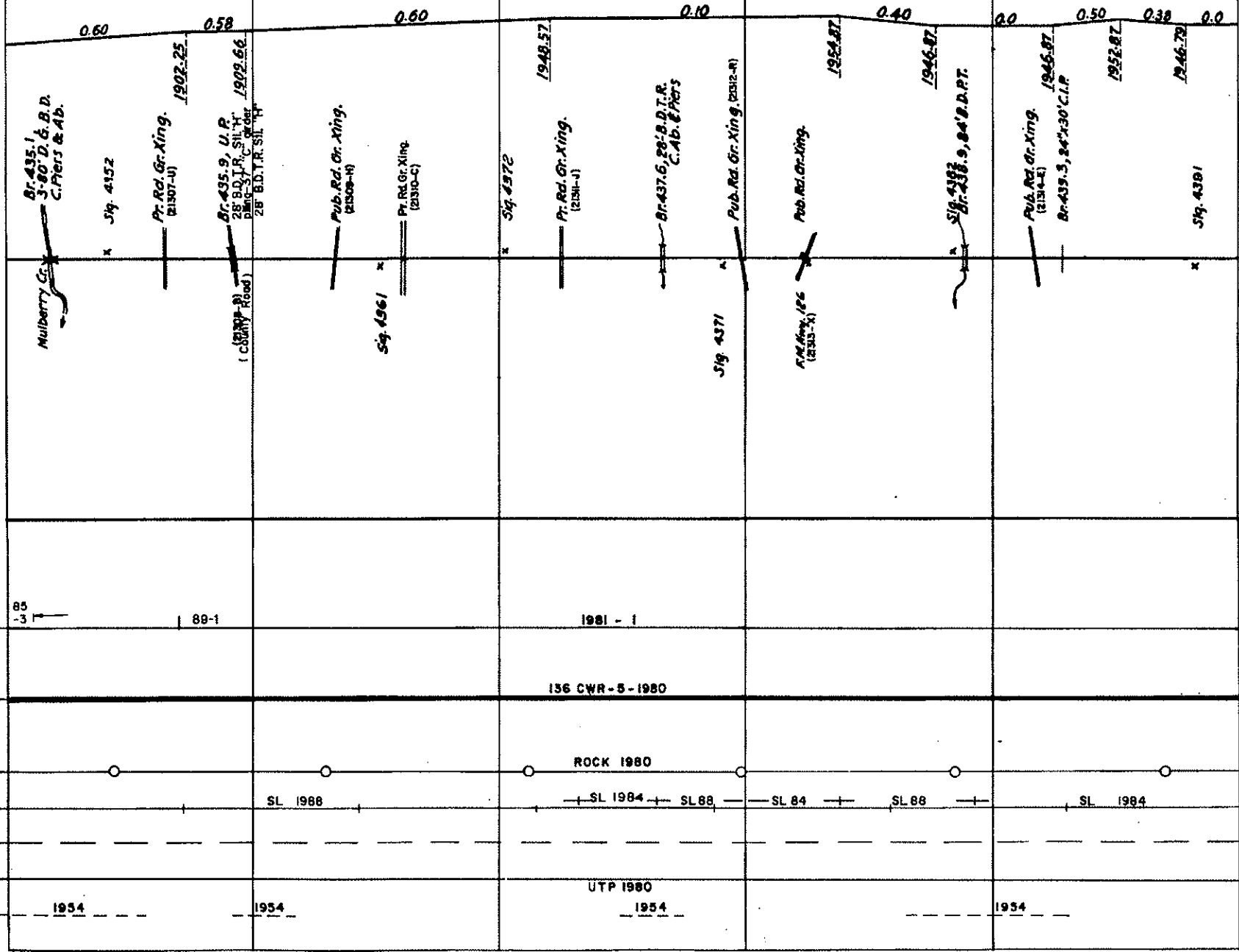
439

440

EL. 1170  
Sp. 170  
444  
0°30'  
1°6'23'

← To Temple

To Sweetwater →



440

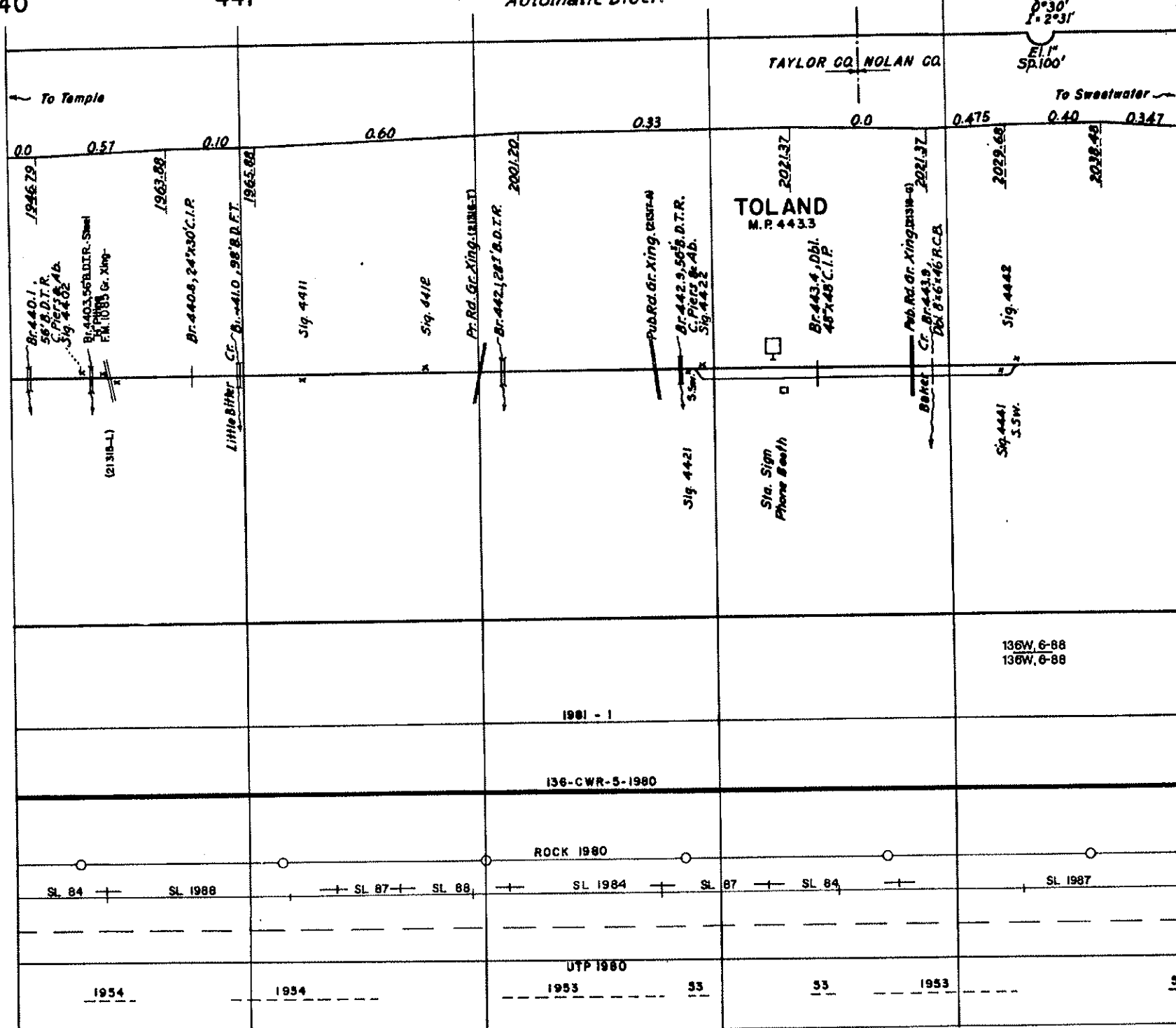
441

442 Automatic Block

443

444

445

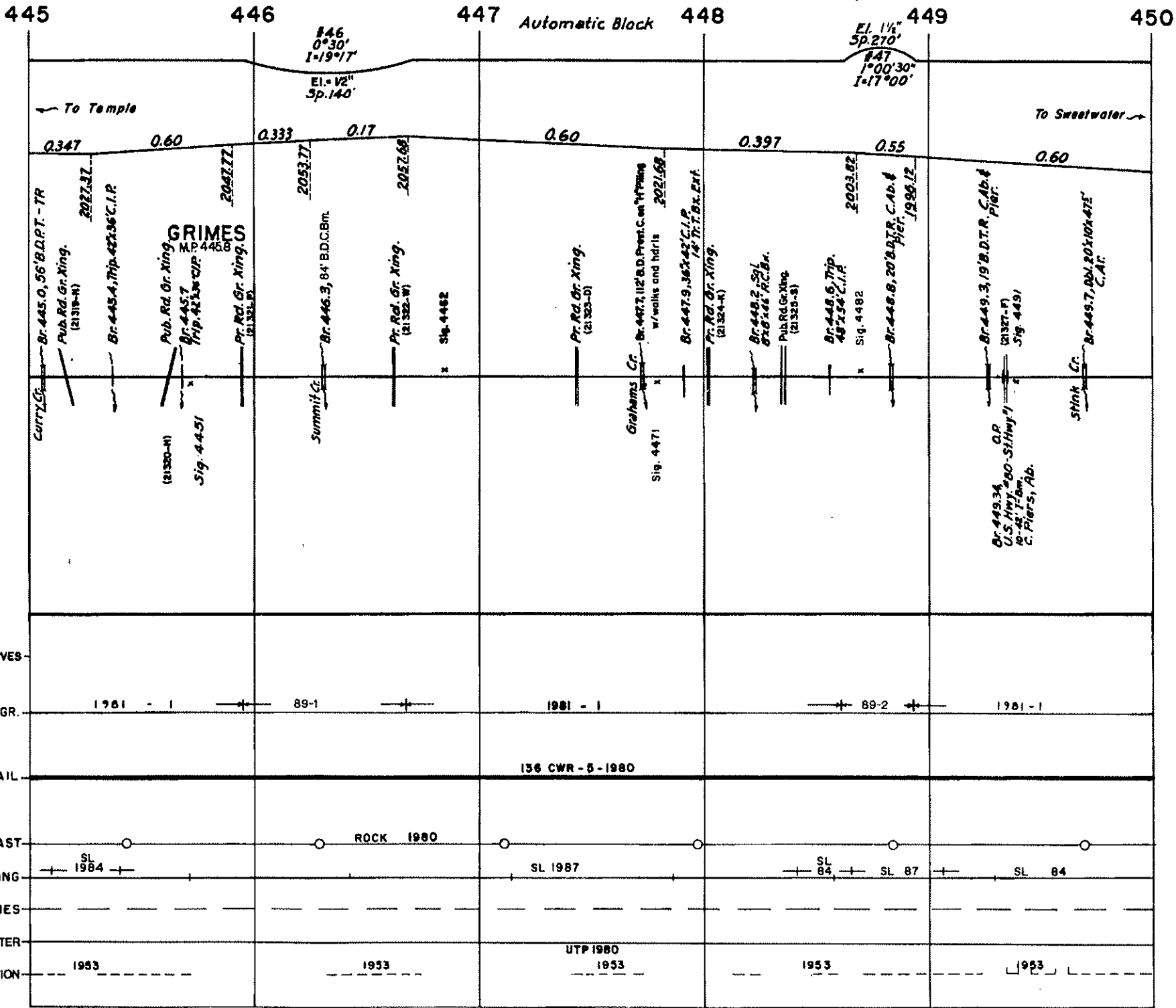


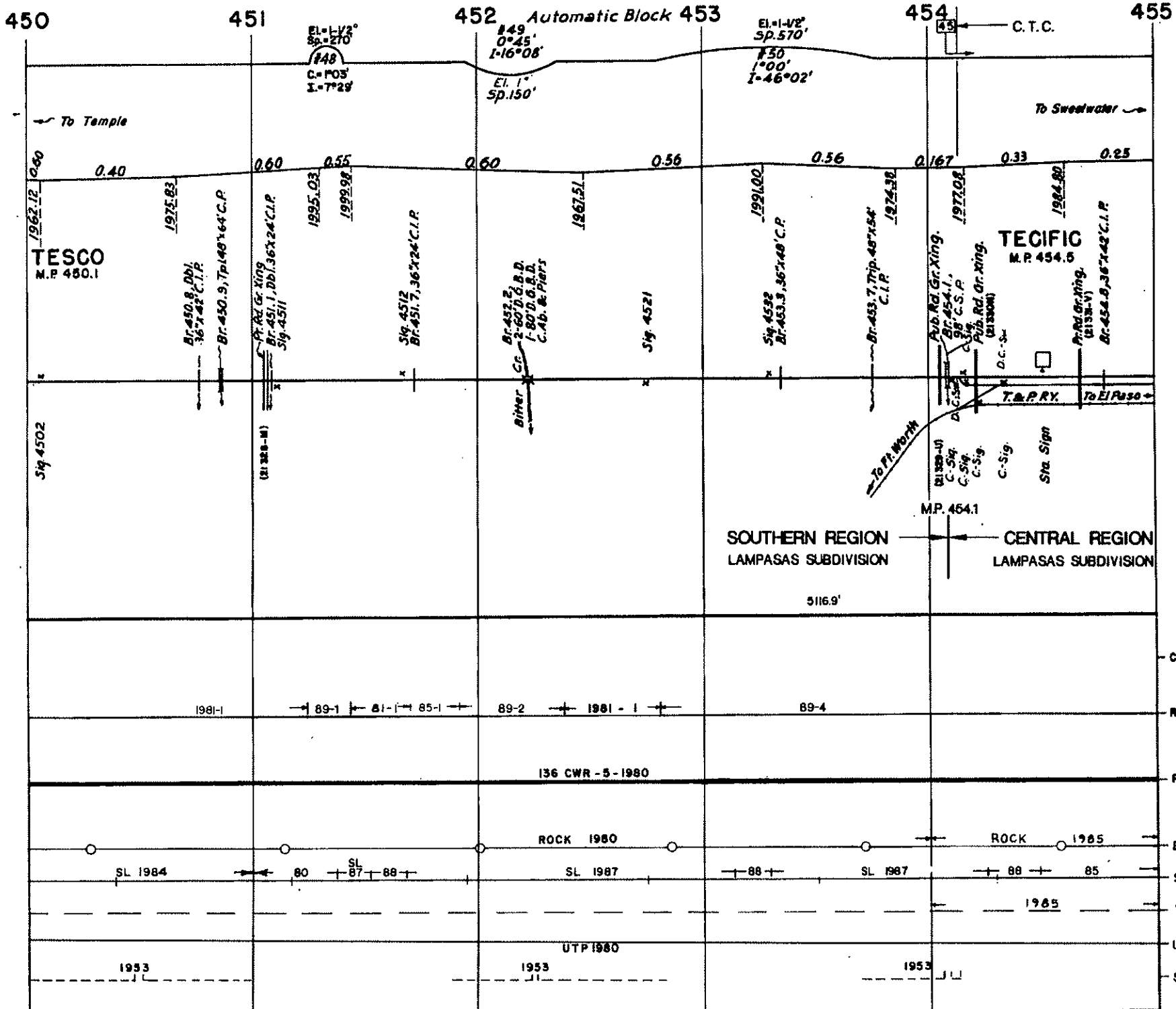
TAYLOR CO. NOLAN CO.

To Sweetwater

To Temple

- CURVES 93'
- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION





REVISED 3-1991

# Lampasas Subdivision

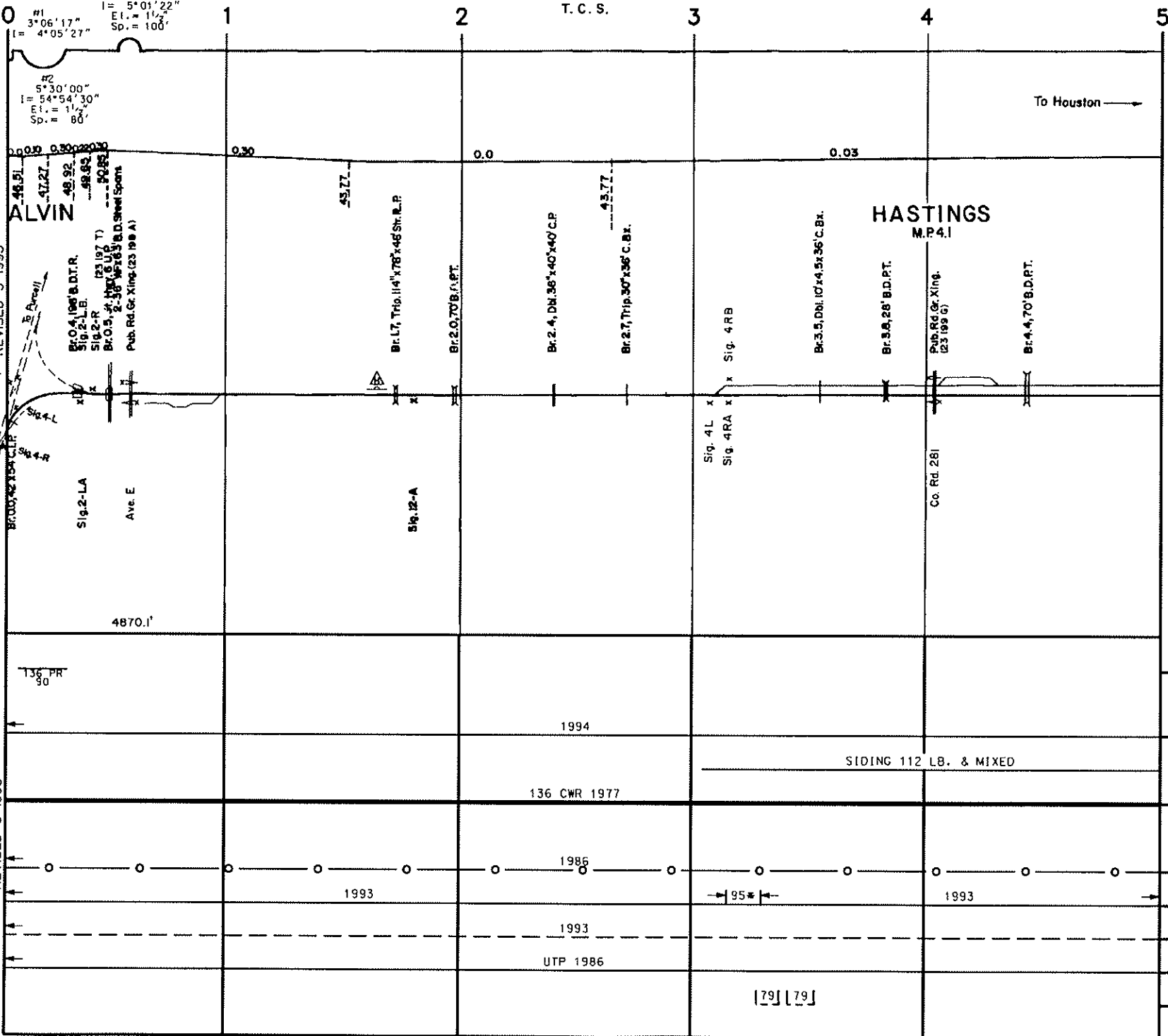
Tecific (M.P. 454.1) to Temple (M.P. 218.1)

*Intentionally Blank*



***Intentionally Blank***

T. C. S.



To Houston →





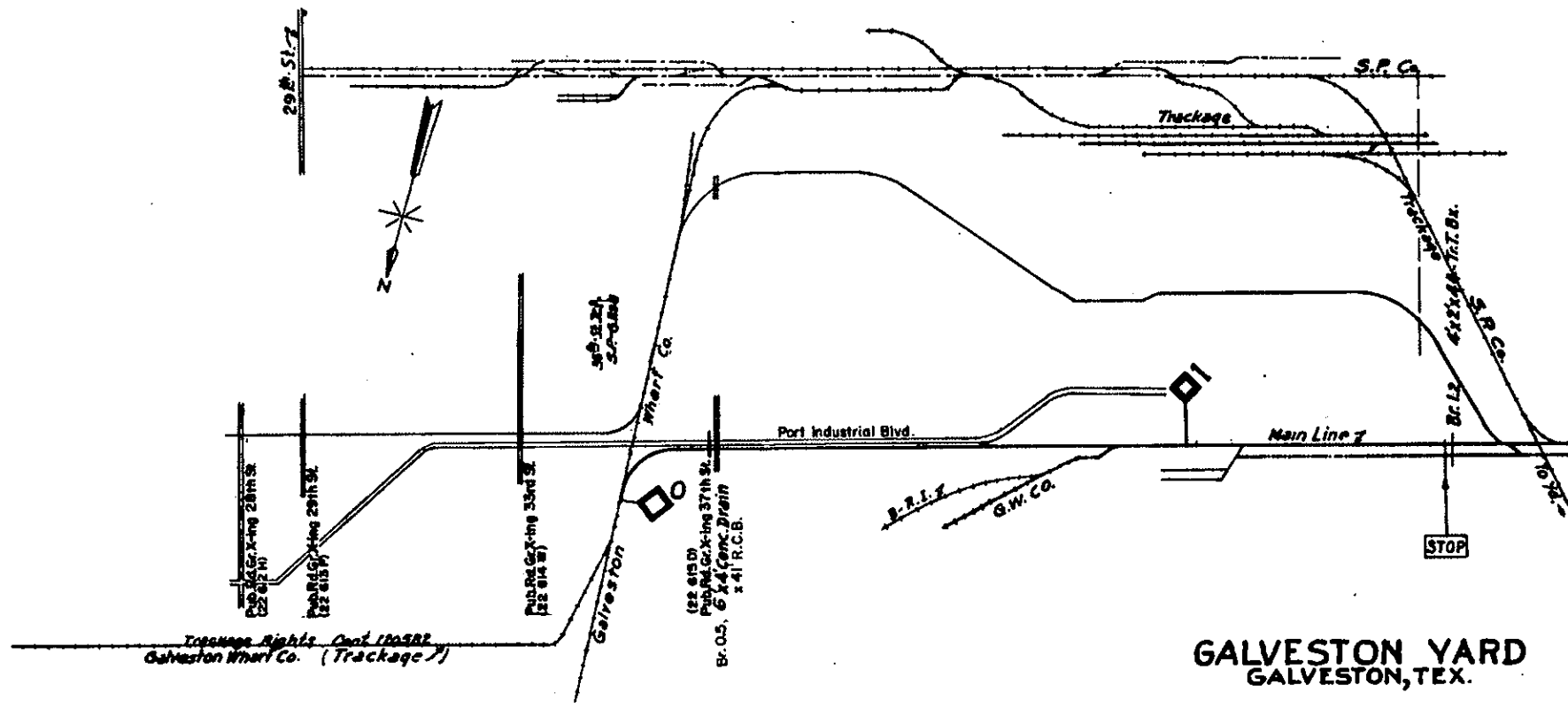


# ***Houston Subdivision***

***Alvin (M.P. 0.0) to Houston (M.P. 19.5)***

*Intentionally Blank*

To Temple →



# GALVESTON YARD GALVESTON, TEX.





0

To Temple →

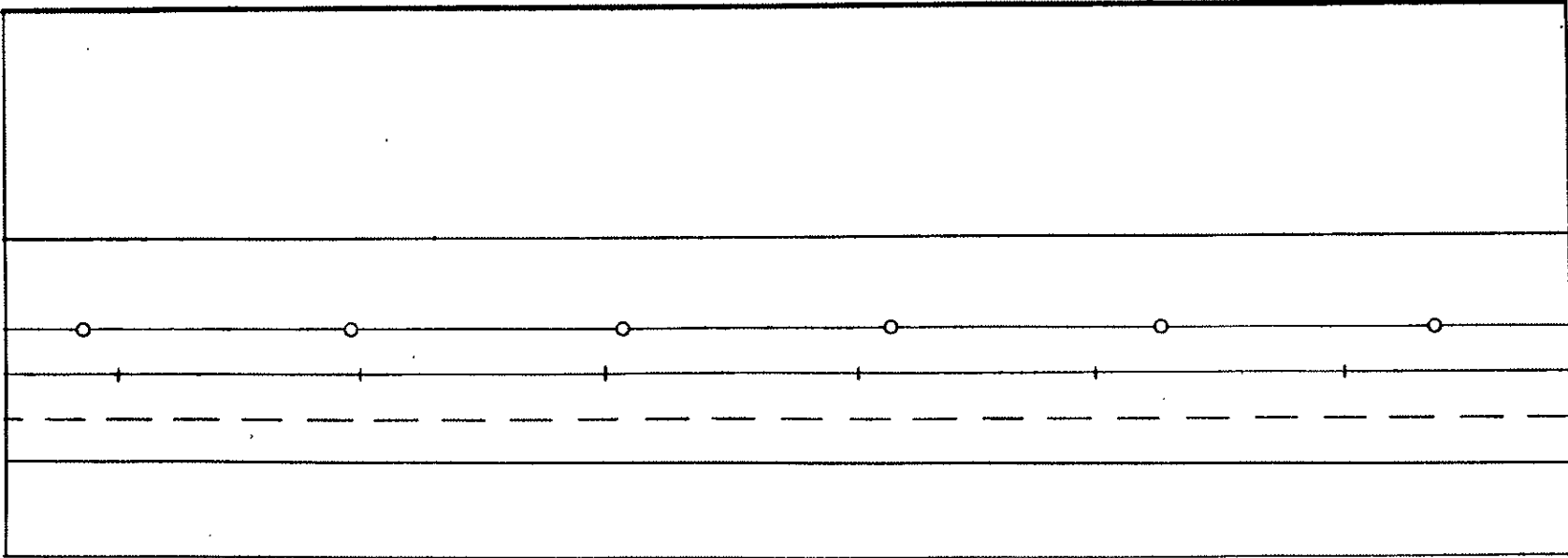
# GALVESTON MINUS 0.4 F.T.

*See separate sketch  
Pages 170 & 171 for Galveston Yard*

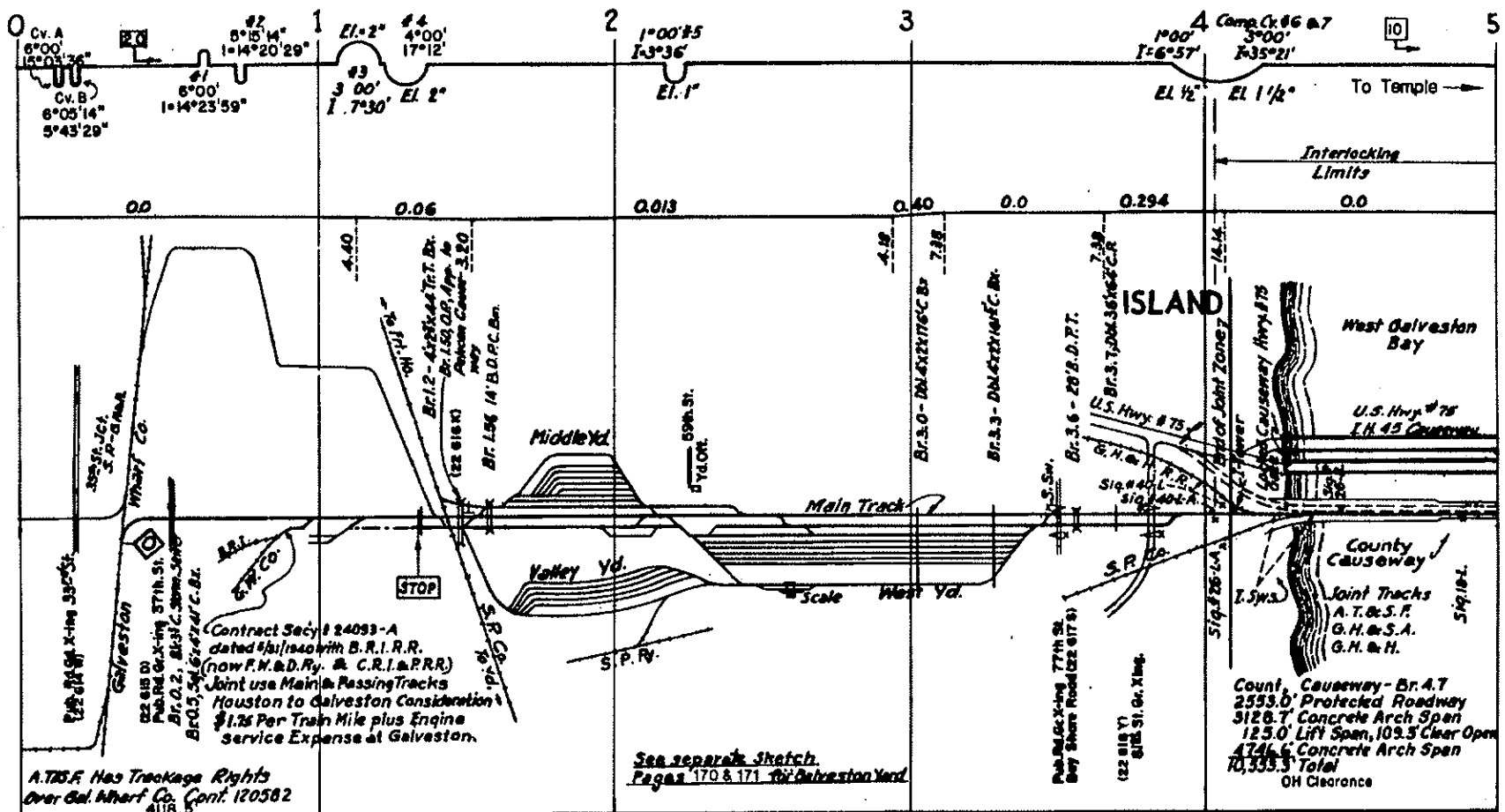
Gal. Wharf Co Trackage  
Industries



Section 31



- CURVES
- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION



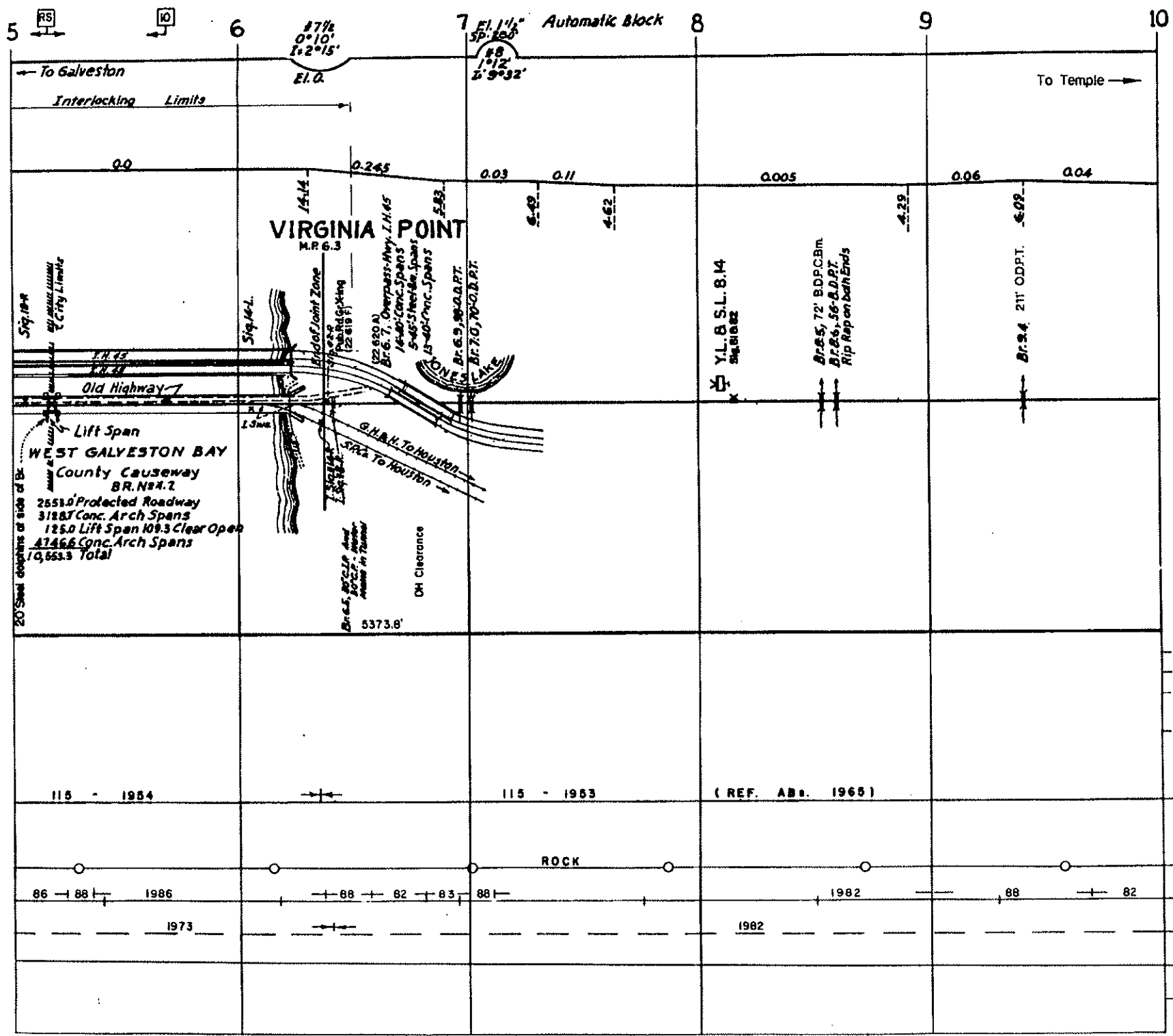
CURVES										
RAIL GR.										
RAIL	90-1926	110-1955						115-1954		
BALLAST			SHELL					ROCK		
SURFACING	SL 84	1984	73	1983	73	88	73	86	1988	1986
TIES					1973					
UNDERCUTTER										
STABILIZATION										

Contract Secy # 24093-A dated 8/1/1940 with B.R.I.R.R. (now F.M.&D.Ry. & C.R.I.&P.R.R.) Joint use Main & Passing Tracks Houston to Galveston Consideration \$1.25 Per Train Mile plus Engine Service Expense at Galveston.

A.T.S.F. Has Trackage Rights over Gal. Wharf Co. Cont. 120582 4118.5

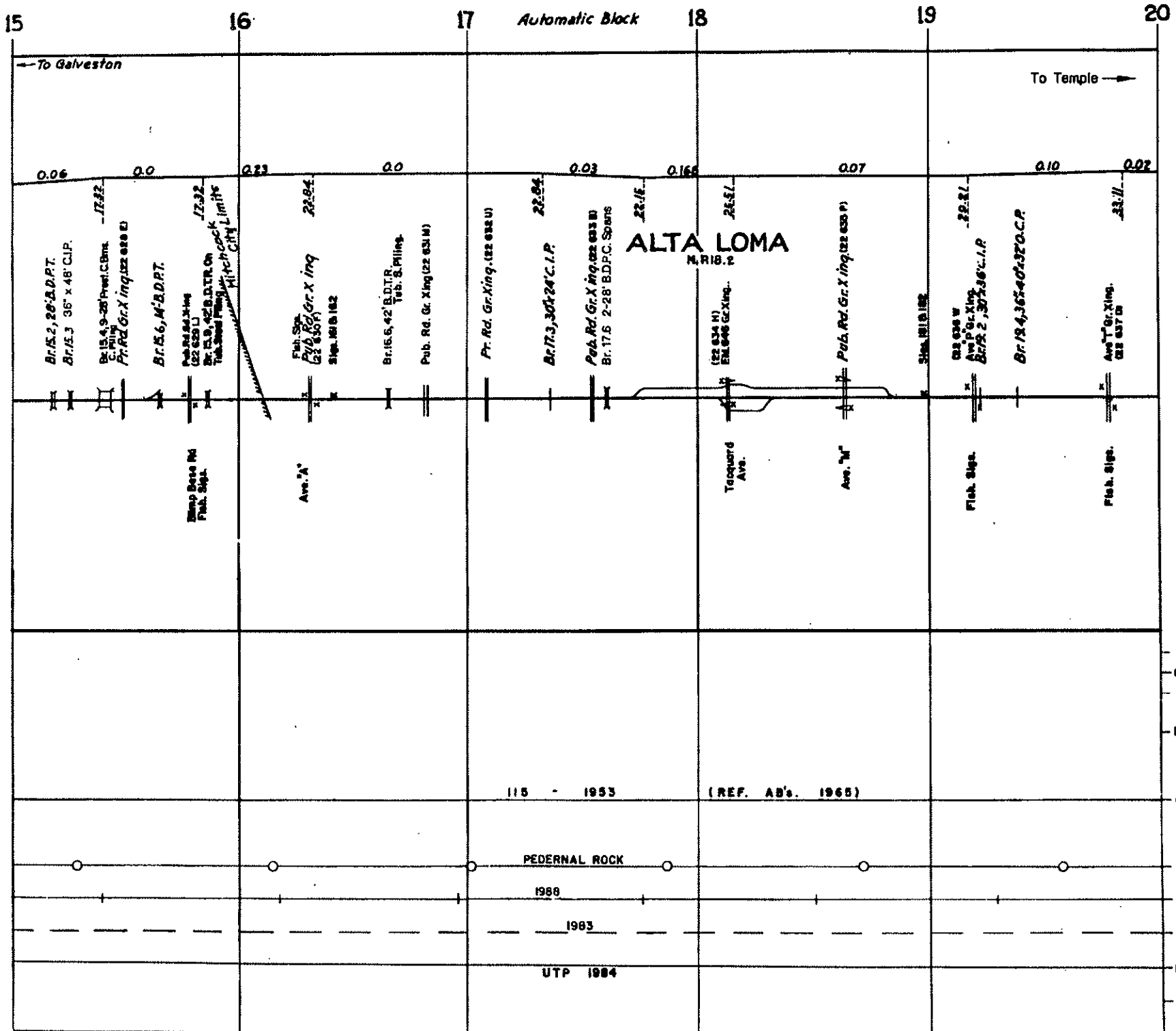
See separate sketch Pages 170 & 171 for Galveston Yard

Count. Causeway - Br. 4.7  
 2553.0' Protected Roadway  
 3126.7' Concrete Arch Span  
 125.0' Lift Span, 109.5' Clear Open  
 474.6' Concrete Arch Span  
 70,353.3' Total  
 OH Clearance



- CURVES
- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION





- CURVES
- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION

115 - 1953 (REF. AB's. 1965)

PEDERNAL ROCK

1988

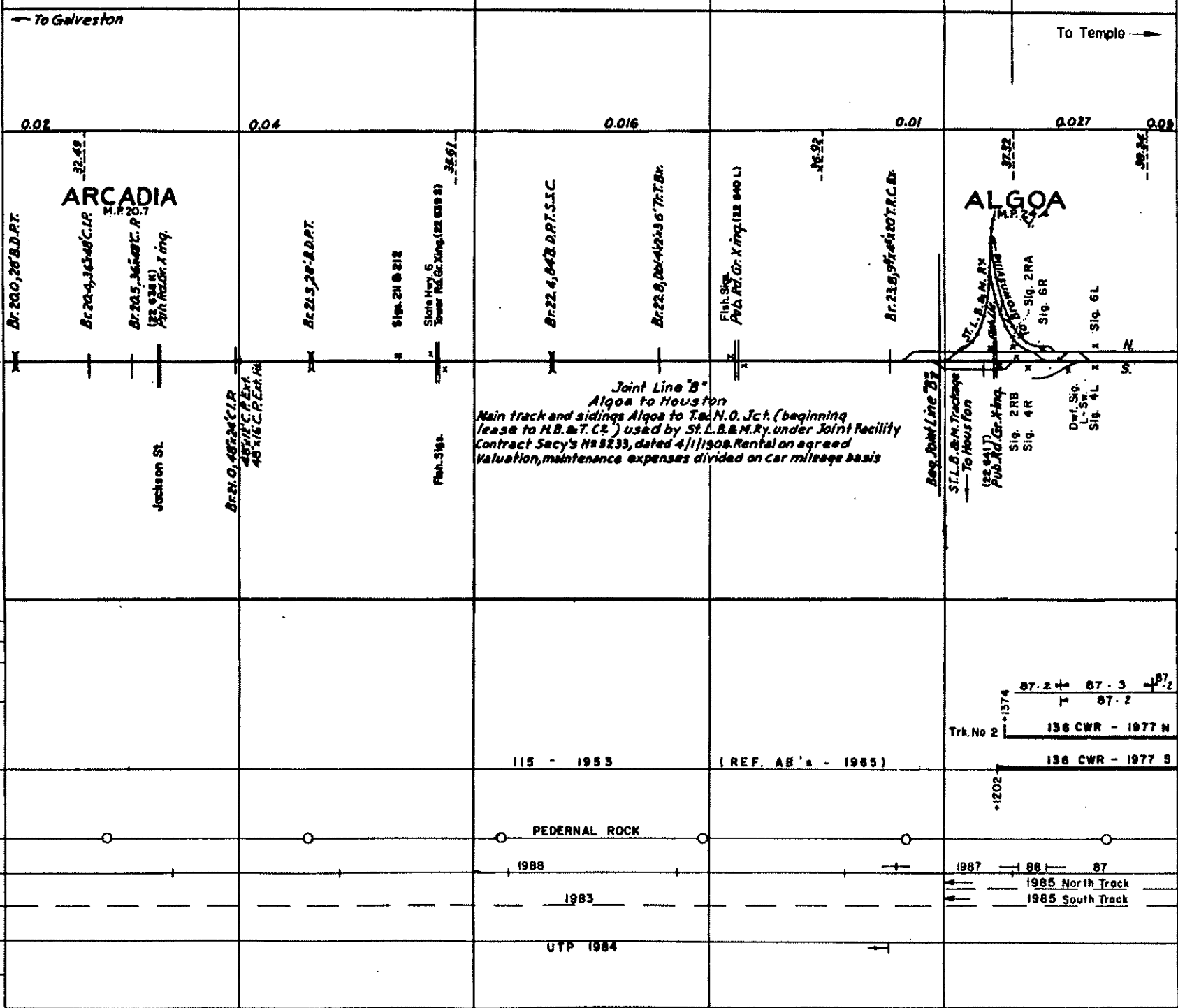
1993

UTP 1984

**ALTA LOMA**  
Z. P. 18. 2

← To Galveston

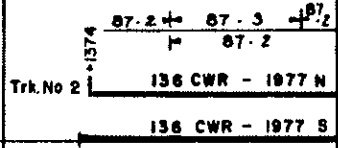
To Temple →



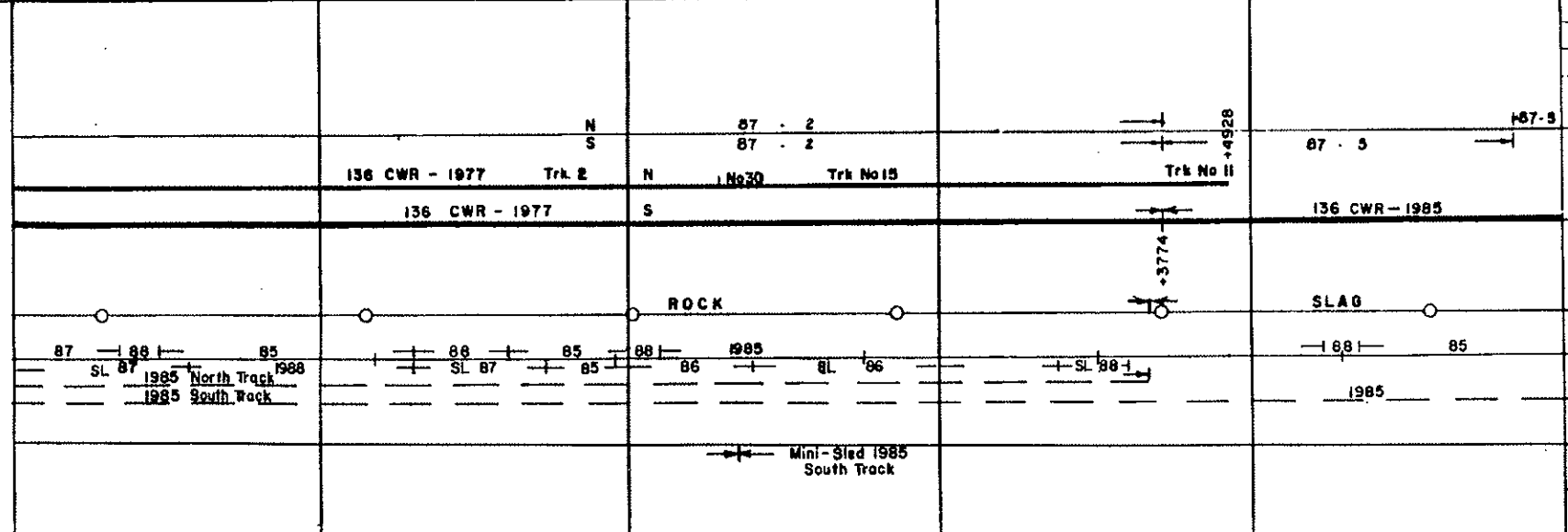
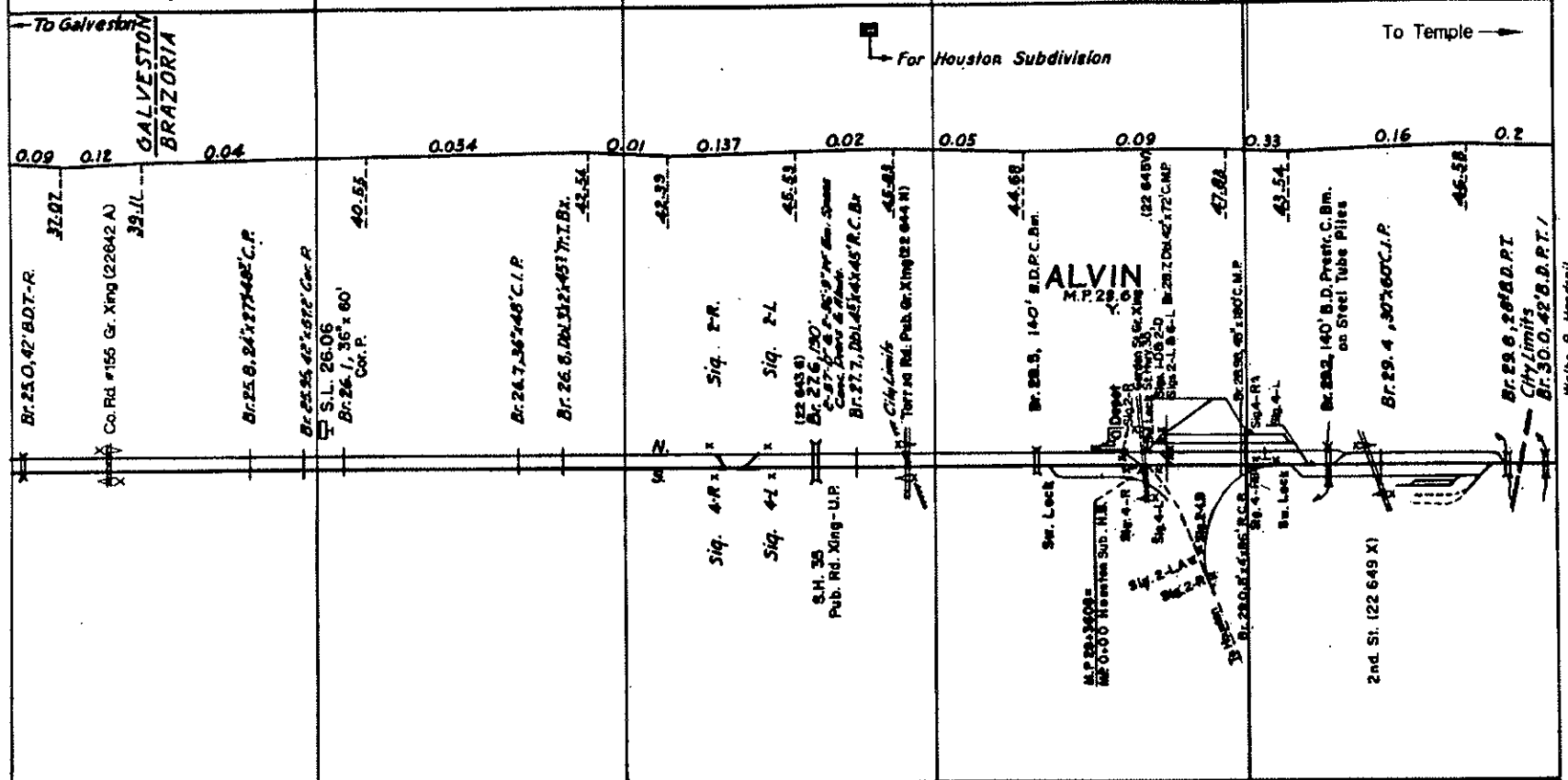
CURVES  
RAIL GR.  
RAIL  
BALLAST  
SURFACING  
TIES  
UNDERCUTTER  
STABILIZATION

Joint Line "B"  
Algoa to Houston  
Main track and sidings Algoa to T&N.O. Jct. (beginning  
lease to H.B. & T. Co.) used by St. L. & M. Ry. under Joint Facility  
Contract Secy's No 8233, dated 4/1/1908. Rental on agreed  
valuation, maintenance expenses divided on car mileage basis

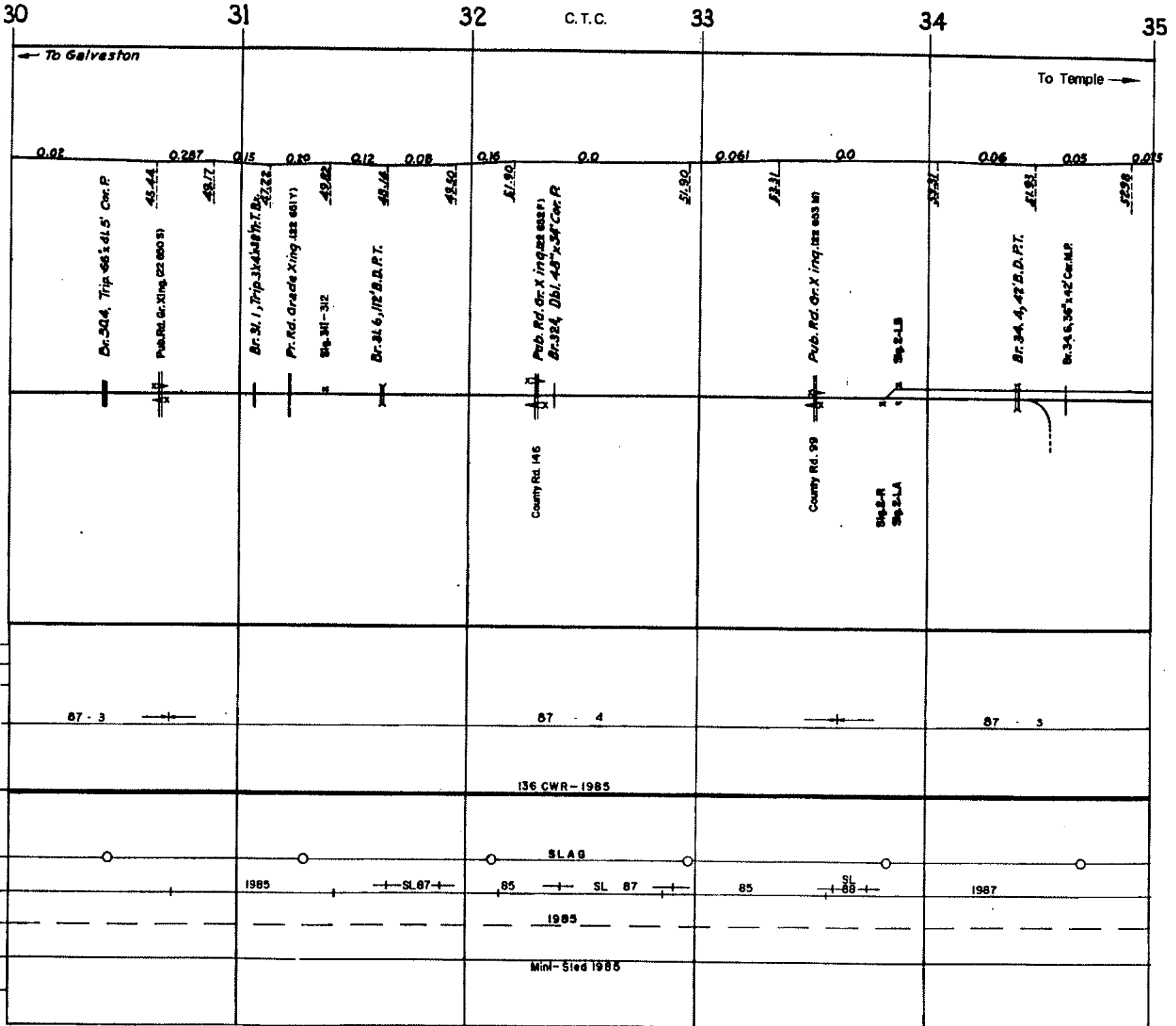
115 - 1983 (REF. AB's - 1965)



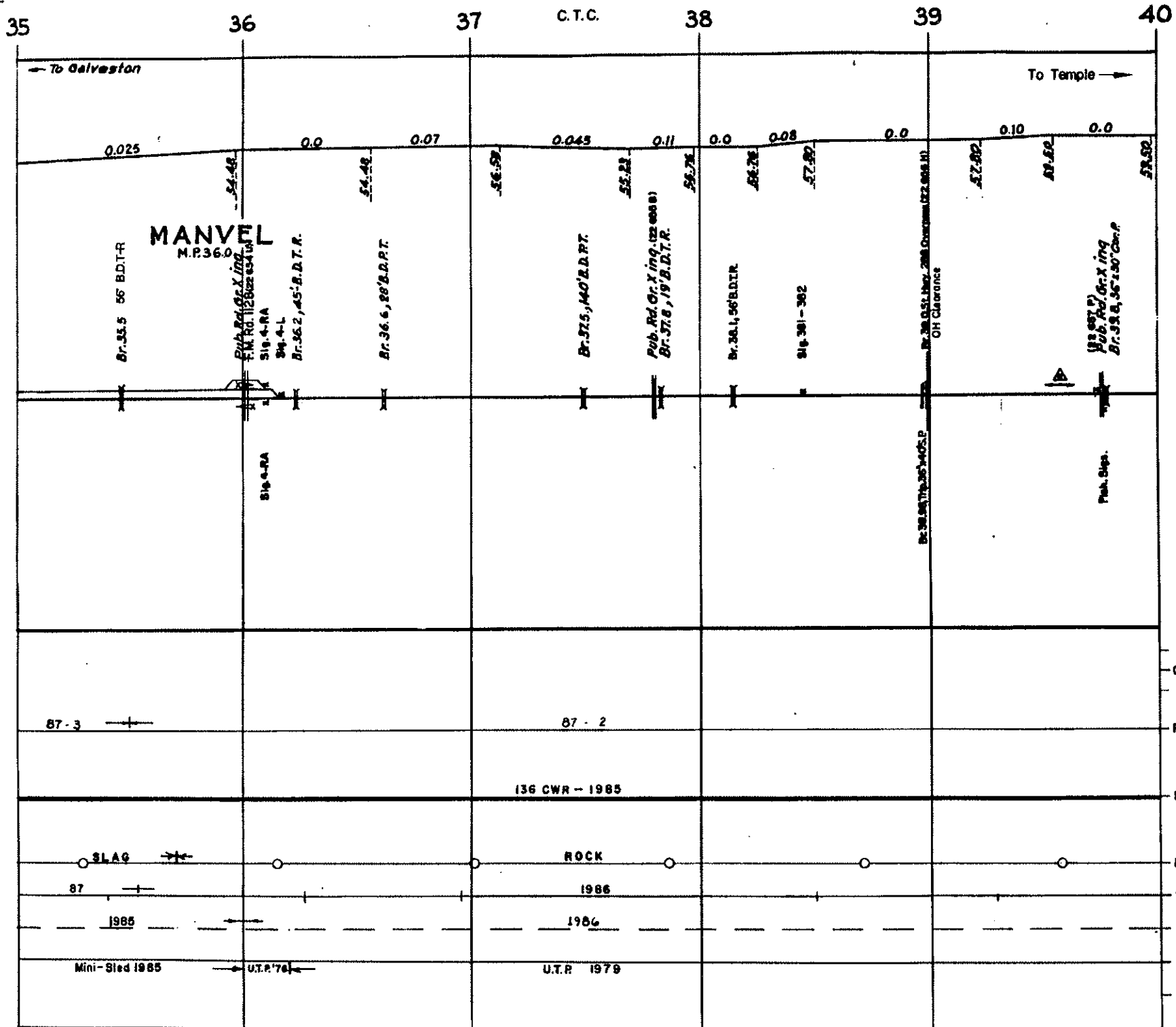
25 26 27 28 29 30 C.T.C.







REVISED 3-1991



40

41

42

C.T.C.

43

[40]

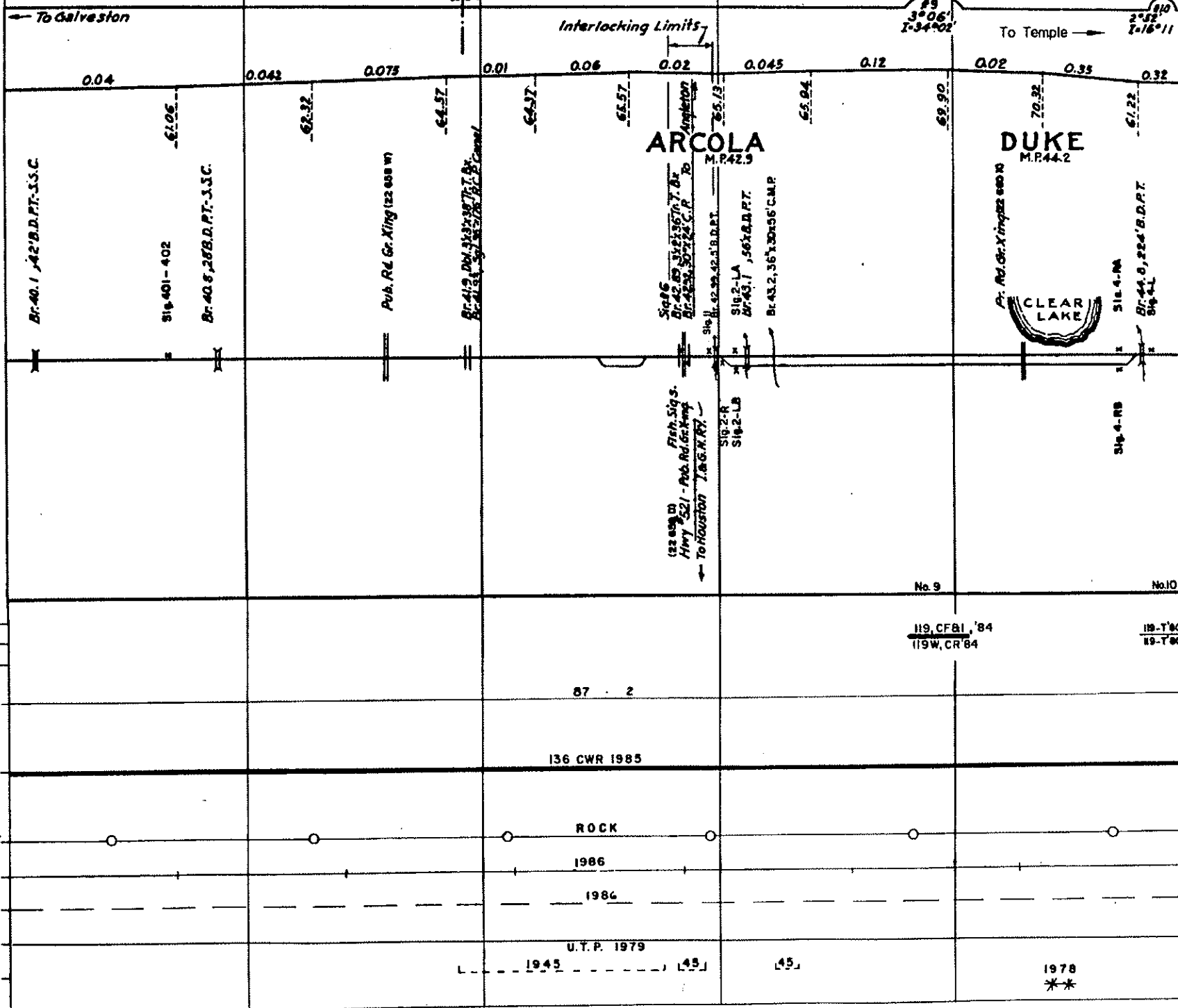
[RS]

44

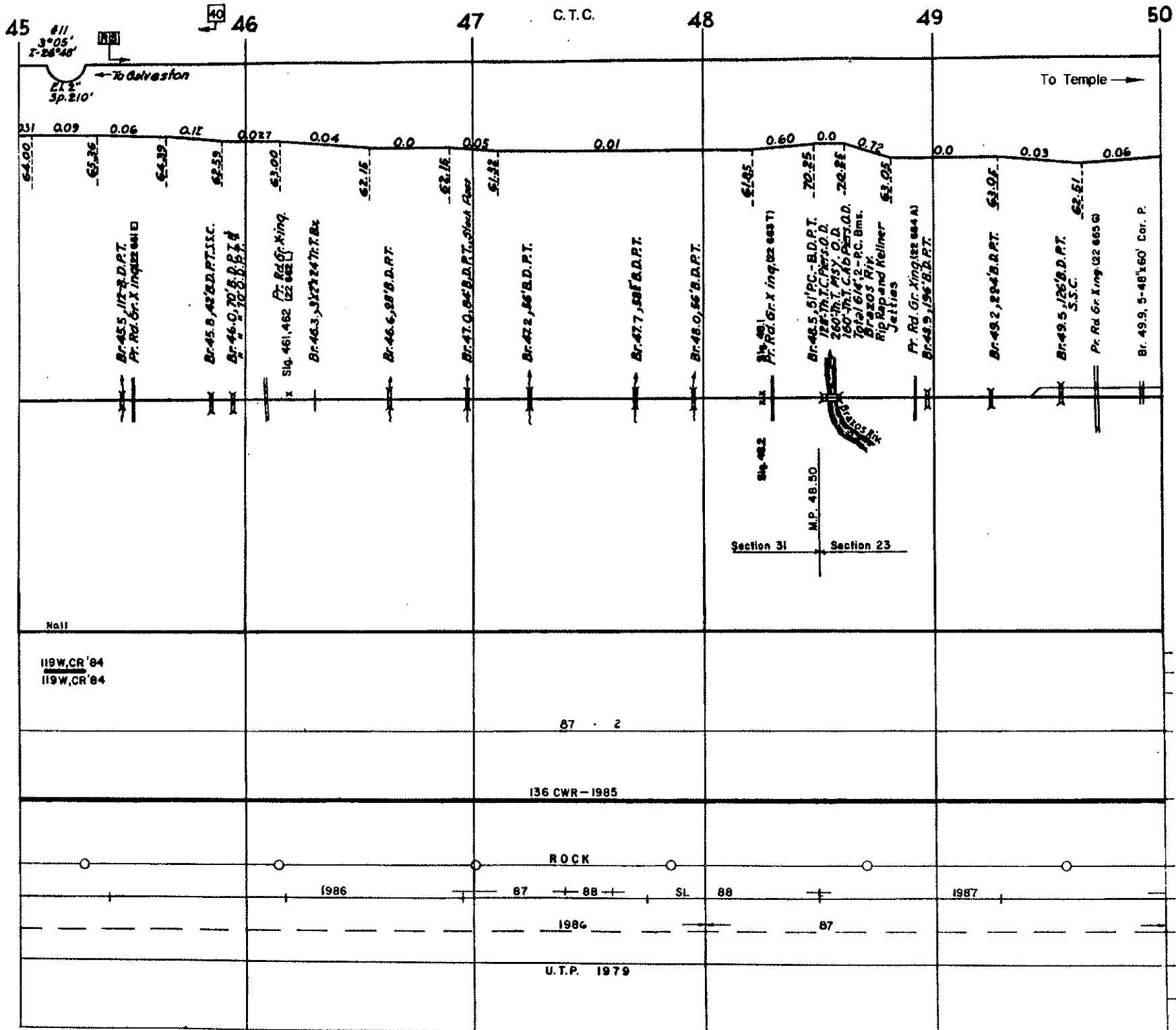
45

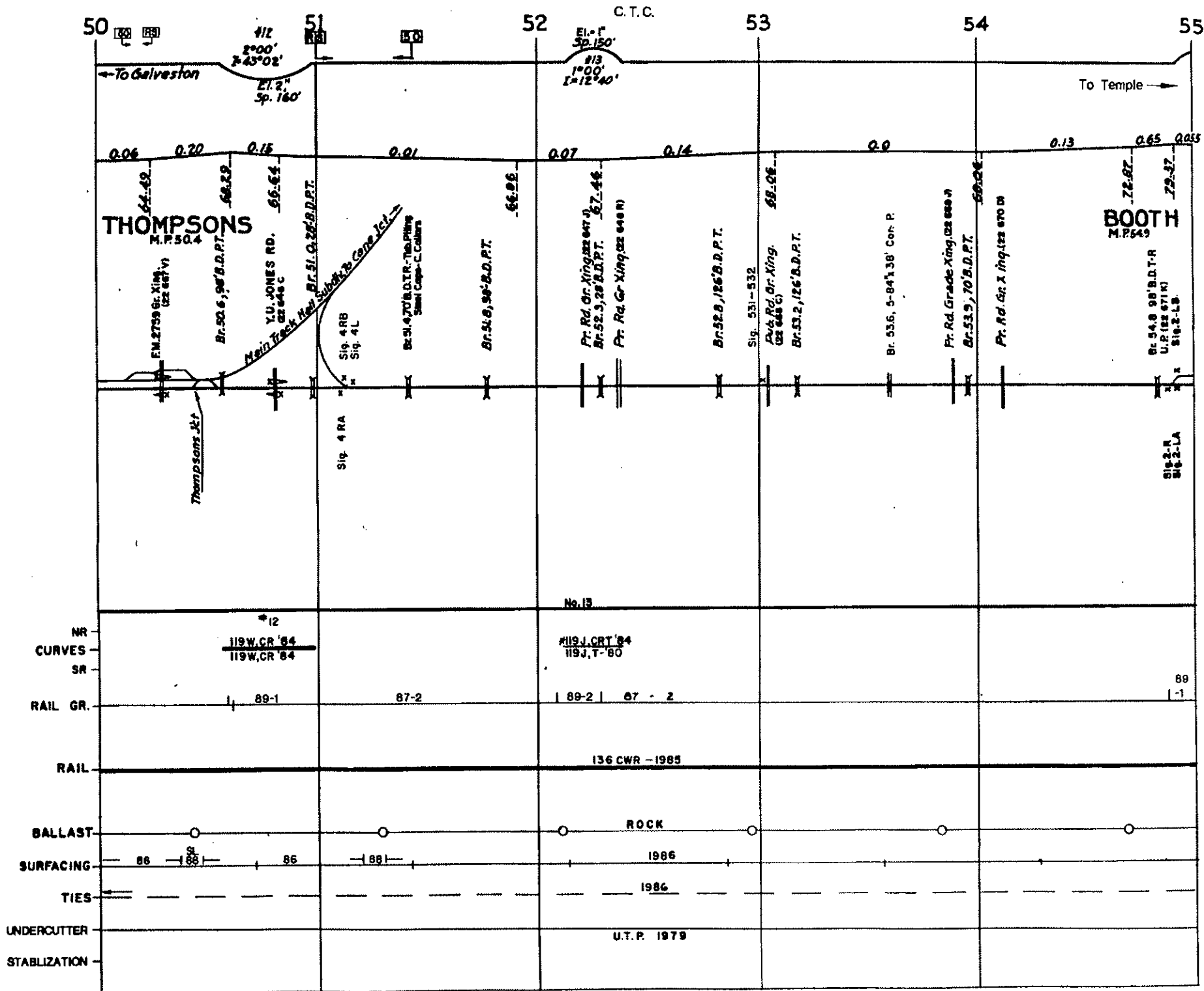
BRAZORIA CO.

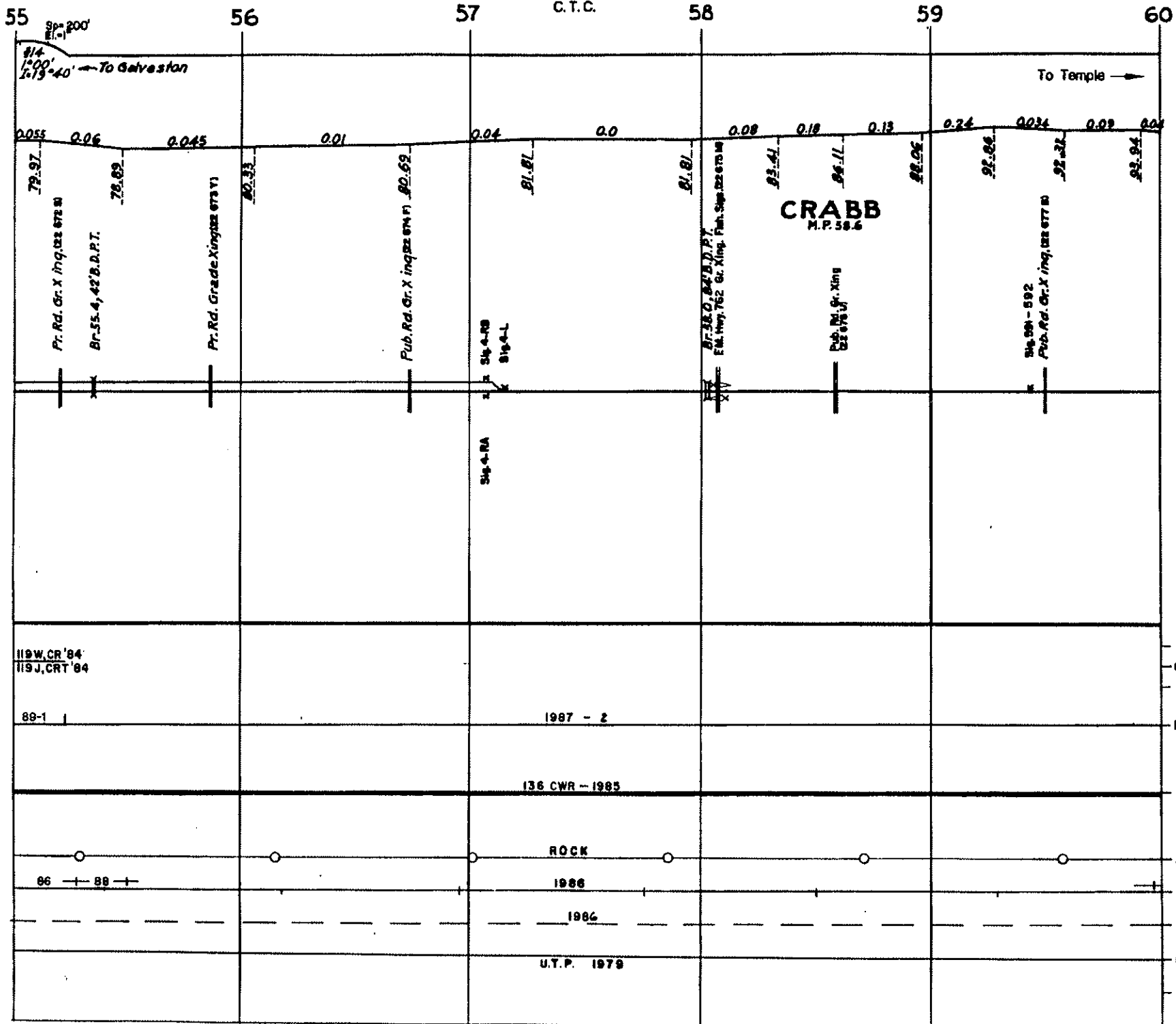
FORT BEND CO.

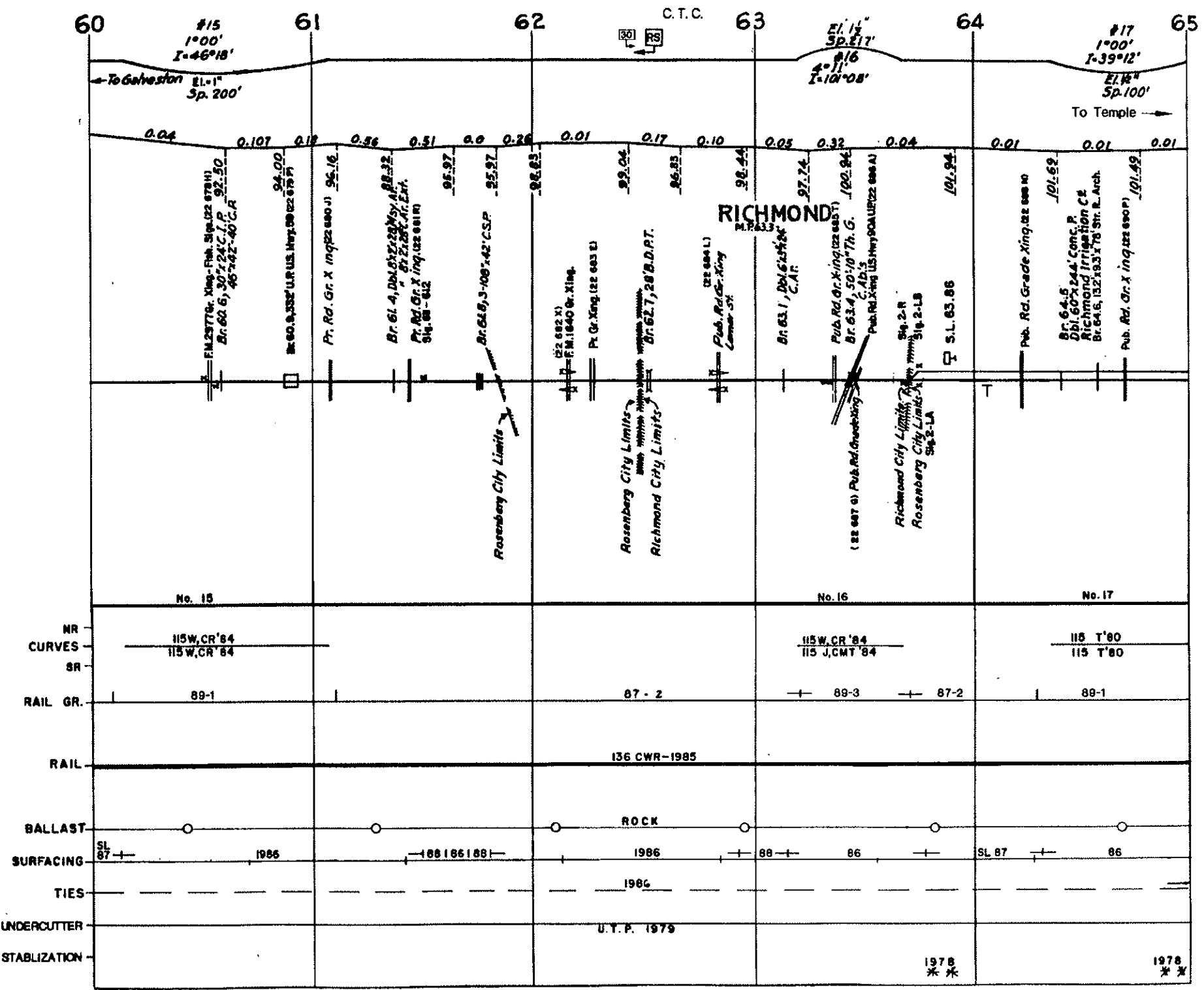


REVISED 3-1991



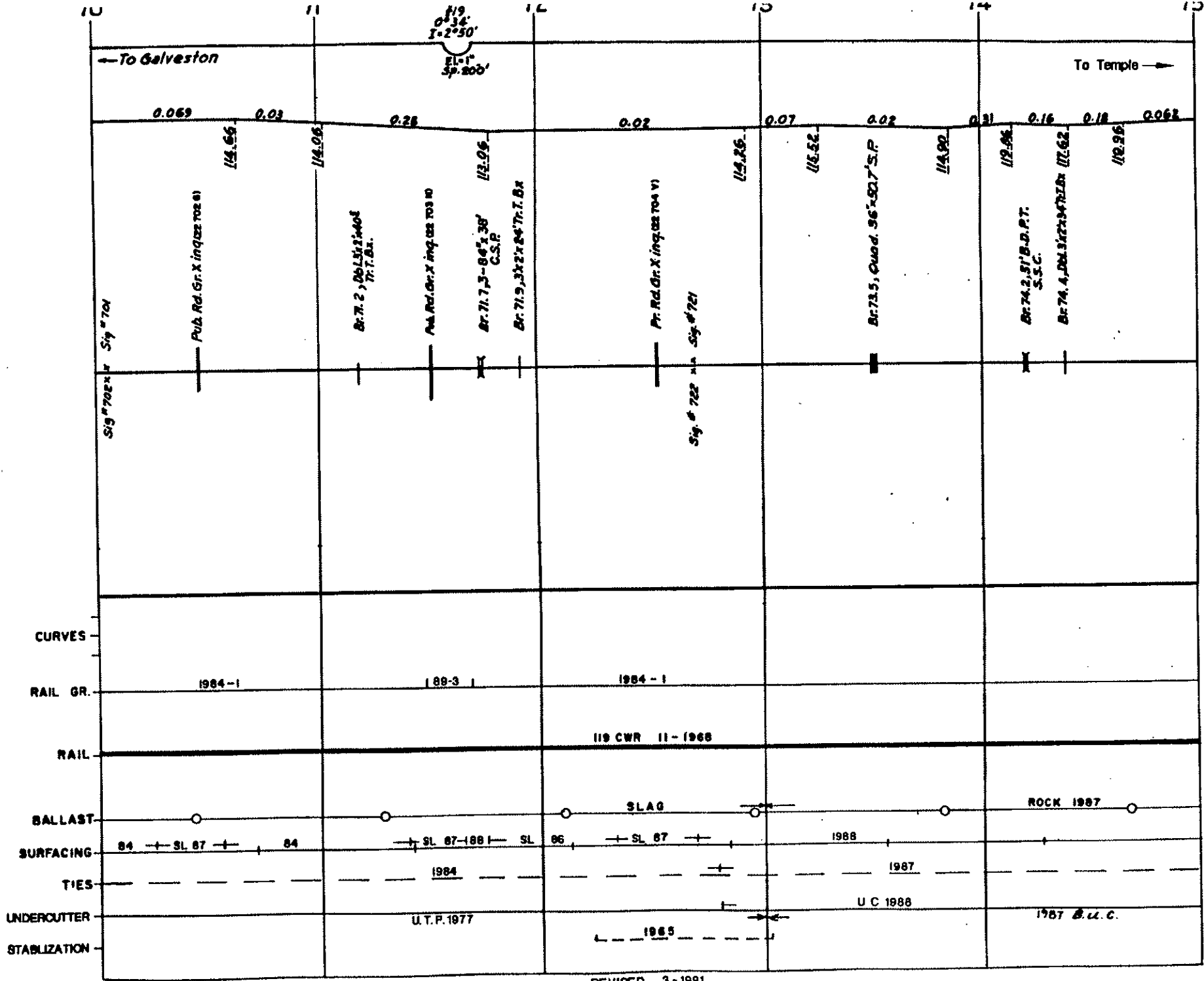




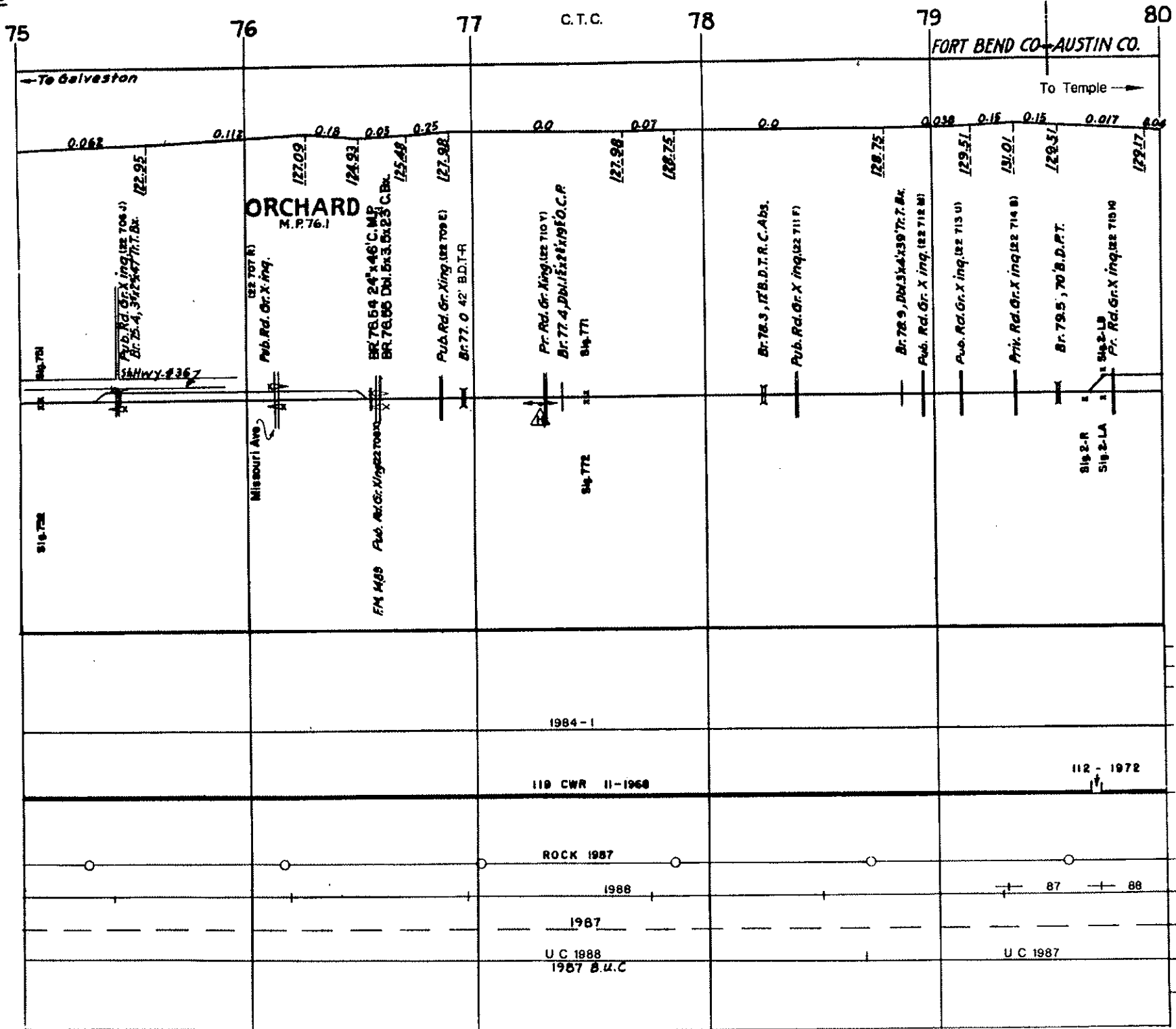


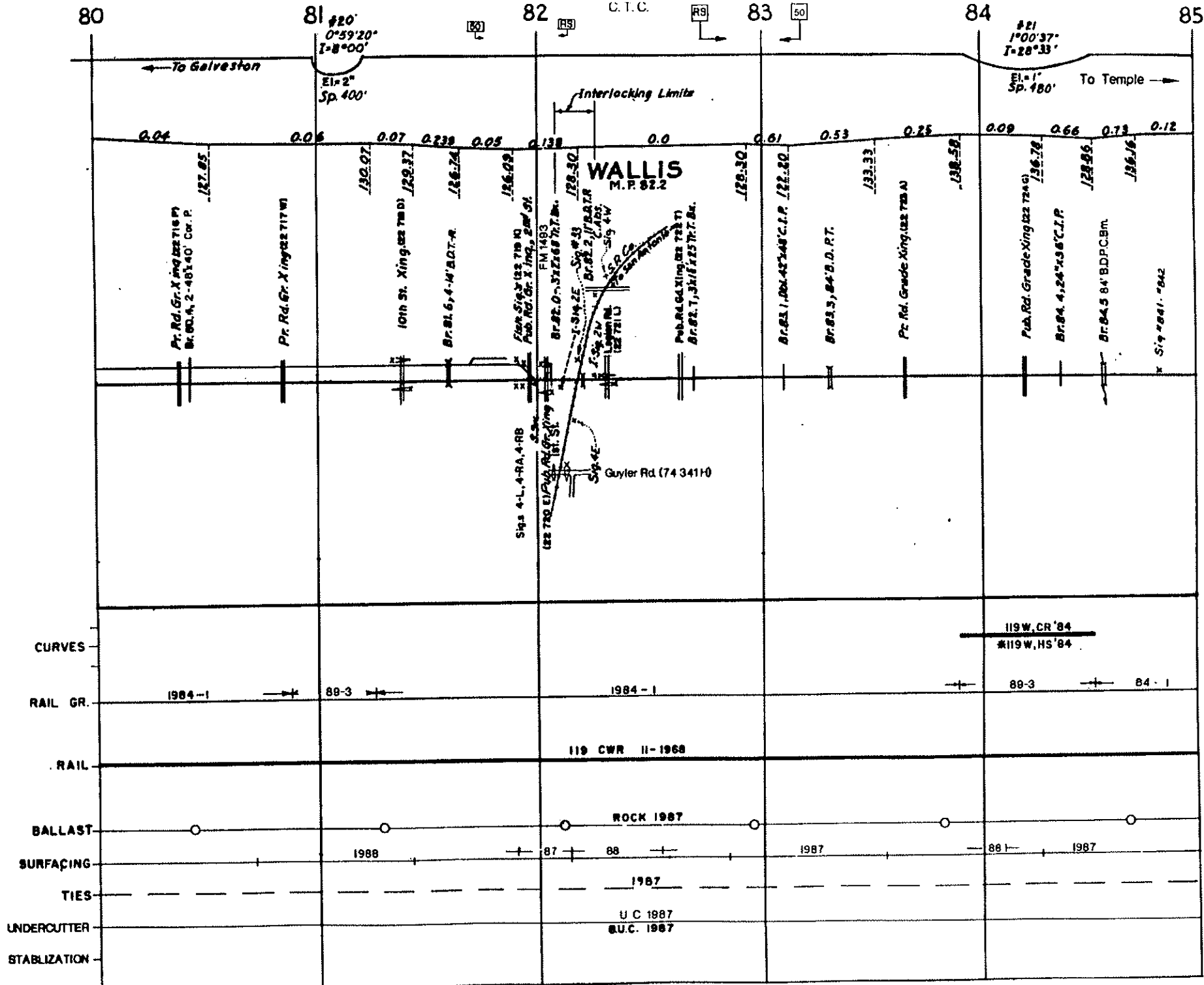


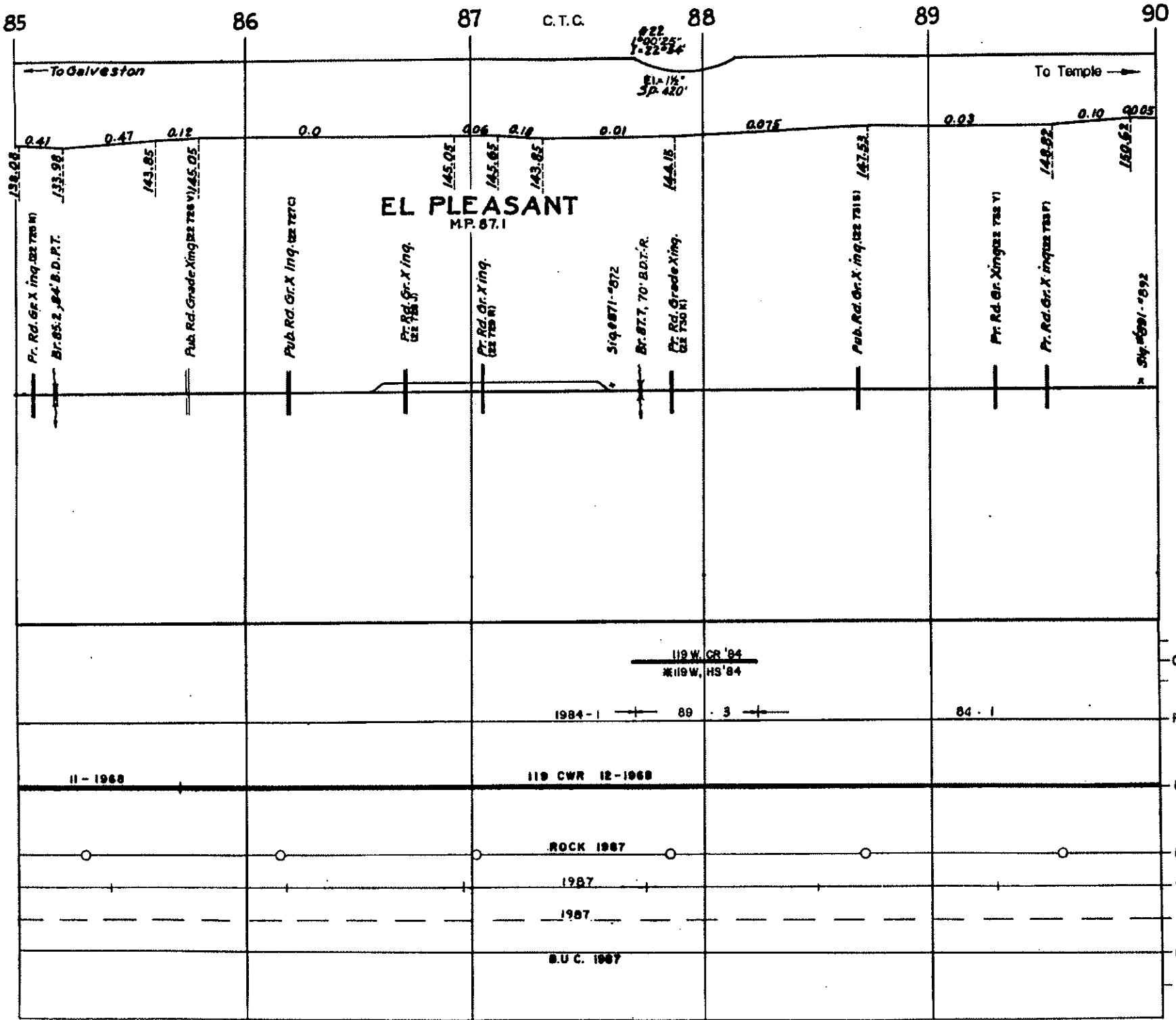




REVISED 3-1981







85 86 87 C.T.C. 88 89 90

To Galveston ←

To Temple →

**EL PLEASANT**  
MP 07.1

119 W. CR '84  
118 W. HS '84

1984 - 1      89      3      84 - 1

11 - 1968

119 CWR 12-1968

ROCK 1987

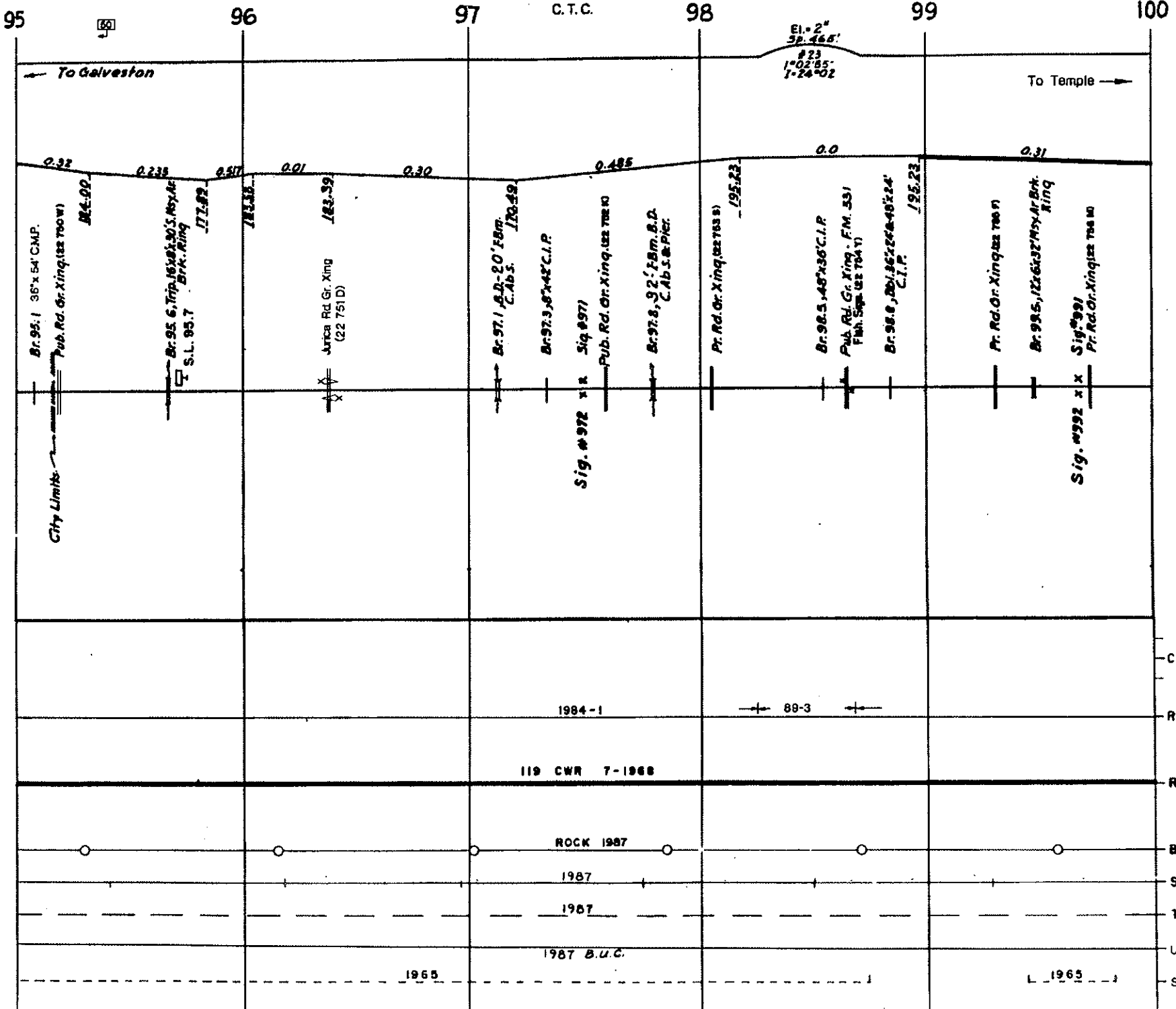
1987

1987

B.U.C. 1987

CURVES  
RAIL GR.  
RAIL  
BALLAST  
SURFACING  
TIES  
UNDERCUTTER  
STABILIZATION





City Limits

To Galveston

To Temple

C.T.C.

El. = 2'  
Sp. 465'  
# 23  
1° 02' 55"  
1-24-02

CURVES

RAIL GR.

RAIL

BALLAST

SURFACING

TIES

UNDERCUTTER

STABILIZATION

1984-1

89-3

119 CWR 7-1968

ROCK 1987

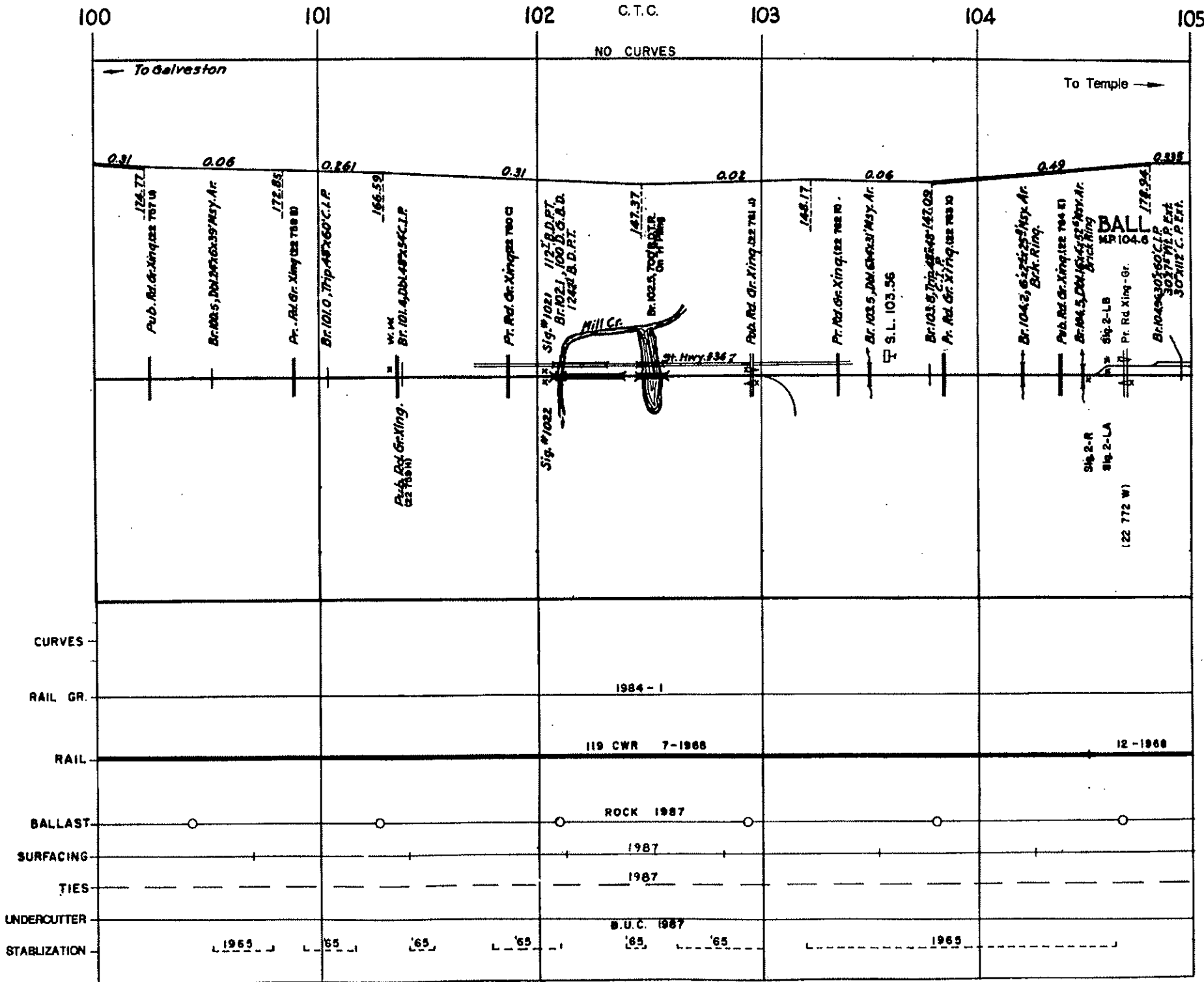
1987

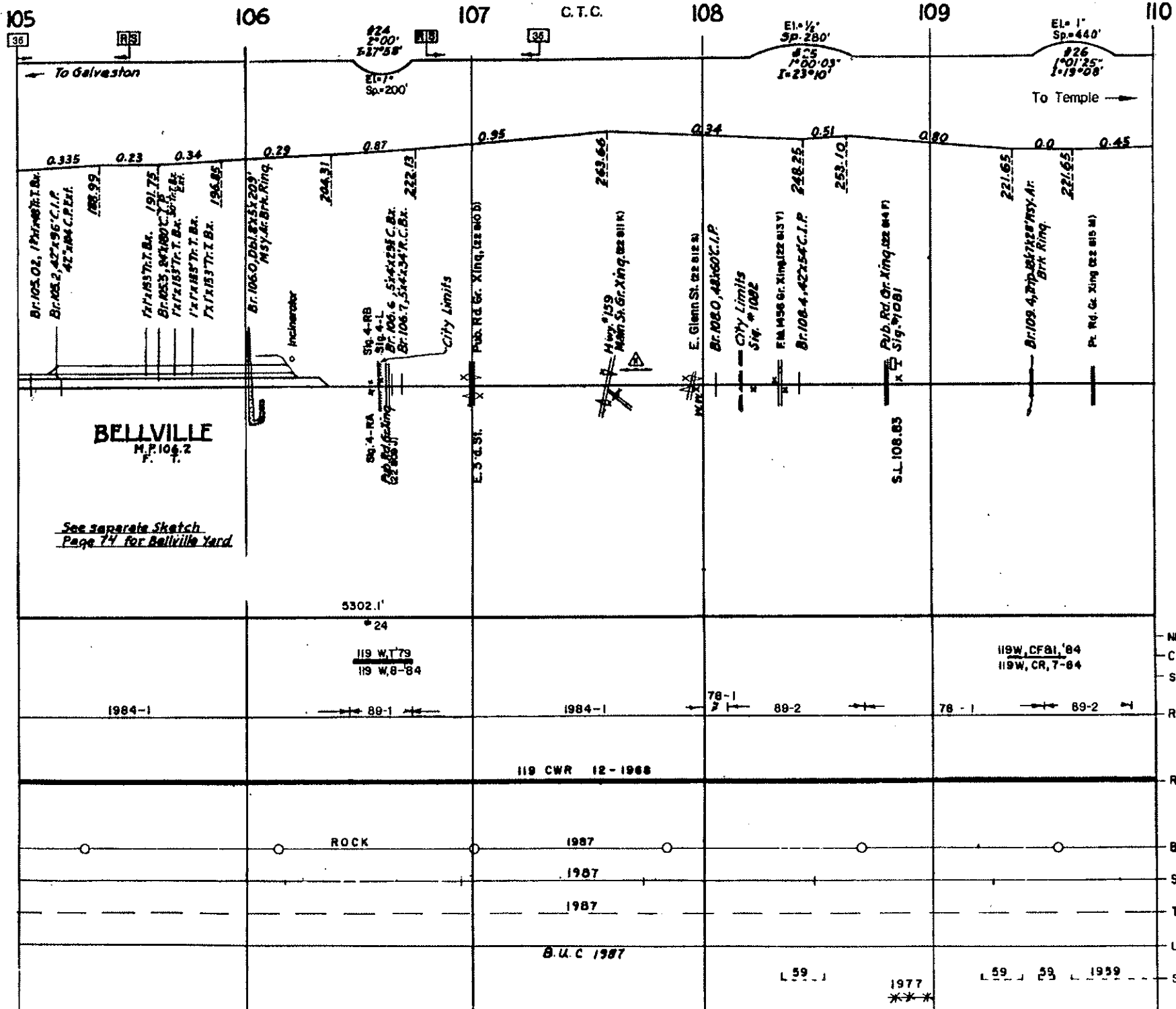
1987

1987 B.U.C.

1965

1965





**BELLVILLE**  
M.P. 106.2

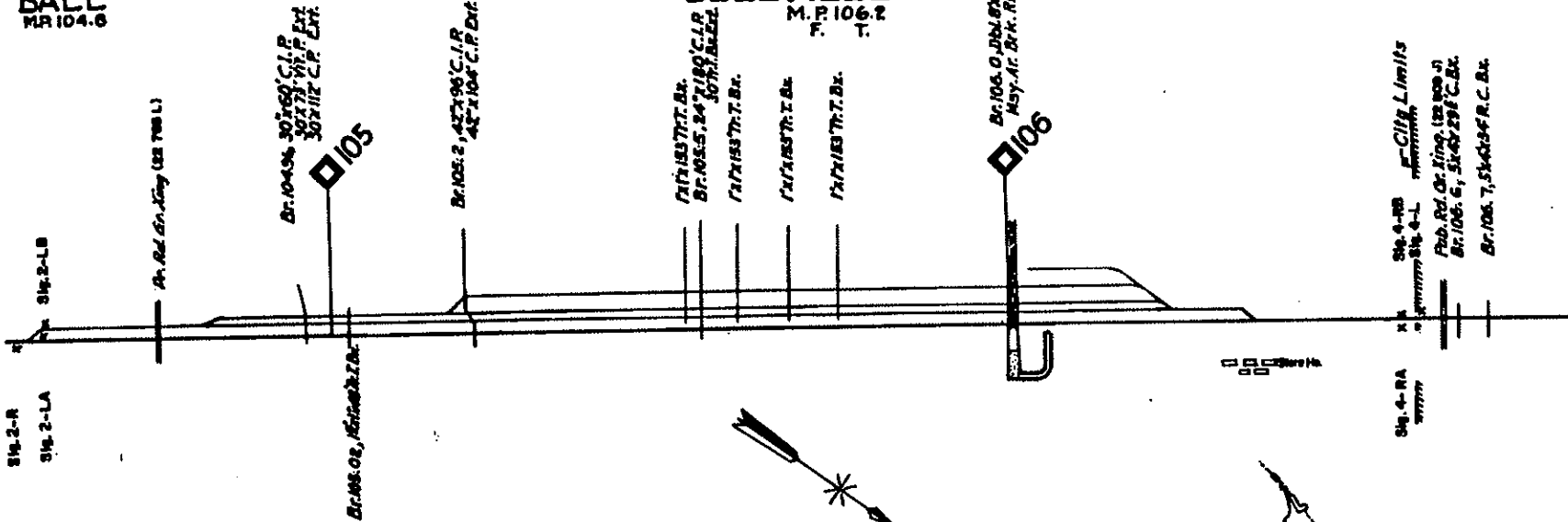
See separate sketch  
Page 74 for Bellville Yard

NR  
CURVES  
SR  
RAIL GR.  
RAIL  
BALLAST  
SURFACING  
TIES  
UNDERCUTTER  
STABILIZATION



← To Galveston

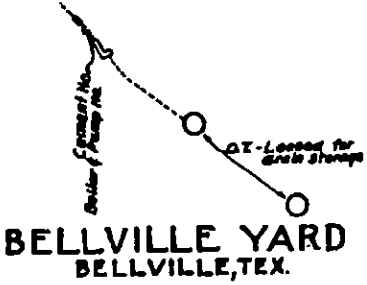
**BALL**  
M.P. 104.6

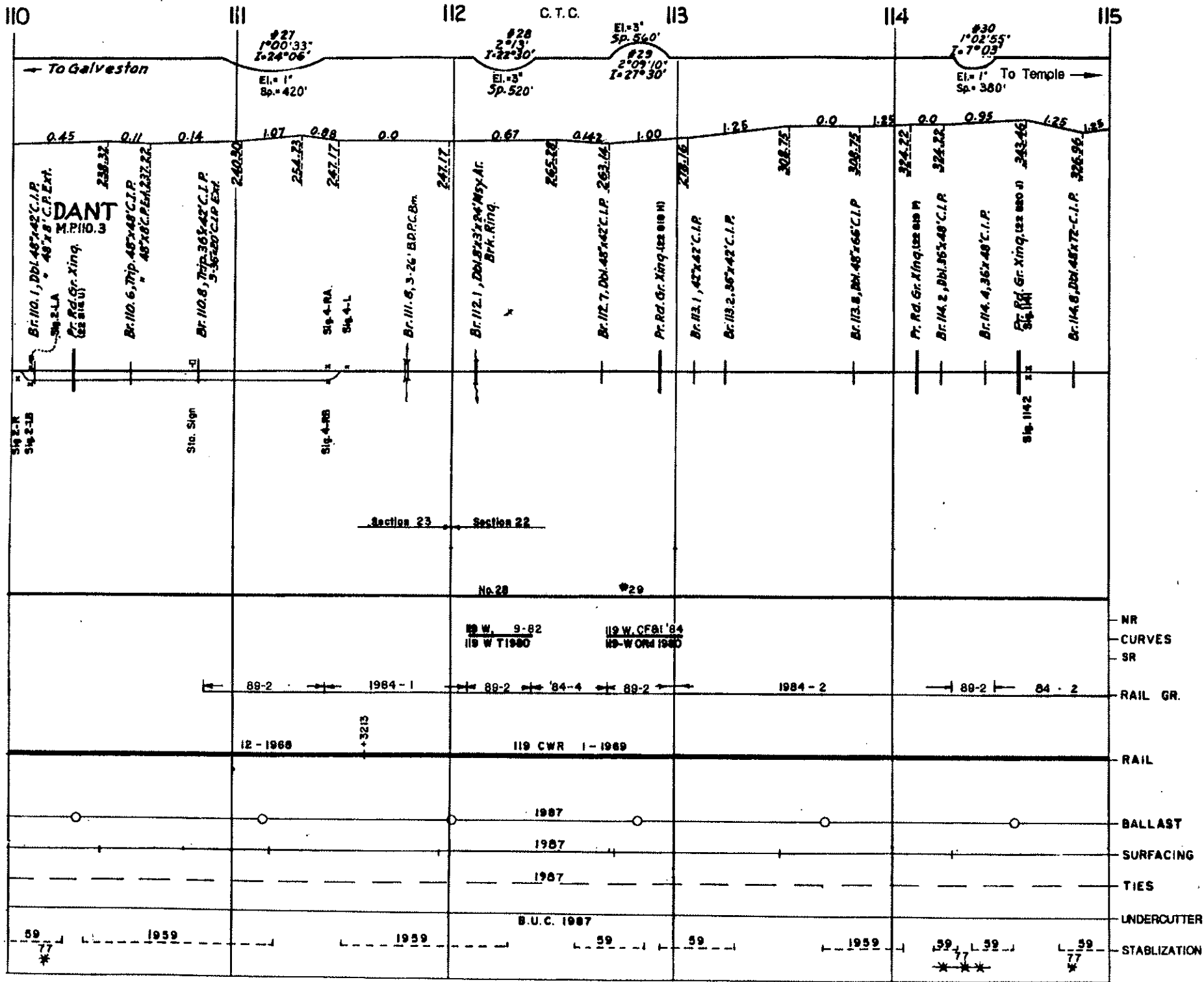


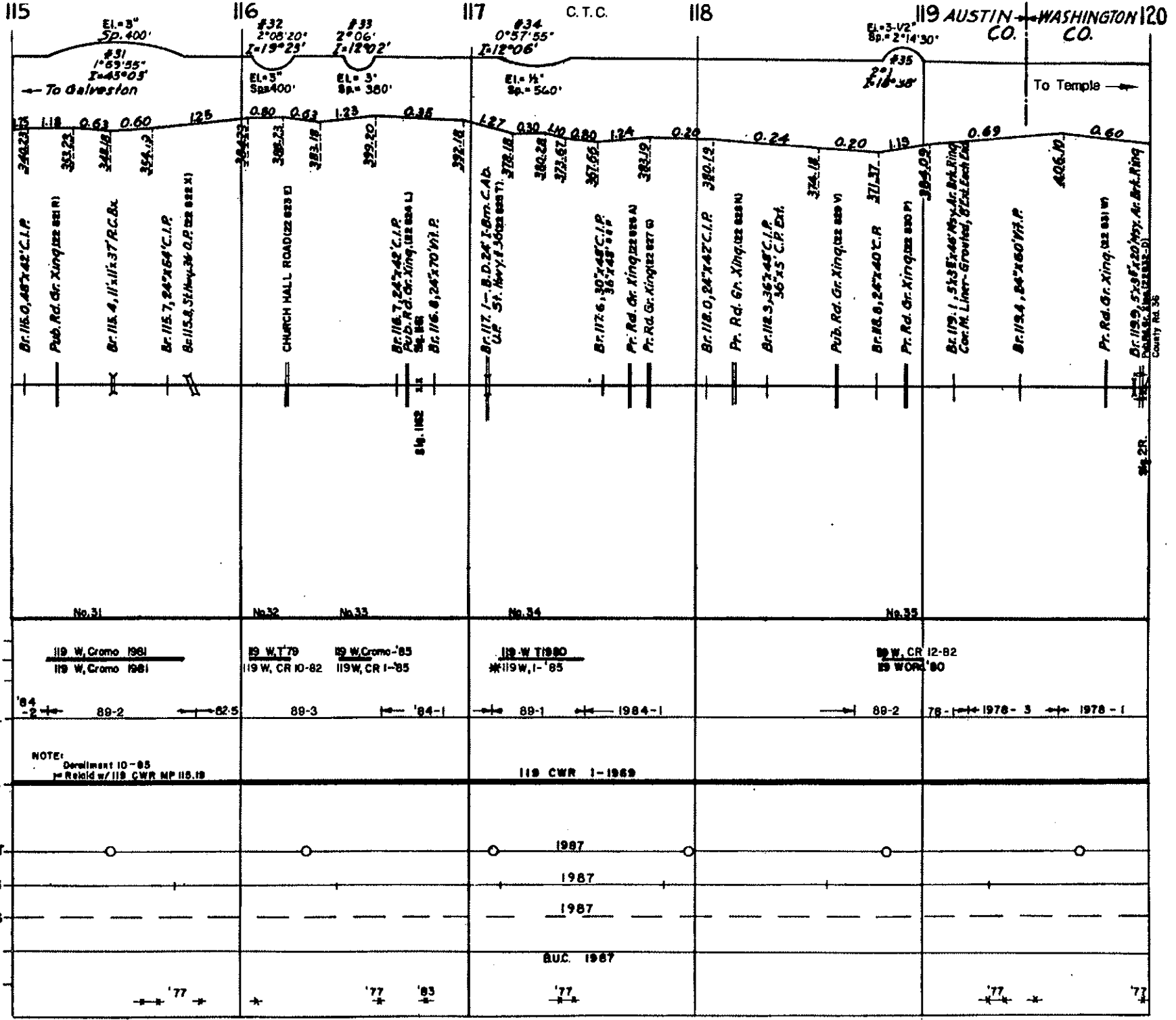
To Temple →

**BELLVILLE**  
M.P. 106.2

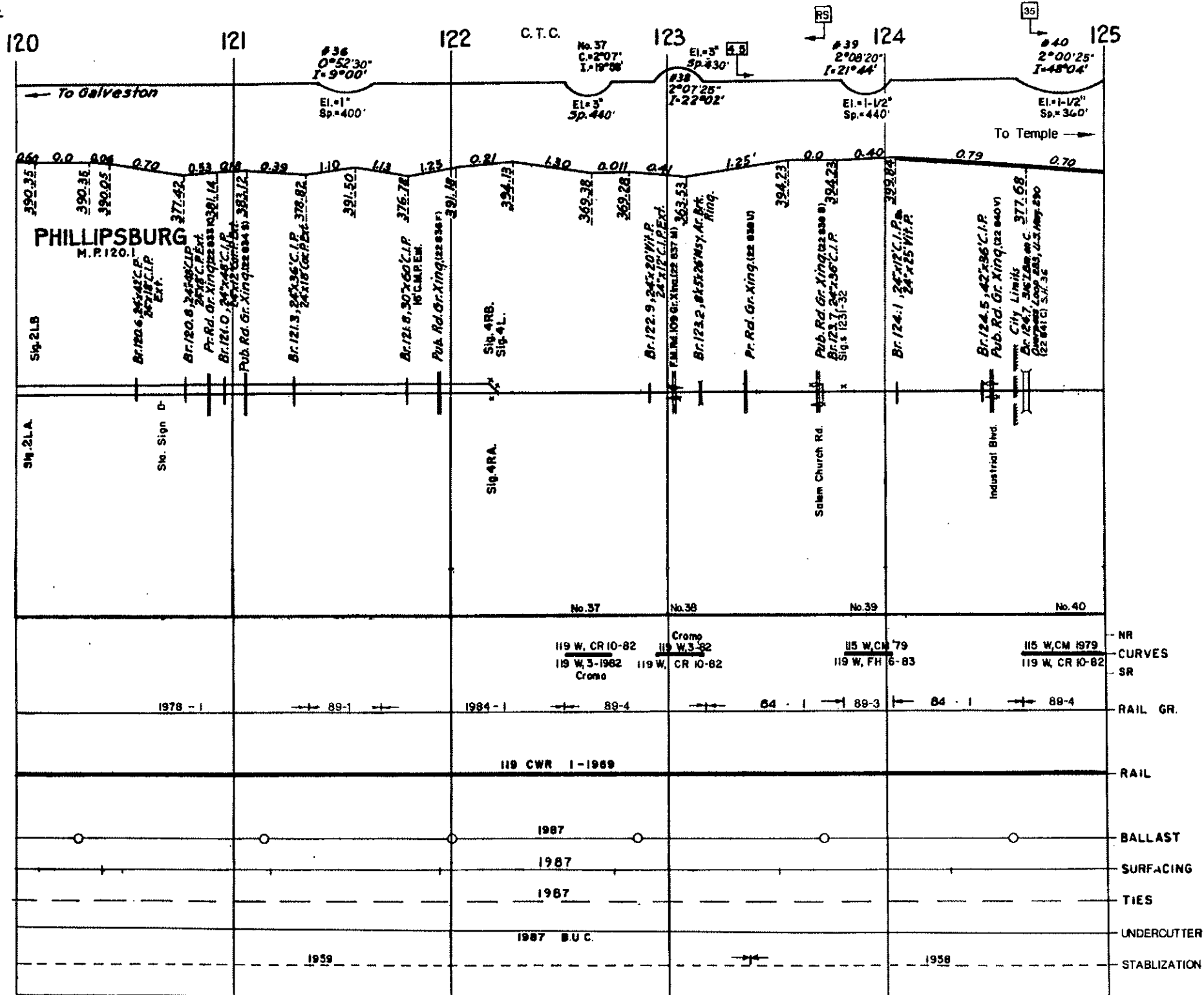
Longest Track = 180 Cars  
Shortest Track = 1 Car  
Total Car Capacity = Cars

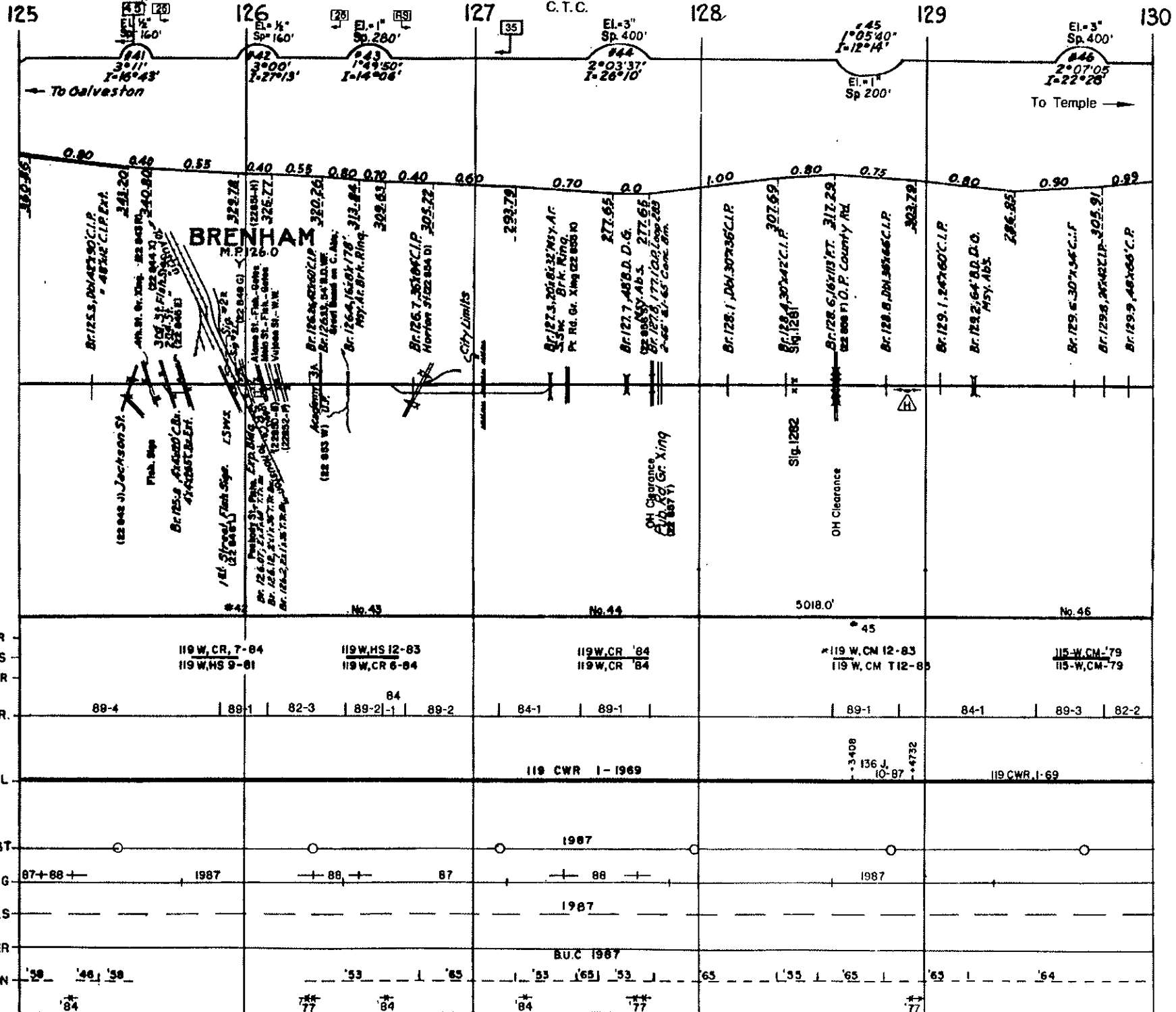


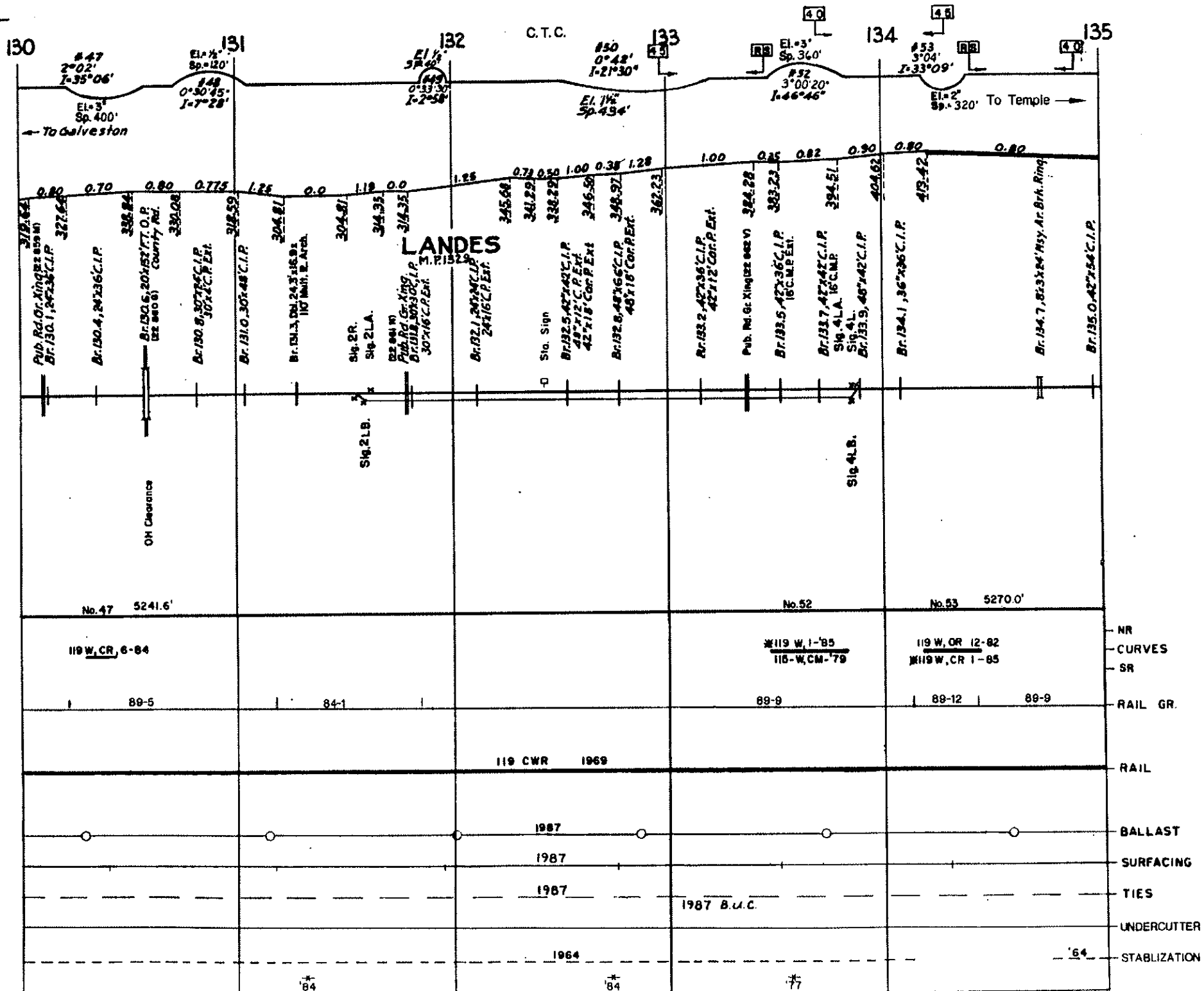




REVISED 3-1991







No. 47 5241.6'

No. 52

No. 53 5270.0'

119 W, CR, 6-84

119 W, 1-85  
 115-W, CM-79

119 W, OR 12-82  
 119 W, CR 1-85

89-5

84-1

89-9

89-12

89-9

119 CWR 1969

1987

1987

1987

1987 B.U.C.

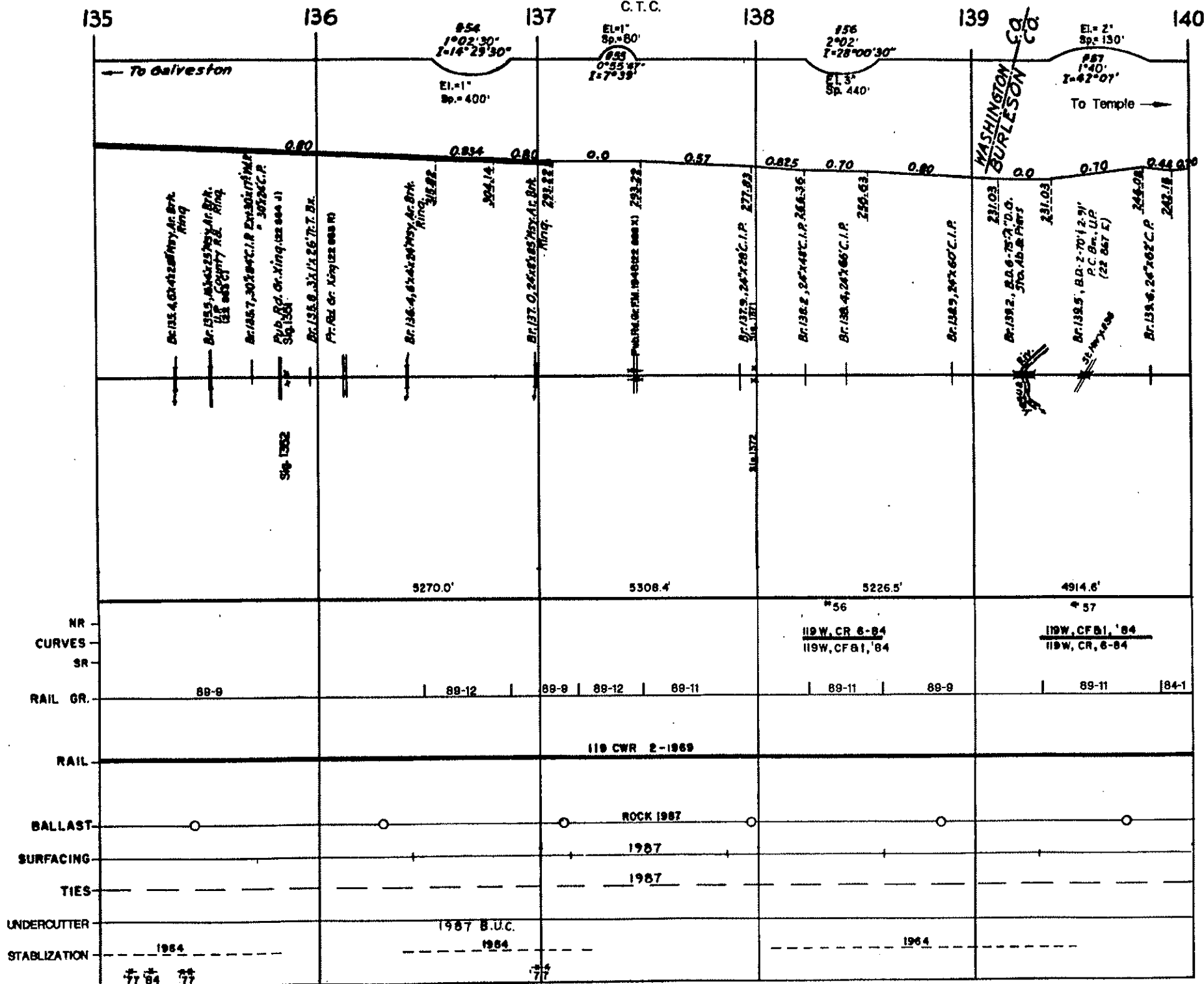
1964

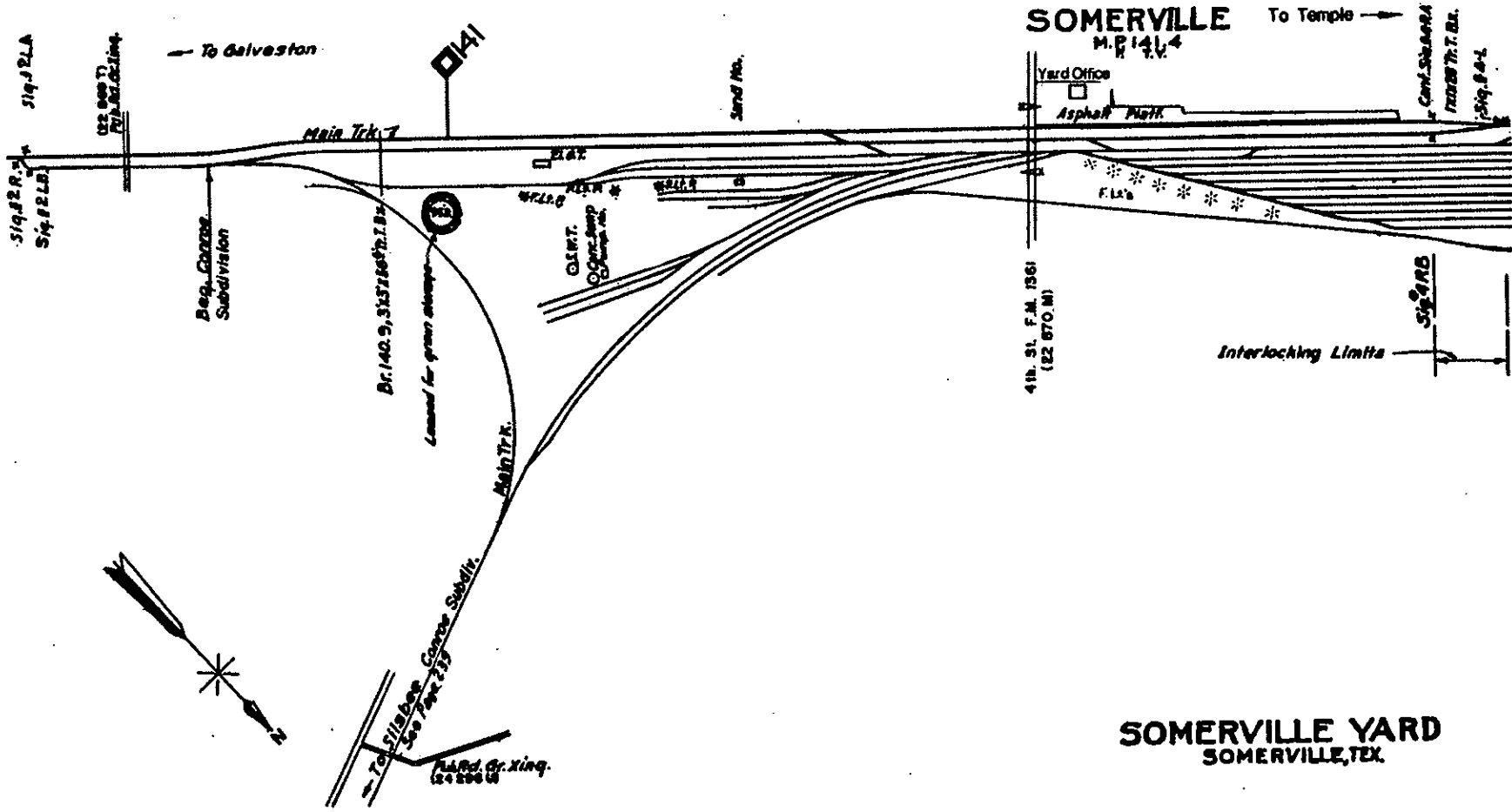
'64

84

84

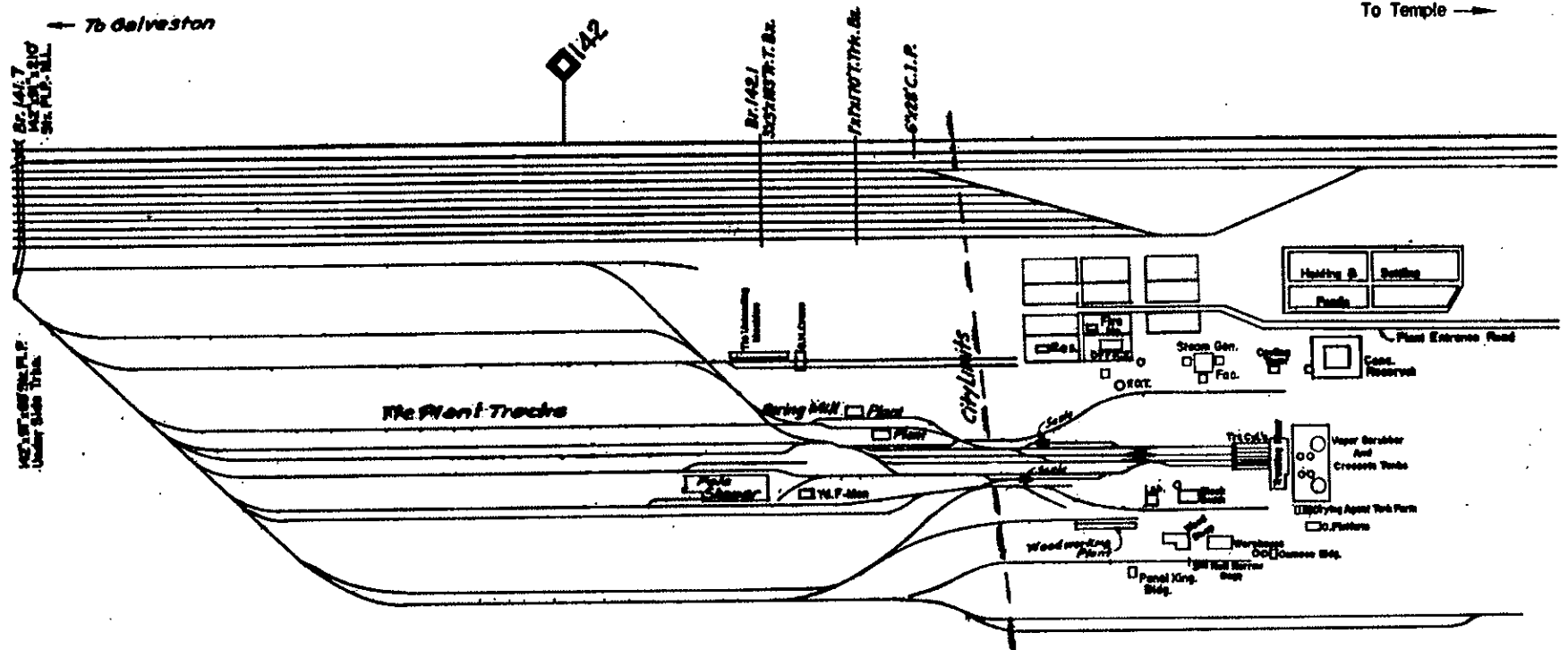
77





**SOMERVILLE YARD**  
SOMERVILLE, TEX.





Br. 142  
142/142/142  
6728 C.I.P.

To Galveston ←

To Temple →

142

Br. 142  
142/142/142

142/142/142

6728 C.I.P.

142/142/142  
6728 C.I.P.

Main Tracks

Spring Mill Plant

City Limits

Steam Gen. Fac.  
Oret.

Holding B. Building  
Ponds

Plant Entrance Road  
Cool. Reservoir

W.P. Shop

Vapor Scrubber  
and  
Crossflow Tanks

30. Flasks

Wood Working Plant

Panel King Bldg.

Longest Track = 114 Cars  
Shortest Track = 2 Cars  
Total Car Capacity = 1342 Cars

SOMERVILLE YARD  
SOMERVILLE, TEX.



145

146

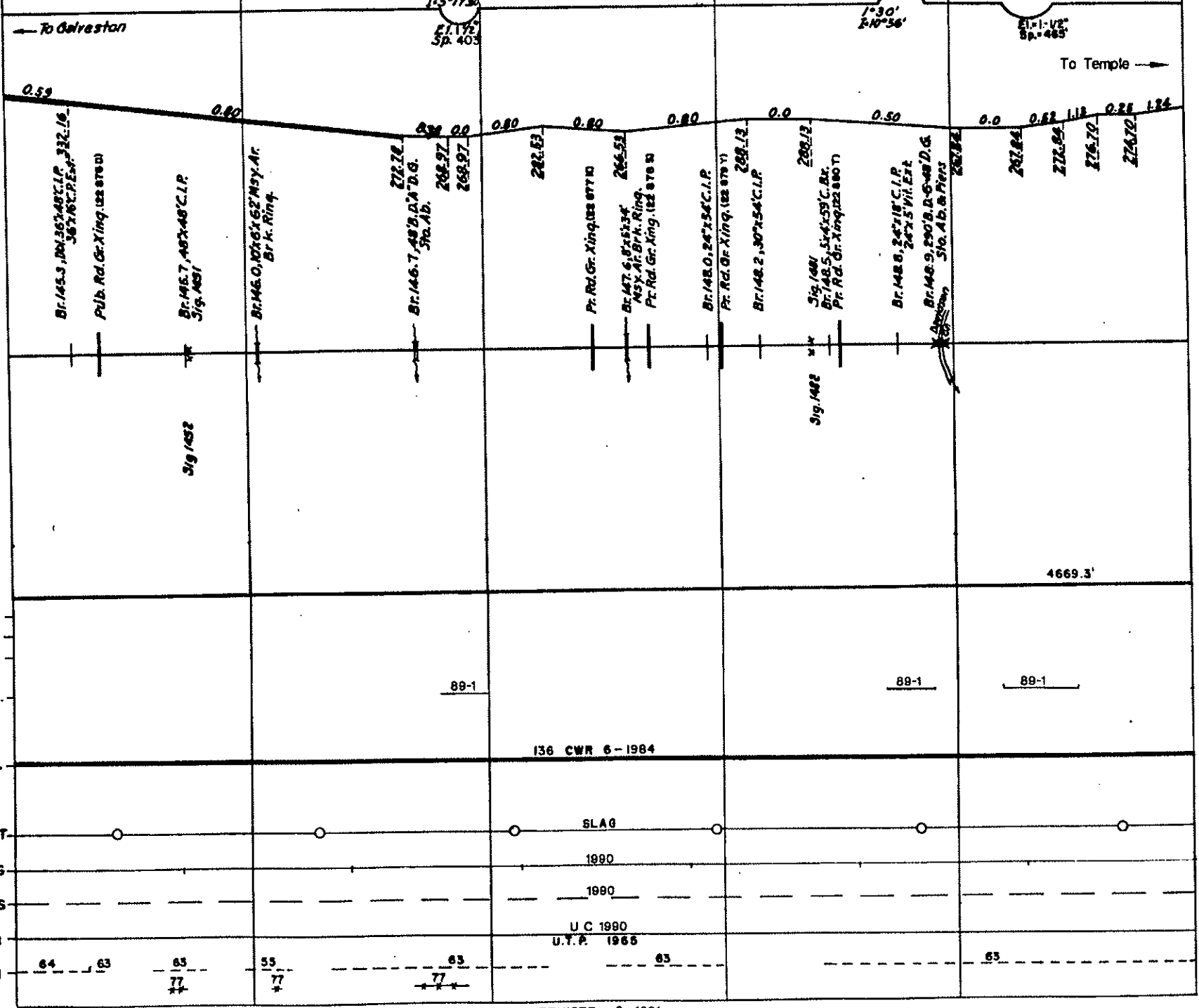
147

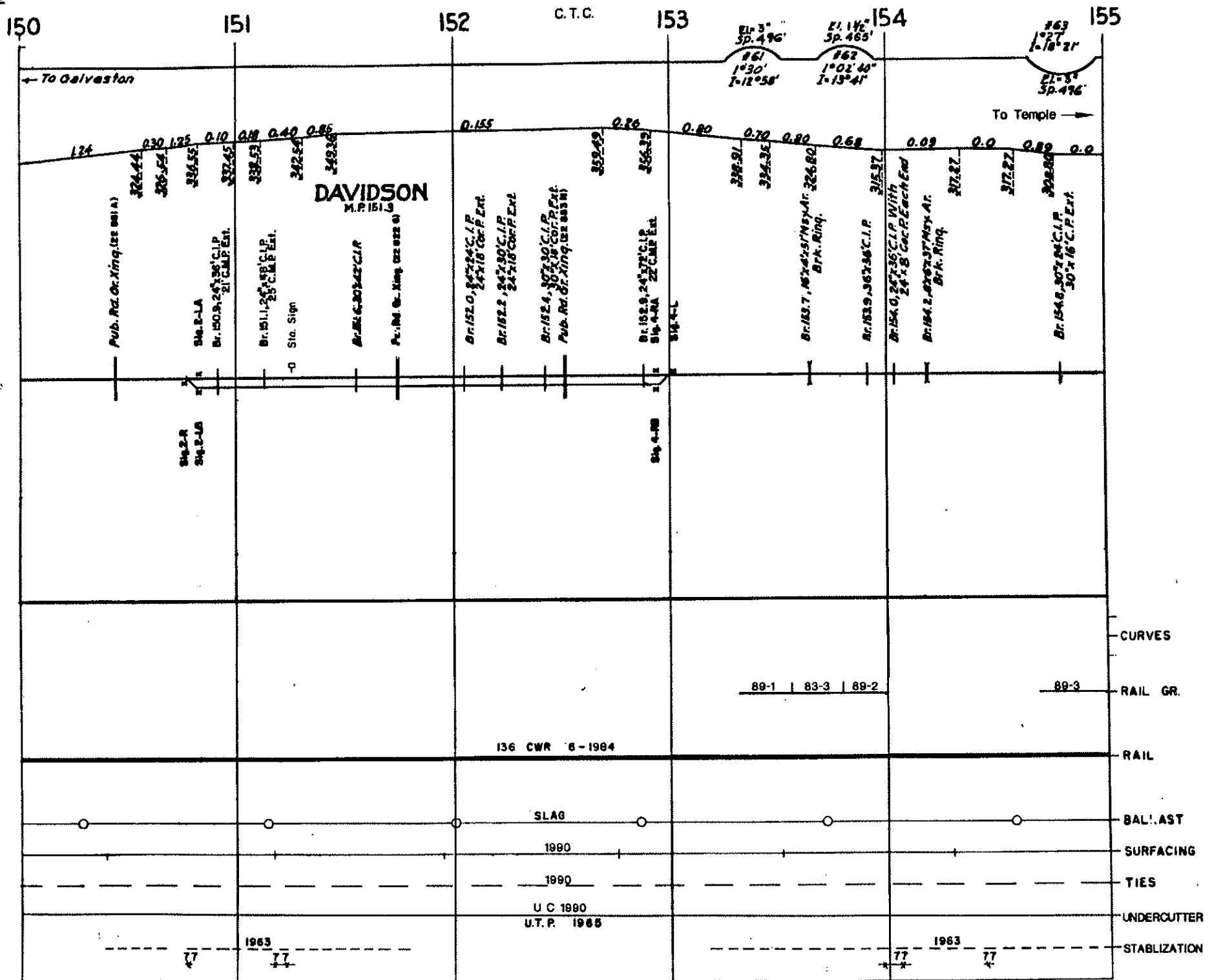
G.T.C.

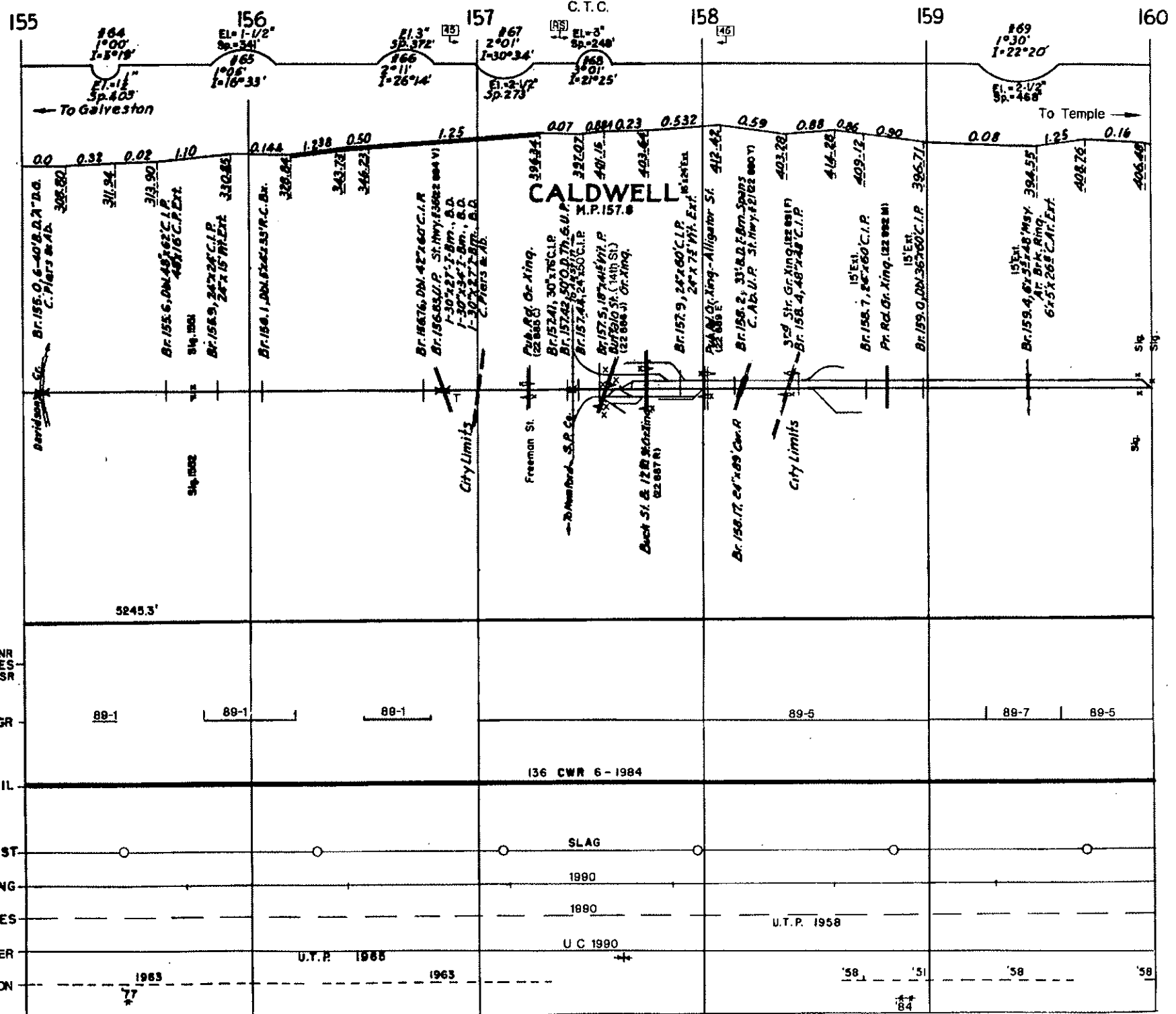
148

149

150







160

161  
970  
1°32'  
1-18°09'

162

C.T.C.

163

164

871  
C=1°34'  
I=16°19'

165

← To Galveston

To Temple →

0.60 0.47 0.09 0.87 0.37 0.80 0.0 0.745 1.00 1.08 0.12 0.75 0.10 1.13

Br. 160-2, 12x12x18' Wsy. 401.23  
Ar. Brk. Ring  
72x18x13' C. Ar. Ext.  
Br. 160-4, 30x48' C.I.P.  
405.86

406.98  
412.10

Pub. Rd. Gr. Xing (see U)  
Br. 161-A, 6x3x120' Wsy.  
Ar. Brk. Ring  
401.98

412.78  
417.76

Br. 162-3, 4x22x62' Wsy. Ar.  
Br. K. Ring  
Sigsbee 17822

Br. 162-6, 2x6x12' D.D.G.  
Wsy. Port & C. Ab.  
371.80

Br. 163-A, 48x42' C.I.P.

Pub. Rd. Gr. Xing  
(see U)

Pub. Rd. Gr. Xing (see U)

Br. 164-3, 24x36' C.I.P.  
24x27x16' C.I.P.  
Ext.

Pub. Rd. Gr. Xing (see U)

Br. 164-9, 48x42' C.I.P.  
24x27x16' C.I.P.  
Ext.

F.M. Hwy. 1363  
Sub. R. Gr. 164-8  
Sub. R. Gr. 164-9  
Sub. R. Gr. 164-10  
Ext.

5143.1'

NR  
CURVES  
SR  
RAIL GR.  
RAIL  
BALLAST  
SURFACING  
TIES  
UNDERCUTTER  
STABILIZATION

89-6 | 89-7 | 89-5 | 89-7 | 89-5 | 89-7 | 89-5 | 89-6

136 CWR 6-1984

SLAG

SL 86 | 87 | SL 1986 | 1990 | 1986 | 87 | 88 | SL 87 | 88 | SL 87 | 88 | SL 87

1990

U C 1990  
U.T.P. 1958

58 | 77\* | 58 | 77\* | 58 | 58 | 58

165

166

167

C.T.C.

168

169

170

873  
1°04'  
I=7°16'  
E11 1/2"  
Sp. 350'

874  
1°00'  
I=17°02'20"

EL=1 1/2"  
Sp. 195'  
BURLESON CO. MILAN CO.

876  
1°04'  
I=10°36'  
E1 1/2"  
Sp. 468'

877  
1°05'  
I=62°02'  
E1 1/2"  
Sp. 390'

878  
1°07'  
I=54°00'  
E1 1/2"  
Sp. 390'



CHRISMAN  
M.R. 169.8

Br. 166.2, 30°24' C.I.P.  
30°24' C.I.P. EXT.  
30°24' C.I.P. EXT.

Pub. Rd. Gr. Xing. (see 808 R)  
Br. 166.2, 30°24' C.I.P.

Pr. Rd. Gr. Xing. (see 808 R)

Br. 166.6, 30°24' C.I.P.  
28° C.M.P. EXT.

Br. 167.1, 48°12' C.I.P.

Pub. Rd. Gr. Xing. (see 808 R)

Pub. Rd. Gr. Xing. (see 808 R)

Br. 167.8, 30°24' C.I.P.  
30°24' C.I.P. EXT.

Pub. Rd. Gr. Xing. (see 808 R)

Br. 168.6, 42°45' C.I.P.

Pub. Rd. Gr. Xing. (see 808 R)  
Br. 169.0, 48°12' C.I.P.

Pub. Rd. Gr. Xing. (see 808 R)  
Br. 169.6, 48°12' C.I.P.

Br. 169.9, 42°19' C.I.P.  
Pr. Rd. Gr. Xing. (see 808 R)

NR  
CURVES  
SR

RAIL GR.

RAIL

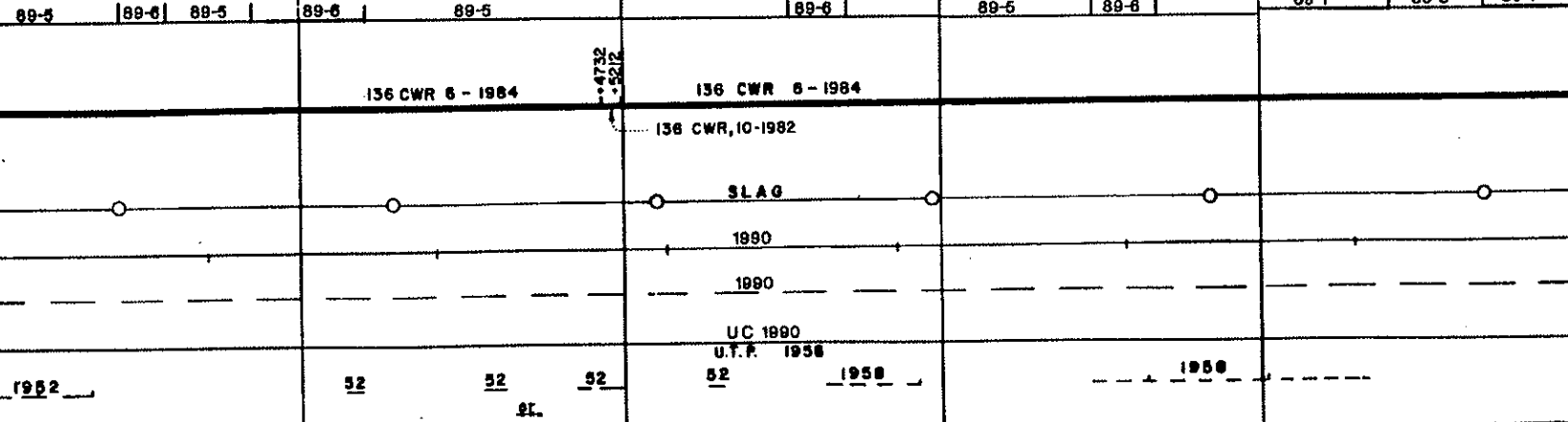
BALLAST

SURFACING

TIES

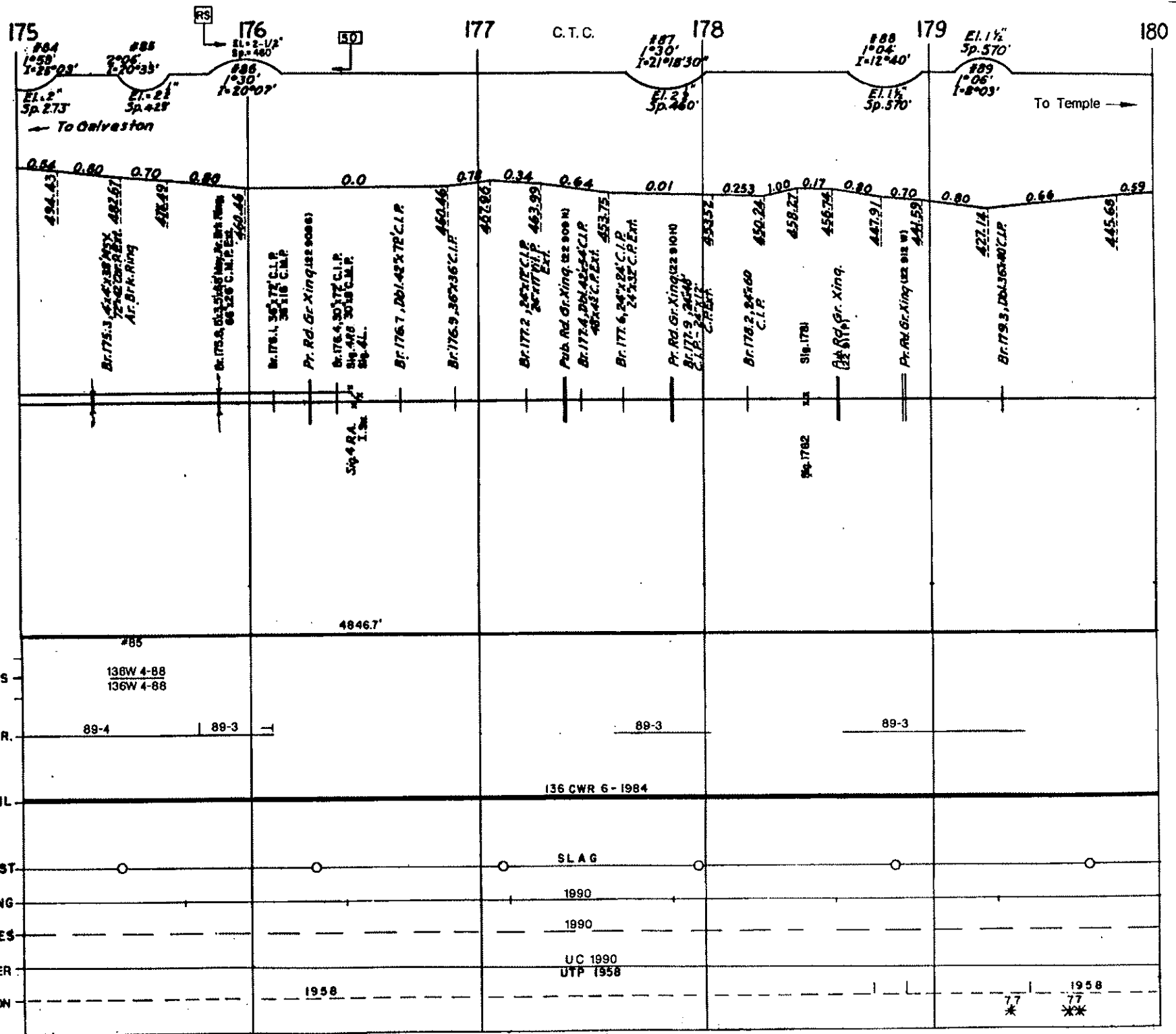
UNDERCUTTER

STABILIZATION



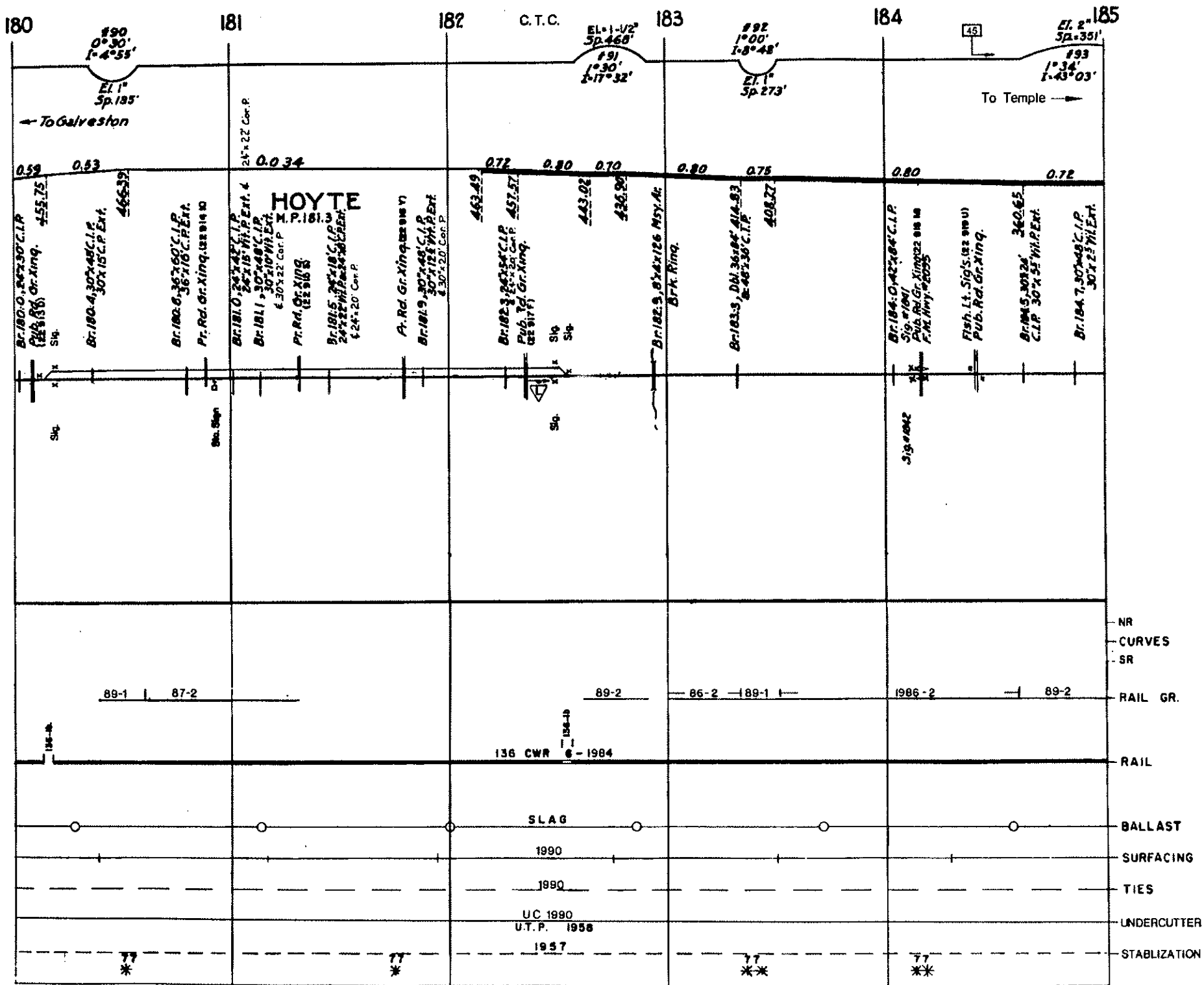


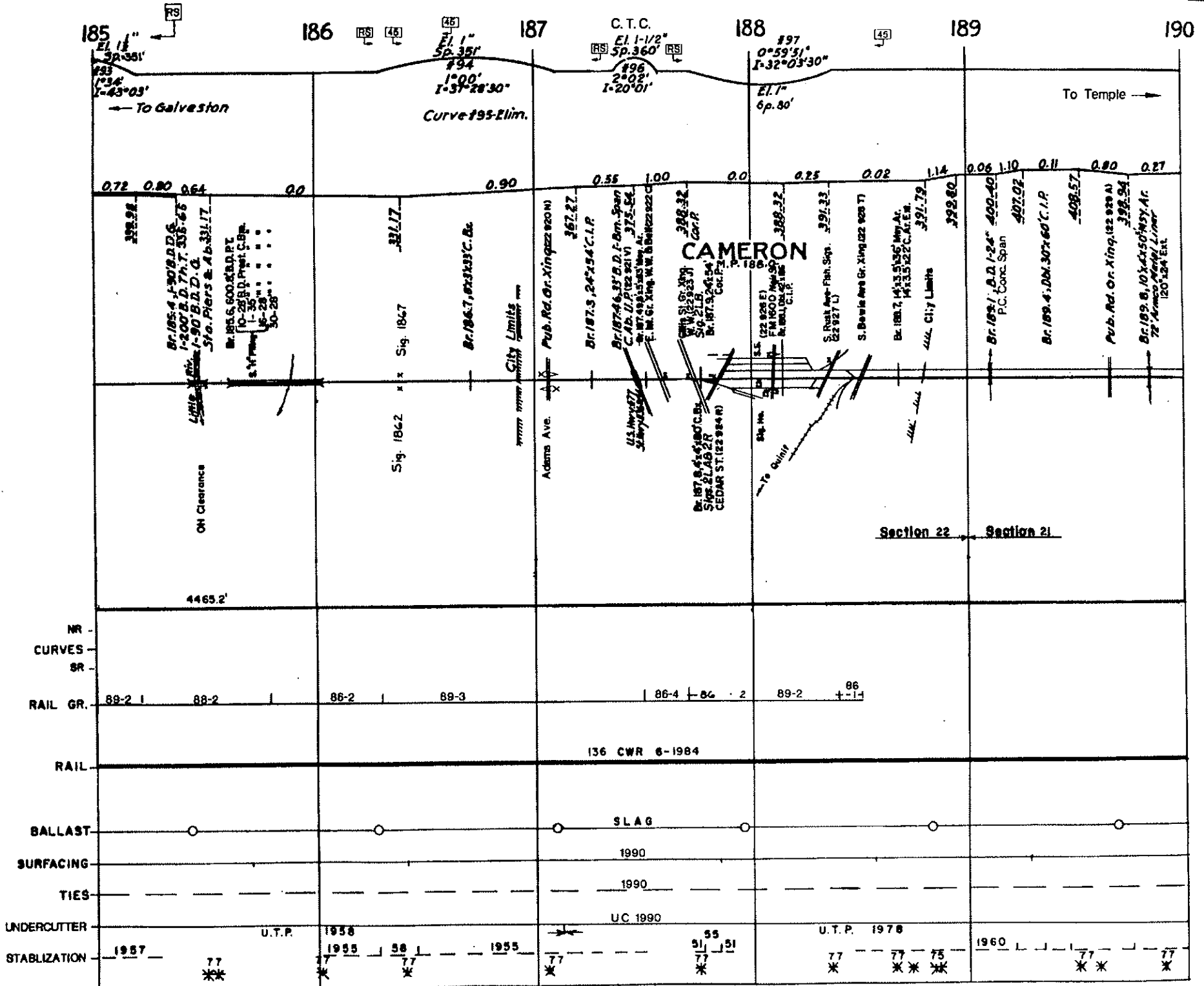


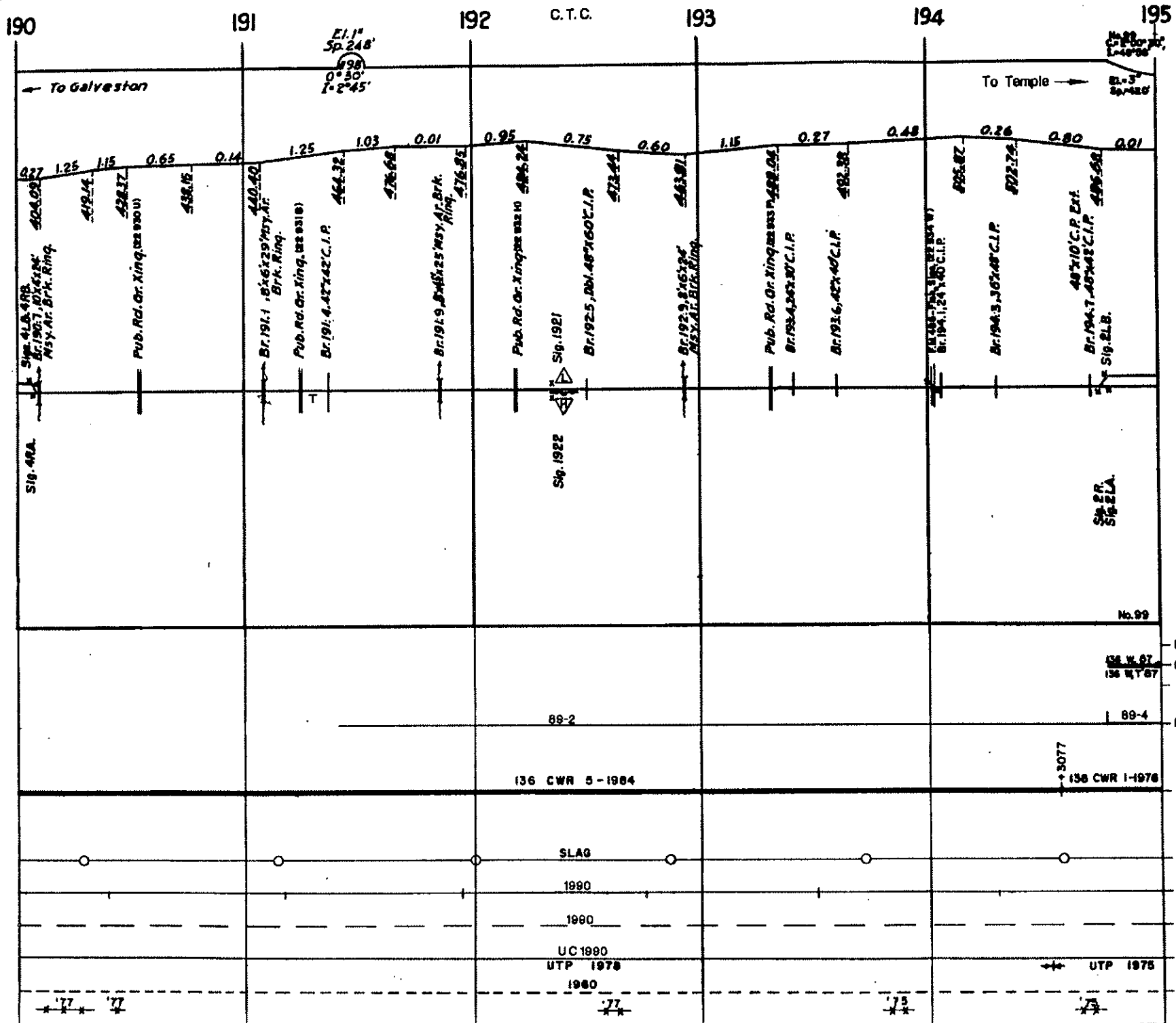


CURVES  
 RAIL GR.  
 RAIL  
 BALLAST  
 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION

1958  
 77  
 \*\*







- NR
- CURVES
- SR
- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION

195

196

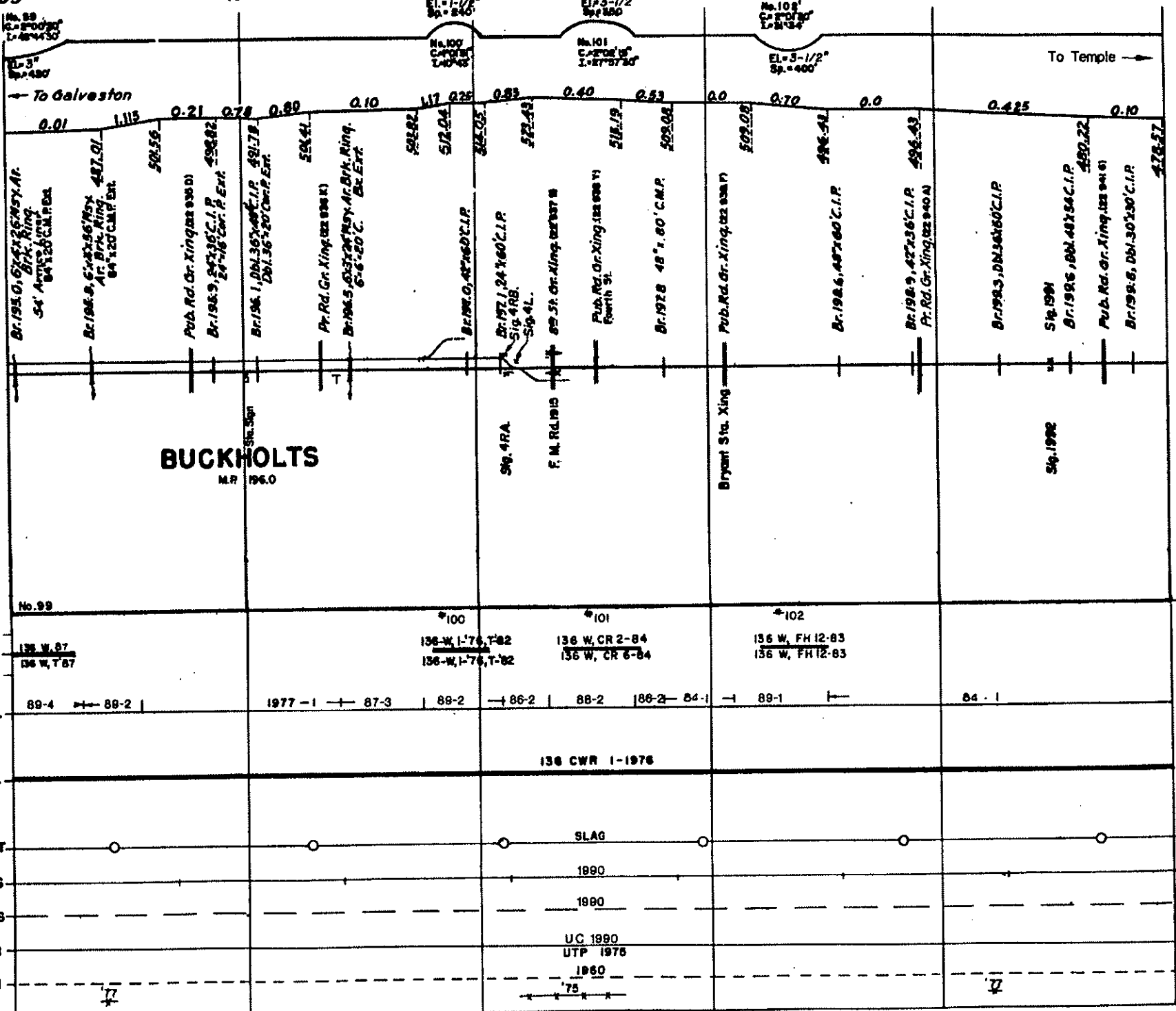
197

C.T.C.

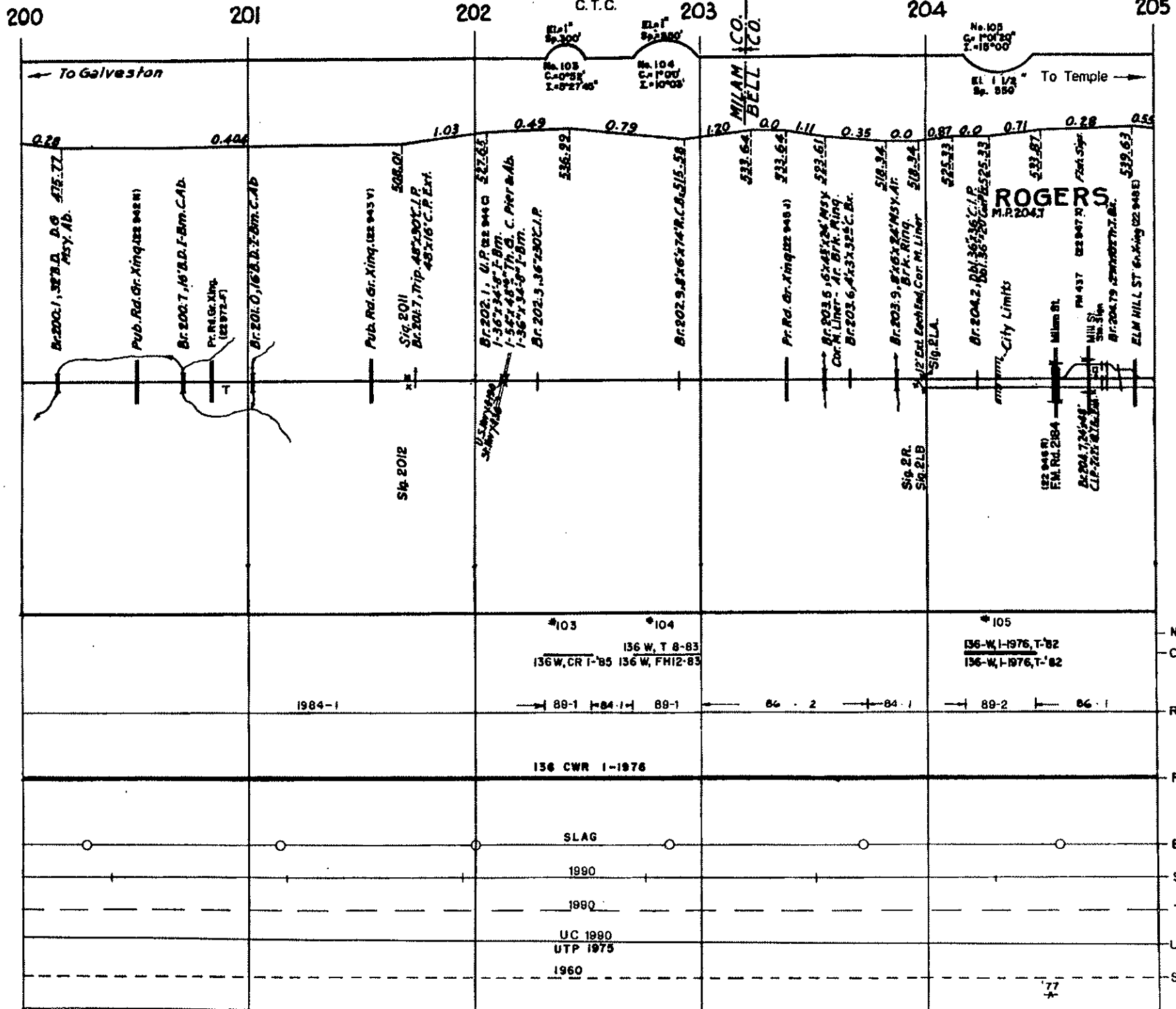
198

199

200

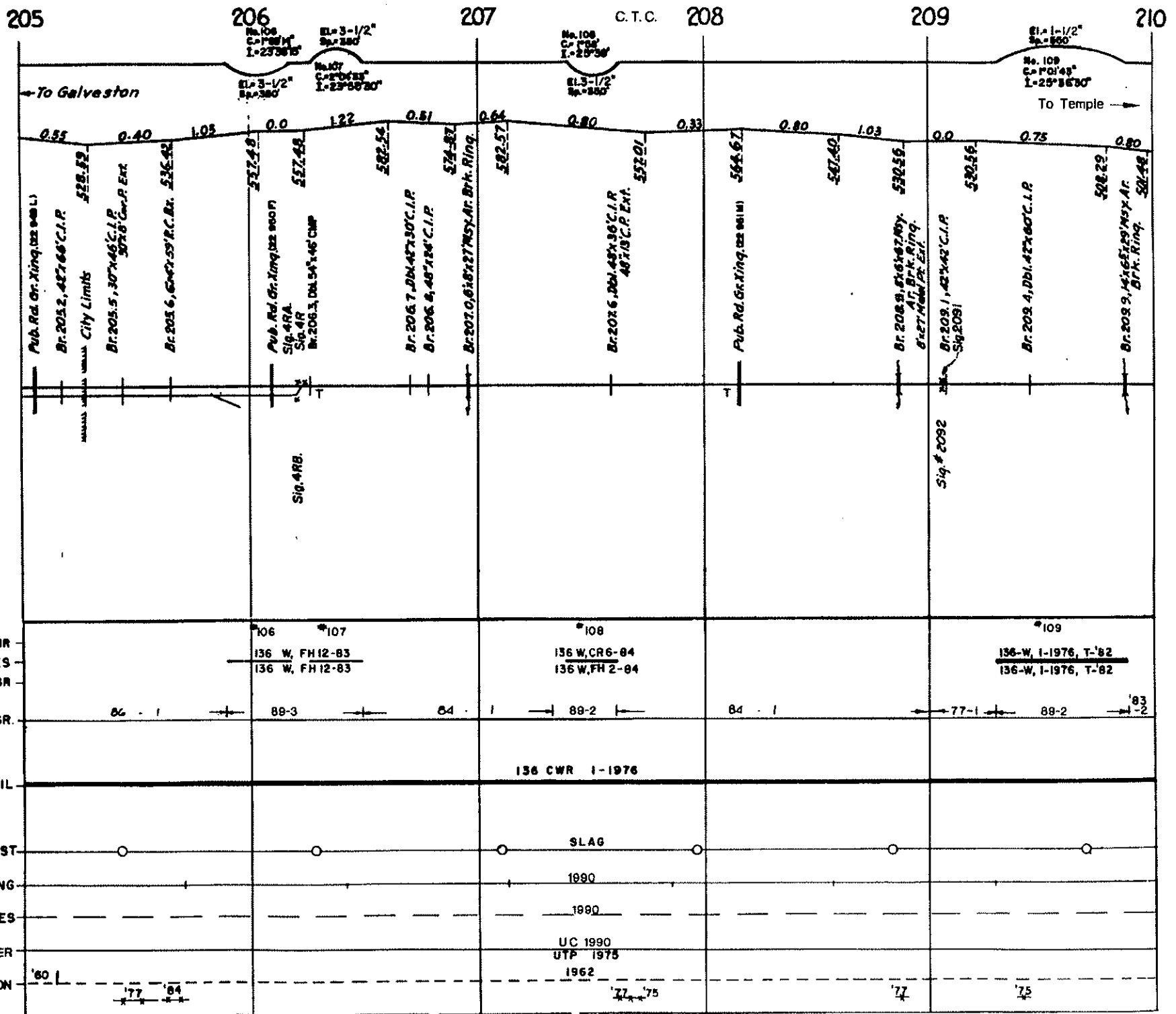


NR	No. 99		*100		*101		*102	
CURVES	136 W, T 87		136 W, I-76, T-82		136 W, CR 2-84		136 W, FH 12-83	
SR	136 W, T 87		136 W, I-76, T-82		136 W, CR 6-84		136 W, FH 12-83	
RAIL GR.	89-4	88-2	1977-1	87-3	88-2	86-2	84-1	89-1
RAIL	136 CWR 1-1976							
BALLAST	SLAG							
SURFACING	1990							
TIES	1990							
UNDERCUTTER	UC 1990 UTP 1976							
STABILIZATION	1960							



NR  
 CURVES  
 RAIL GR.  
 RAIL  
 BALLAST  
 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION

77



REVISED 3-1991

210

211

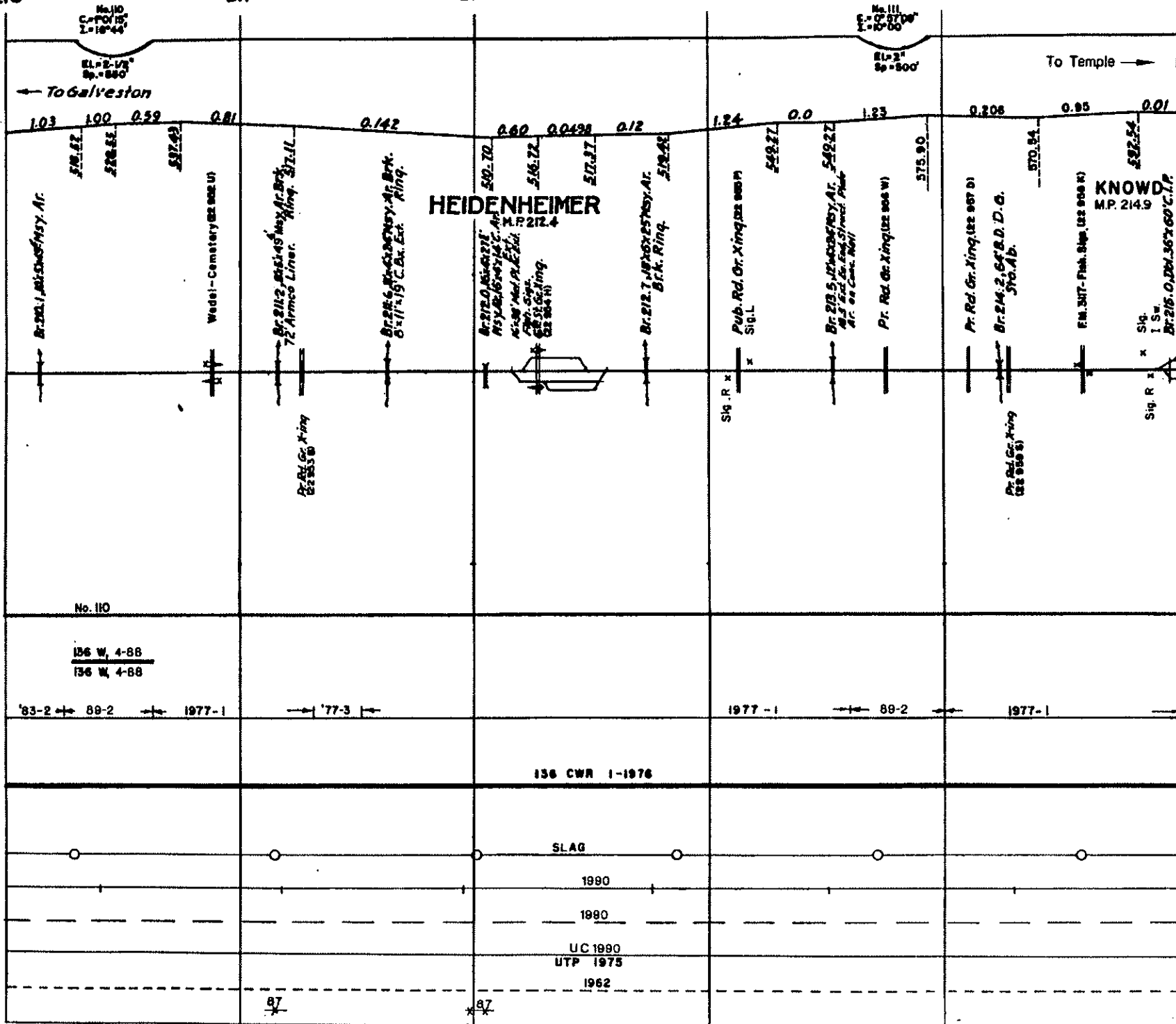
212

C.T.C.

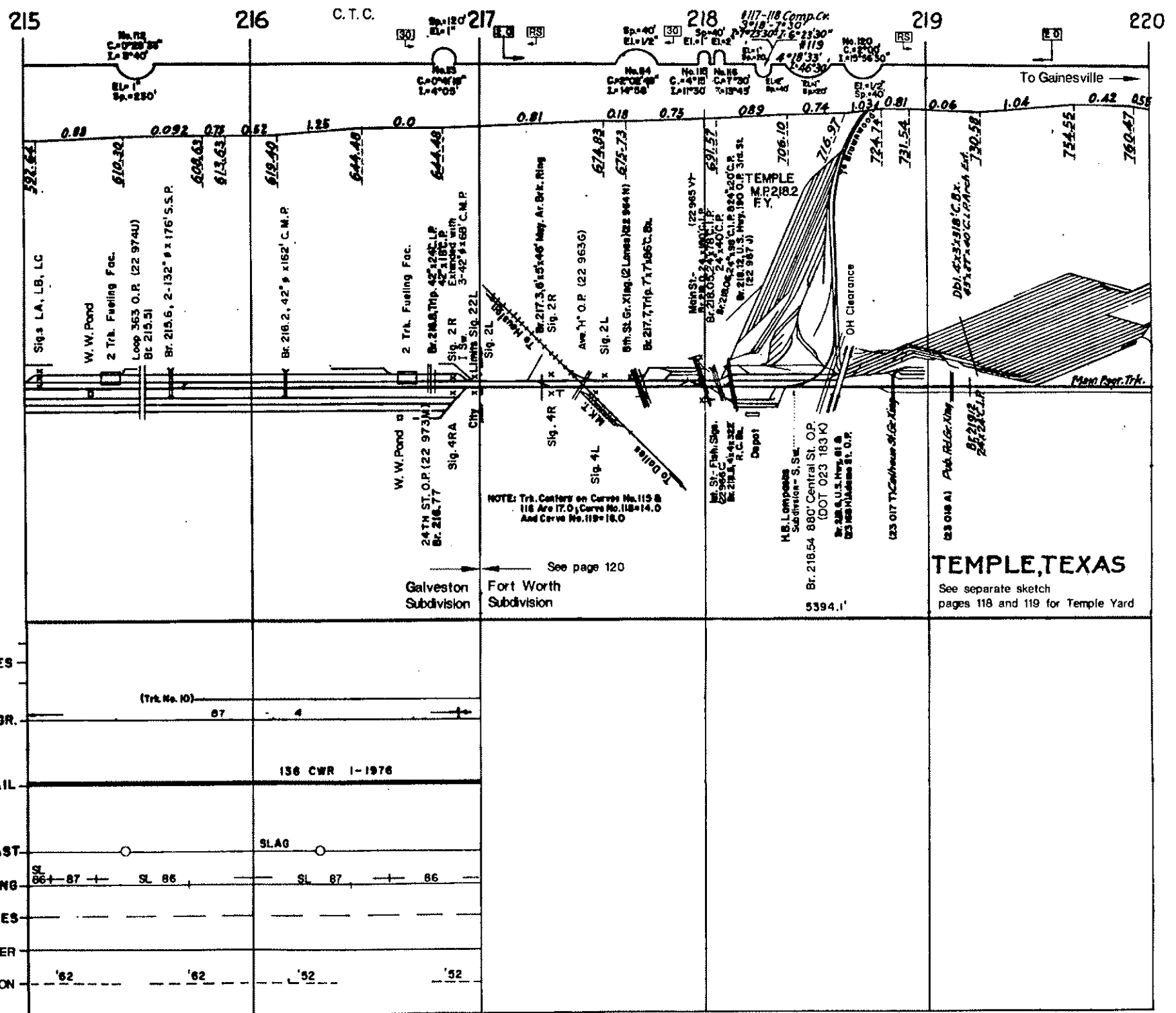
213

214

215







# TEMPLE, TEXAS

See separate sketch pages 118 and 119 for Temple Yard

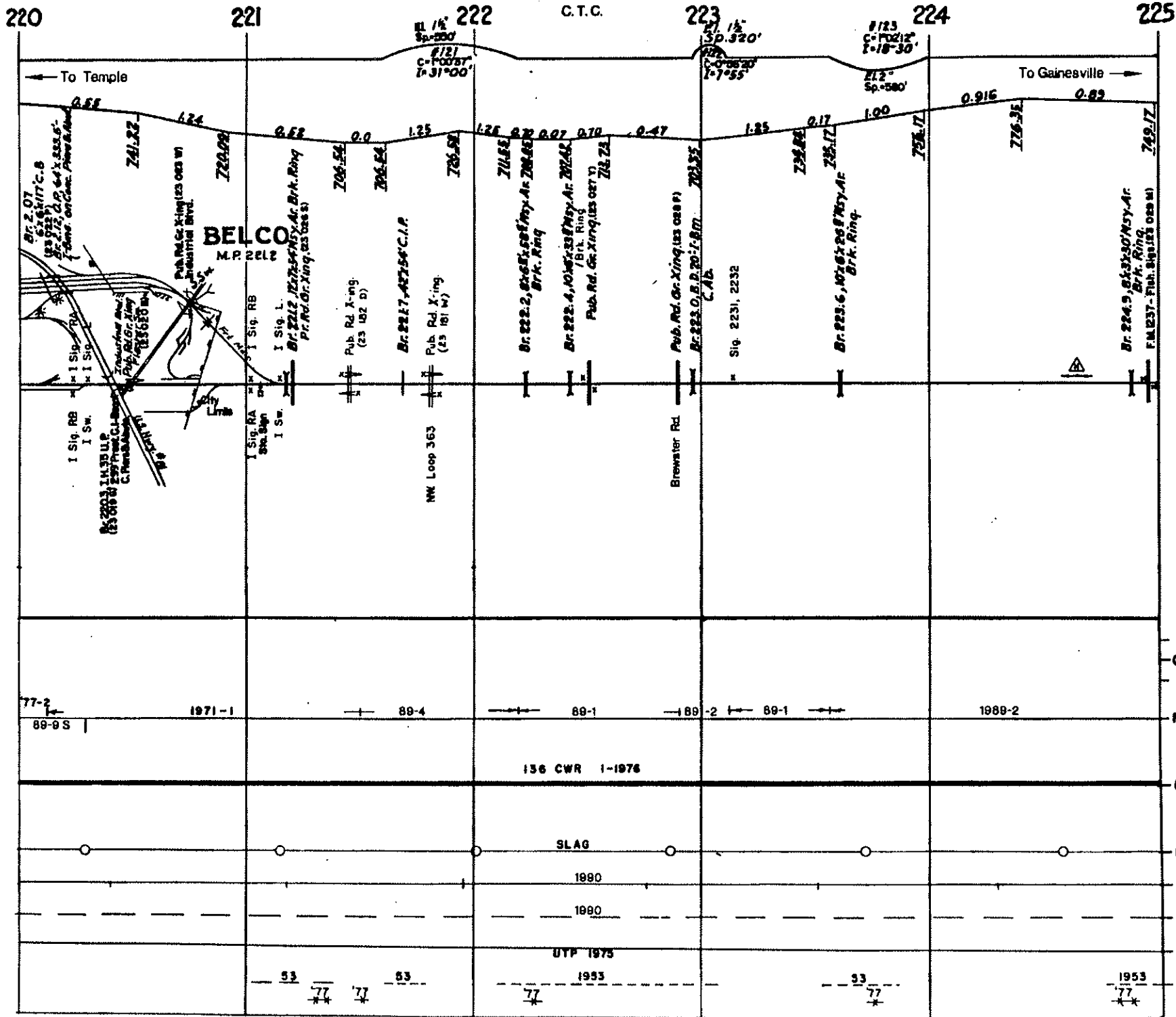
# Galveston Subdivision

Temple (M.P. 218.2) to Galveston M.P. 0.0)









225

226

227

228

229

230

← To Temple

To Gainesville →

749.17

# PENDLETON

I Sw.  
I Sig. LB  
Br. 225.3, Dbl. 487x60  
C.I.P.  
(25 030 8)  
Pub. Rd. Gr. Xing.  
5-0-76  
Pub. Rd. Gr. Xing.  
5-0-76  
R.C. Bk.  
Br. 225.6, Dbl. 487x60  
C.I.P.  
15 035 6 Xing.  
Br. 225.9, Trip. 487x64  
Z22-64  
Pub. Rd. Gr. Xing.  
5-0-76  
C.I.P.  
Br. 226.0, 5-0-76 Hwy. 729.64  
A.R. Br. K. Xing.  
5-0-76 Conc. Bk. Ext.

I Sig. R  
I Sig. LA  
Sta. Sign  
F.M. Hwy. No 1237, 2nd St.  
Area B

Br. 227.0, Dbl. 367x56 C.I.P.

I Sig. RB  
I Sig. L  
Pub. Rd. Gr. Xing. 03 034 J  
Br. 227.3, Dbl. 367x56 C.I.P.

I Sig. RA  
I Sw.

Br. 227.6, 36 256 C.P.  
Pub. Rd. Gr. Xing. 12 3 036 10  
761.53

Pub. Rd. Gr. Xing. 03 038 X

Br. 228.4, Dbl. 487x56 C.P.

Pub. Rd. Gr. Xing. 03 037 6  
Br. 228.6, 36 256 C.P.

Br. 229.0, 30 760 C.I.P.

Pub. Rd. Gr. Xing. 03 040 U

Br. 229.5, 30 736 C.I.P.  
Pub. Rd. Gr. Xing. 03 039 T

Pub. Rd. Gr. Xing. 03 040 W  
Br. 229.8, 36 736 C.I.P.  
775.10

0.60 0.04 0.63 0.0 0.46 0.91 0.88 0.80 0.66 0.68 0.16 1.00 0.29 1.25

CURVES

RAIL GR. 89-2 89-5 89-2

RAIL 136 CWR 2-1976

BALLAST SLAG

SURFACING 1990

TIES 1990

UNDERCUTTER UTP 1975

STABILIZATION 1949 '53 '53

Section 21





235

236

237

C.T.C.

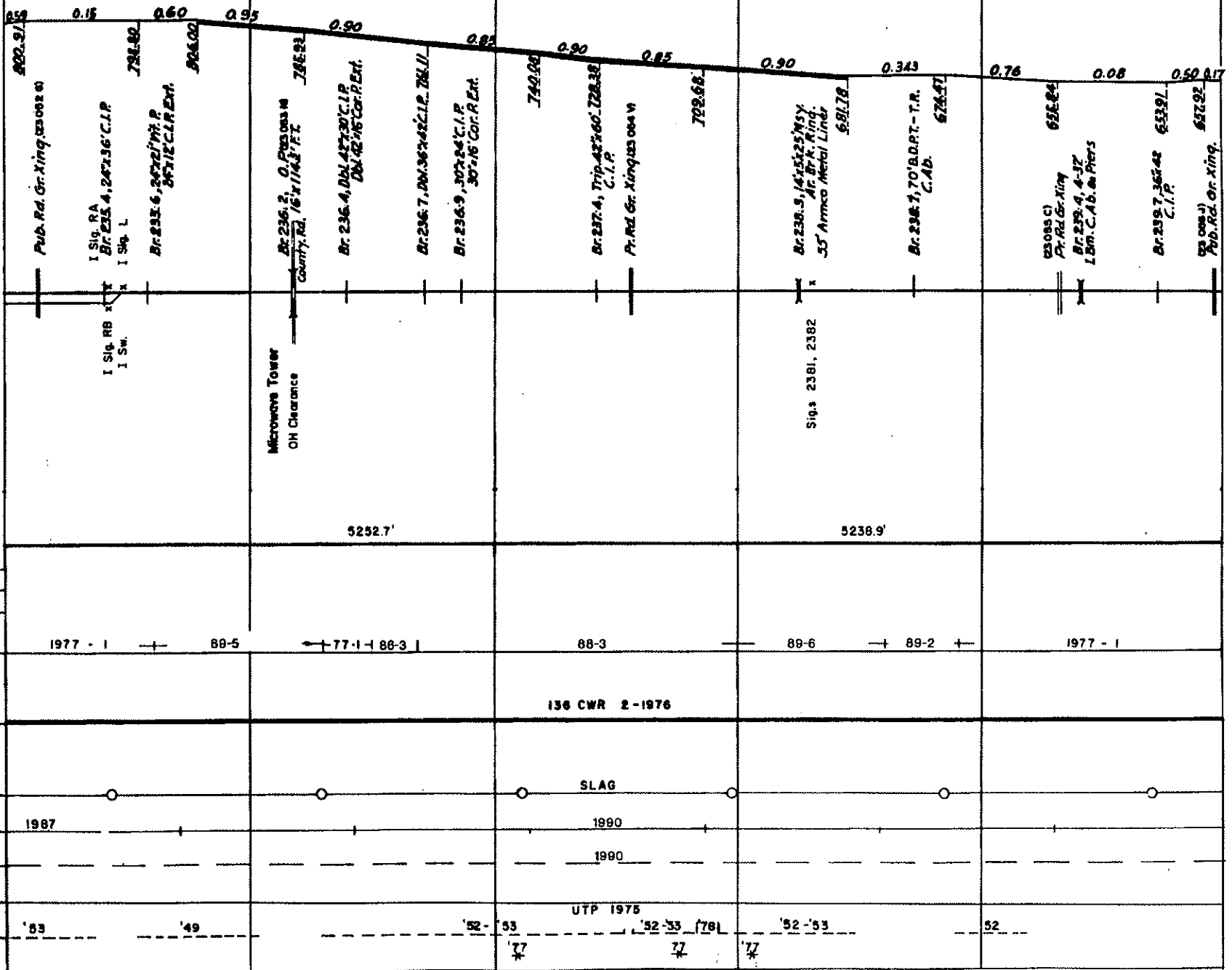
238

239

240

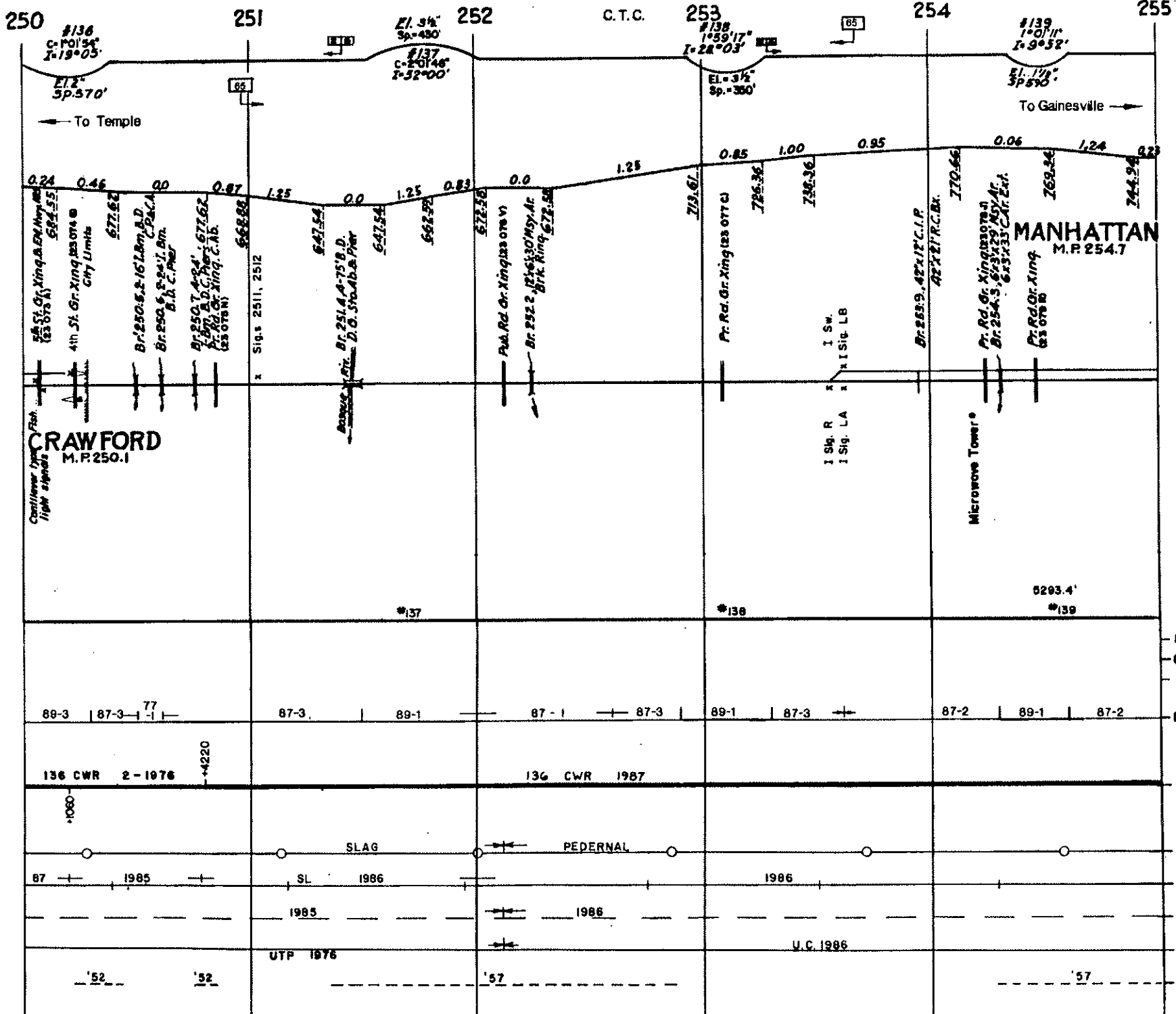
← To Temple

To Gainesville →









255

256

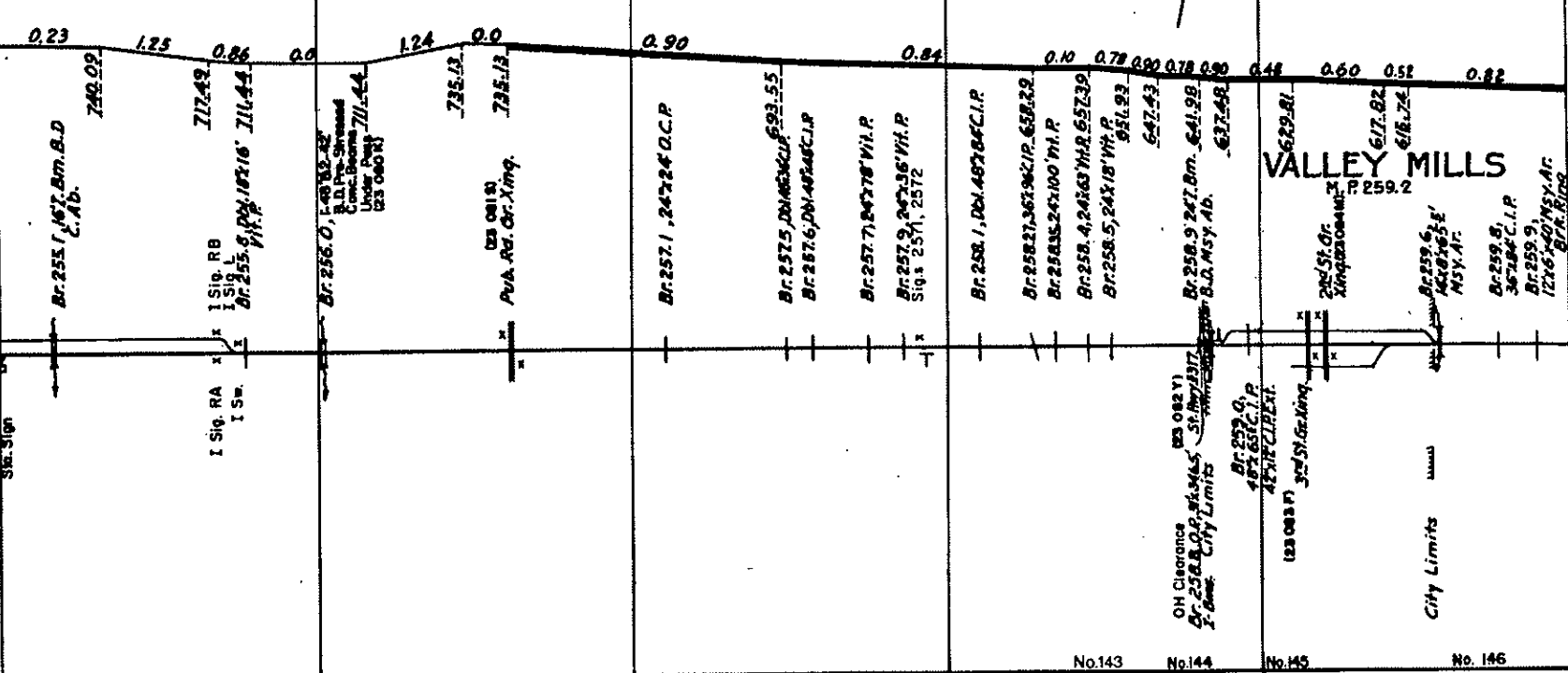
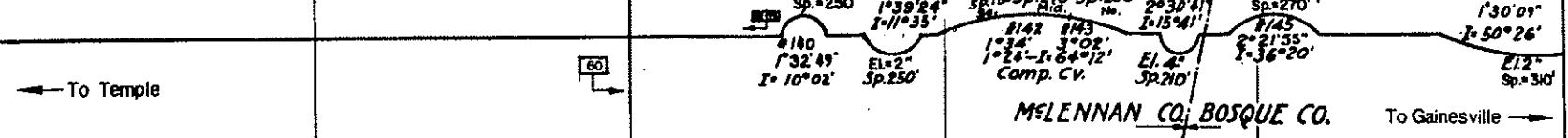
257

258

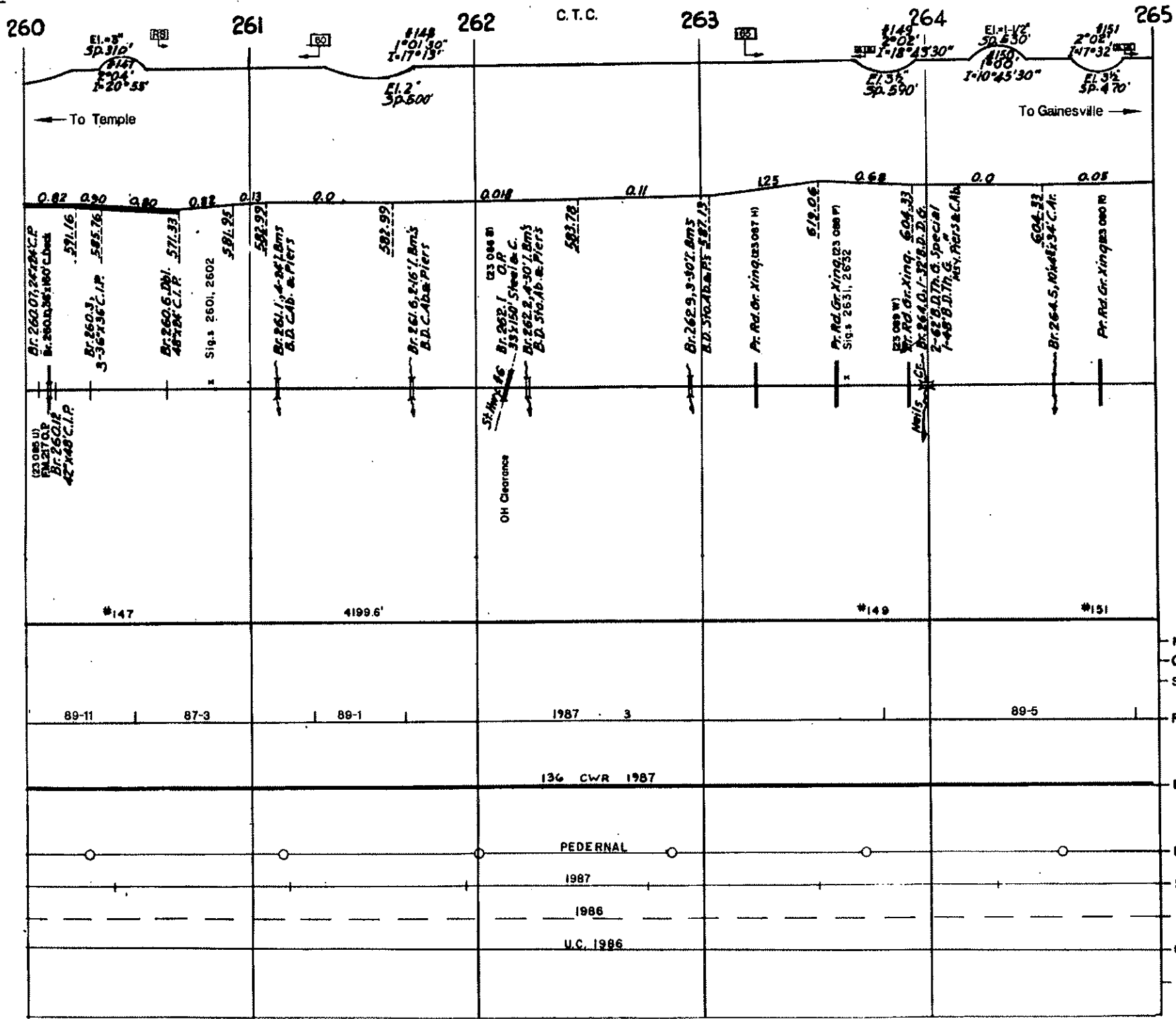
259

260

C.T.C.



NR				
CURVES			136W, CR, 87 136W, CR, 87	
SR				
RAIL GR.	87 - 2	87 - 3	87 - 3	89-11
RAIL			136 CWR 1987	
BALLAST			PEDERNAL	
SURFACING			1986	1987
TIES			1986	
UNDERCUTTER			U.C. 1986	
STABILIZATION	'57	1957	'57	1957



C.T.C.

0.82 0.90 0.80 0.82 0.13 0.0 0.018 0.11 0.25 0.68 0.0 0.05

Br. 260.01, 247.84 C.P.  
R. 260.0, 261.80 C.D. & P.  
- 571.16  
Br. 260.3  
3-36" x 36" C.I.P. - 595.76  
Br. 260.6, 261.1  
46" x 24" C.I.P. - 571.33  
Sig. 260, 2602  
- 591.25  
- 592.99  
Br. 261.1, 4-24" Bms  
B.D. CAB. & Piers  
- 592.99  
Br. 261.6, 2-16" I. Bms  
B.D. CAB. Piers  
- 592.78  
Br. 262.1  
123.048 W  
C.P.  
33' x 150' Steel & C.  
Br. 262.2, 4-30" I. Bms  
B.D. 516" Ab. & Piers  
- 592.78  
Br. 262.9, 3-30" I. Bms  
B.D. 516" Ab. & Piers - 592.78  
Pr. Rd. Gr. Xing. 123.087 W  
- 612.06  
Pr. Rd. Gr. Xing. 123.088 W  
Sig. 2651, 2652  
- 604.33  
Pr. Rd. Gr. Xing. 604.33  
B.D. 40' x 32' B.D. & C.  
7-62" B.D. Th. G. Special  
7-48" B.D. Th. G.  
454' Piers & CAB

#147

4199.6'

#149

#151

NR  
CURVES  
SR  
RAIL GR.  
RAIL  
BALLAST  
SURFACING  
TIES  
UNDERCUTTER  
STABILIZATION

89-11 | 87-3 | 89-1 | 1987 3 | 89-5

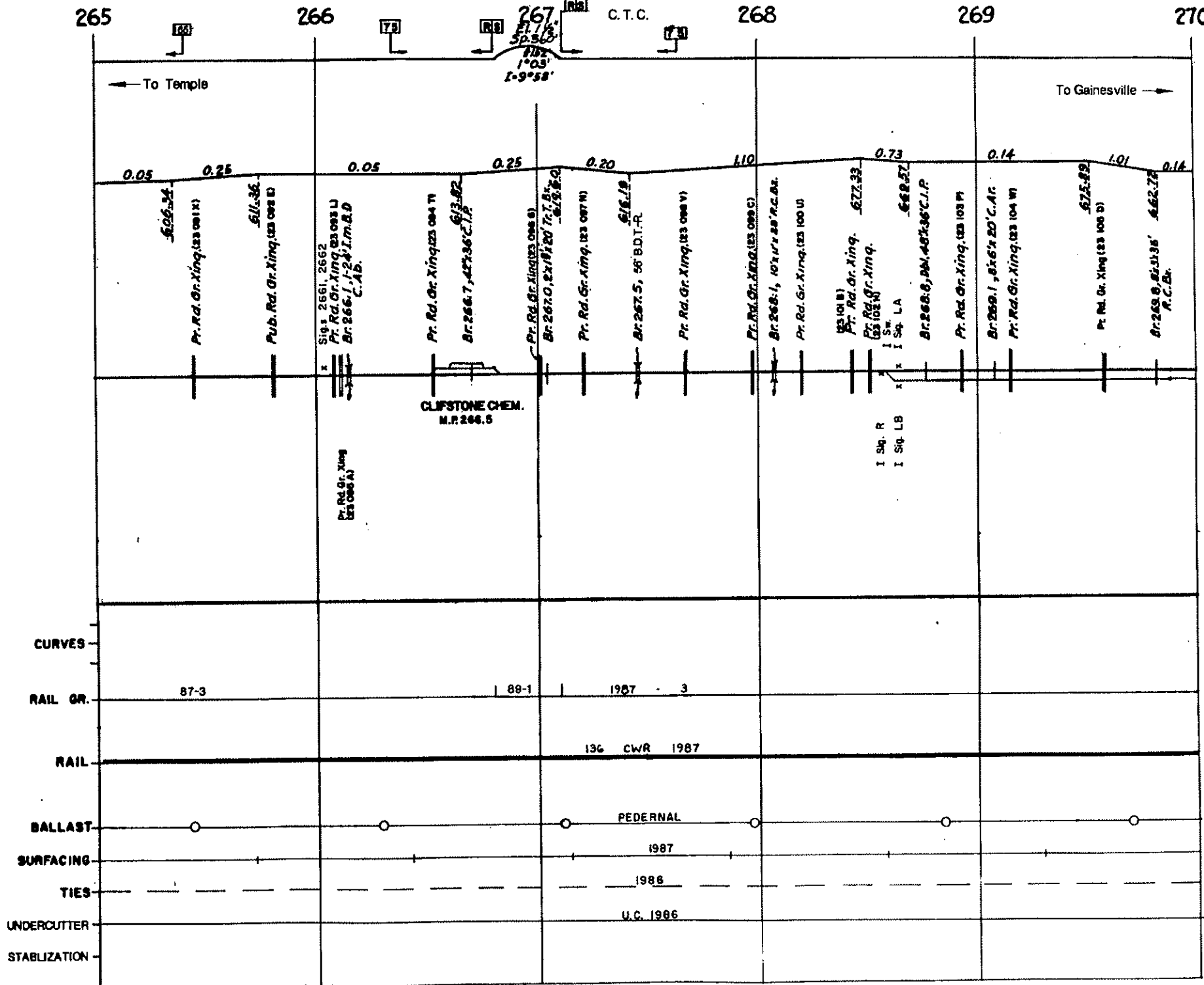
136 CWR 1987

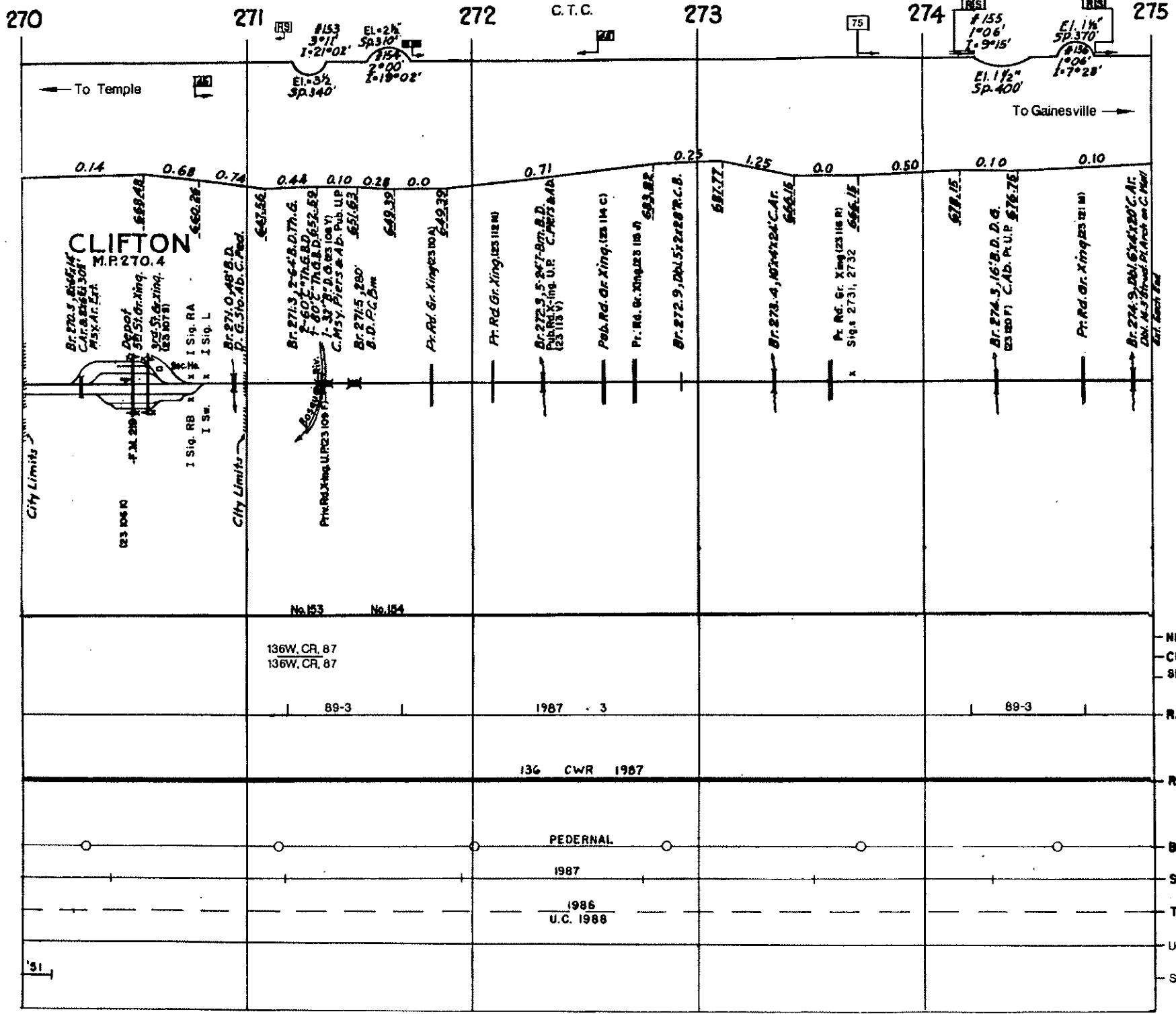
PEDERNAL

1987

1986

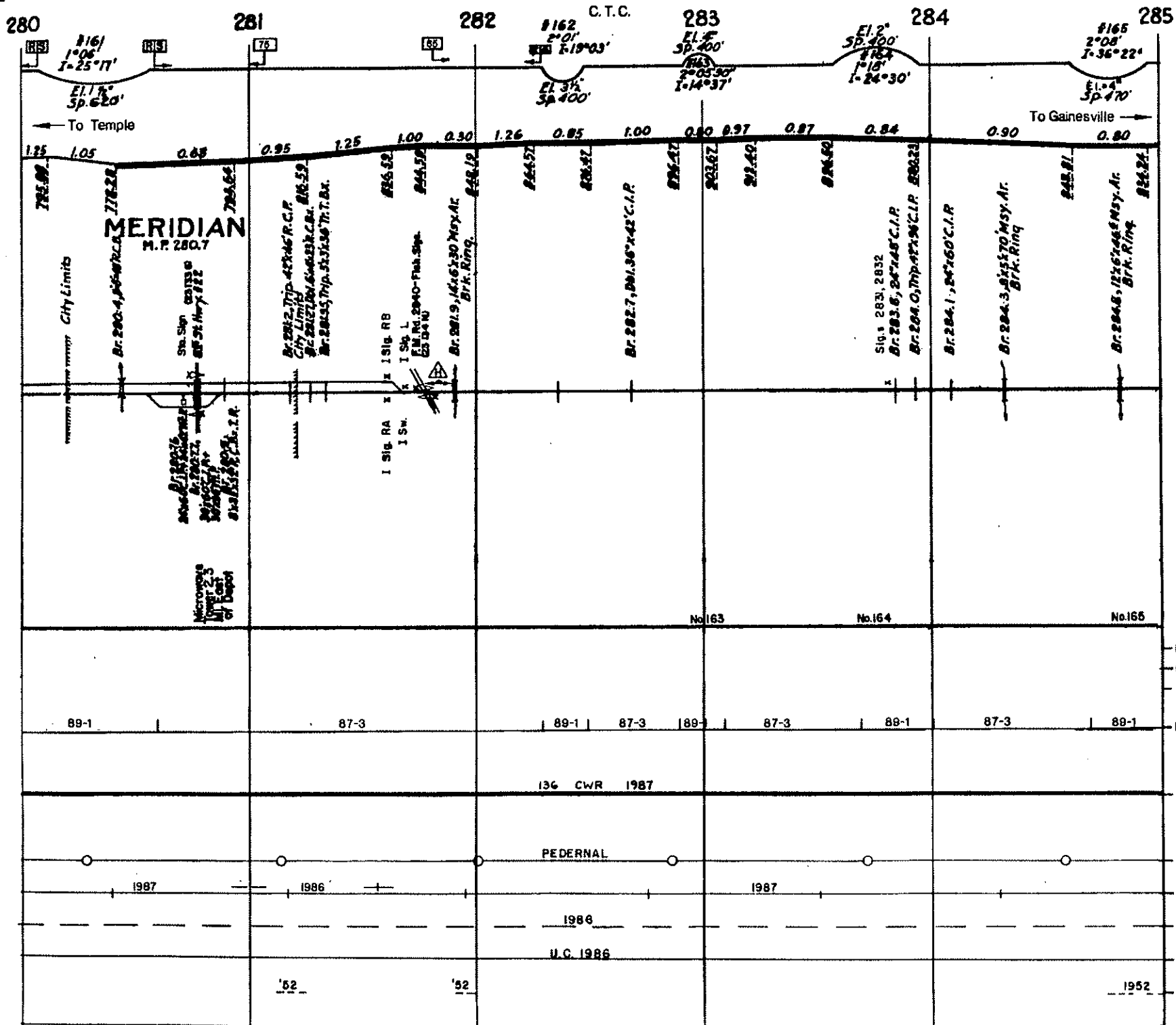
U.C. 1986





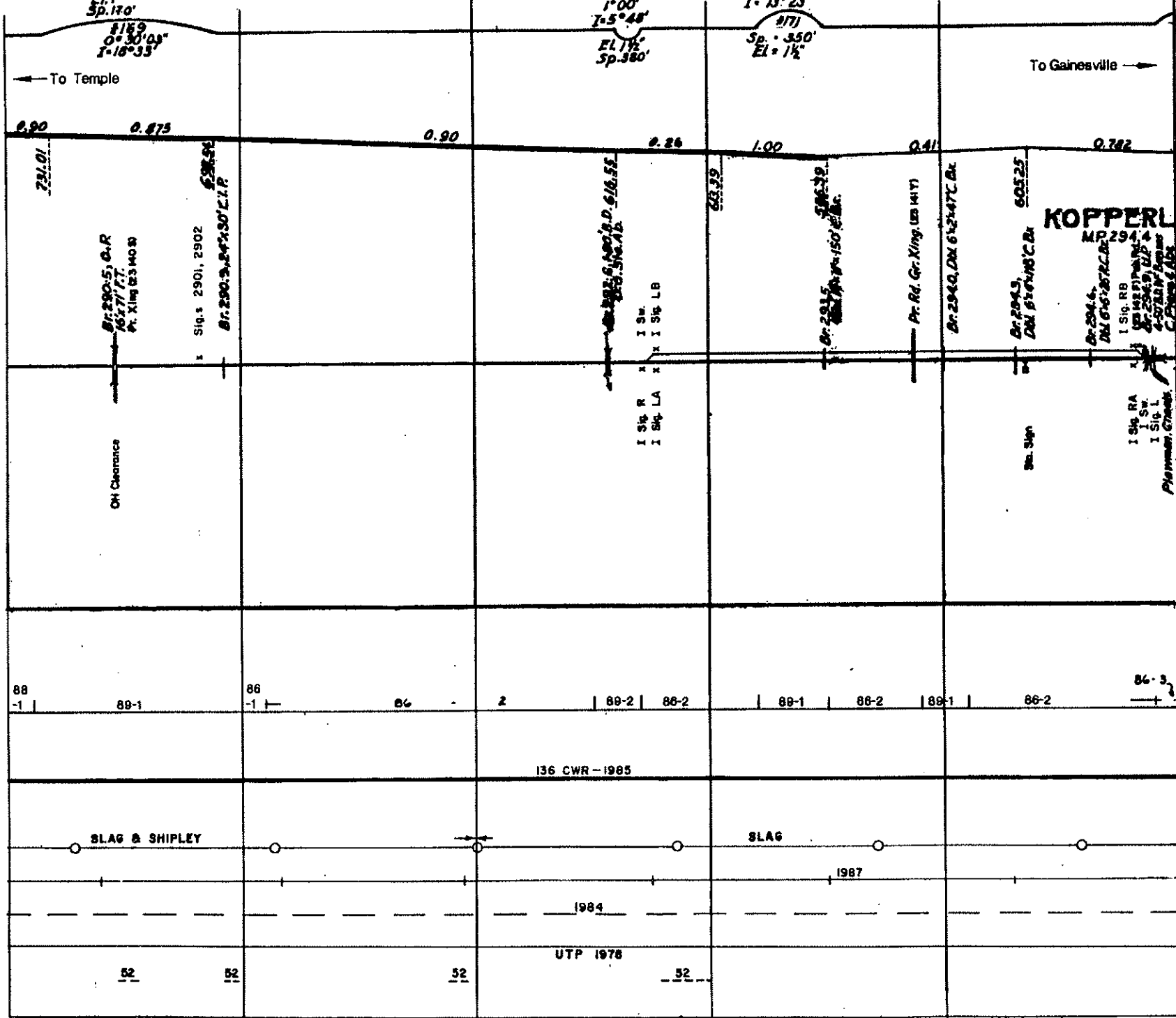


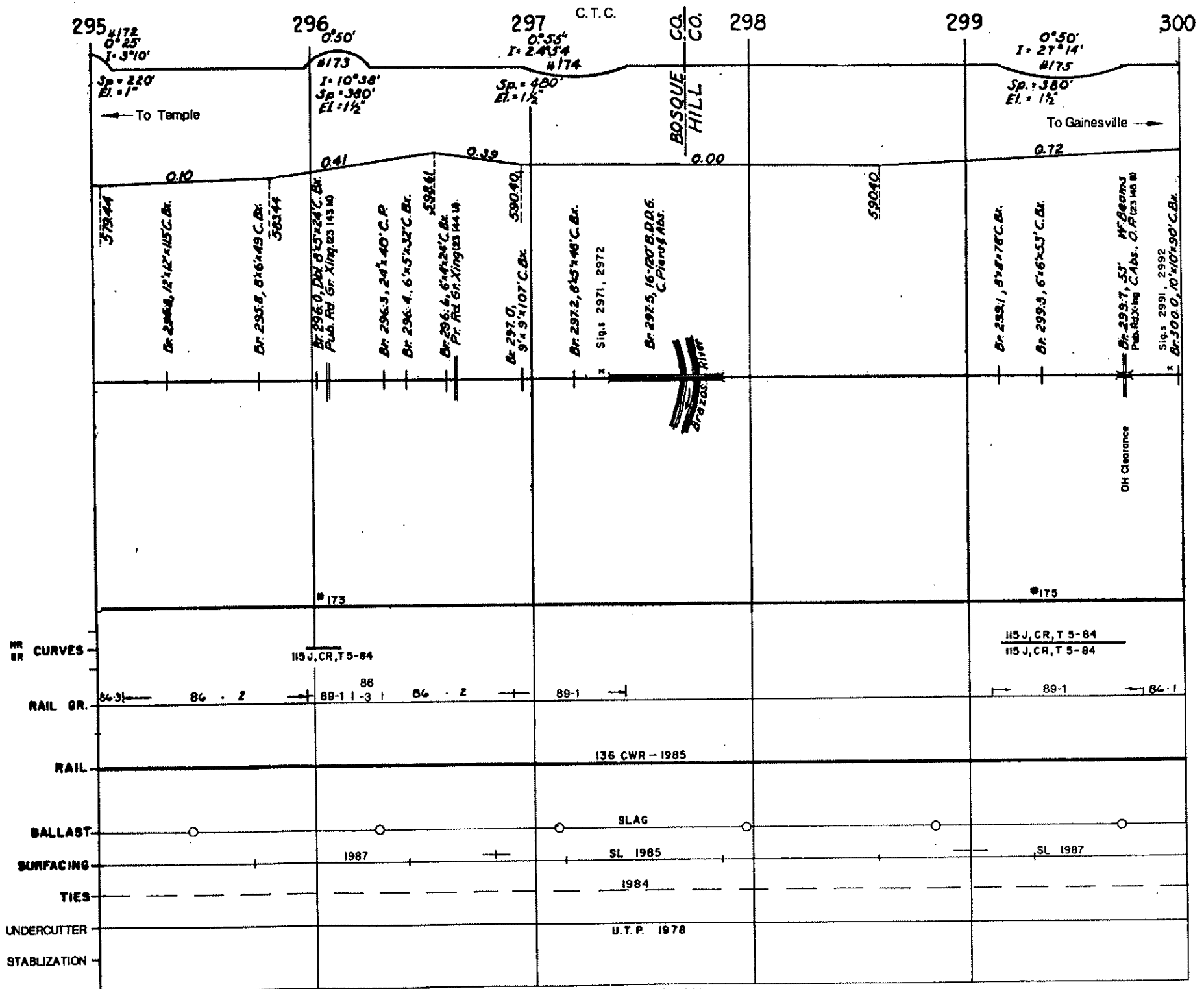




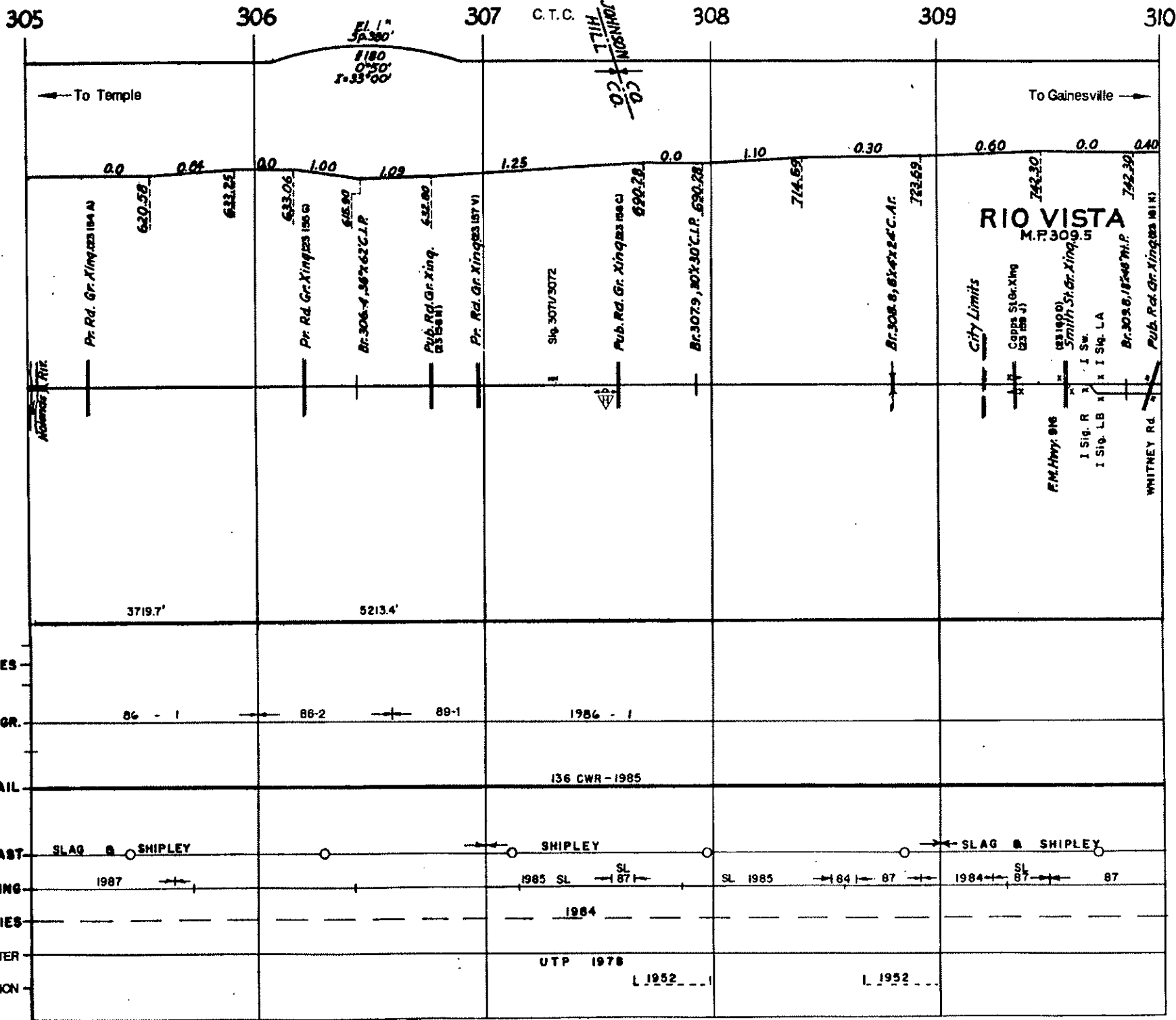


290 291 C.T.C. 292 293 294 295









310

311

312

C.T.C.

313

314

315

NO CURVES

← To Temple

To Gainesville →

0.40 0.80 0.0 0.70 0.40 0.80 0.25 0.90 0.84 0.32 0.0 1.00 0.20 0.0 1.00 0.882 0.859 0.92 0.74

741.37 741.00 741.00 741.31 741.38 743.10 749.57 770.12 781.44 741.83 754.84 759.84 769.84 761.11 767.80 763.82 763.64

City Limits

Br. 310-4, 48°14' C.I.P. 46 X 16' C.P. Ext.

Br. 311-1, 104.48°30' C.I.P. 741.38

Br. 311-7, 101.16°48' C.I.P.

Br. 313-0, 116°17' 00" B.D. 741.83  
Pub. Rd. Gr. Xing (as 162 ft)

Pub. Rd. Gr. Xing (as 103 ft)

Br. 314-3, 77°10' 45" 24' C.I.P. 48°16' C.P. Ext.

Br. 315-0, 102°12' 15" C.I.P. on Sta. 44

I Sig. RB x I Sig. RA  
I Sig. L x I Sig. L

I Sig. RB x I Sig. RA  
I Sw. x I Sig. L

County Rd. #1205

I Sig. R x I Sig. L

Y. L. & S. L. 31500

CURVES

RAIL GR.

RAIL

BALLAST

SURFACING

TIES

UNDERCUTTER

STABILIZATION

1986

136 CWR-1985

SLAG & SHIPLEY

SLAG

SLAG & SHIPLEY

1987

SL 1985 87 SL 85

1987

1984

U.T.P. 1978

1952

1952

1952

1952



315

316

140

317

C.T.C.

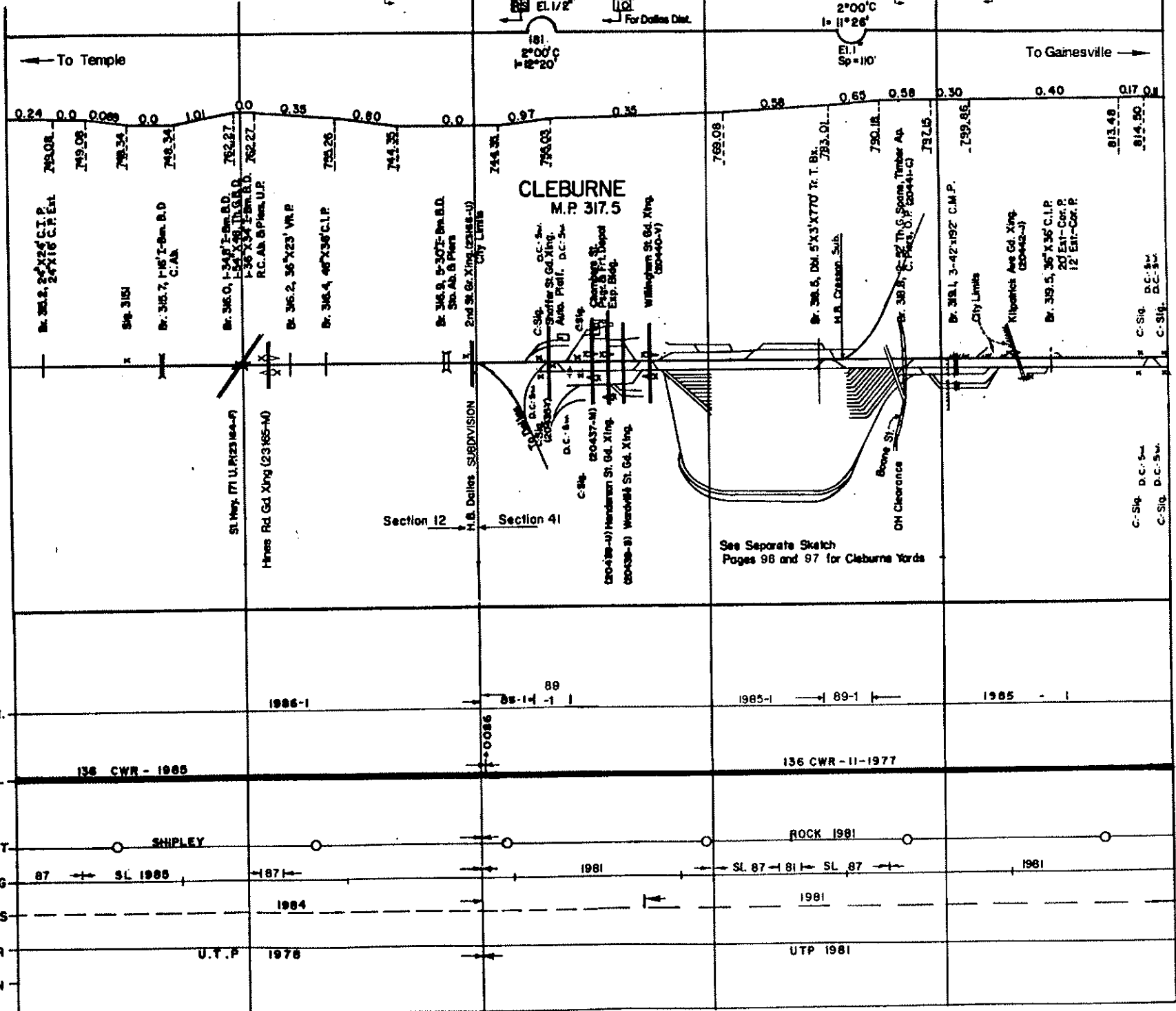
318

150

319

140

320



REVISED 3-1991





320

321

322

323

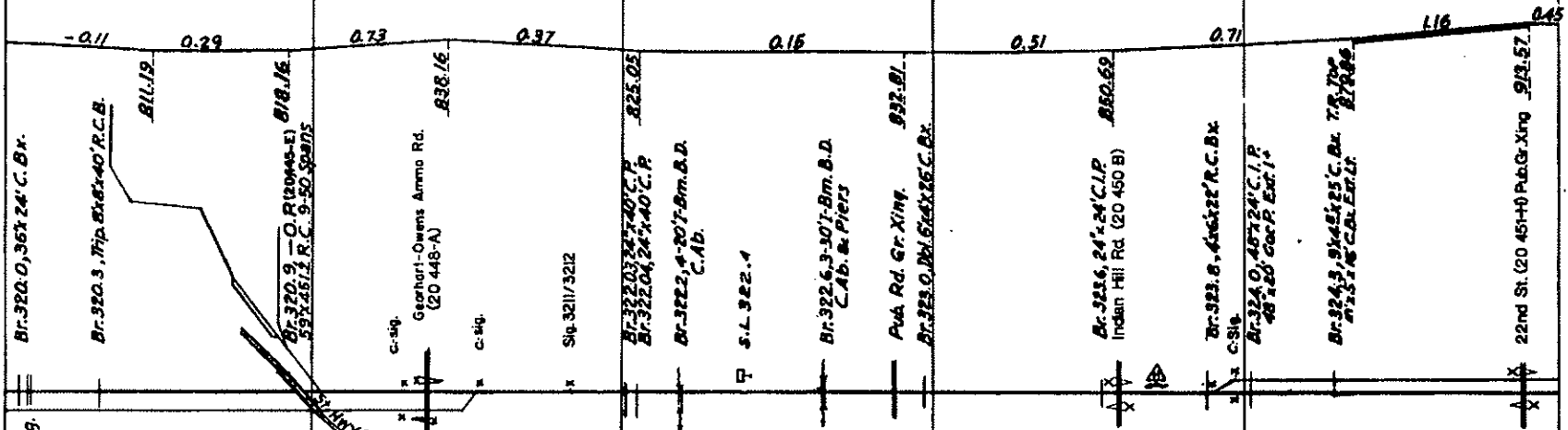
324

325

C.T.C.  
NO CURVES

To Temple ←

To Gainesville →



325

# 183  
0°31'  
1°4'01'

326

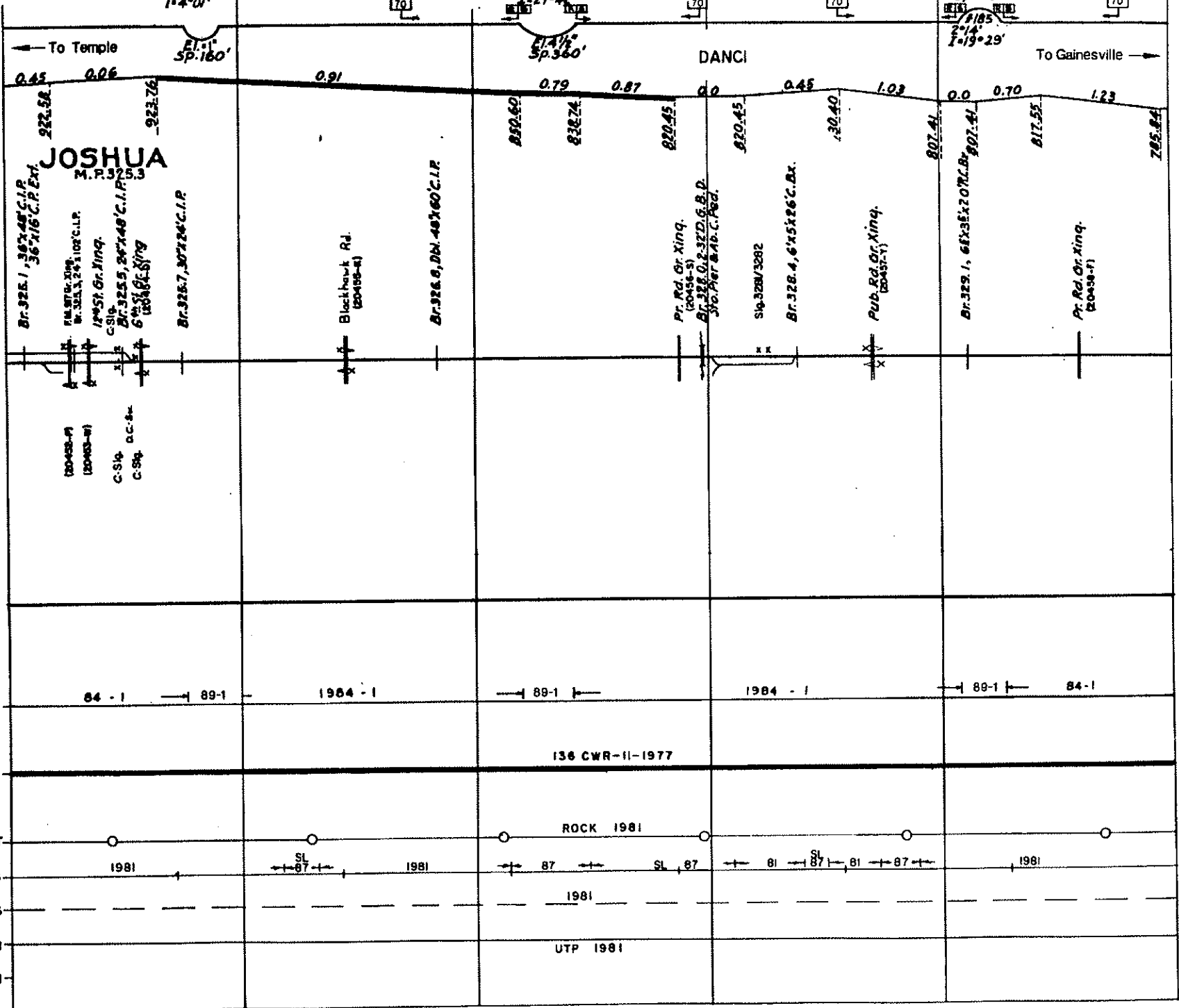
327

C.T.C.

328

329

330



330

331

332

333

334

335

C.T.C.

JOHNSON CO.  
TARRANT CO.

Fl. 1" = 50' 160'  
1188  
0° 30' 30"  
7.8° 14'

← To Temple

To Gainesville →

0.125

0.155

0.52

0.50

0.54

0.47

0.17

0.60

1.10

0.0

1.16

0.25

Br. 330, O. 237, B.D.D. G. U.R.  
Sta. Plat. & Ab.

Br. 330, O. 237, B.D.D. G. U.R.  
Sta. Plat. & Ab.

Br. 330, O. 237, B.D.D. G. U.R.  
Sta. Plat. & Ab.

Pub. Rd. Gr. Xing.  
(20440-4)

Br. 331, 9, 30-T, Bm. B.D.C. Ab.  
Pub. Rd. Gr. Xing. City Limits

Pr. Rd. Gr. Xing.  
(20442-4)

Br. 332, 5, 48-T, D.D. G.  
Sta. Ab.

Br. 332, 9, 51-T, 40 R.C. Bx  
(Magnolia St.)

Pub. Rd. Gr. Xing. (Magnolia St.)  
Br. 333, 1-24, 40 C.I.P.  
24' x 16' C.P. Ext.

**CROWLEY**  
M.P. 333.7

Br. 333, 4, 24-T, 40 R.C. Bx  
At. Bm. R.R. 24' x 16' C.P. Ext.

Pub. Rd. Gr. Xing.  
F.M. 1187

Pub. Rd. Gr. Xing.  
(Hunting Rd.)

Br. 334, 3, 45-T, D.D. G.  
M.Sy. Ab.

Br. 335, 0, 24-T, 26' C.P.

Co. Rd. 1016  
(20448-N)

C-519  
D.C.-SW  
C-519

(20464-J)  
(20460-R)

C-519  
D.C.-SW

Section 41 ← → Section 43

89-1

1984 -1

136 CWR - 11-1977

ROCK 1981

1981

1981

1981

1981

UTP 1981

RAIL GR.

RAIL

BALLAST

SURFACING

TIES

UNDERCUTTER

STABILIZATION

555

550

551

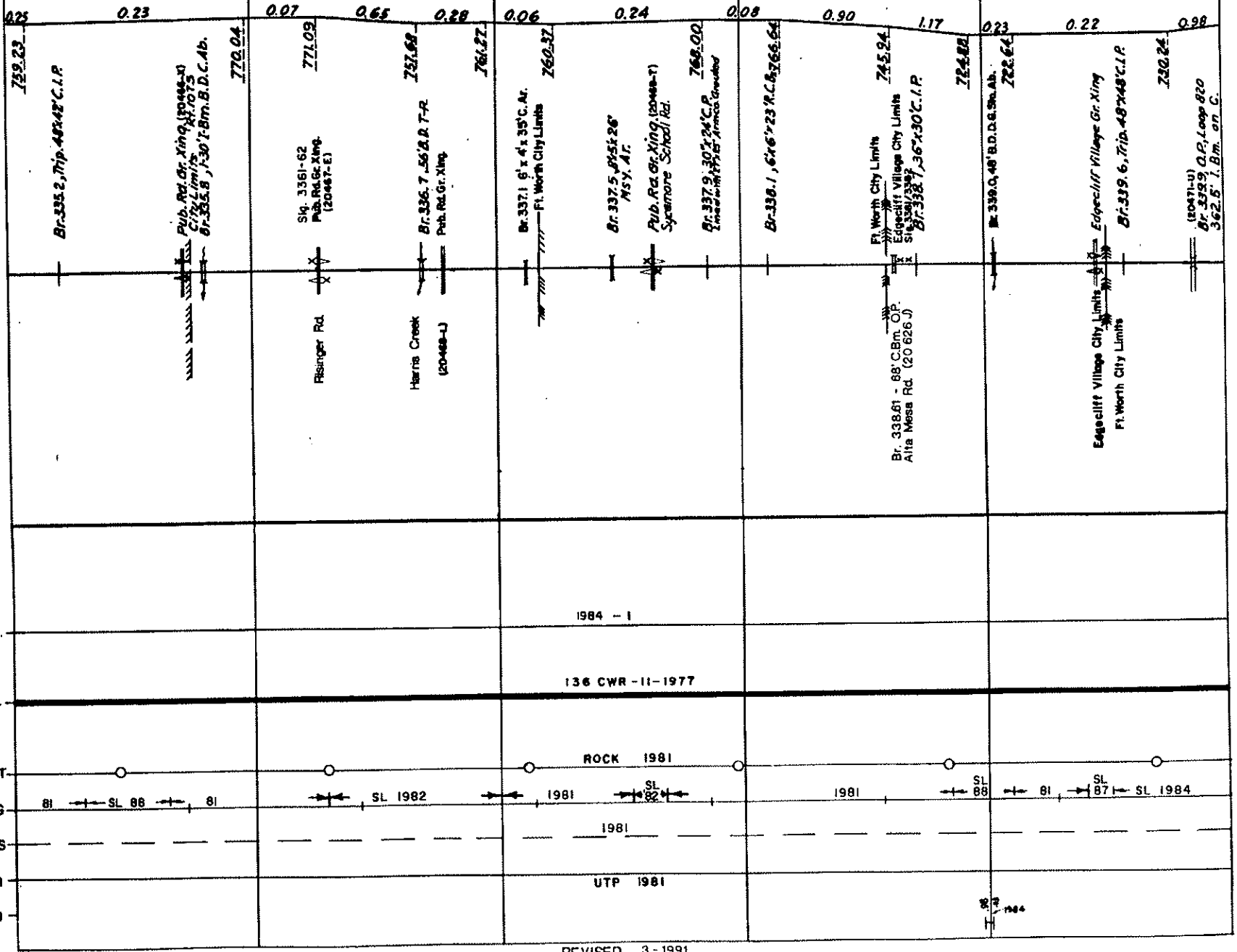
550

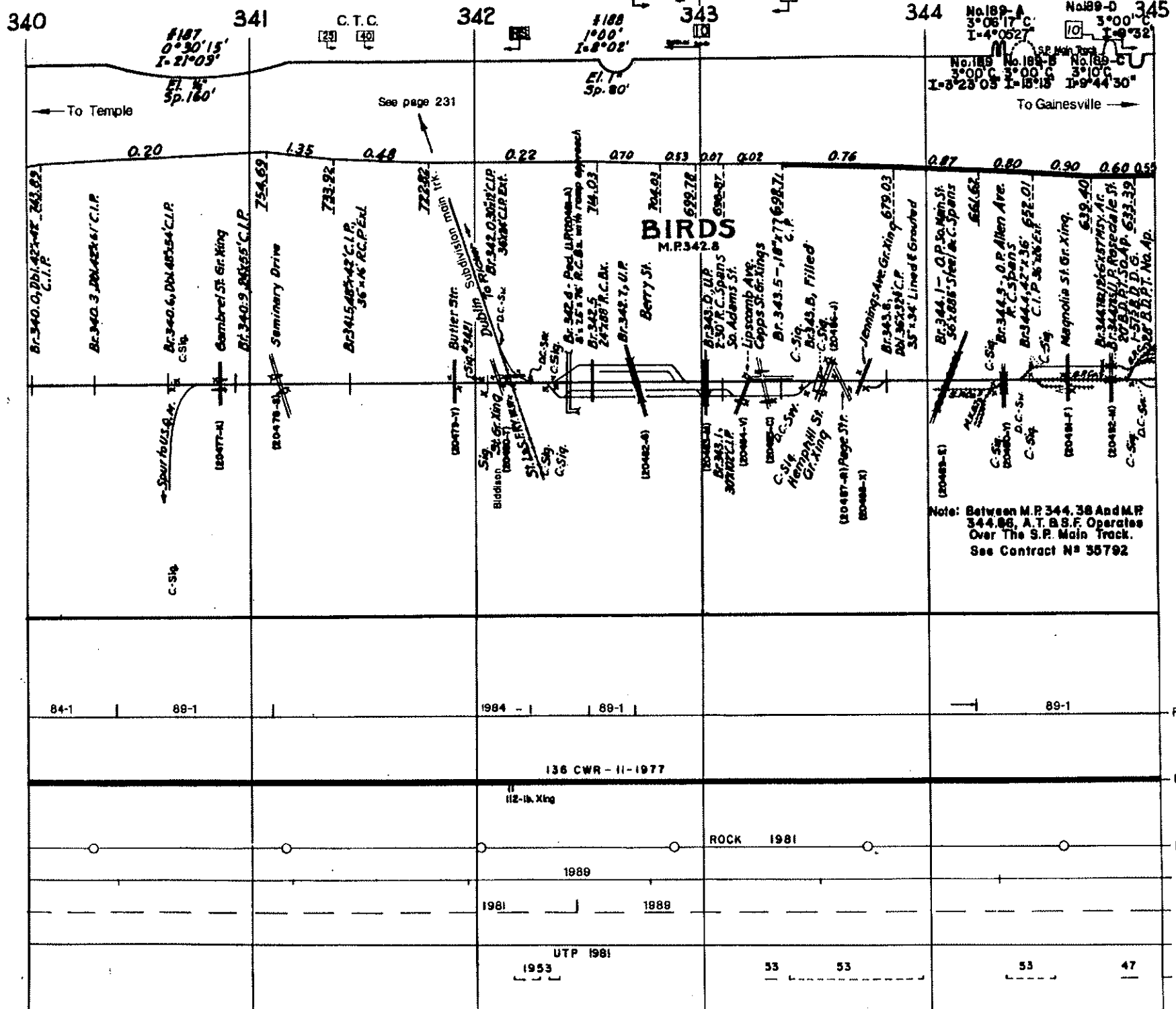
550

540

← To Temple

To Gainesville →





C.T.C.

**BIRDS**  
M.P. 342.8

Note: Between M.P. 344.38 And M.P. 344.88, A.T.B.S.F. Operates Over The S.P. Main Track. See Contract N# 55792







350

351

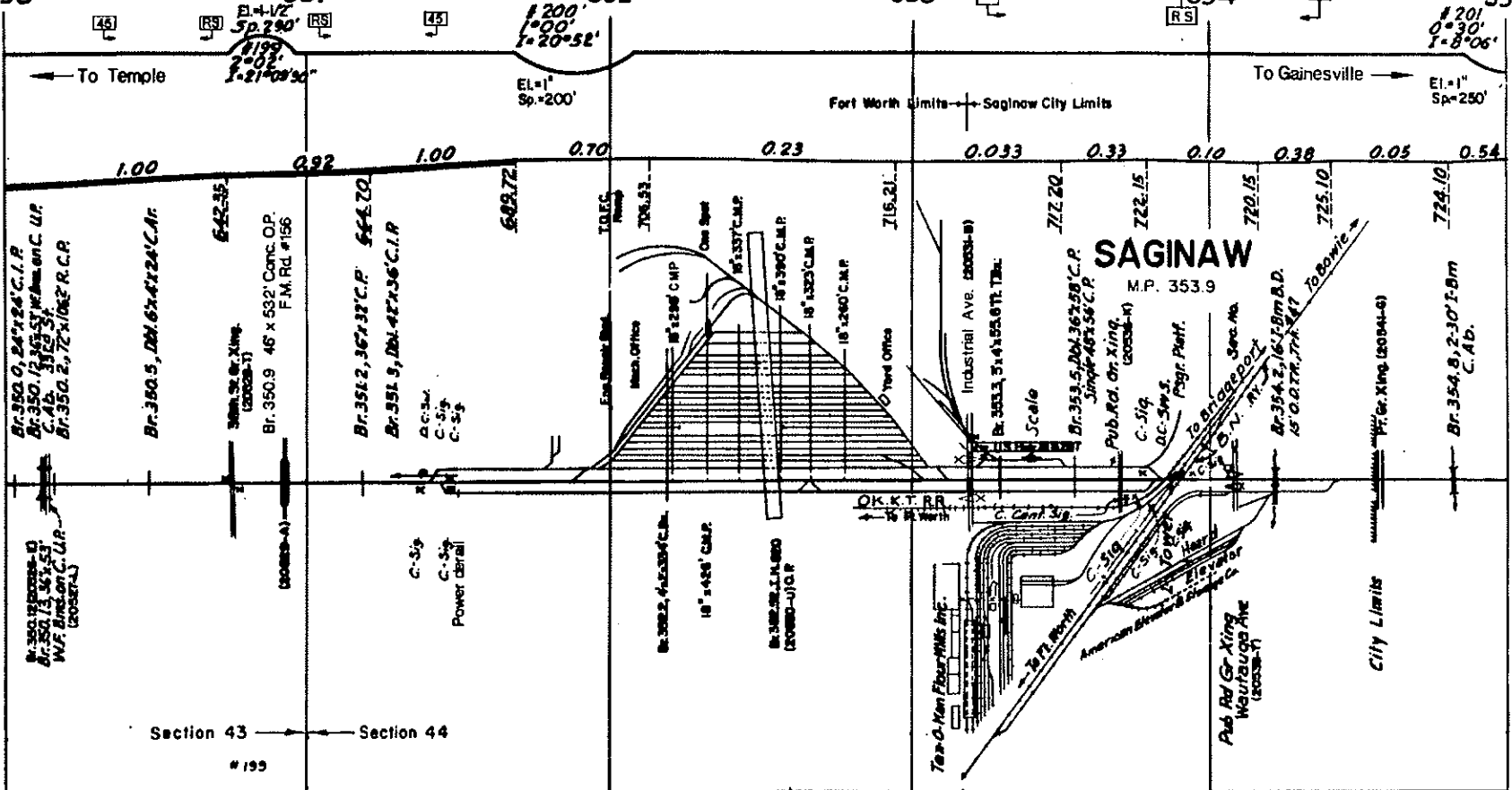
352

C.T.C.

353

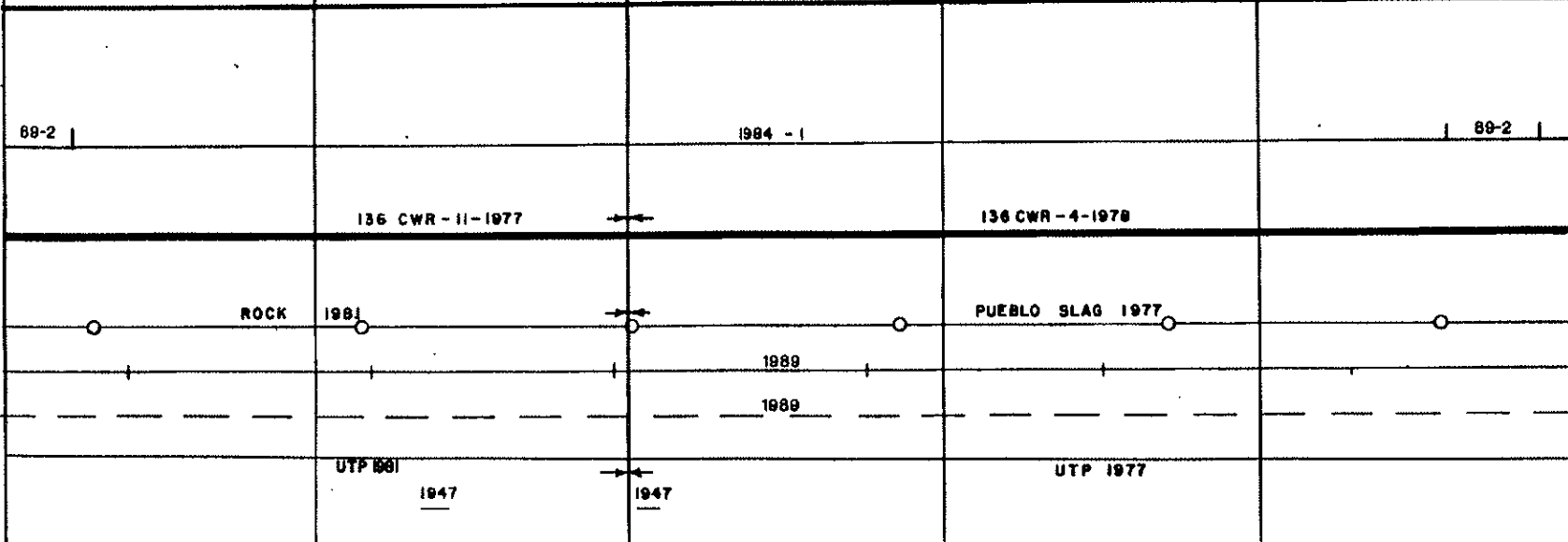
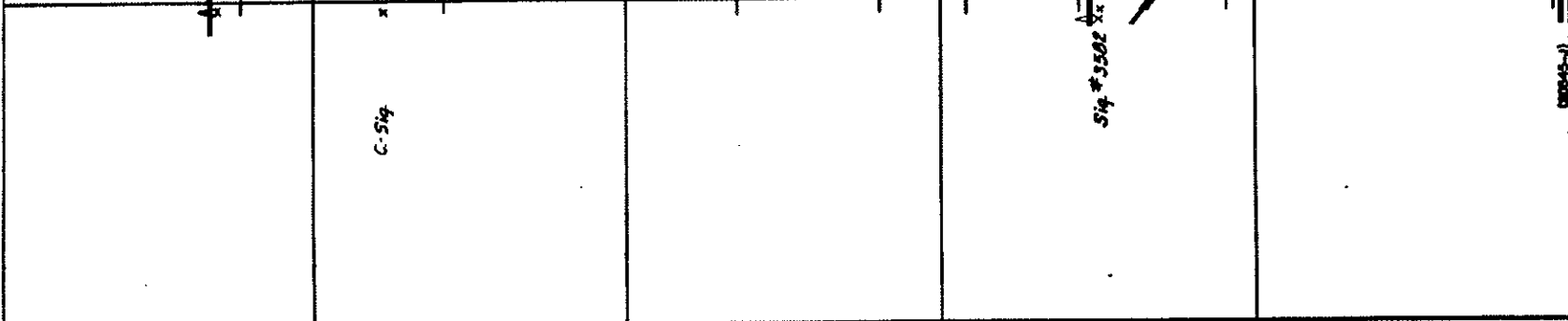
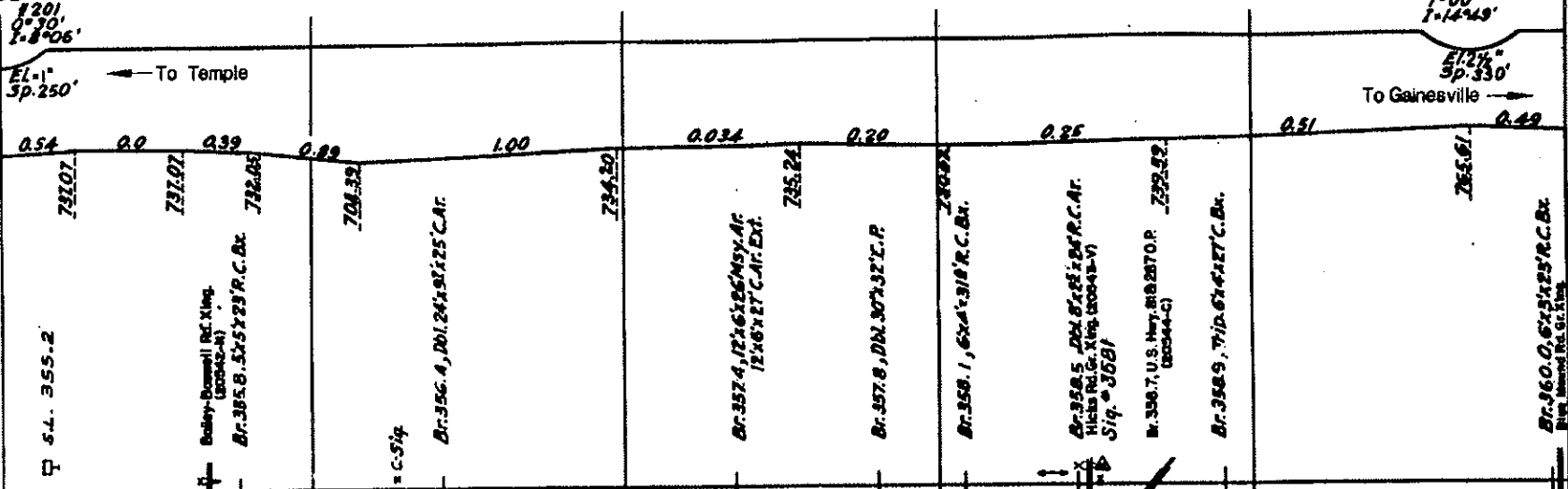
354

355



CURVES	136 W.C.M.T 136 W.C.M.T	6-87 6-87								
RAIL GR.	1984-1	89-2	86-3	86-1	89-3		88-2	78-1	1984-1	89-2
RAIL						Siding - 115 W. 1981				
BALLAST						136 CWR - 11-1977				
SURFACING								1989		
TIES								1989		
UNDERCUTTER								UTP 1981		
STABILIZATION									1947	

355 356 357 C.T.C. 358 359 360



RAIL GR.  
 RAIL  
 BALLAST  
 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION

360

361

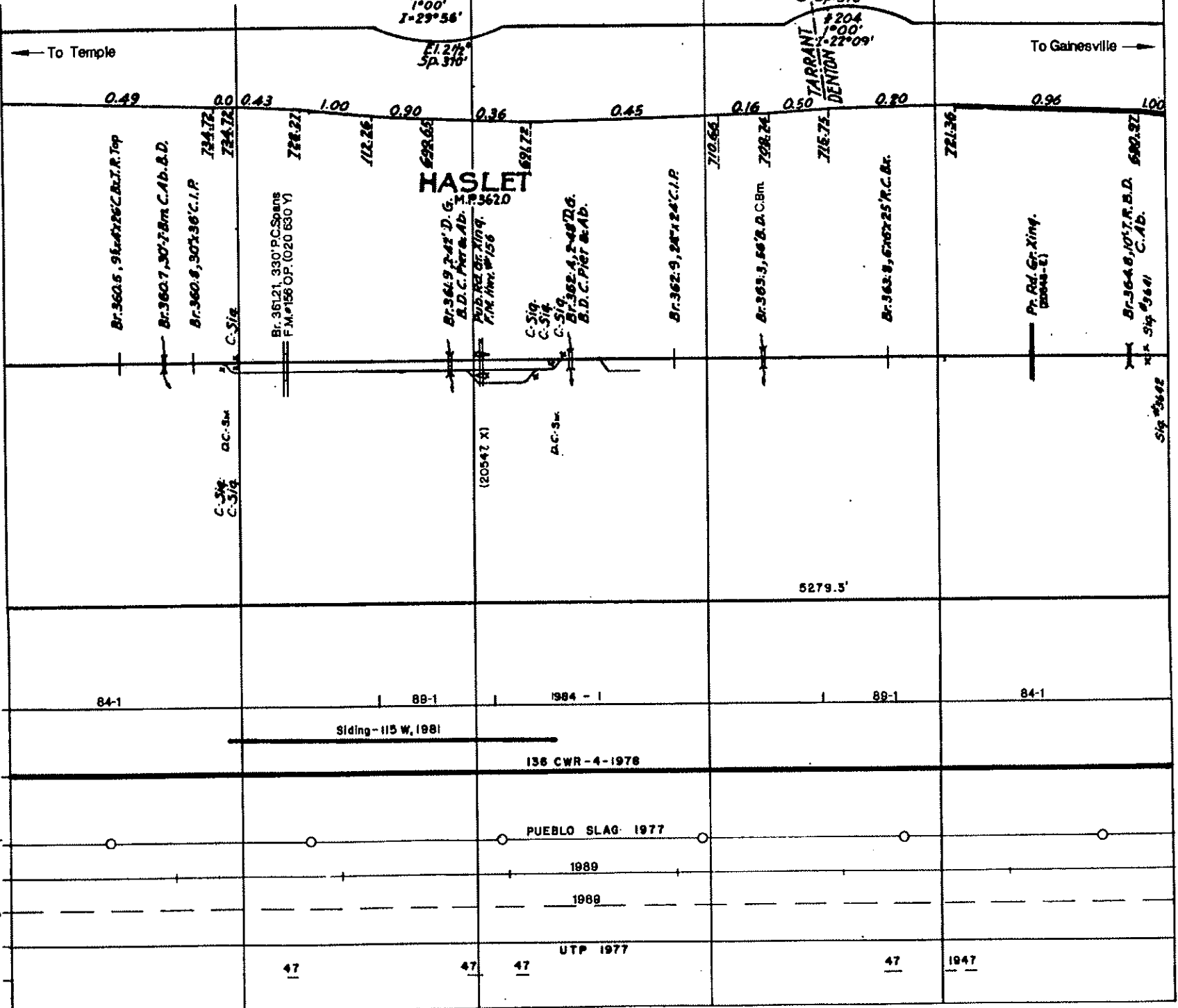
362

C.T.C.

363

364

365



365

366  
E121/2°  
Sp. 370'  
#205  
1°00'  
1-10°45'

367

1206  
0°30'  
1-4°14'  
E1. = 1/2"  
Sp = 250'

C.T.C.

368

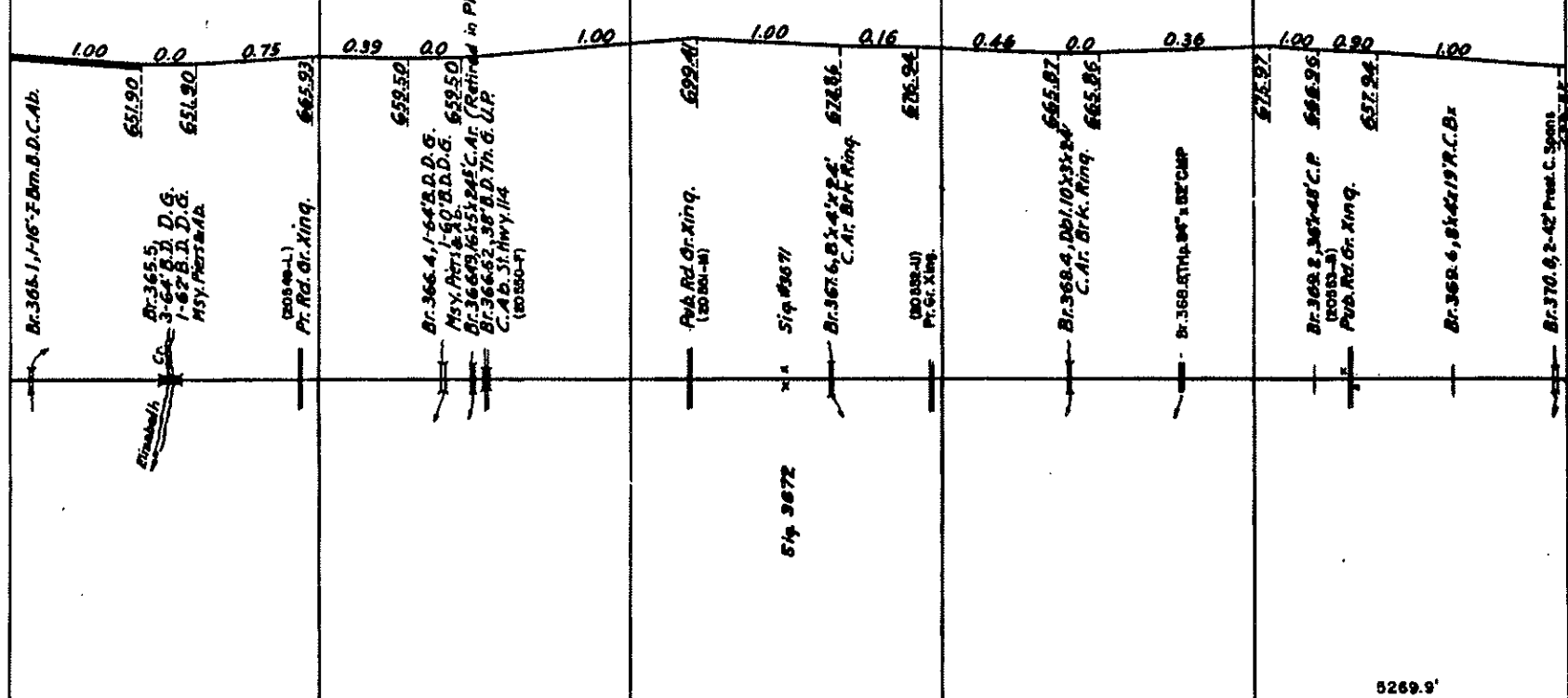
369

E121 1/2°  
Sp. 370'  
#201  
1°00'  
1-19°34'

370

← To Temple

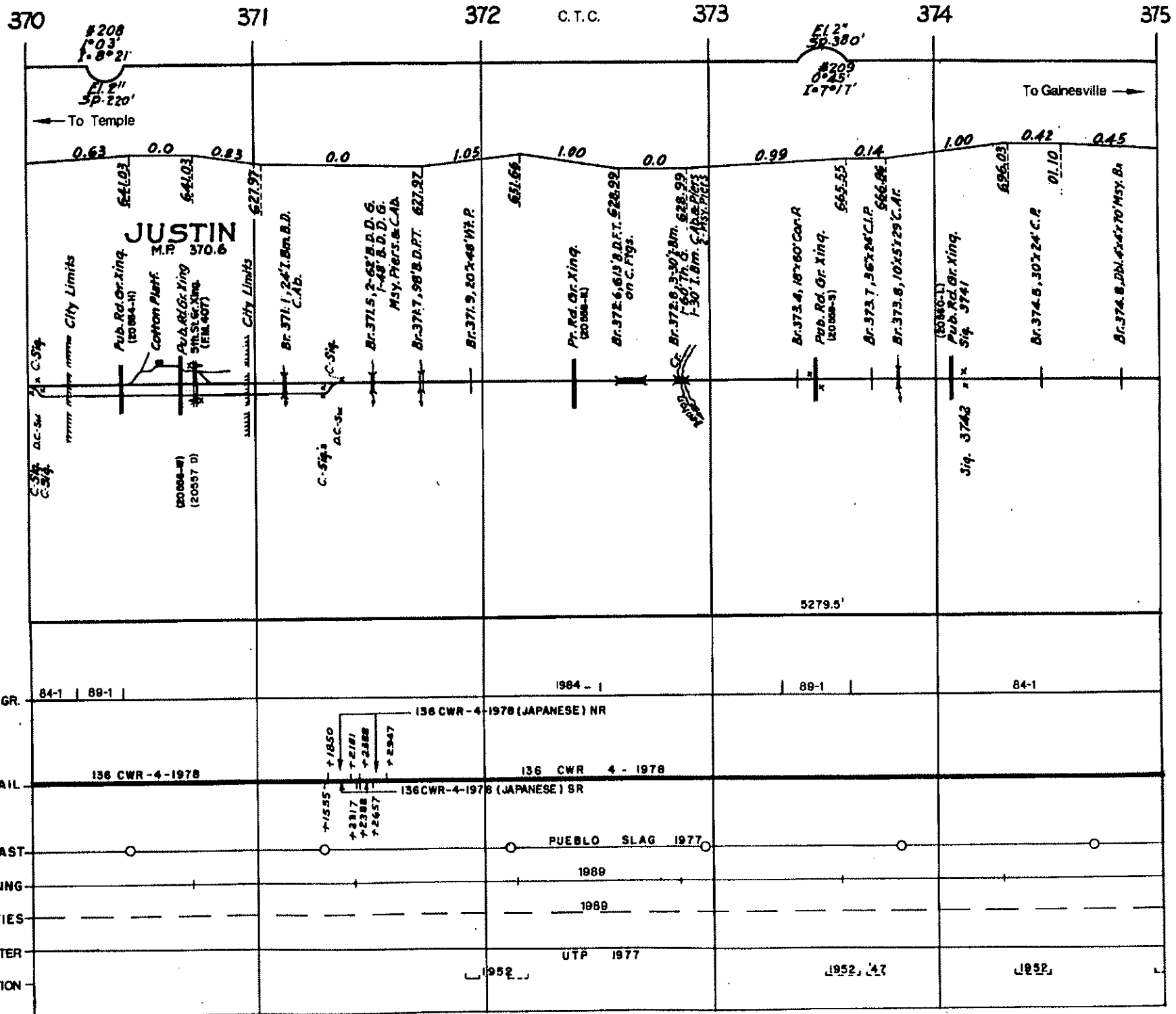
To Gainesville →

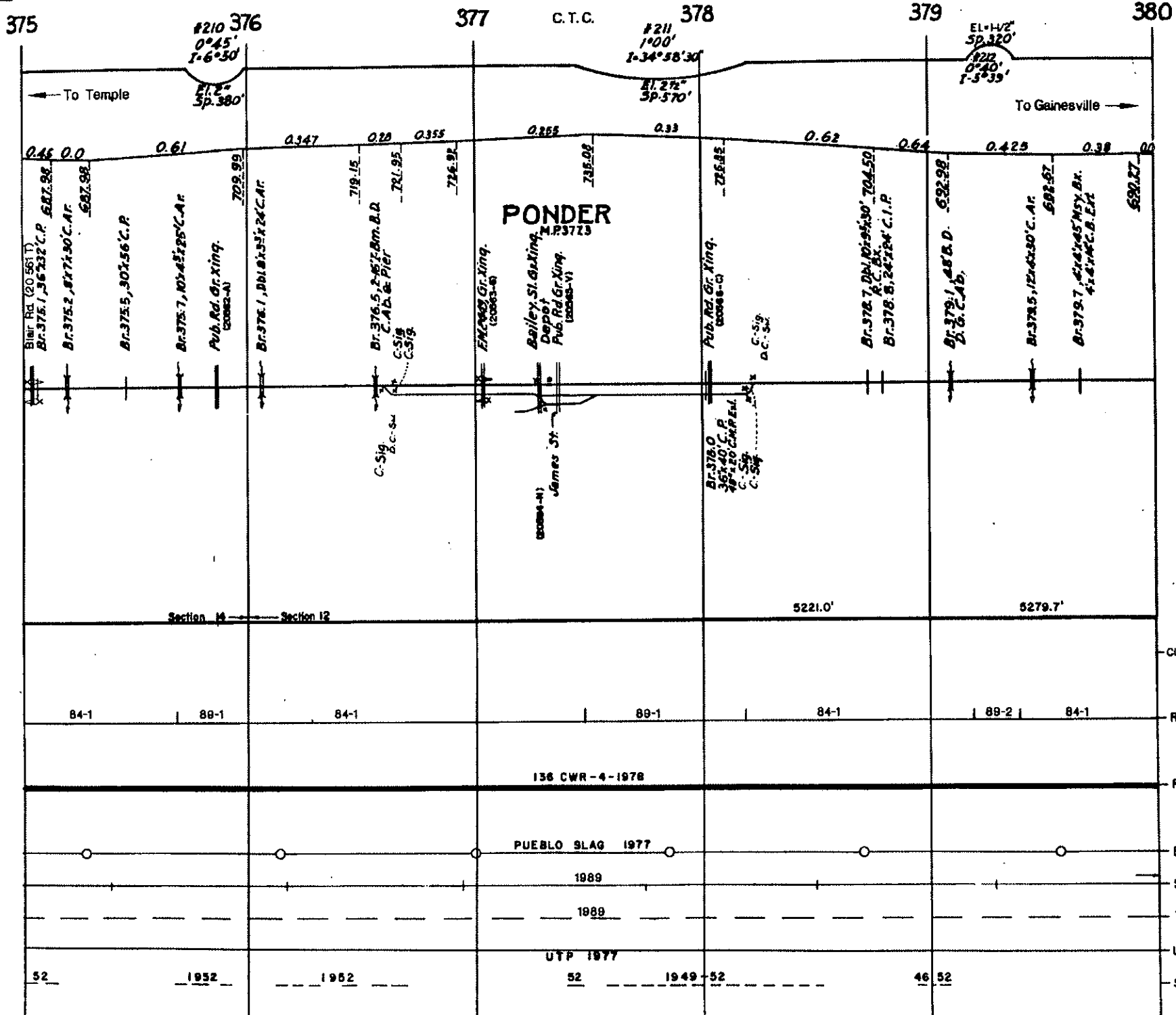


5269.9'

84-1	89-2	89-2	1984 - 1	89-2	84-1
136 CWR - 4 - 1978					
PUEBLO SLAG 1977					
UTP 1977					
1947	47	1947	1947	1947	

RAIL GR.  
 RAIL  
 BALLAST  
 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION







380

381

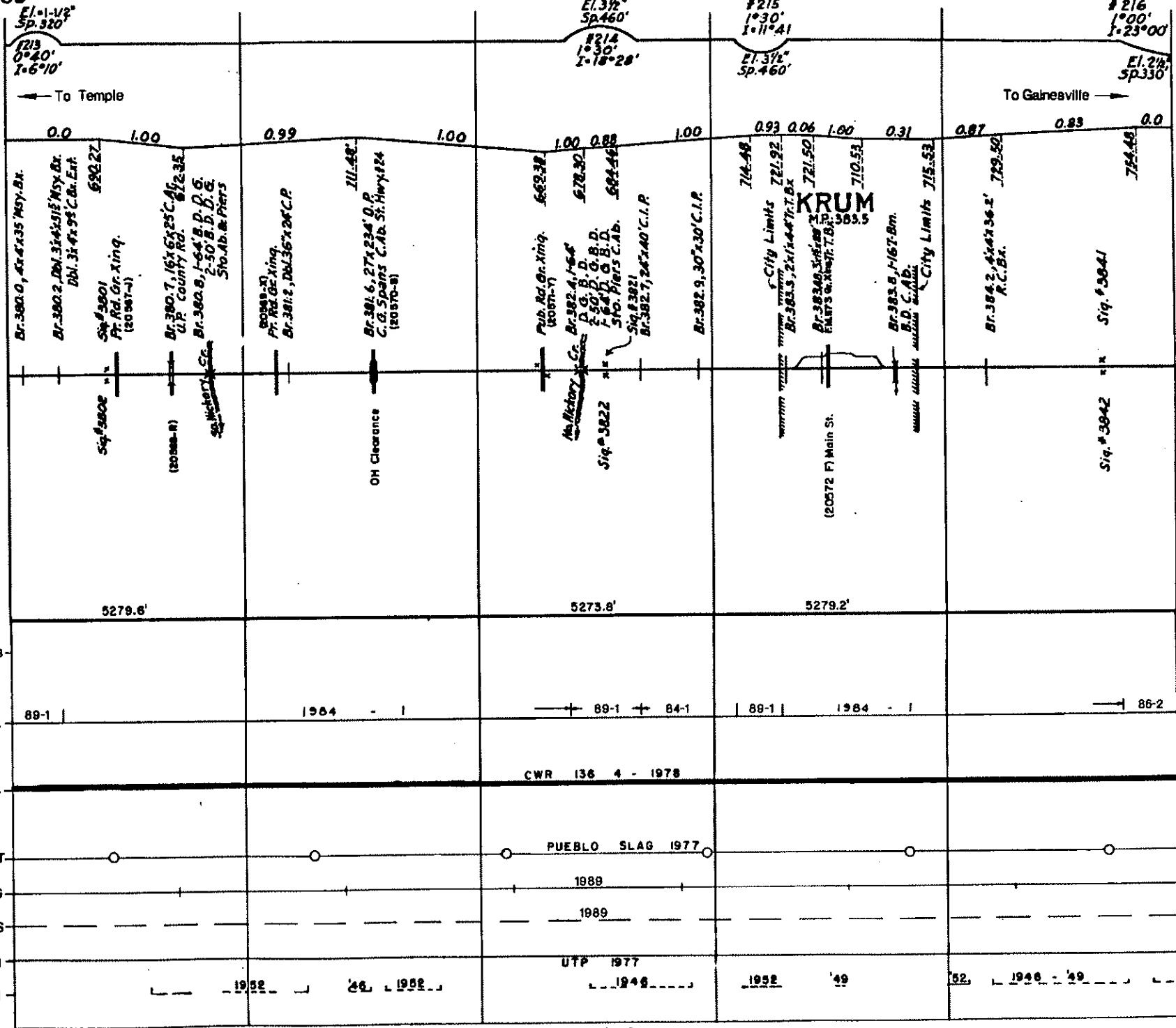
382

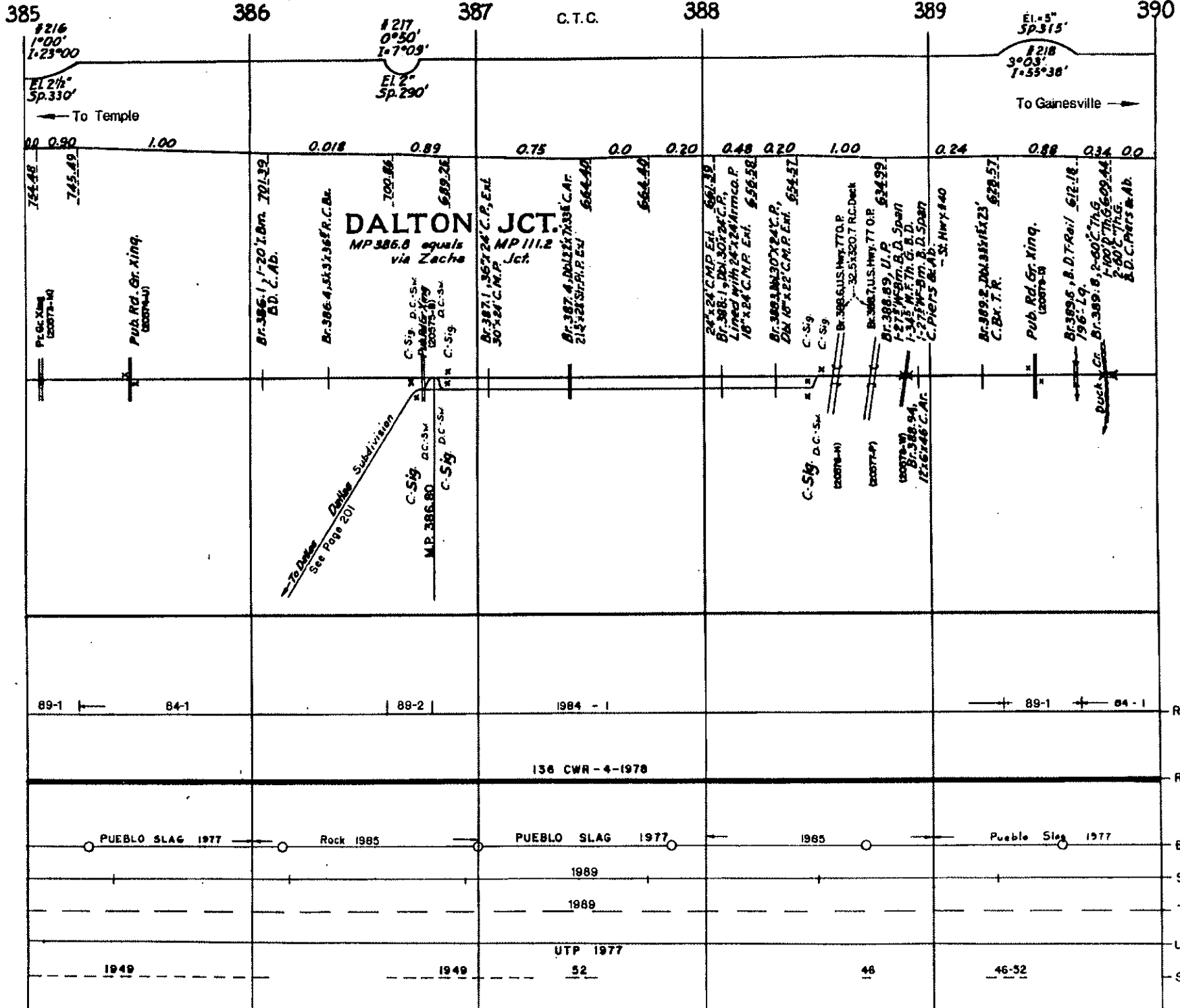
383

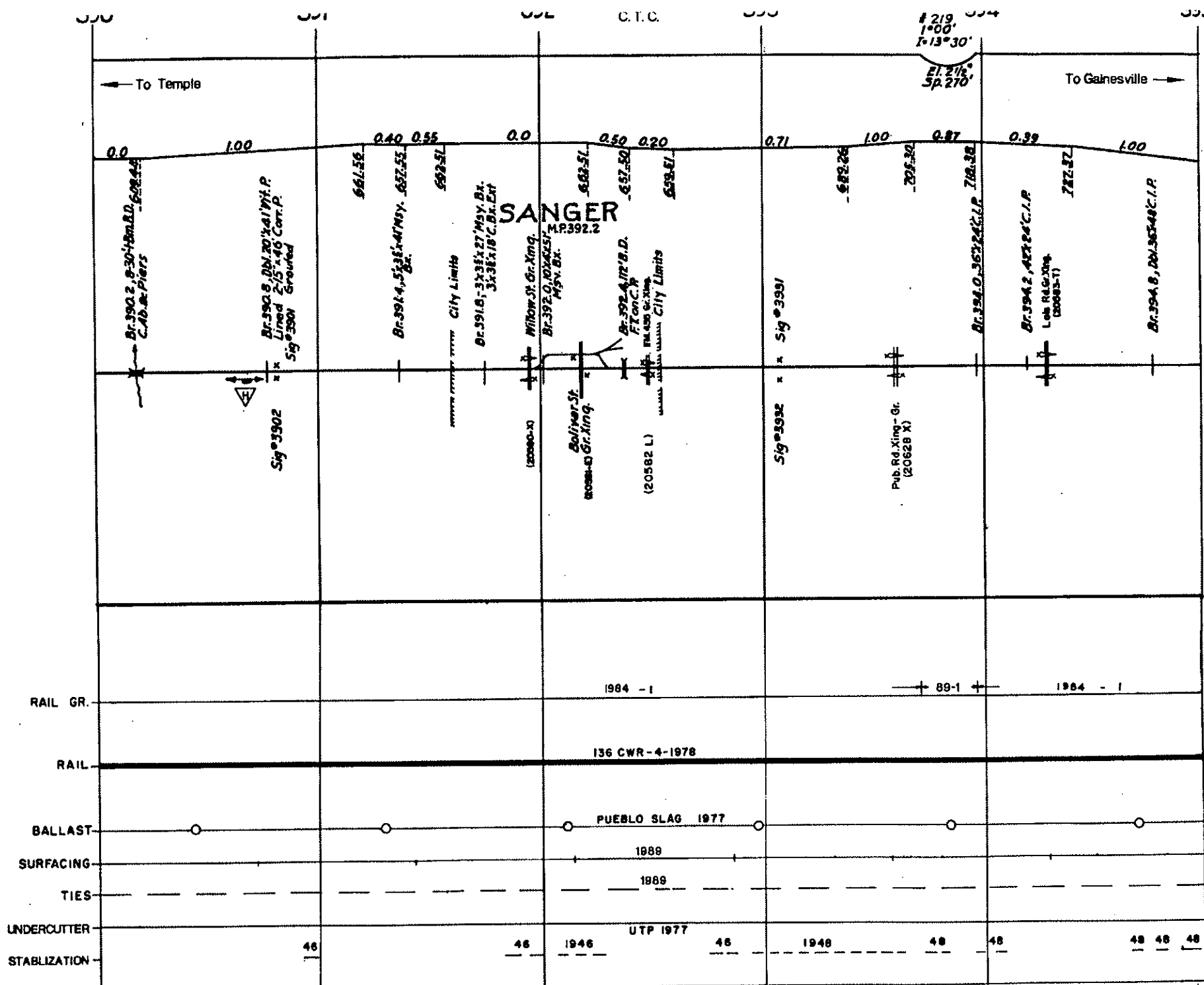
384

385

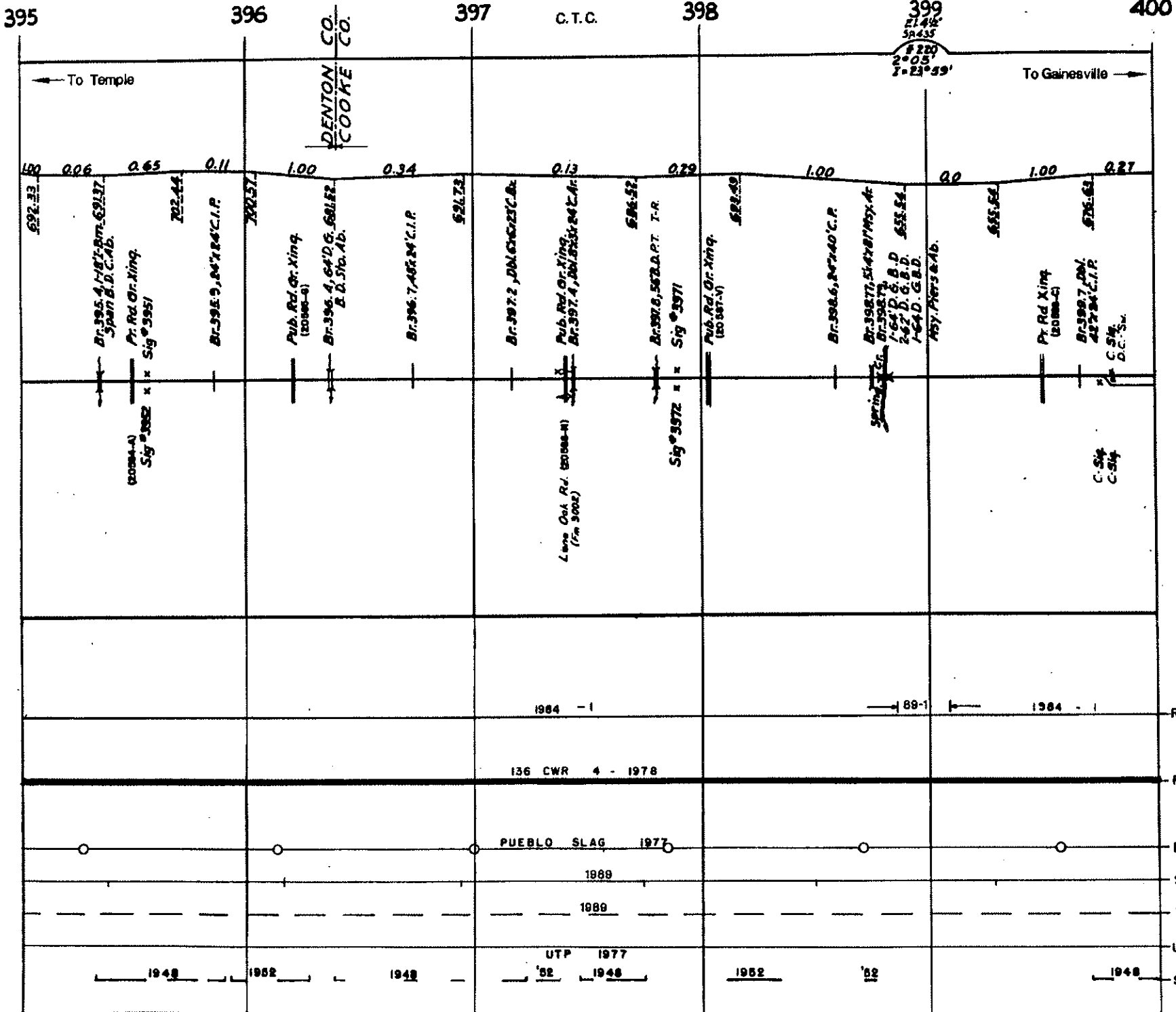
C.T.C.







REVISED 3-1991



400

401

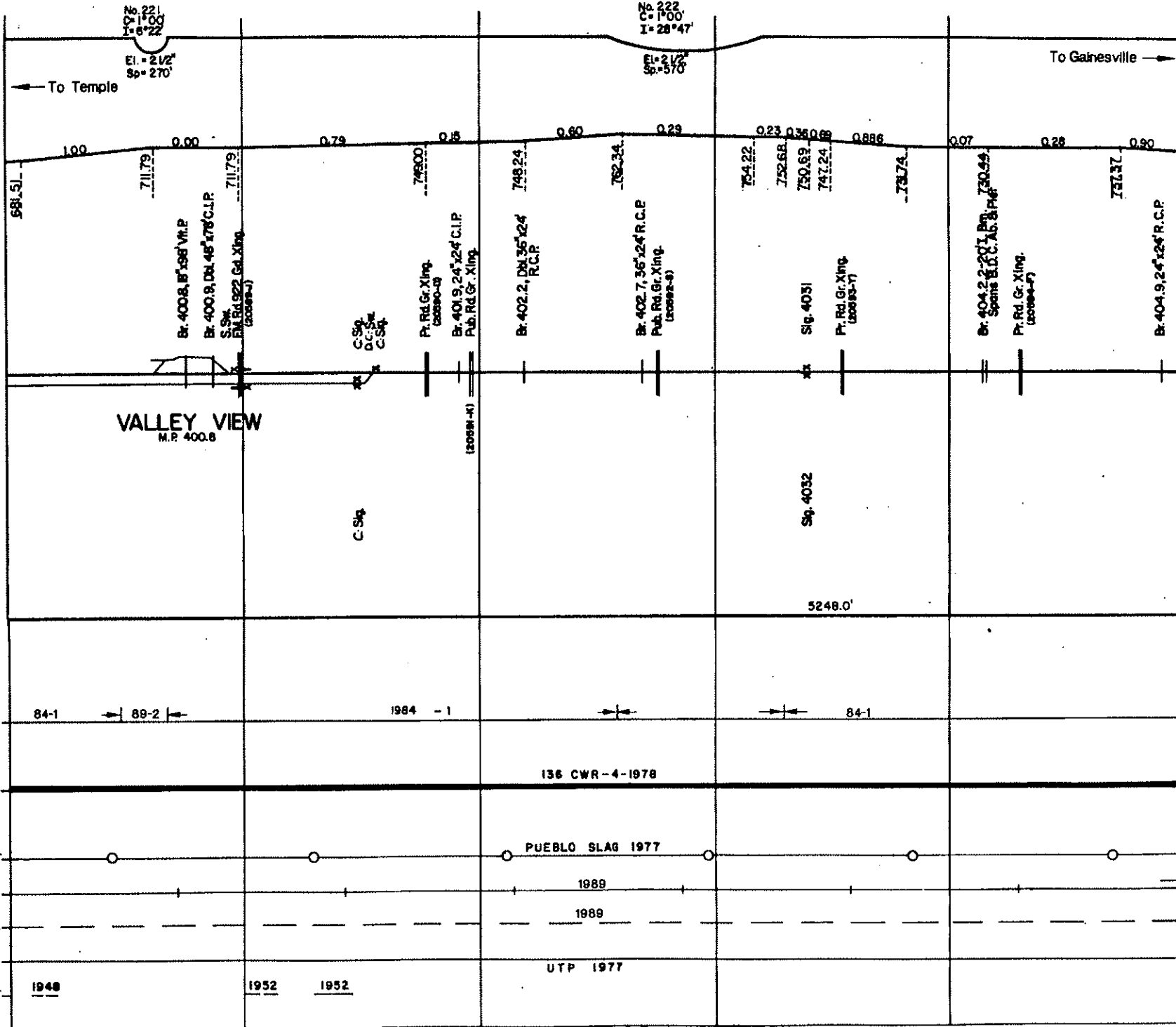
402

C.T.C.

403

404

405



← To Temple

To Gainesville →

VALLEY VIEW  
M.P. 400.8

RAIL GR. 84-1 89-2

RAIL

BALLAST

SURFACING

TIES

UNDERCUTTER

STABILIZATION 1948 1952 1952

1984 - 1

136 CWR-4-1978

PUEBLO SLAG 1977

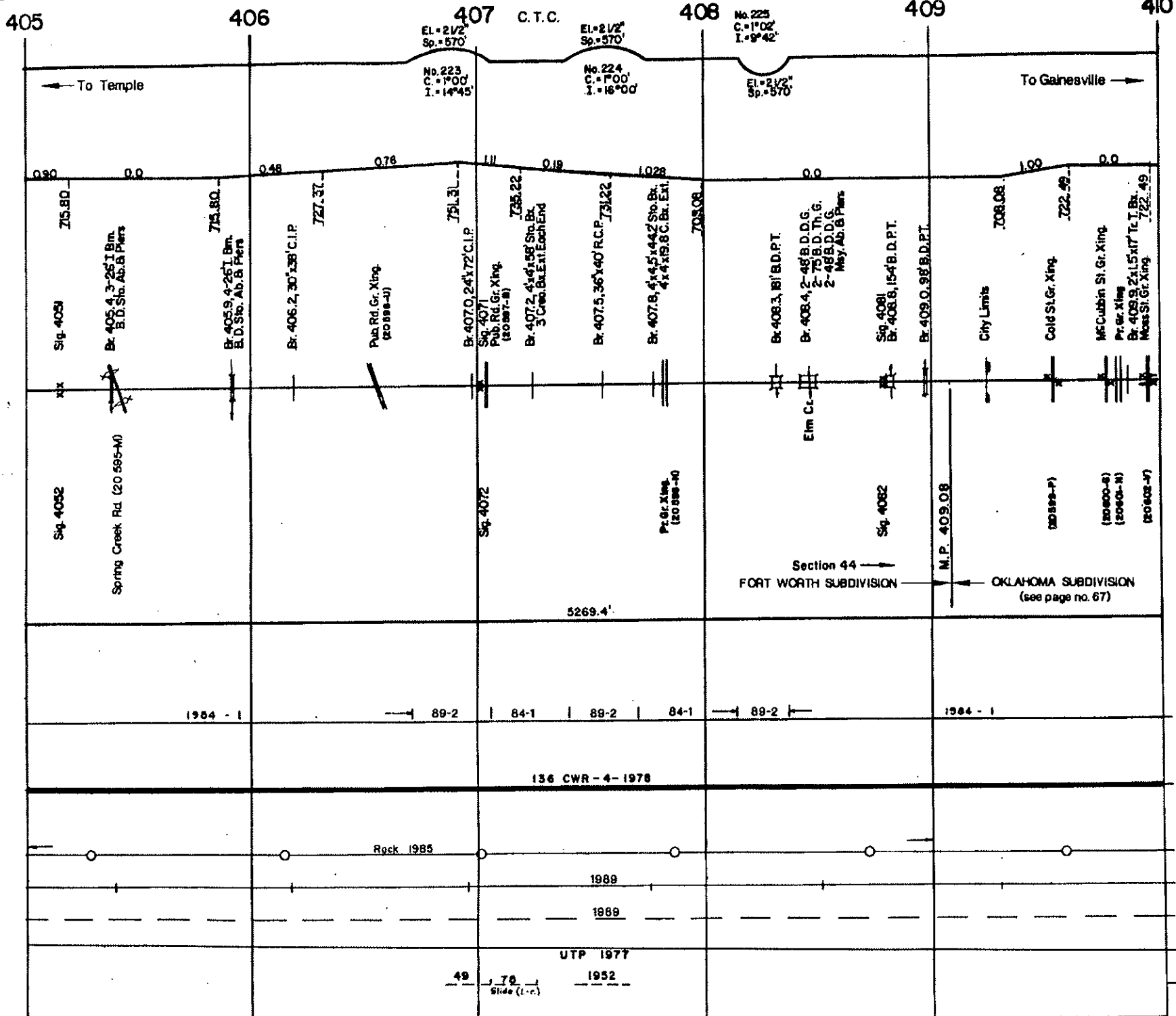
1989

1989

UTP 1977

Sig. 4032

5248.0'



# Ft. Worth Subdivision

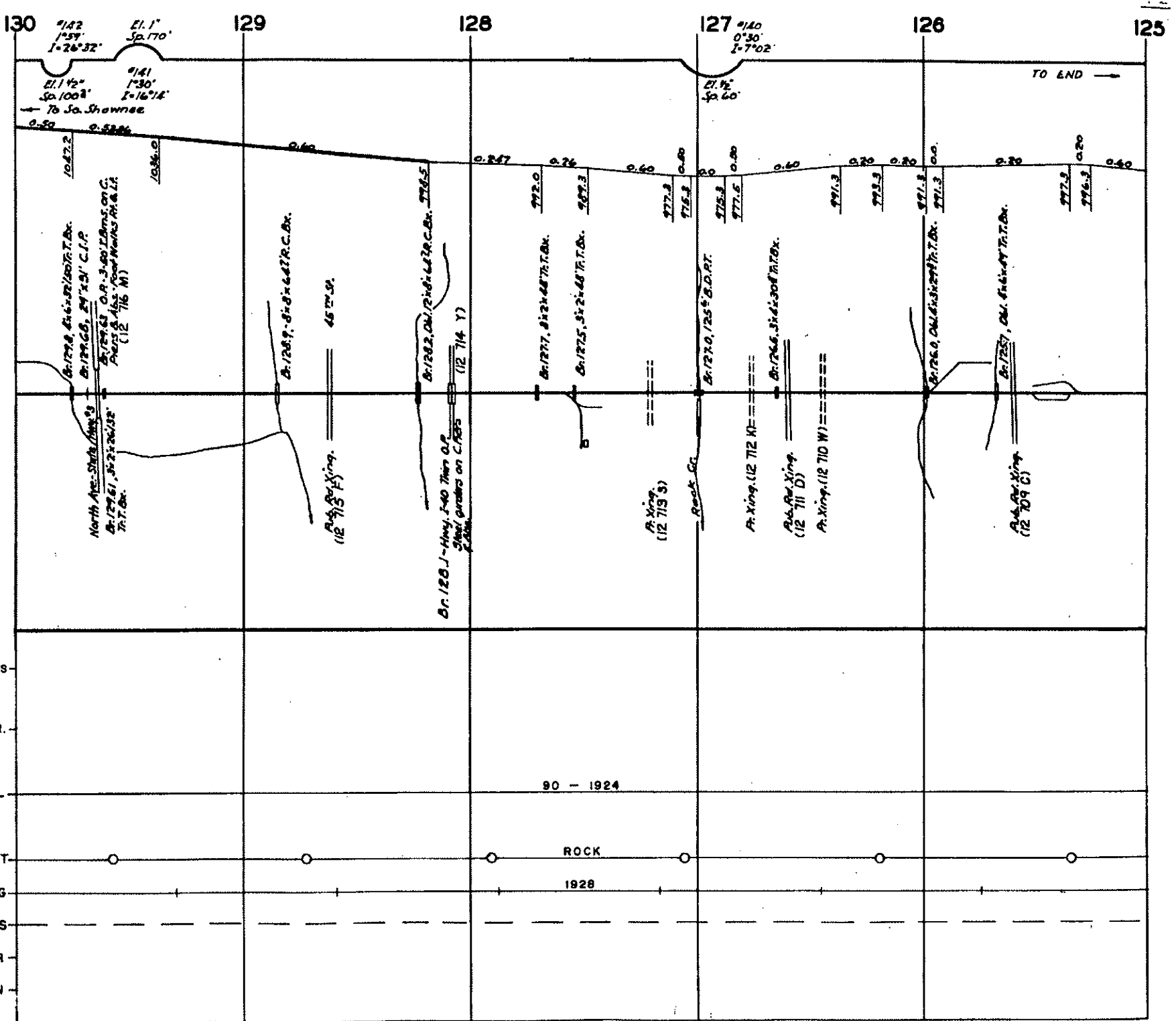
Gainesville (M.P. 409.1) to Temple (M.P. 218.2)

*Intentionally Blank*



*Intentionally Blank*





130

129

128

127

126

125

#142  
1°59'  
I=26°32'

El. 1°  
Sp. 170'

#141  
1°30'  
I=16°14'

To So. Shawnee

#140  
0°30'  
I=7°02'

El. 42°  
Sp. 60'

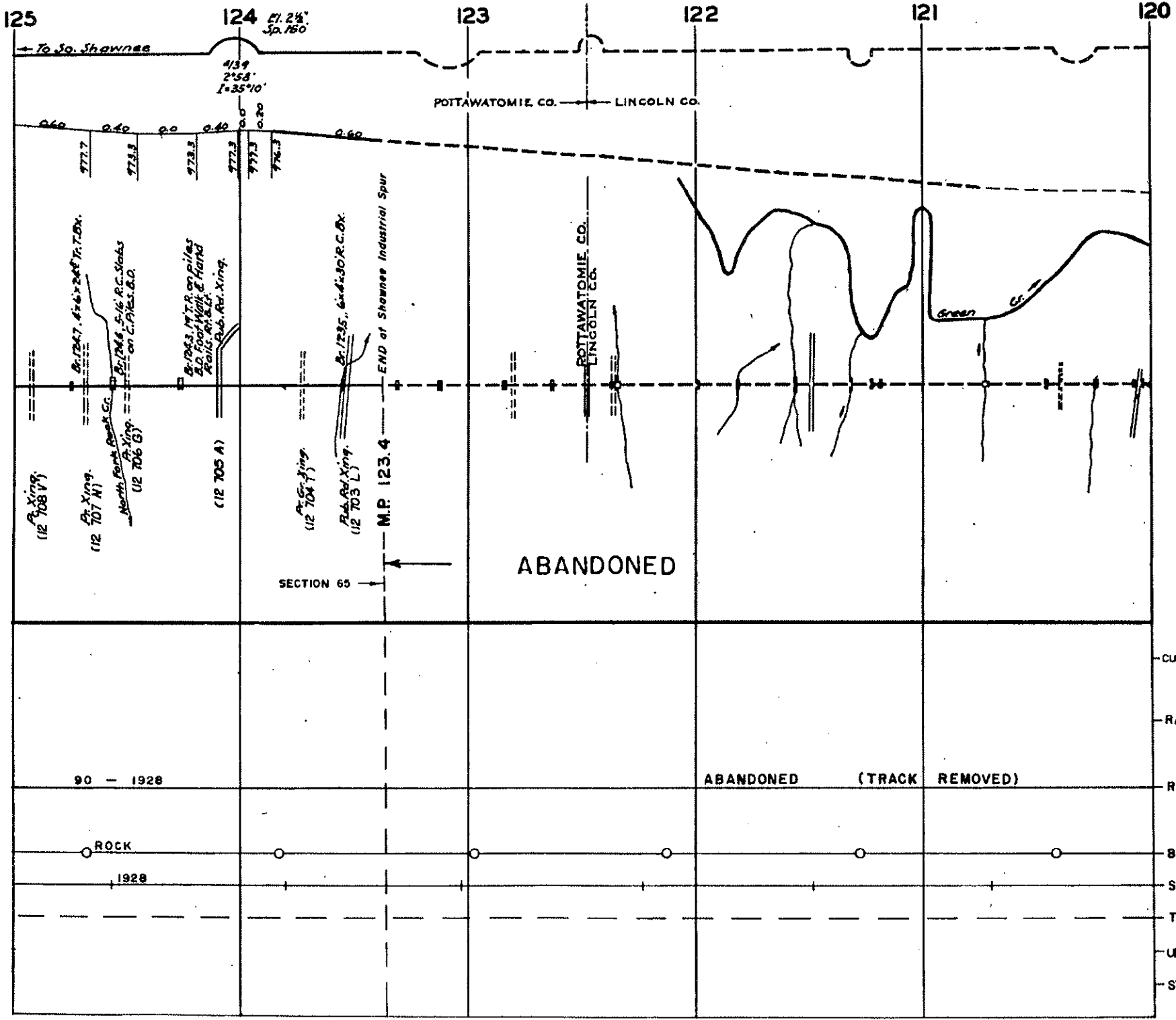
TO END →

95 CURVES  
RAIL GR.  
RAIL  
BALLAST  
SURFACING  
TIES  
UNDERCUTTER  
STABILIZATION

90 - 1924

ROCK

1928



SECTION 65

ABANDONED

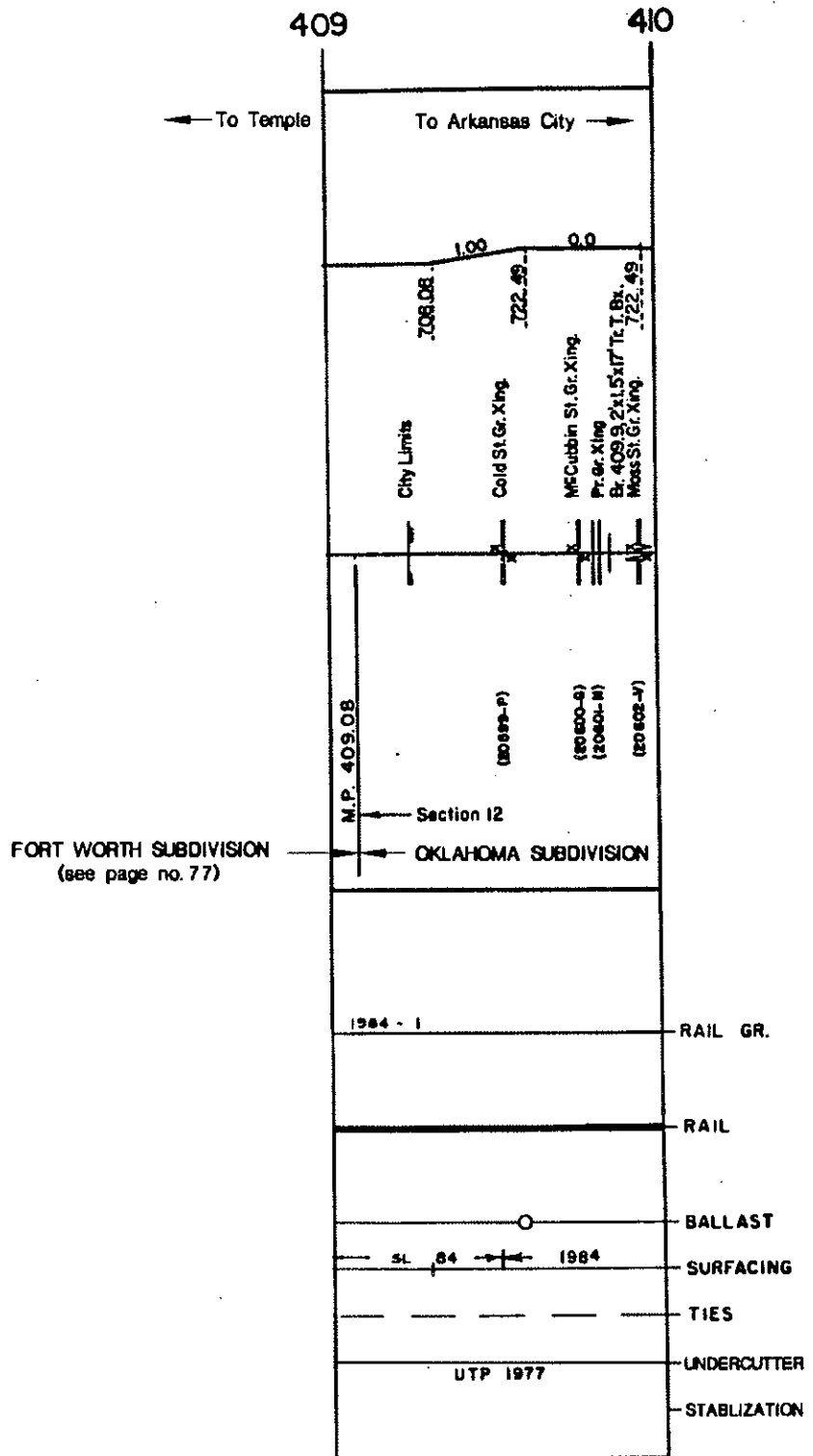
ABANDONED (TRACK REMOVED)

- CURVES NR BR
- RAIL GR.
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABILIZATION

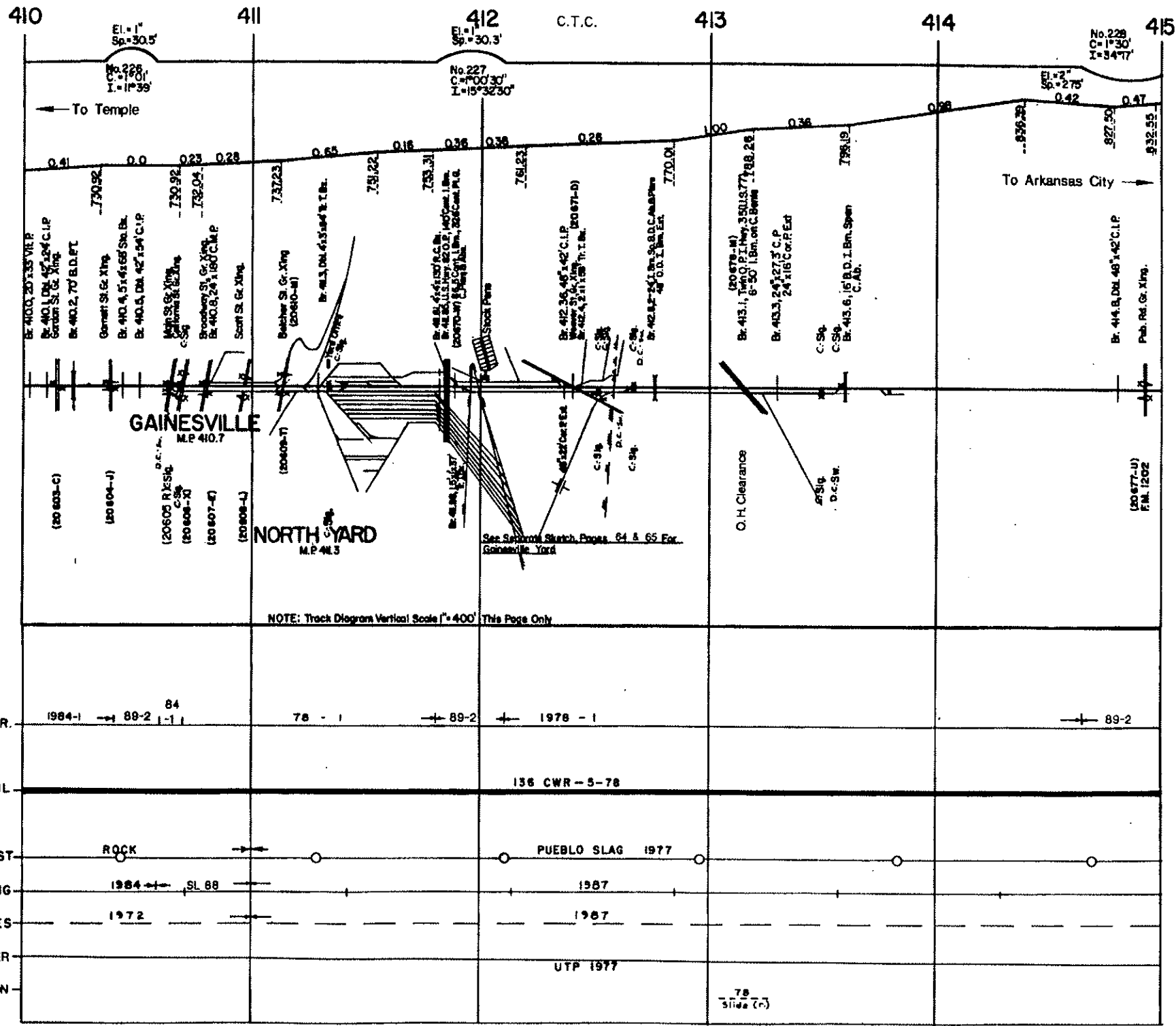


Shawnee Industrial Spur  
M.P. 123.4 to M.P. 134.0

*Intentionally Blank*





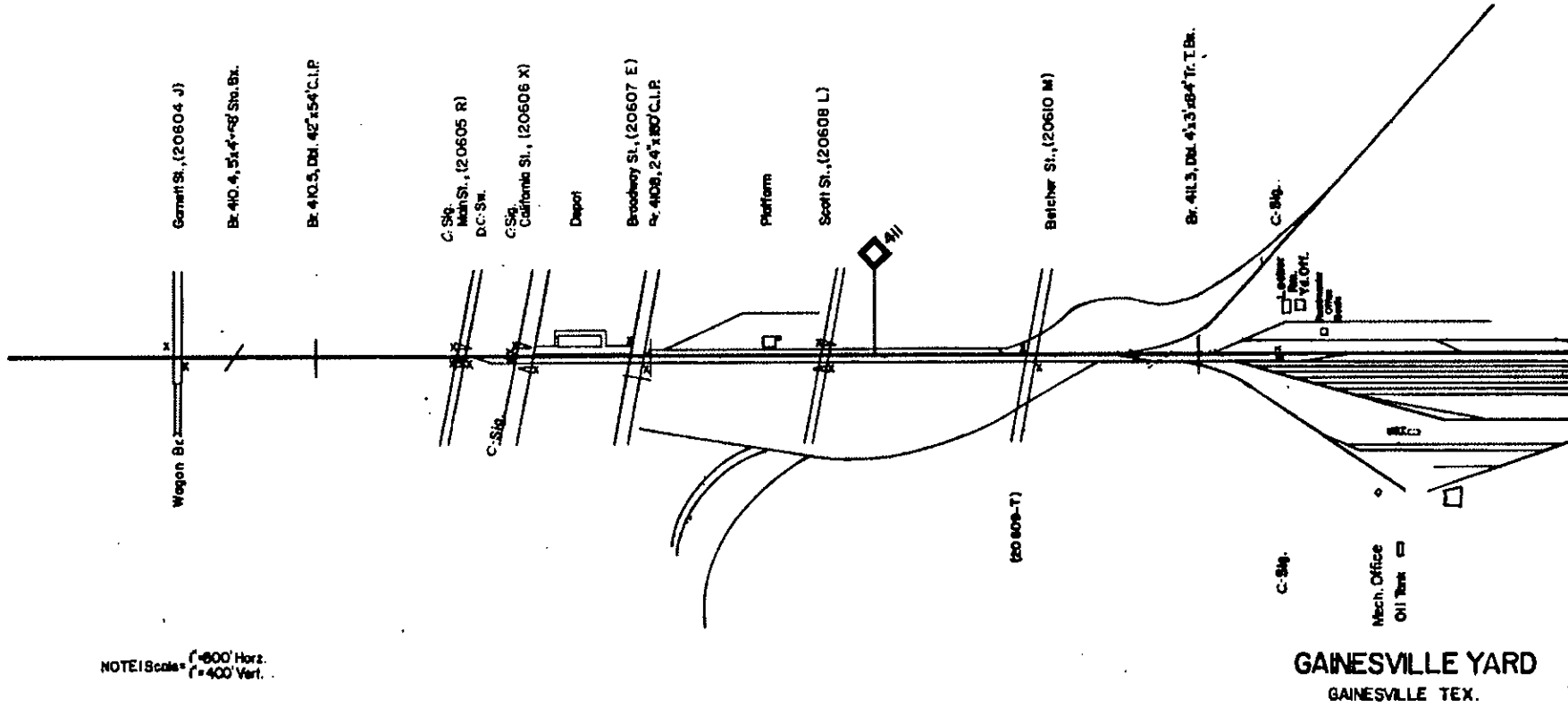


NOTE: Track Diagram Vertical Scale 1" = 400' This Page Only

← To Galveston

### GAINESVILLE M.P. 410.7

To Arkansas City →

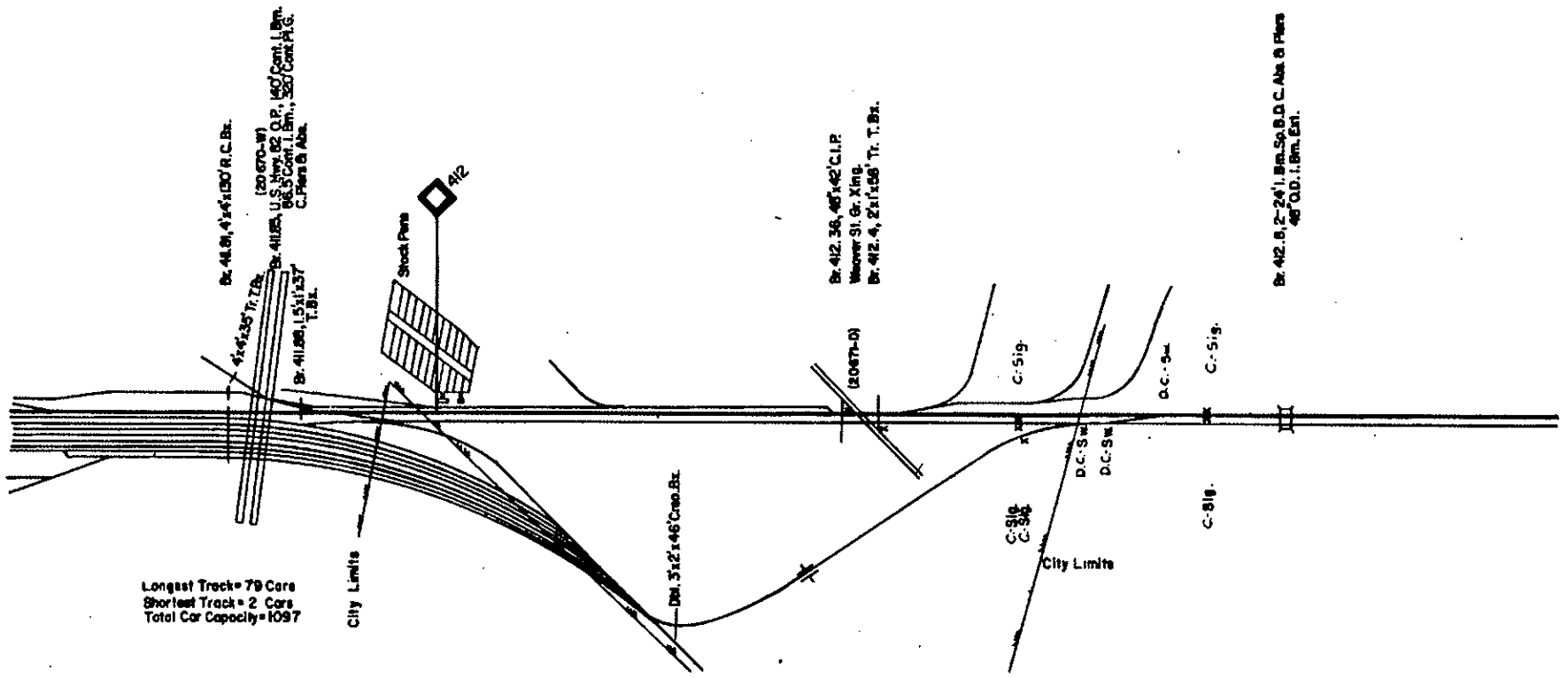


NOTE: Scale = 1" = 600' Horiz.  
1" = 400' Vert.

**GAINESVILLE YARD**  
GAINESVILLE TEX.

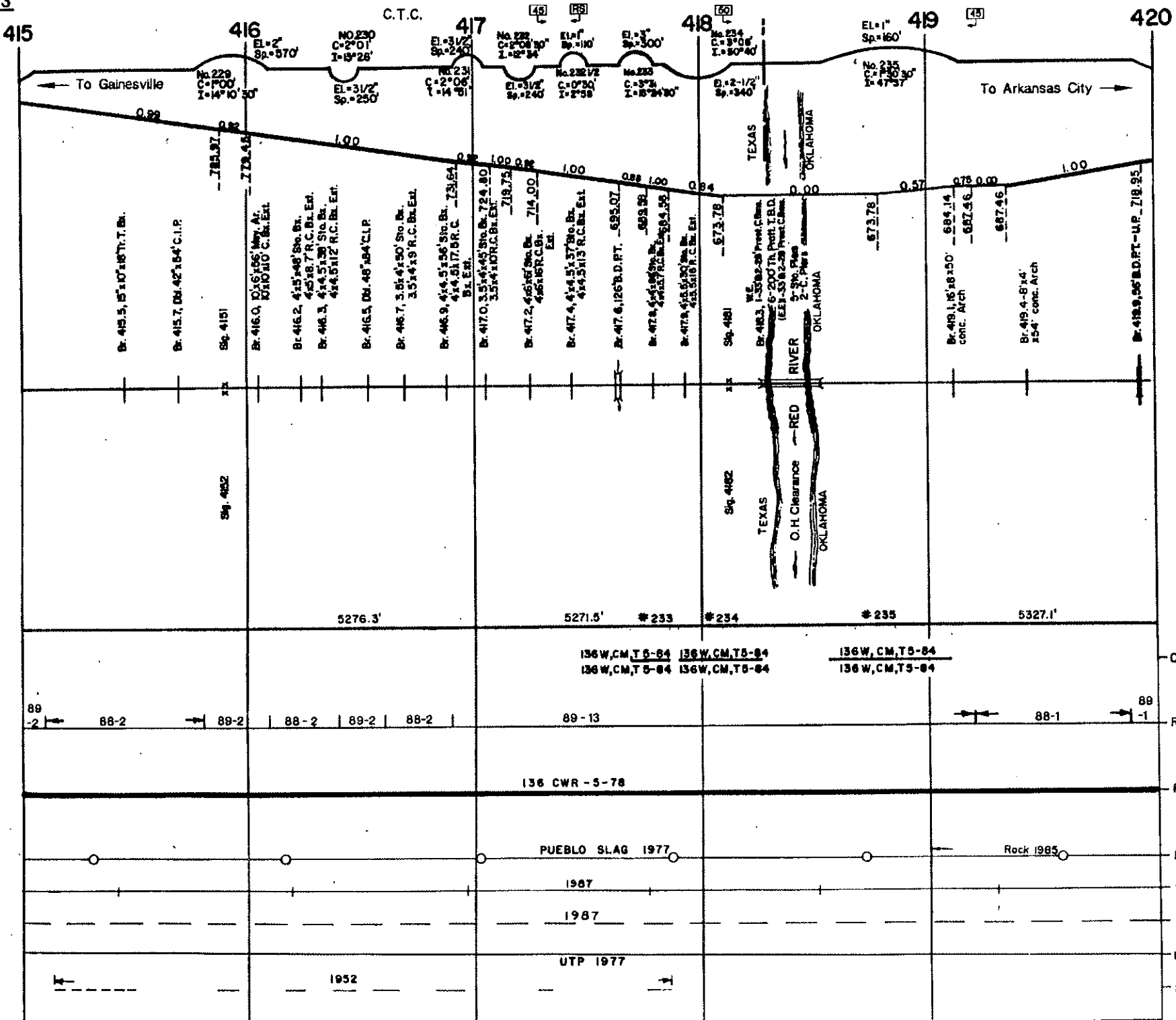
← To Galveston

To Arkansas City →

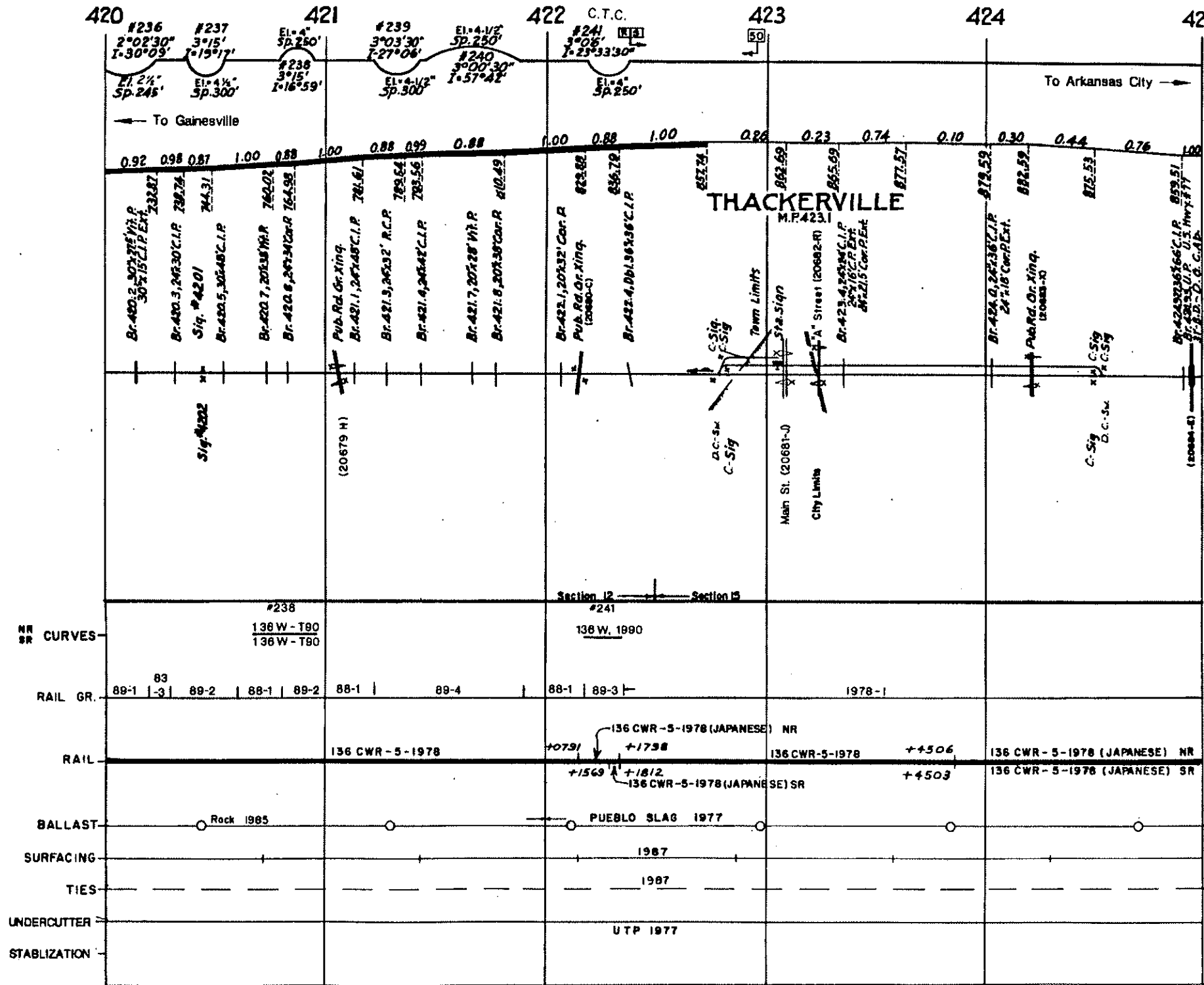


Longest Track = 79 Cars  
 Shortest Track = 2 Cars  
 Total Car Capacity = 1097

**GAINESVILLE YARD**  
 GAINESVILLE TEXAS



CURVES NR  
 RAIL GR.  
 RAIL  
 BALLAST  
 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION



425

426

427

C.T.C.

428

429

430

4242  
100'  
T=10°30'

El. 174'  
Sp. 300'

← To Gainesville

To Arkansas City →

1.00 0.35 1.00 0.12 1.00 0.32 0.82 0.22 1.00 1.00

879.58

895.39

867.57

832.03

823.43

792.90

Pub. Rd. Gr. Xing.  
(20085-L)

Br. 426.1 O.P. I.H. 35  
(20086-T)

Co. Rd. Gr. Xing.  
(20087-A)

Br. 426.6, DN, 487'4" C.I.P. 868.80

Br. 426.8, 427'72" C.I.P.

54'4072" Sig. 44871

Co. Rd. Gr. Xing.  
(20088-4)

Br. 427.4, DN, 487'38" C.I.P.

Br. 427.7, 816'52" R.C.A.F.

Pr. Gr. Xing. (20089-4)

Br. 428.5, DN,  
457'72" C.I.P.

Br. 428.8, 724'85" R.C.P.

54'4072" Sig. 44871

Br. 429.0, 807'16" K.C.P.  
307'12" C.I.P. Ent. - 293.90

Br. 429.5, 881'74" C.A.F.  
766.11

CURVES NR SR

1978 - 1

77-1 | 89 - 1 |

1977 - 1

RAIL GR.

136 CWR - 5-78 ( JAPANESE ) NR

119 CWR - 4-1964

RAIL

136 CWR - 5-78 ( JAPANESE ) SR

PUEBLO SLAG

1977

ROCK 1979

BALLAST

1987

SURFACING

1987

TIES

UTP 1979

UNDERCUTTER

UTP 1977

STABILIZATION

430

431

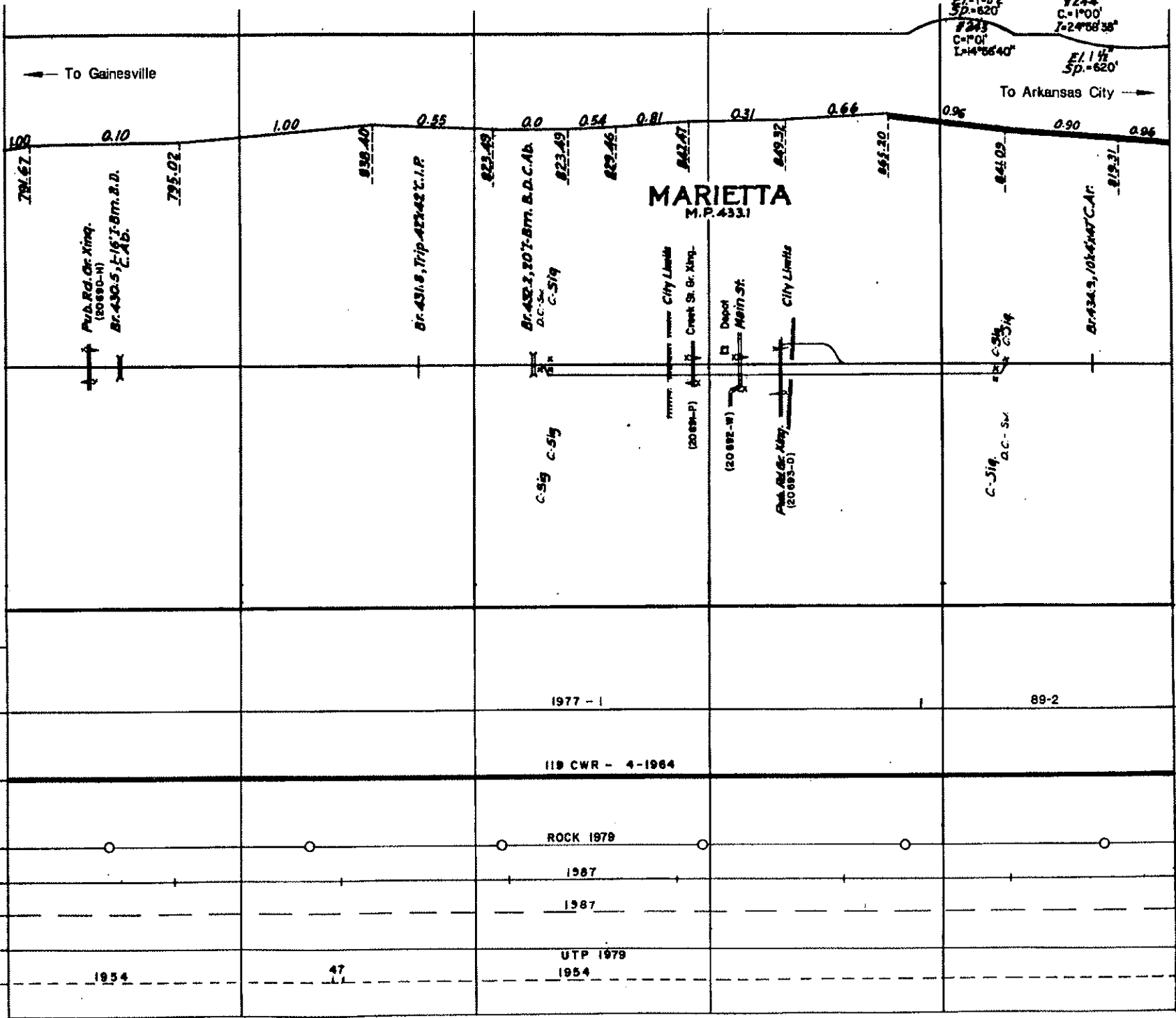
432

C.I.C.

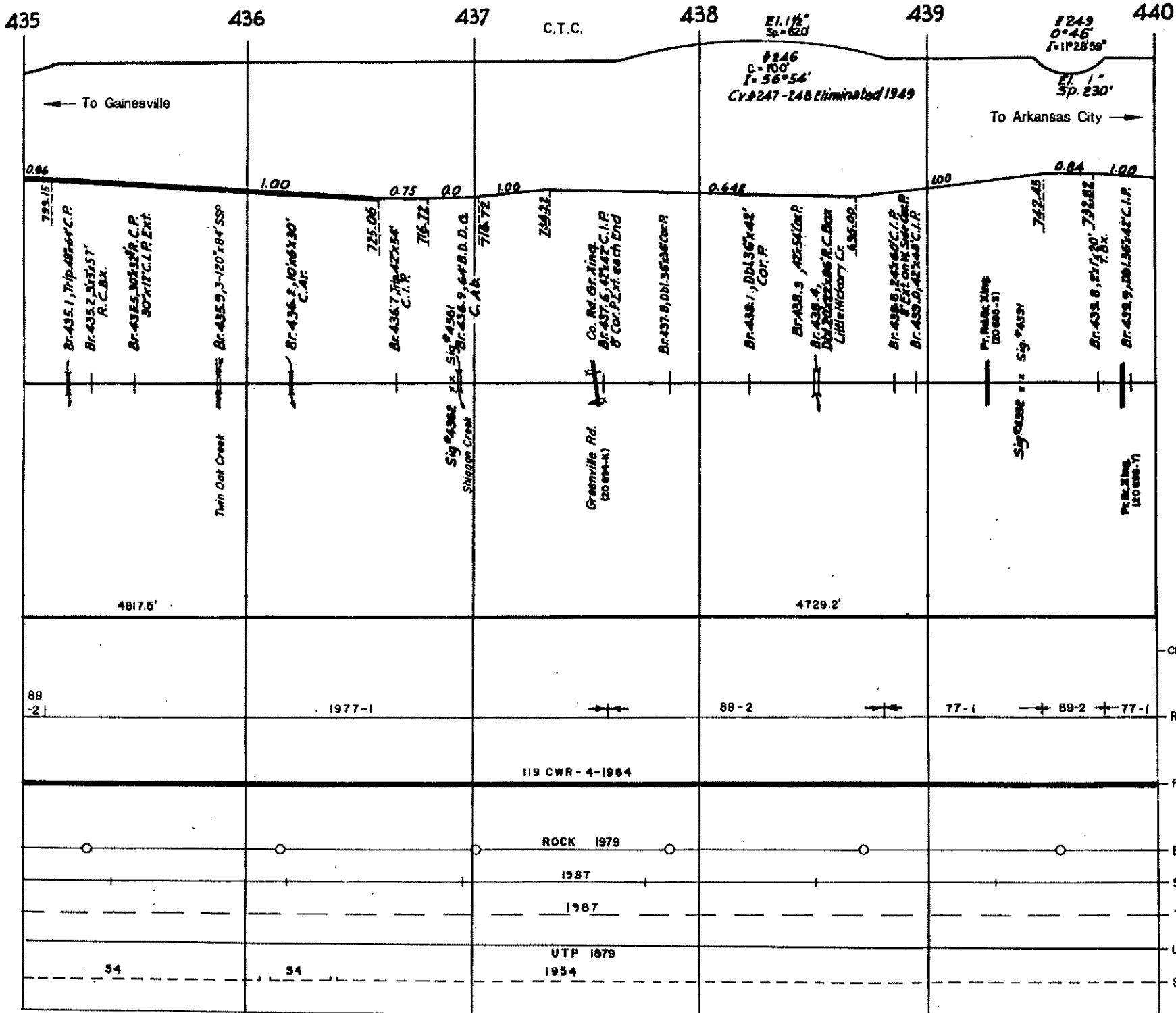
433

434

435



STAB  
RAIL GR.  
RAIL  
BALLAST  
SURFACING  
TIES  
UNDERCUTTER  
STABILIZATION







445

446

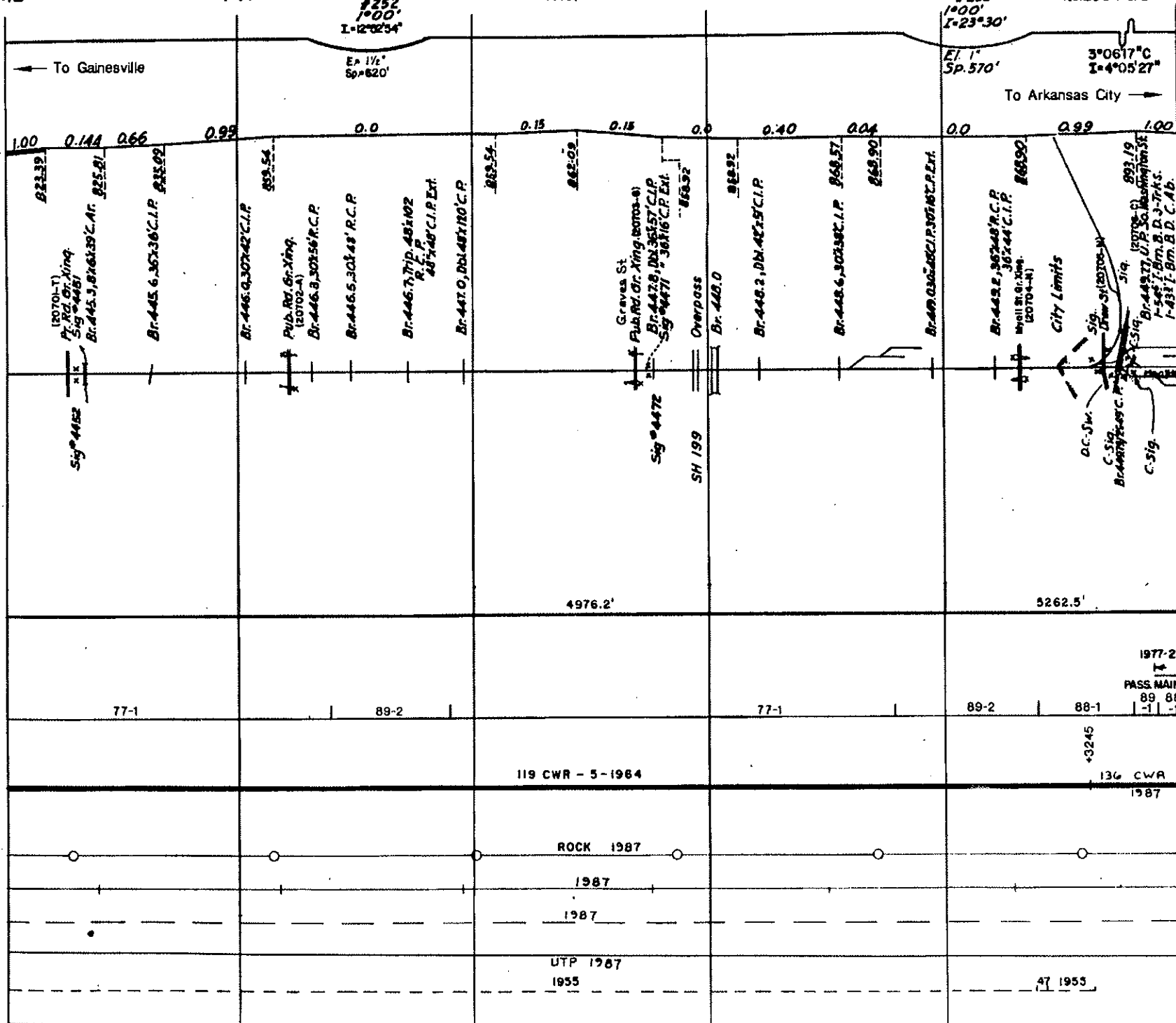
447

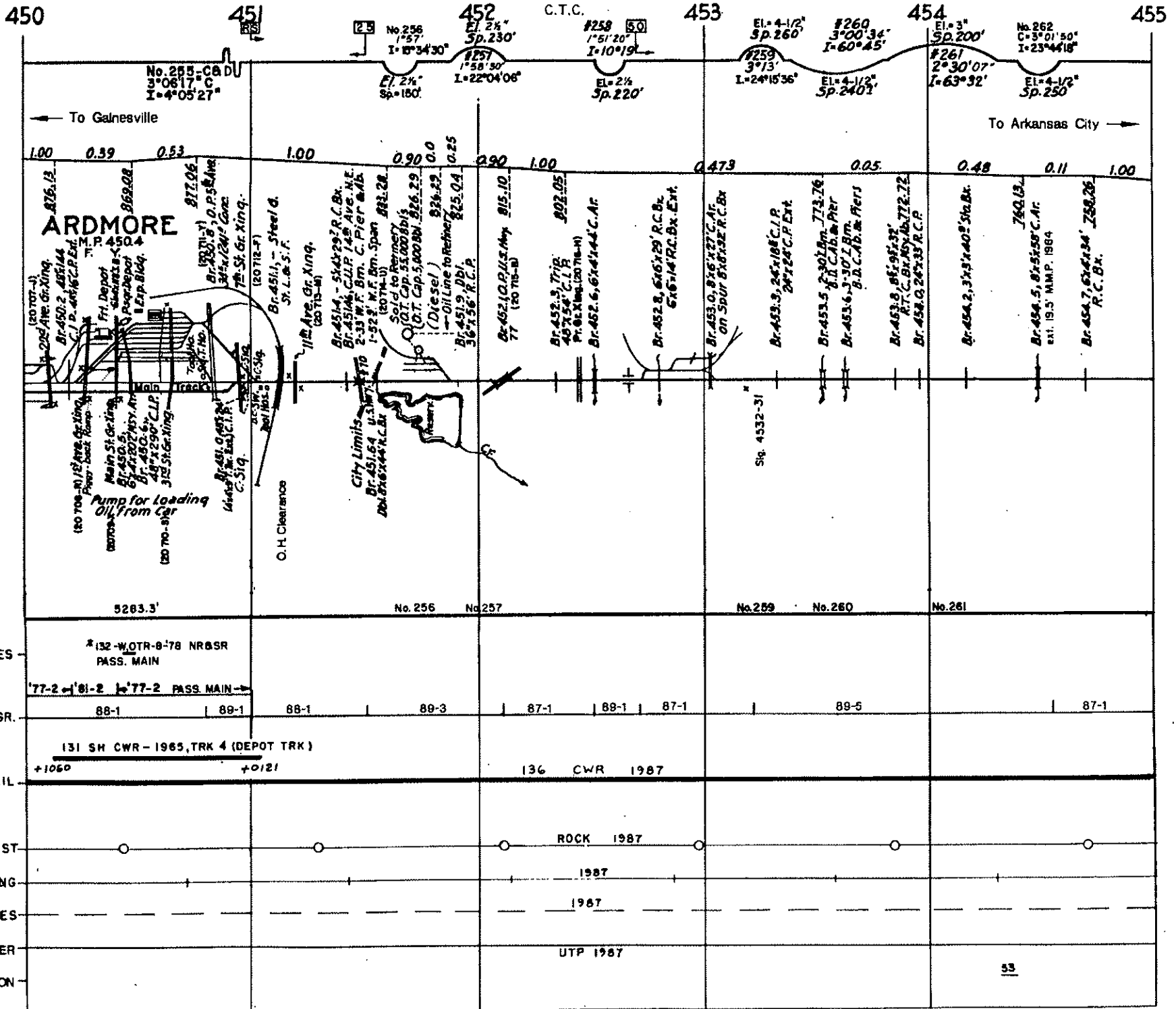
C.T.C.

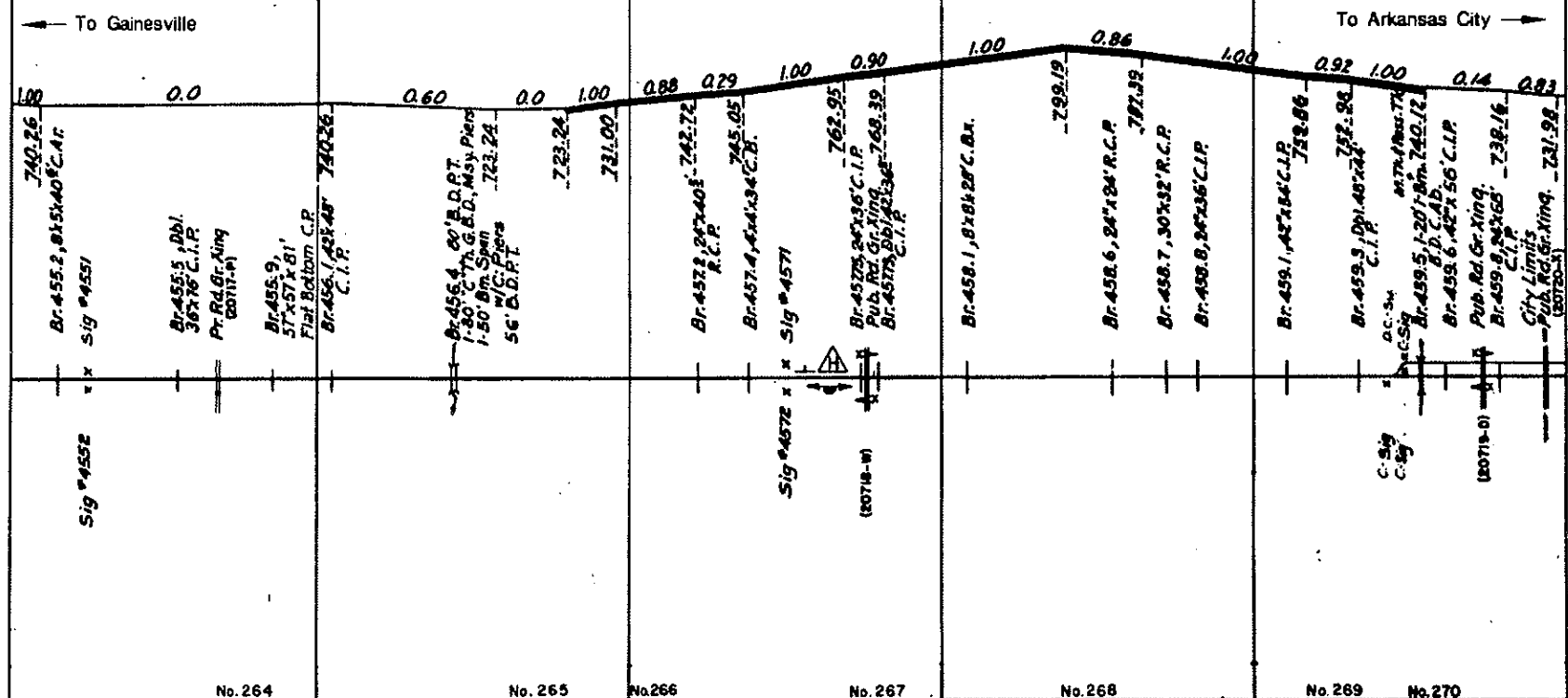
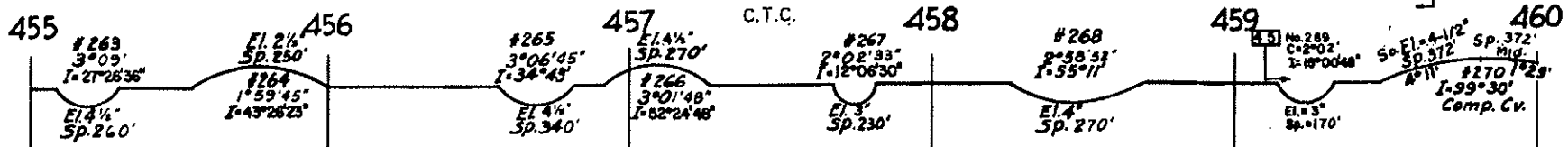
448

449

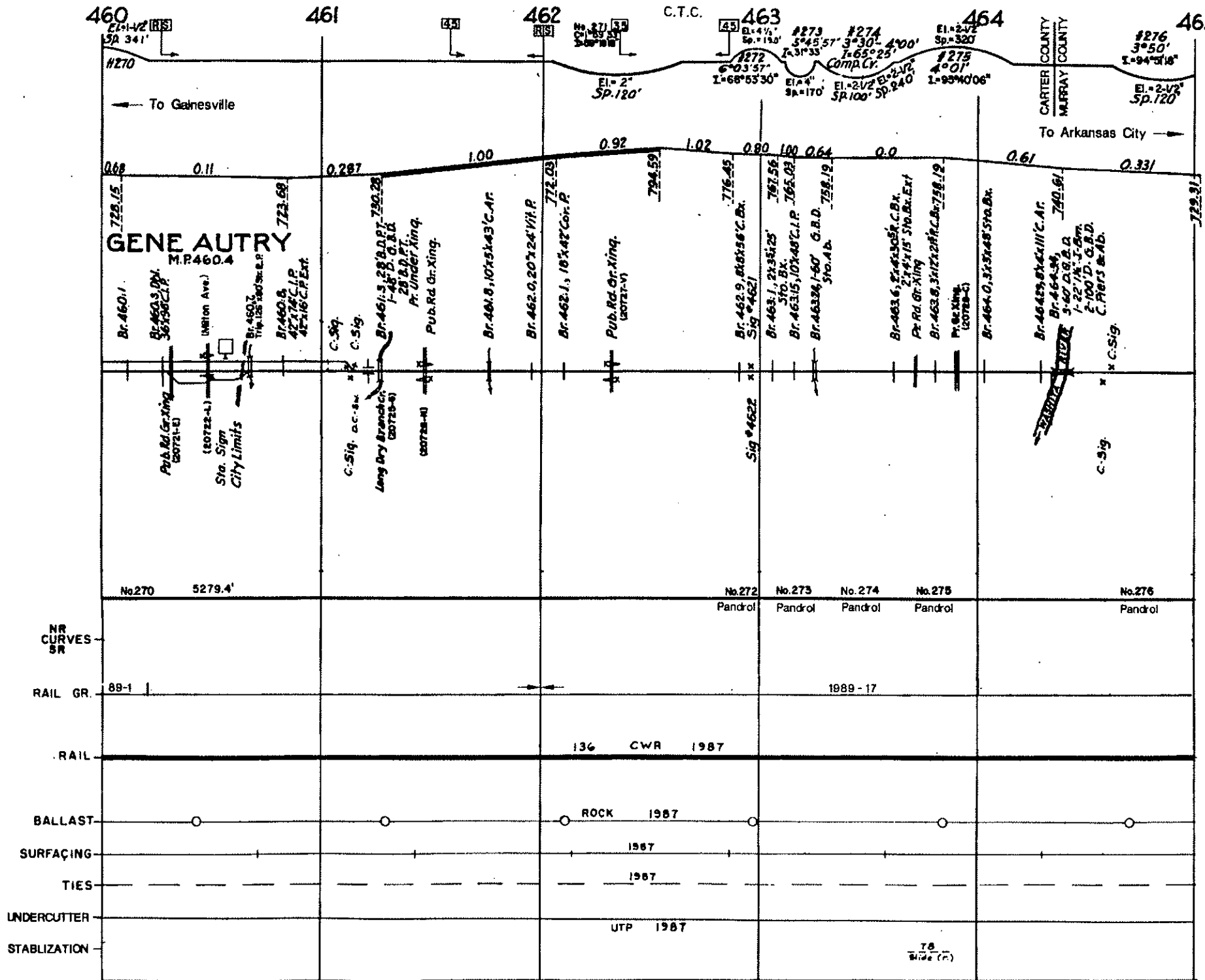
450



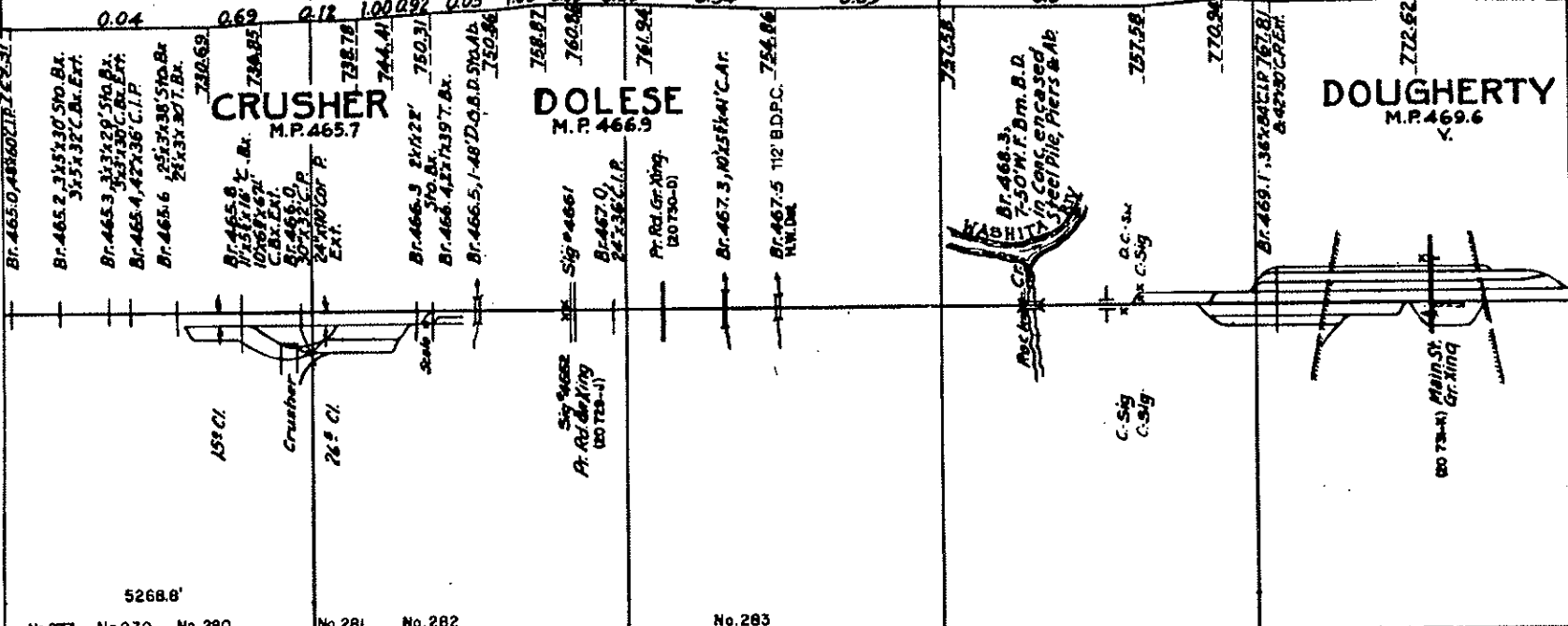
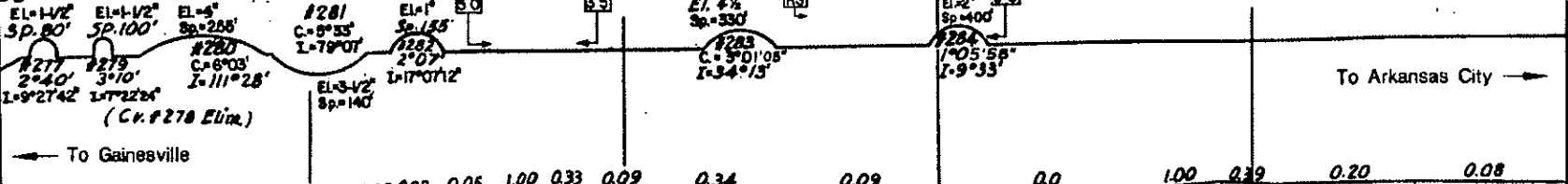




No. 264		No. 265			No. 266		No. 267		No. 268			No. 269		No. 270	
NR CURVES SR															
RAIL GR.															
RAIL															
BALLAST															
SURFACING															
TIES															
UNDERCUTTER															
STABILIZATION															



465 466 467 C.T.C. 468 469 470



5268.8'											
No.277	No.279	No.280	No.281	No.282	No.283						
Pandrol	Pandrol	Pandrol	Pandrol								
1989-17			87-3			89-1			87-3		
						136 CWR 1987					
						ROCK 1987					
						1987					
						1987					
						UTP 1987					
						56			1956		
									56		
									57 56		

NR  
CURVES  
SR

RAIL GR.

RAIL

BALLAST

SURFACING

TIES

UNDERCUTTER

STABILIZATION

To Arkansas City →

← To Gainesville

**CRUSHER**  
M.P. 465.7

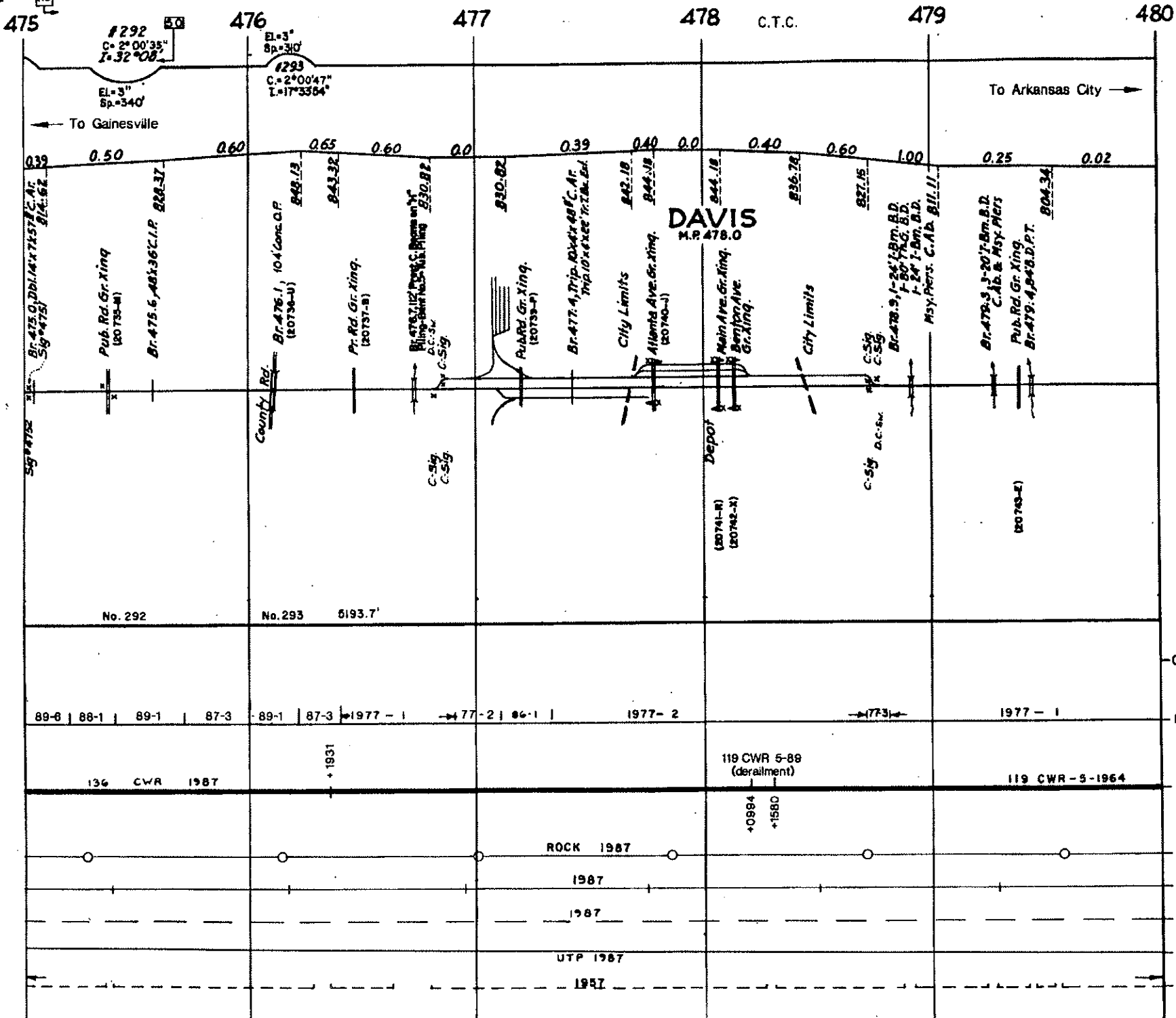
**DOLESE**  
M.P. 466.9

**DOUGHERTY**  
M.P. 469.6

WABHITA  
Rockhouse Cr.  
Br. 468.3, 7-50' W.F. Dm. B.D.  
In concrete enclosed  
Steel Plug, Piers & Ab.

Main St.  
Gr. Sing  
(80 78-X)





**DAVIS**  
M.P. 478.0

CURVES  
RAIL GR.  
RAIL  
BALLAST  
SURFACING  
TIES  
UNDERCUTTER  
STABILIZATION







490

491

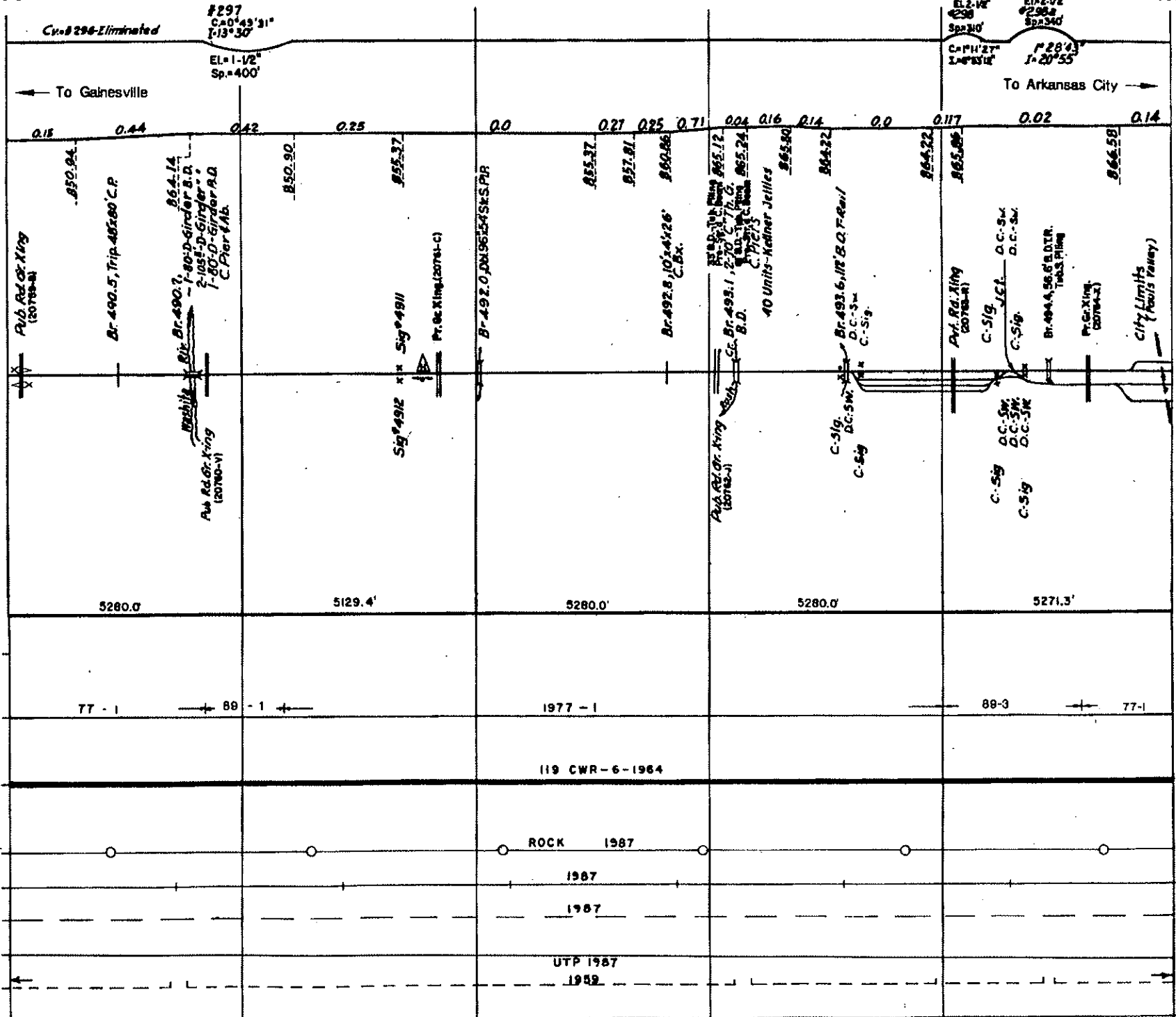
492

C.T.C.

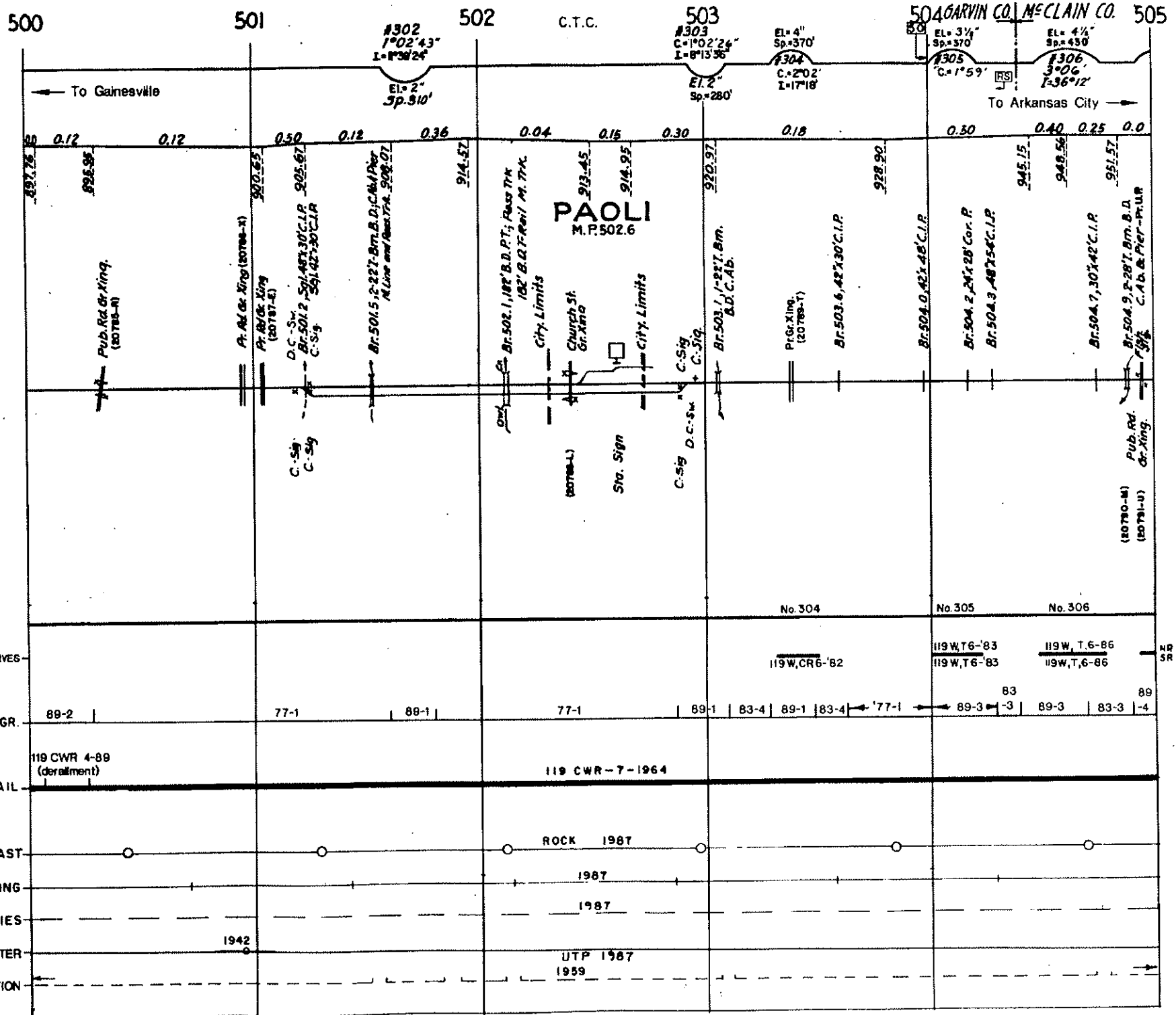
493

494

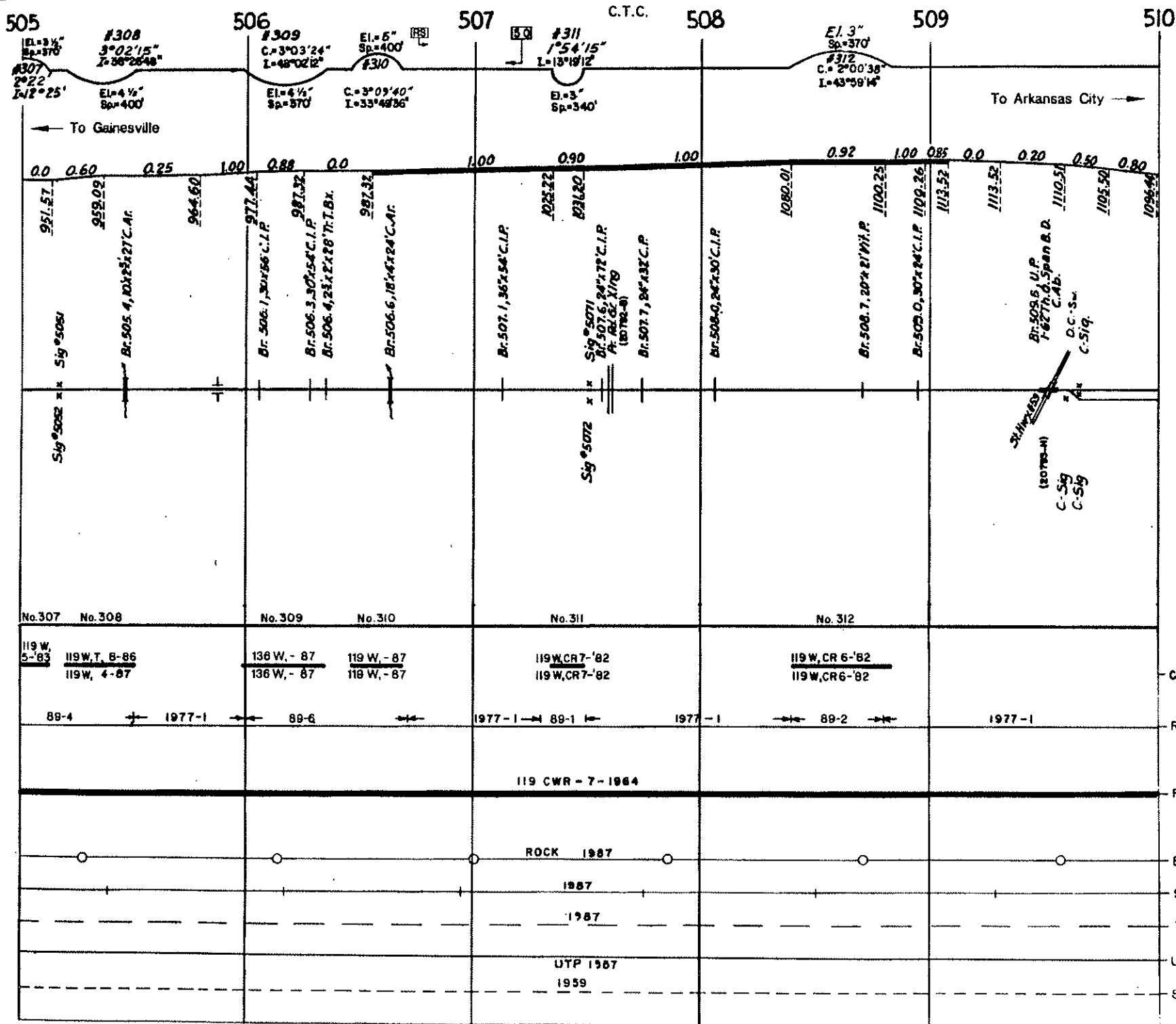
495







REVISED 3-1991



510

511

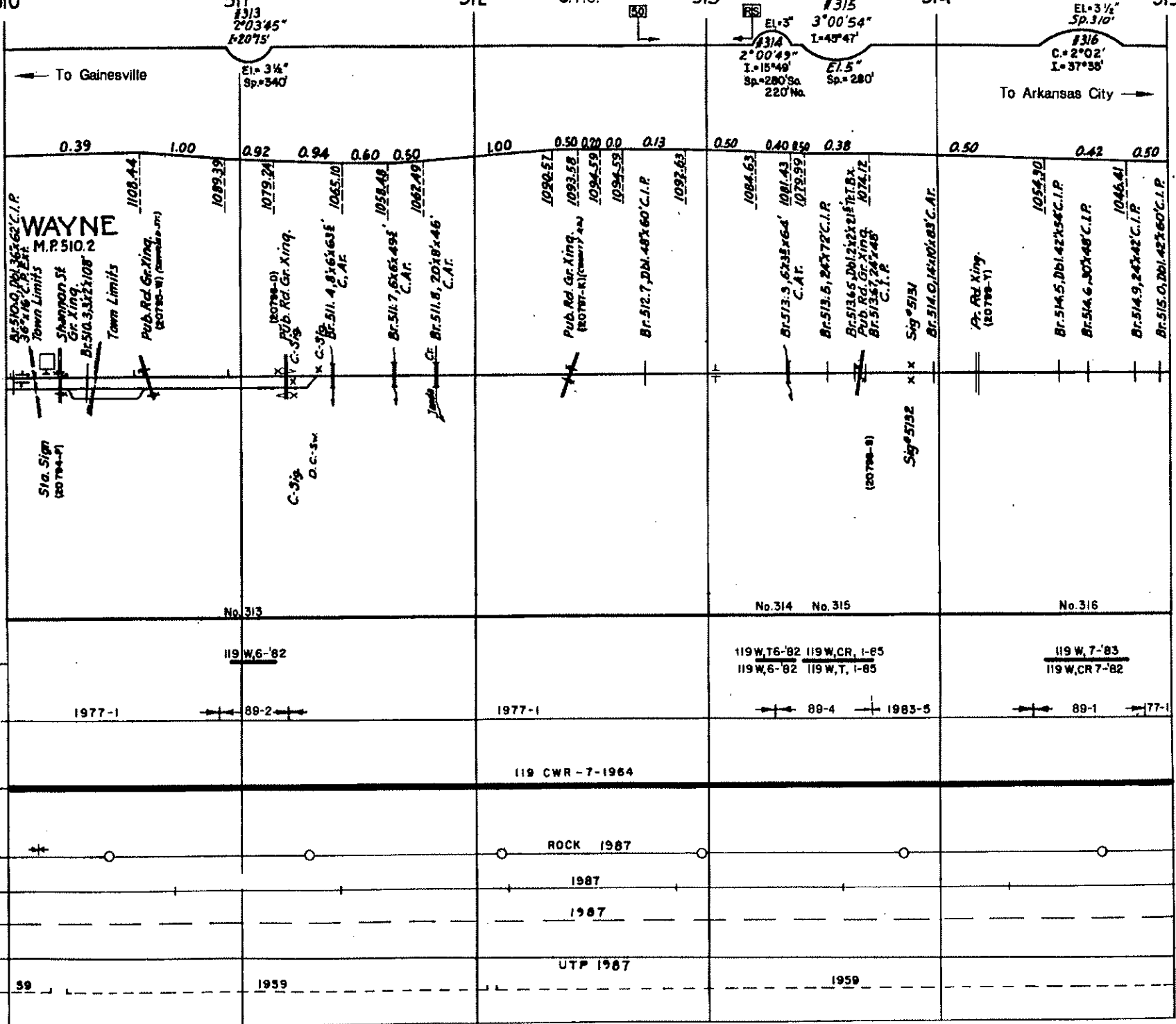
512

G.T.C.

513

514

515



NR CURVES

RAIL GR.

RAIL

BALLAST

SURFACING

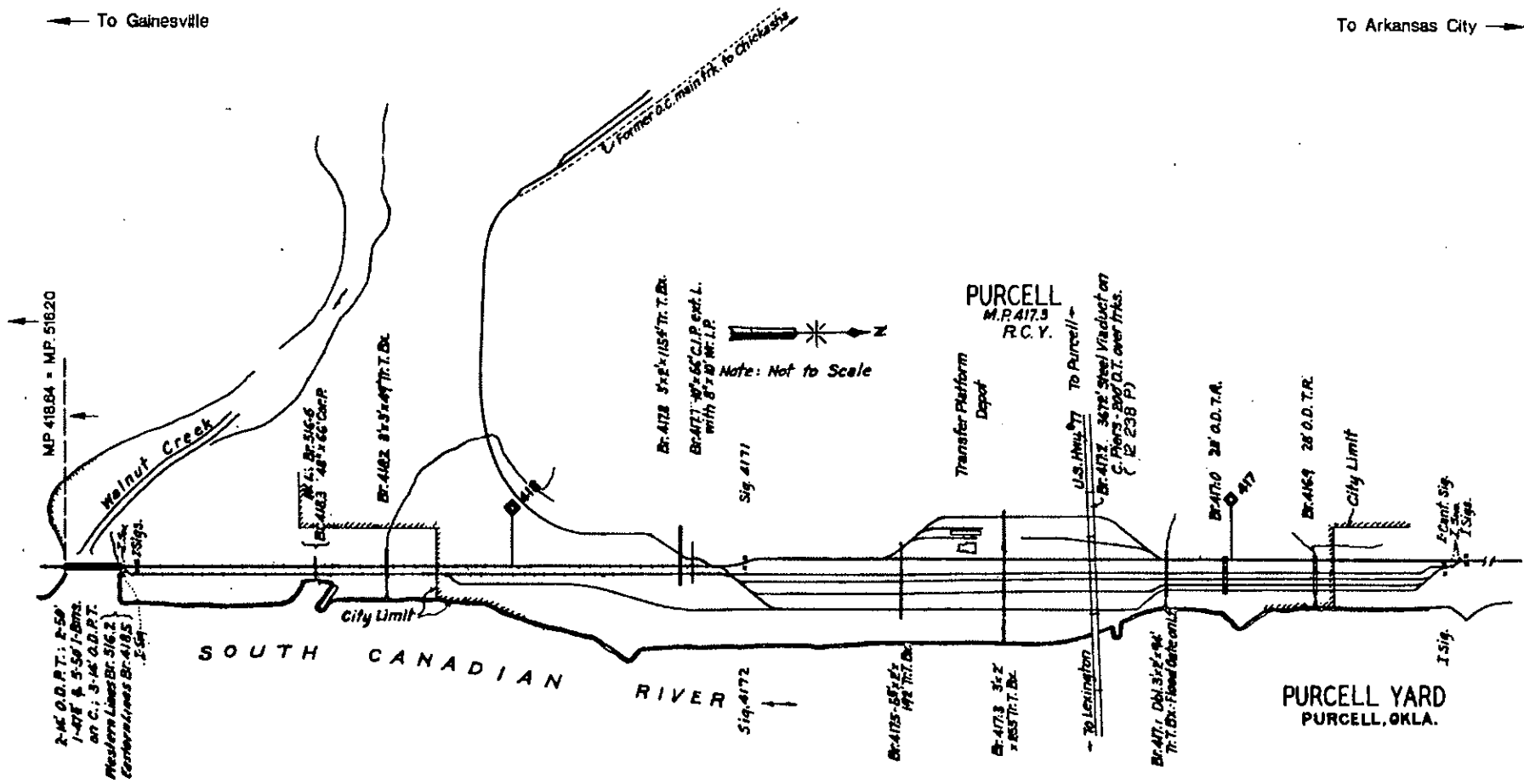
TIES

UNDERCUTTER

STABILIZATION







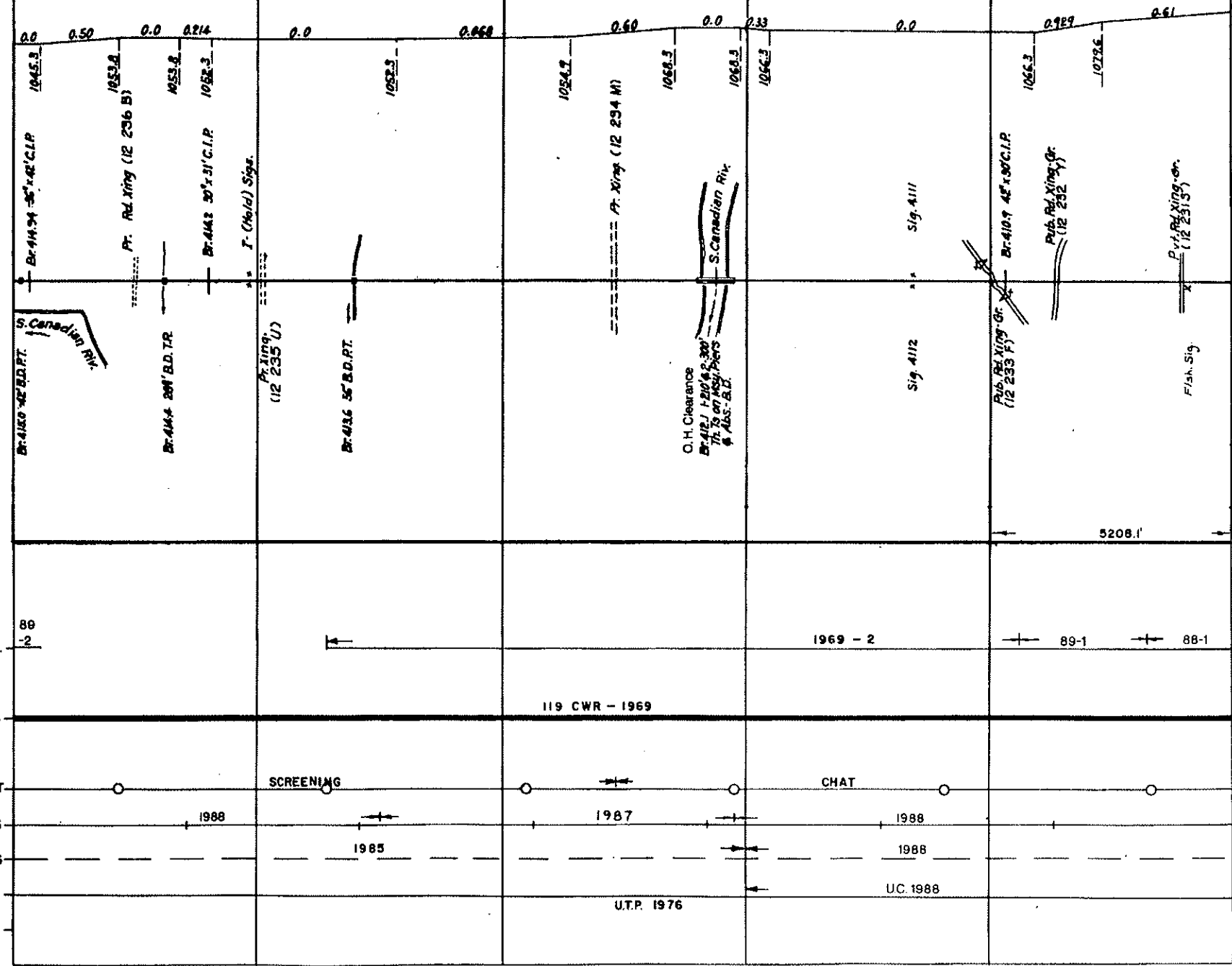


9138  
0°59'48"  
1-26°59'52"  
El. 1'  
Sp. 480'

El. 2'  
Sp. 500'  
9139  
705'  
1-1201'

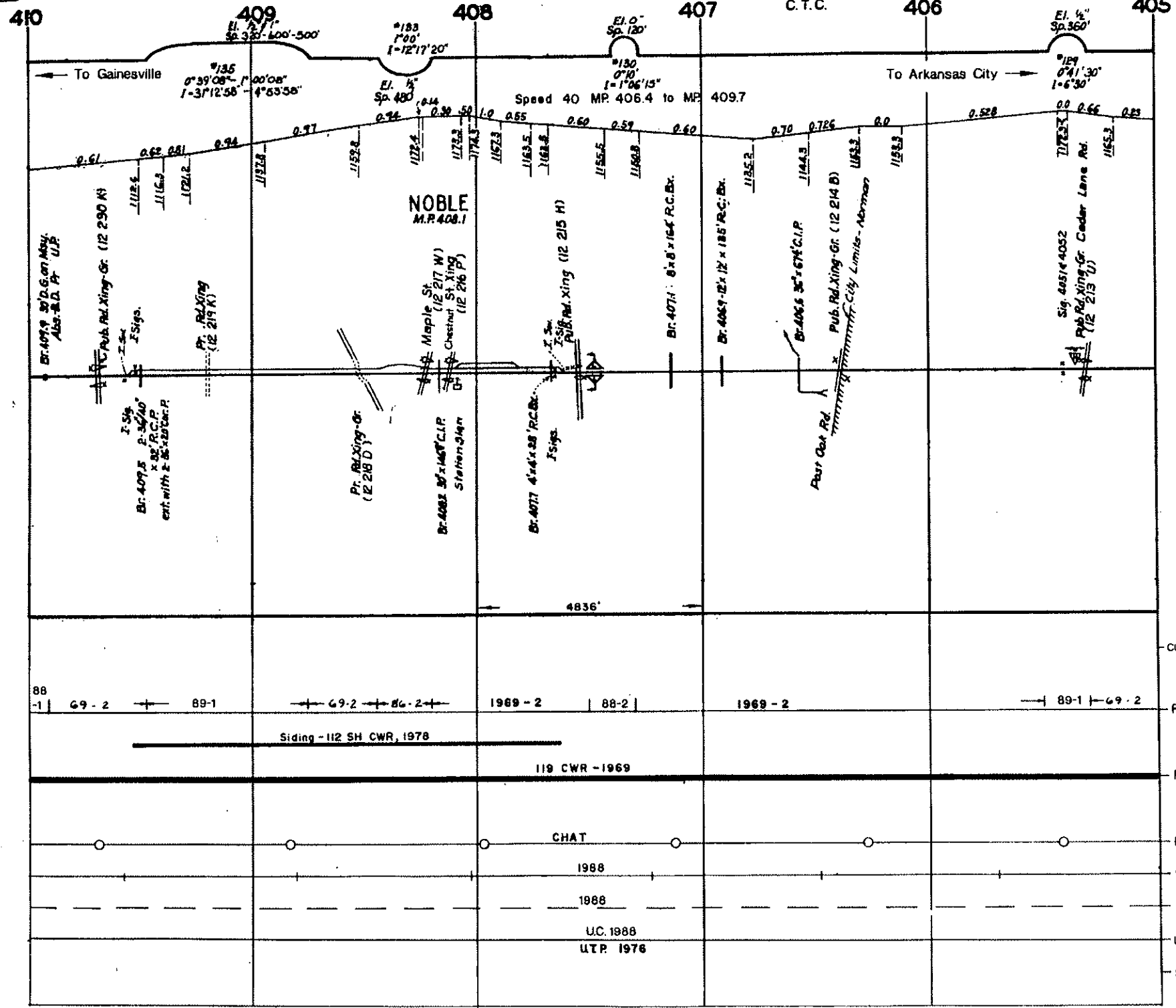
To Gainesville ←

To Arkansas City →



RAIL GR.	89 -2		1969 - 2	89-1	88-1
RAIL			119 CWR - 1969		
BALLAST		SCREENING		CHAT	
SURFACING		1988	1987	1988	
TIES		1985		1988	
UNDERCUTTER			U.T.P. 1976	UC. 1988	
STABILIZATION					

5208.1'



CURVES NR SR

RAIL GR.

RAIL

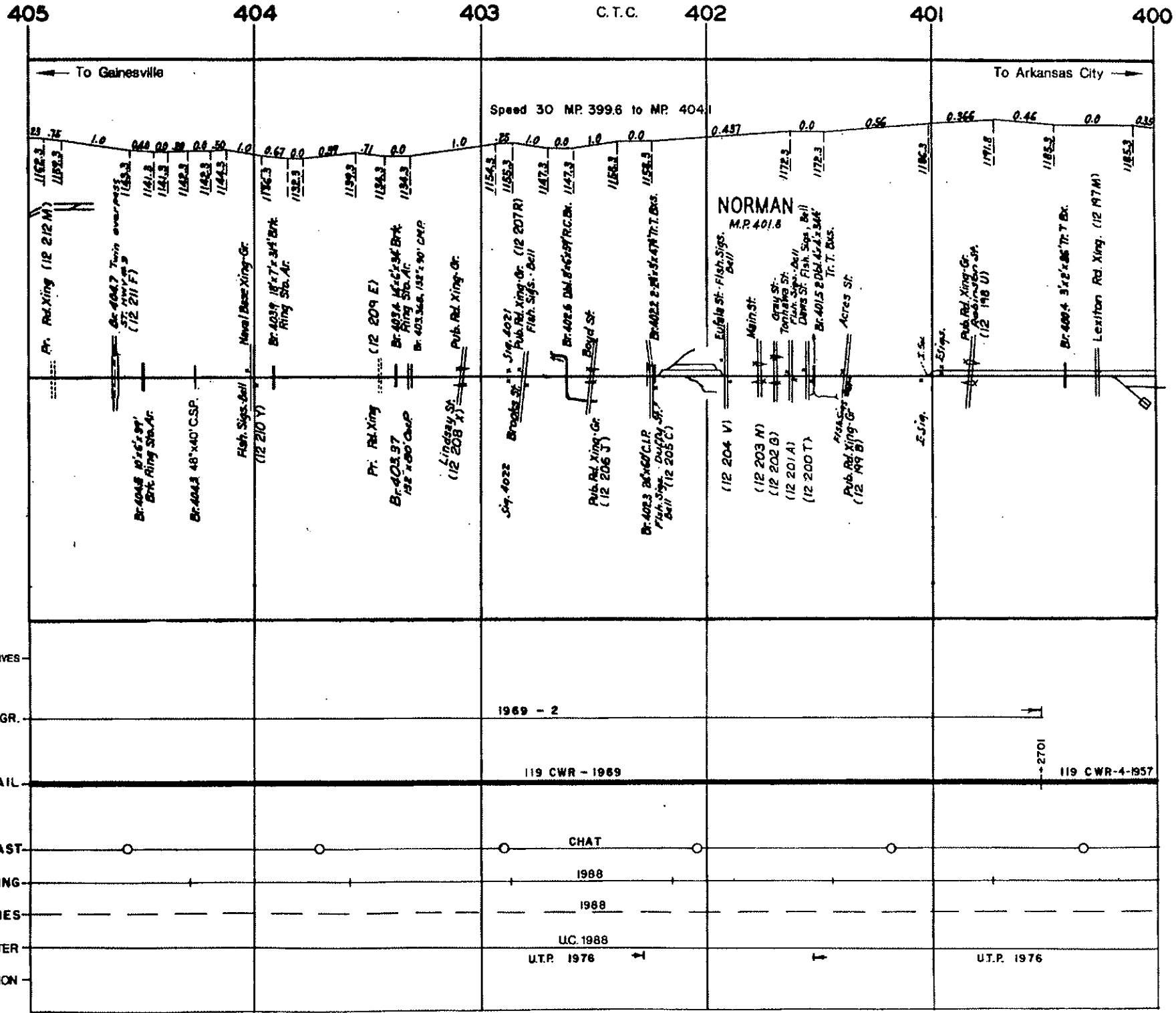
BALLAST

SURFACING

TIES

UNDERCUTTER

STABILIZATION





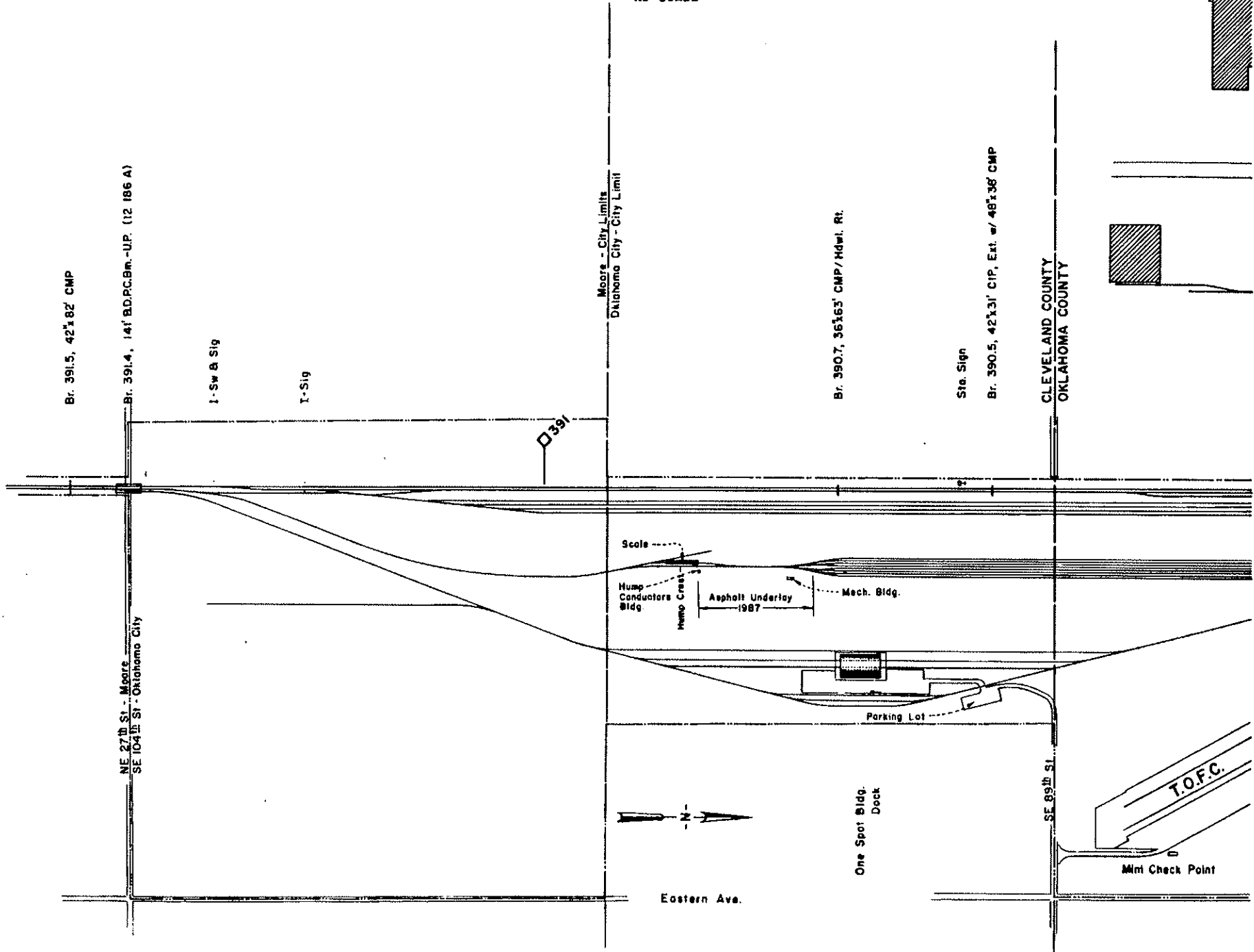


← To Gainesville

To Arkansas City →

# FLYNN YARD

- NO SCALE -



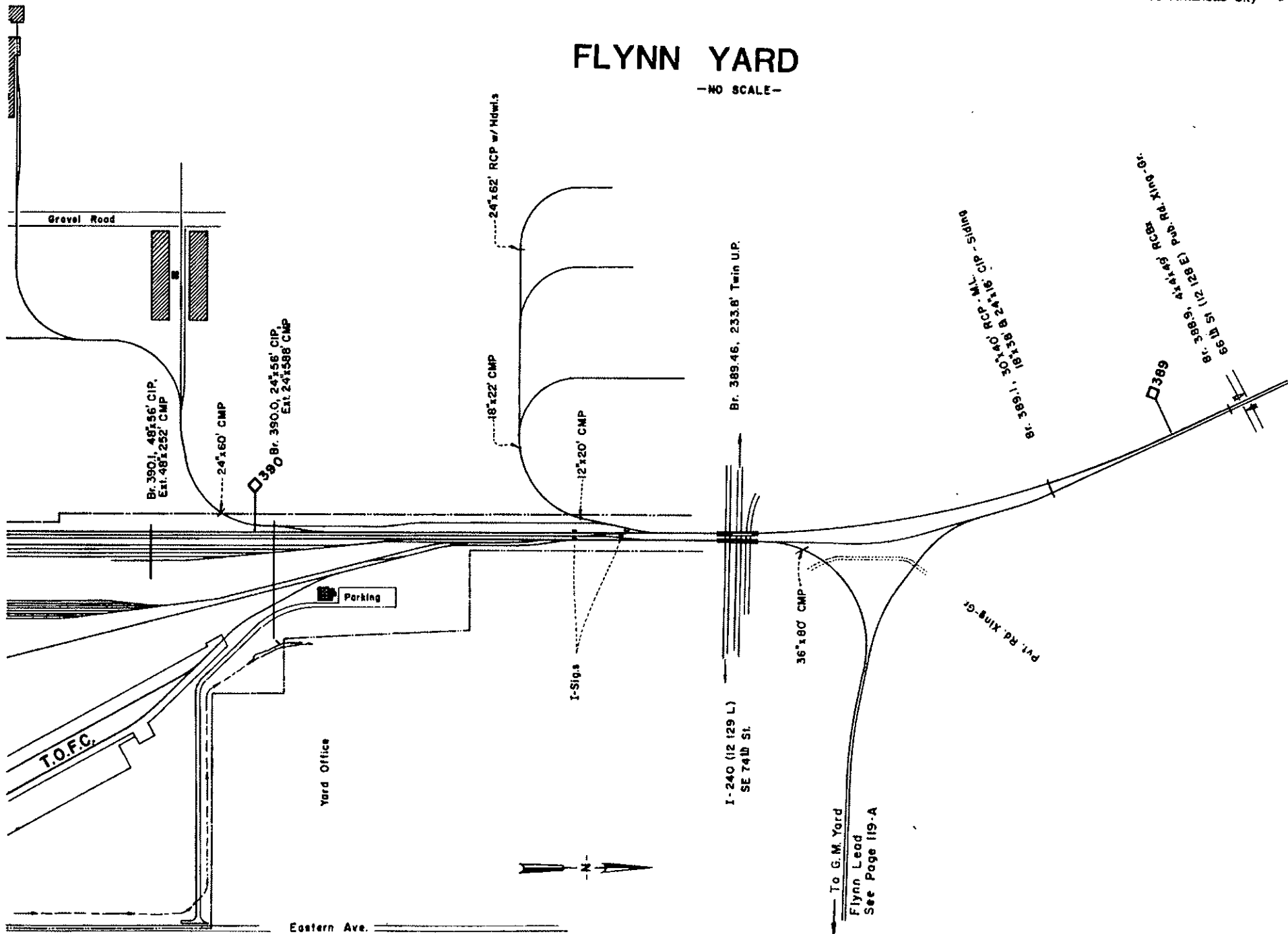


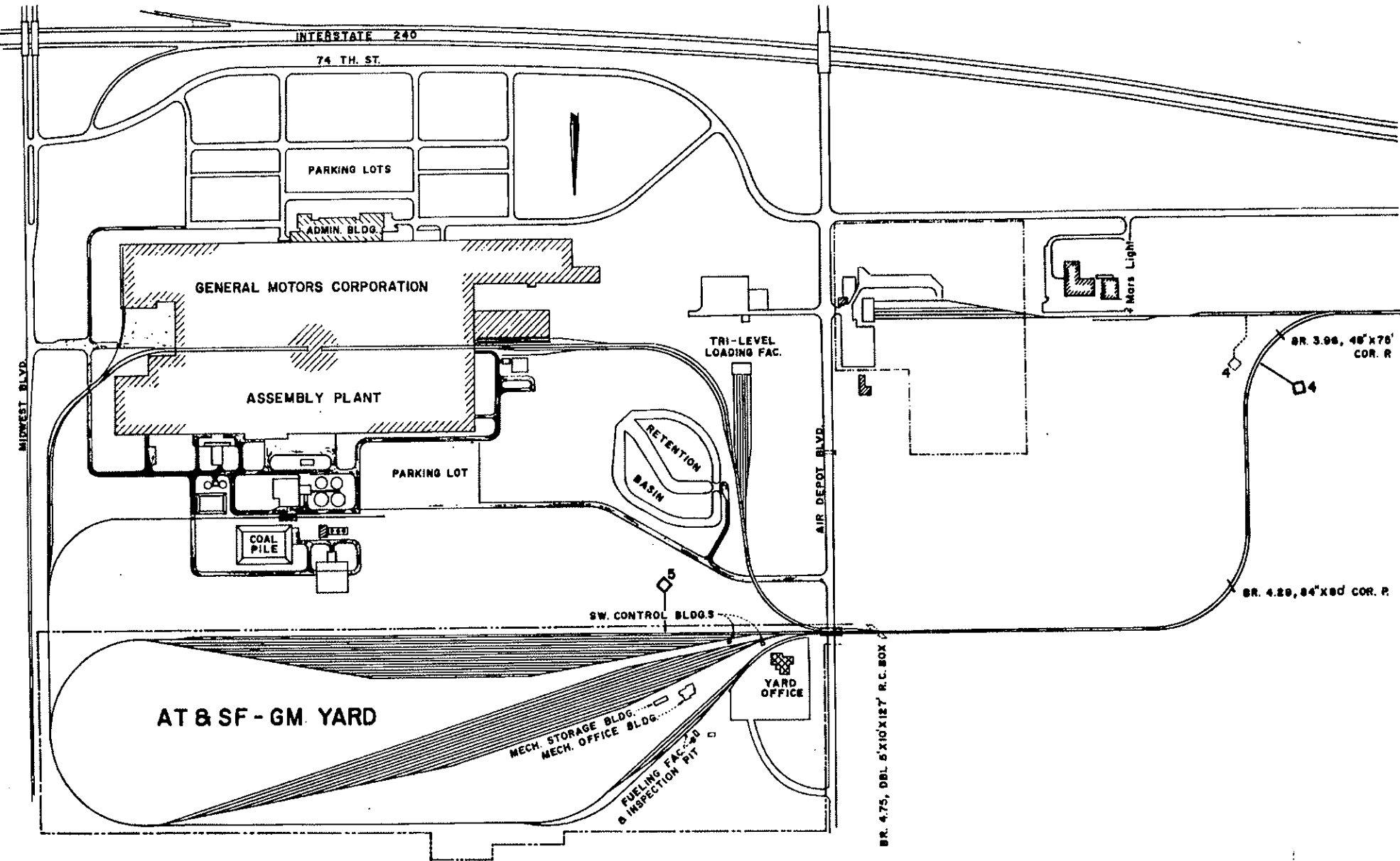
To Gainesville

To Arkansas City

# FLYNN YARD

-NO SCALE-





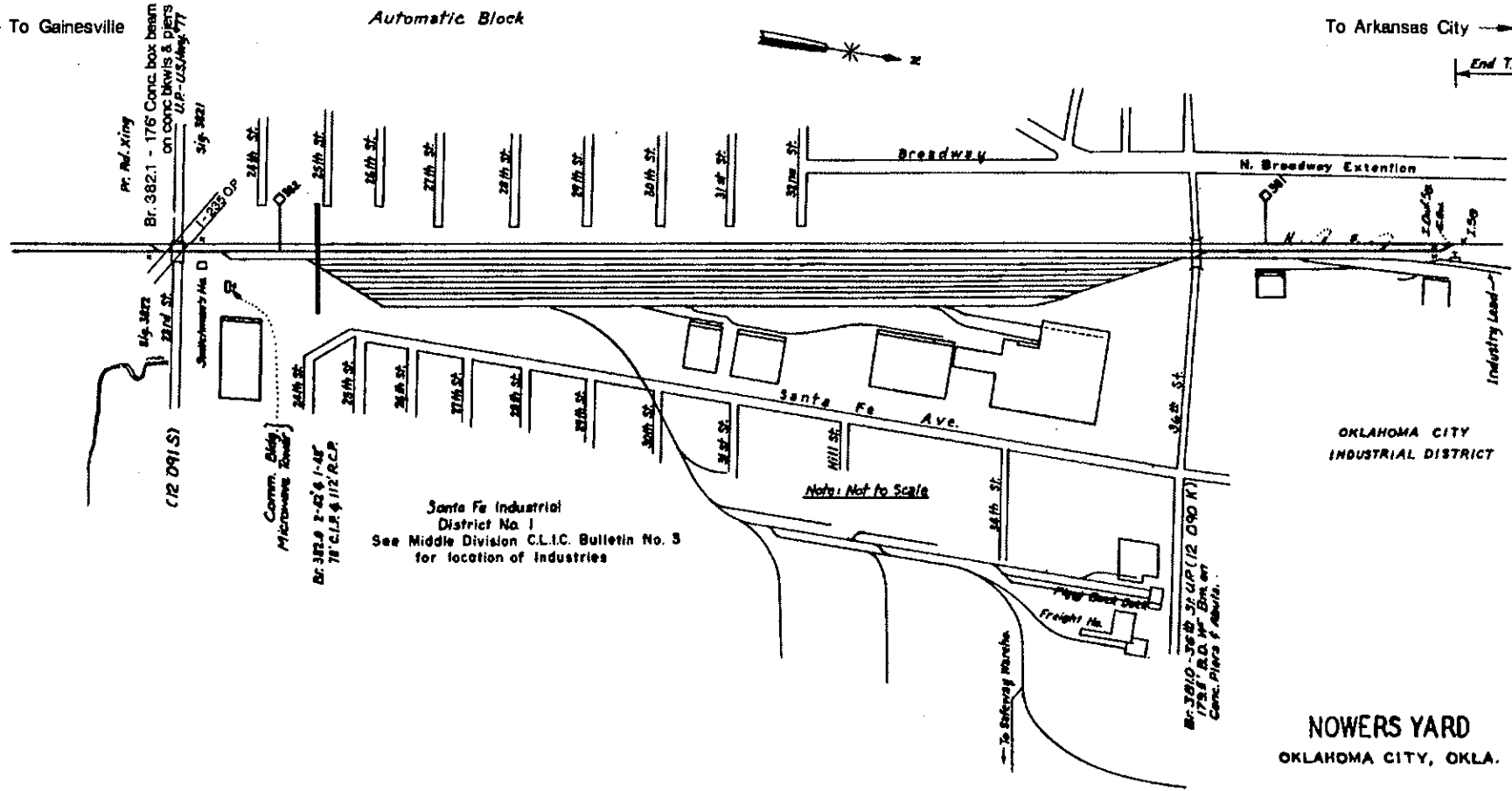




To Gainesville

To Arkansas City

End T.C.S.



Pr. Ref. Xing  
 Br. 3821 - 176' Conc. box beam  
 on conc. bents & piers  
 U.P.-O.S. Hwy. 77  
 Sig. 3821

(12 091 S)

Comm. Dist.  
 Microwaves

Br. 3822 2'-42" x 1'-48"  
 78' C.L.P. & 112' R.C.P.

Santa Fe Industrial  
 District No. 1  
 See Middle Division C.L.I.C. Bulletin No. 3  
 for location of industries

Note: Not to Scale

Br. 3810 - 26'-0" ST. U.P. (12 090 K)  
 175' R.O. w/ 8m. int.  
 Conc. Piers & Abutts.

OKLAHOMA CITY INDUSTRIAL DISTRICT

**NOWERS YARD**  
 OKLAHOMA CITY, OKLA.

385

384

Automatic Block

383

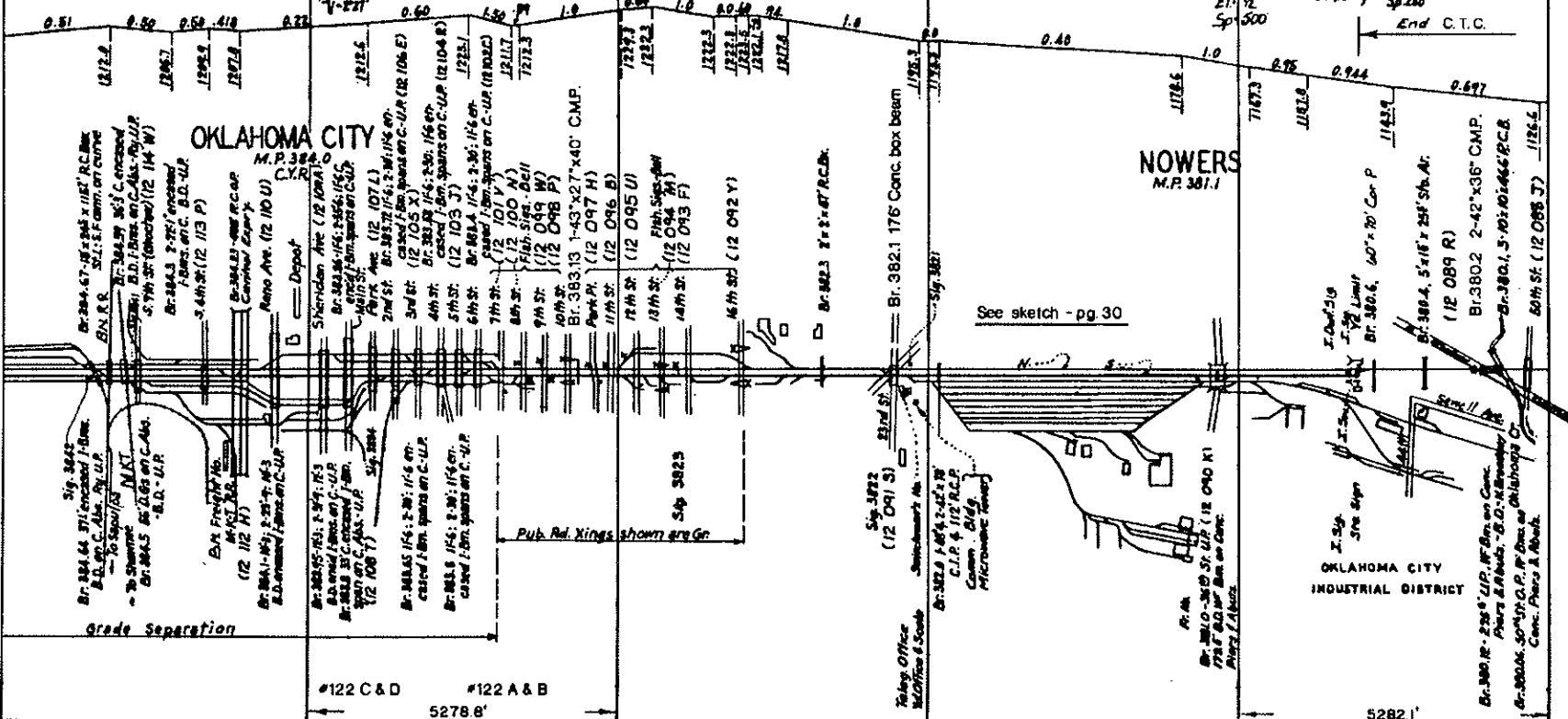
382

381

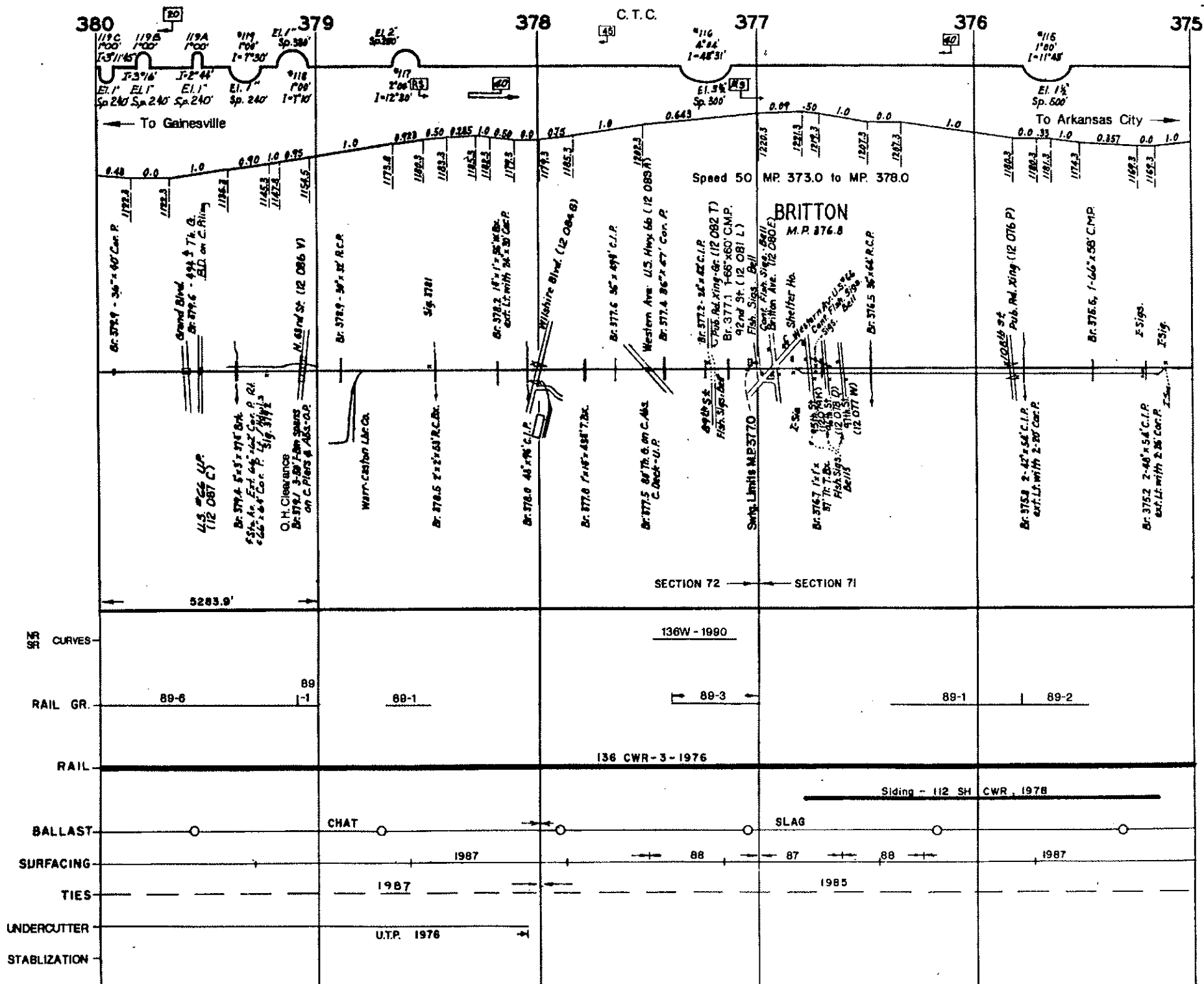
380

To Gainesville

To Arkansas City



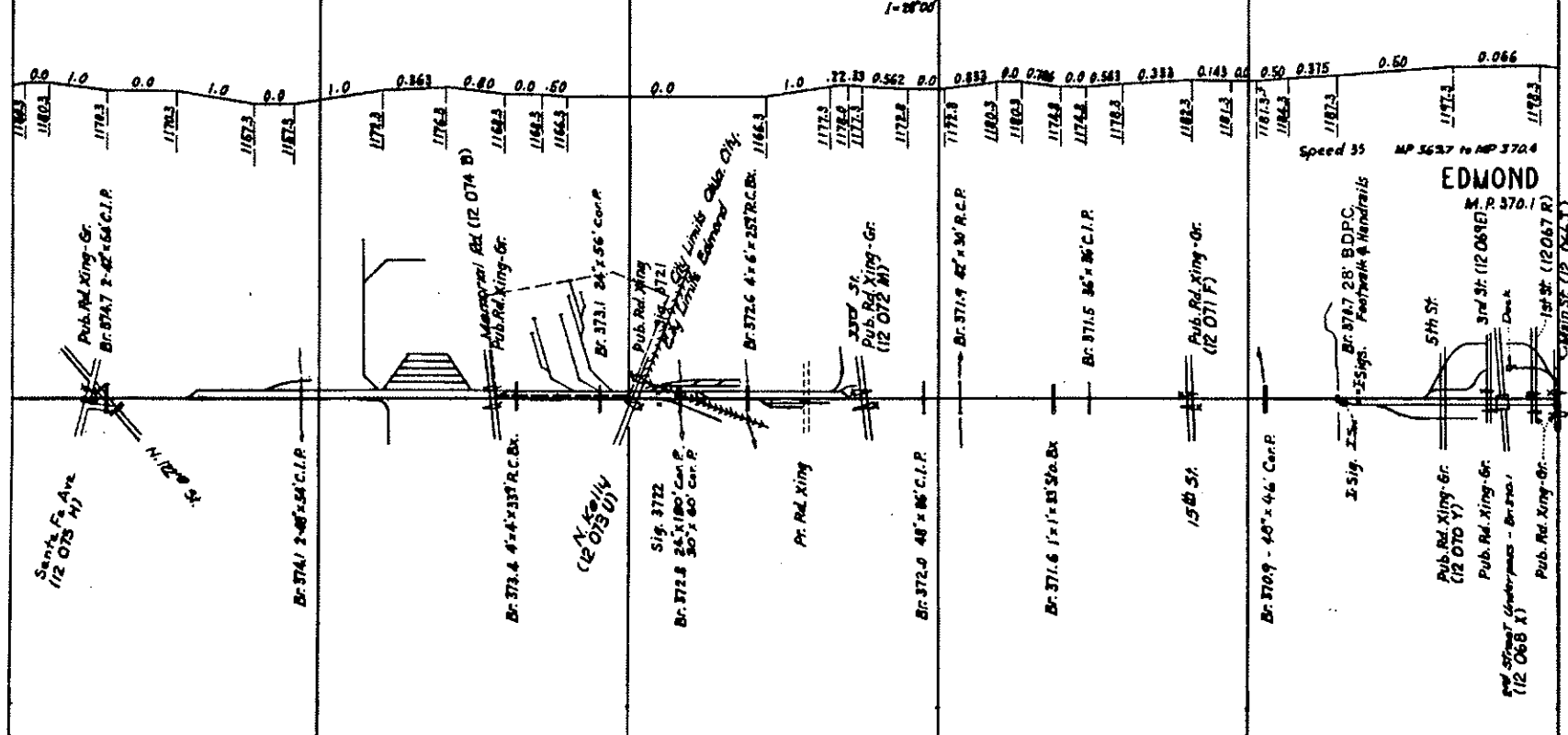
#122 C & D 5278.8'		#122 A & B 5282.1'	
136W-T90 136W-T90 136W-T90 136W-T90	136W-T90 136W-T90 136W-T90 136W-T90		
88 88-3N 88-6N	86 88-3N 88-6N	89-1N 89-1S	88-3N 89-6
89-1 86-3 89-6N	88 88-3N 88-6N	89-1N 89-1S	88-3N 89-6
89-6S 88 4S	88-2S 88 2S		
		136 CWR-7-1977 N 136 CWR-7-1977 S	136 CWR-3-1976
		SLAG N SLAG S	SLAG
1987	1988	1987 N SL 1988 S	1987
78		1977	78
			UTP '76



375 374 373 C.T.C. 372 371 370

To Gainesville

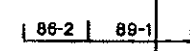
To Arkansas City



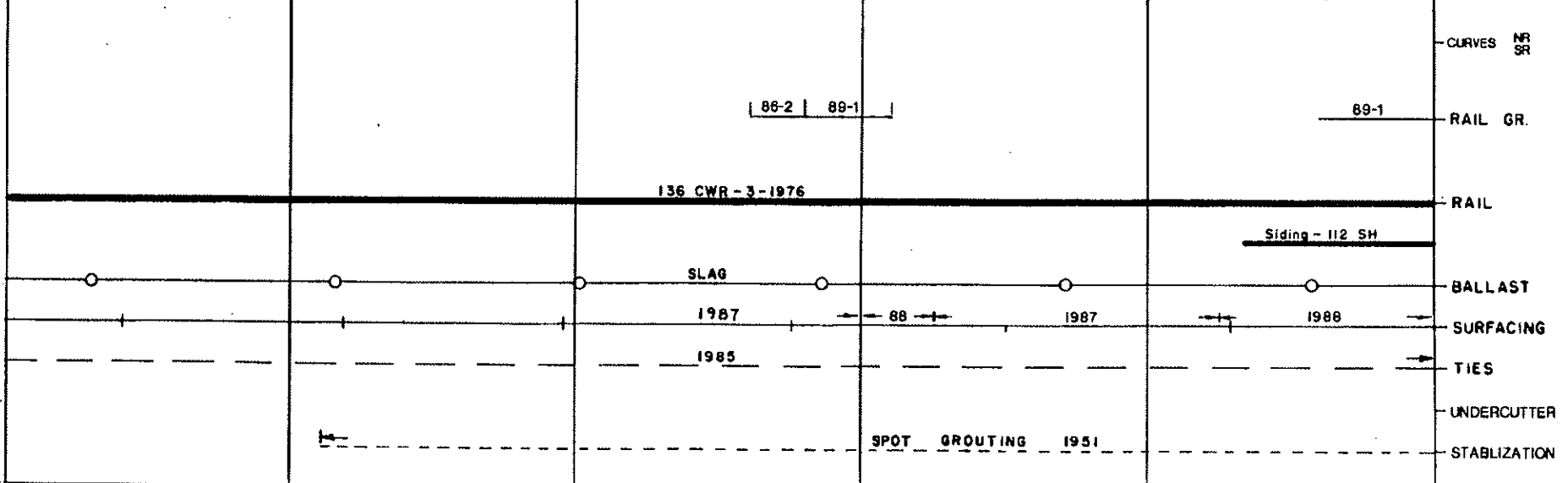
E1.31 Sp. 610  
114.2' 04'  
1-28' 00"

Speed 35 MP 3637 to MP 3704

EDMOND  
M.P. 370.1



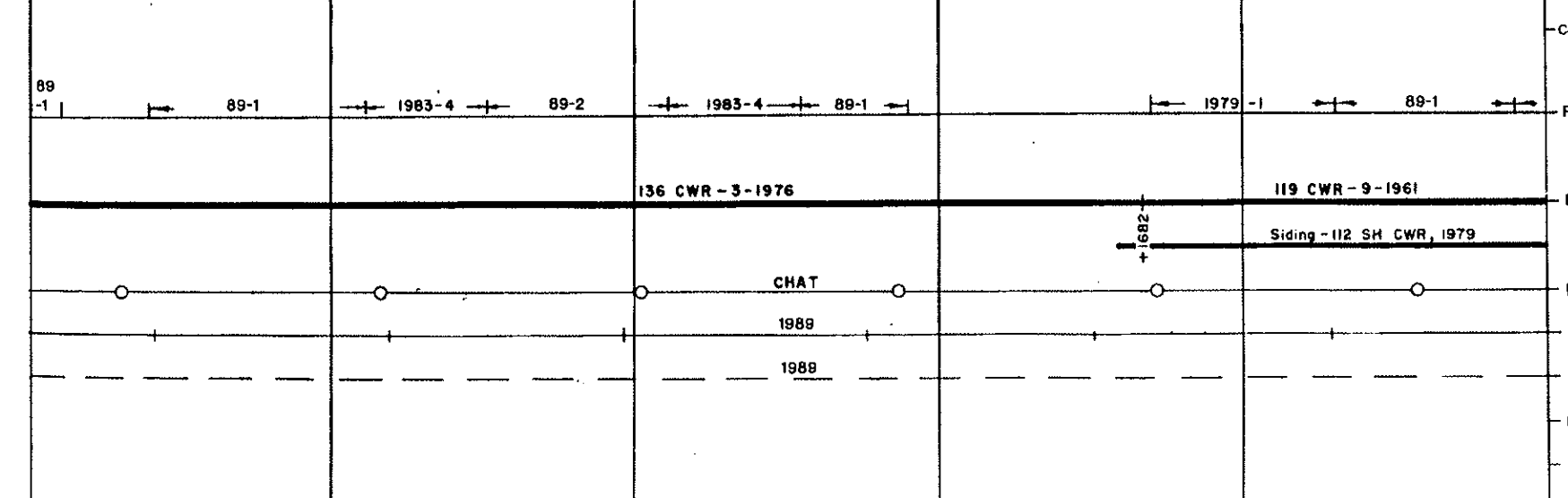
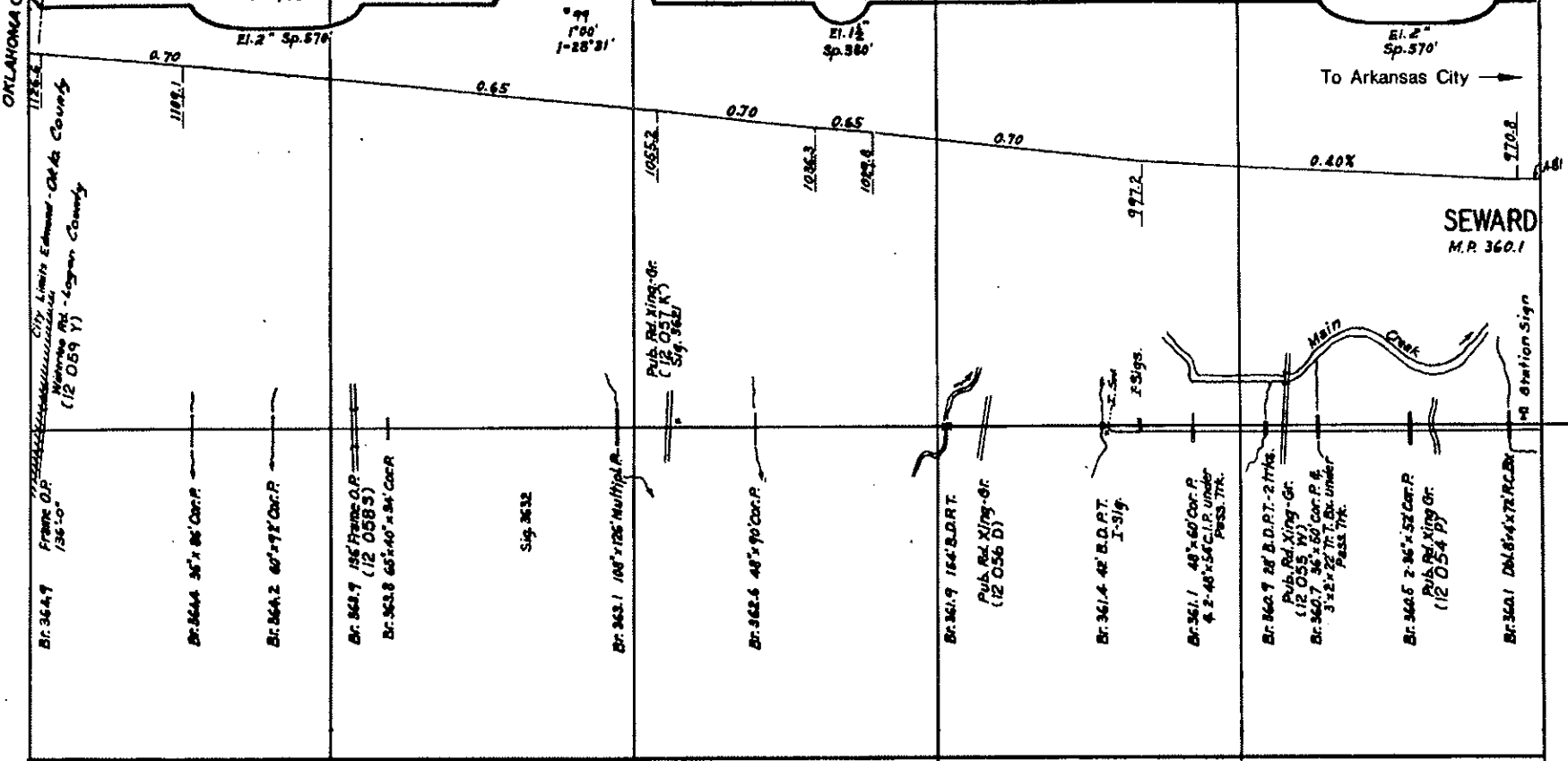
136 CWR - 3 - 1976



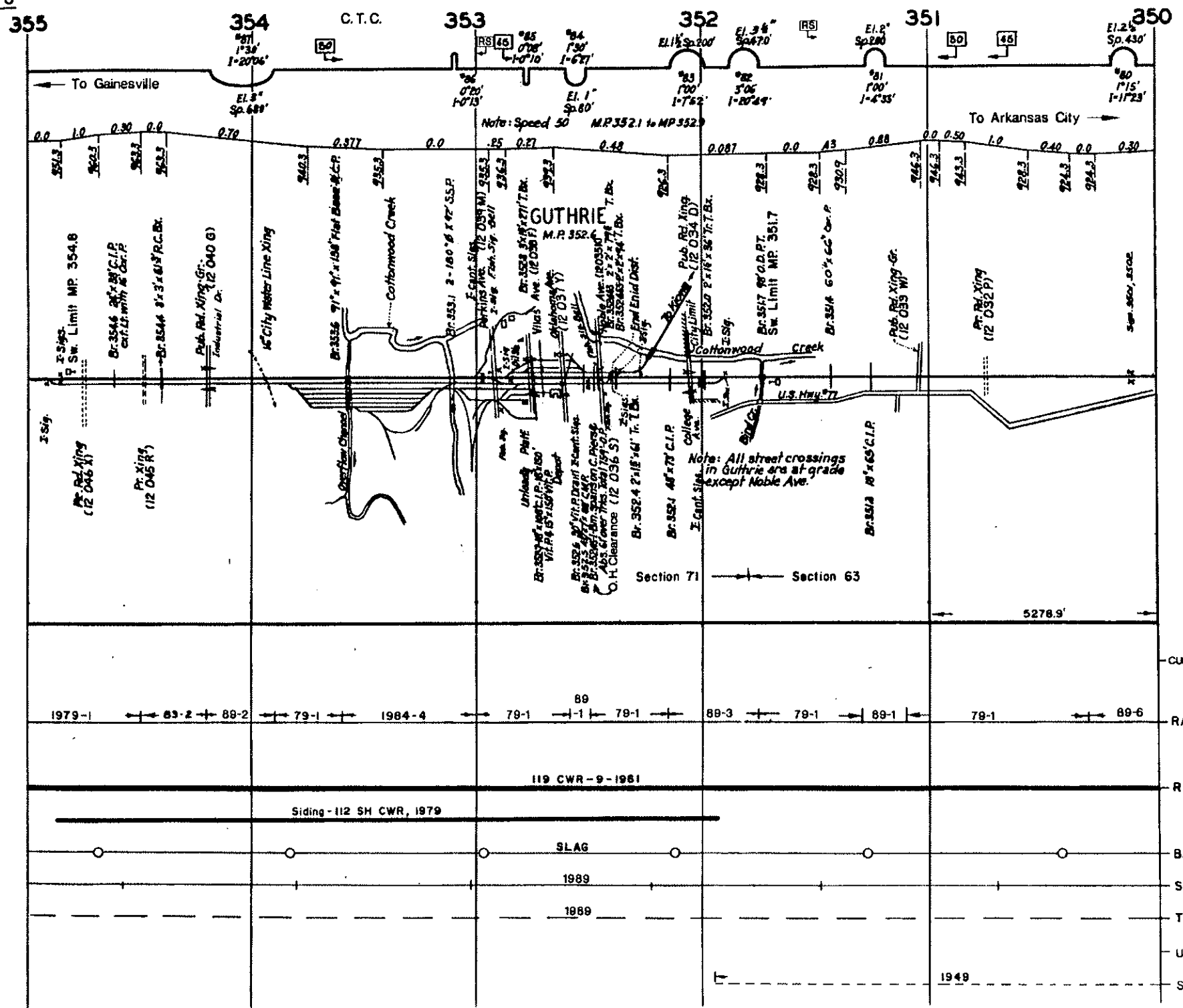




365 LOGAN CO. To Gainesville  
 364  
 363 C.T.C. 362  
 361  
 360 To Arkansas City







C.T.C.

Note: Speed 50 M.P. 352.1 to MP 352.9

To Arkansas City

To Gainesville

GUTHRIE

Note: All street crossings in Guthrie are at grade except Noble Ave.

Section 71      Section 63

CURVES 915

RAIL GR.

RAIL

BALLAST

SURFACING

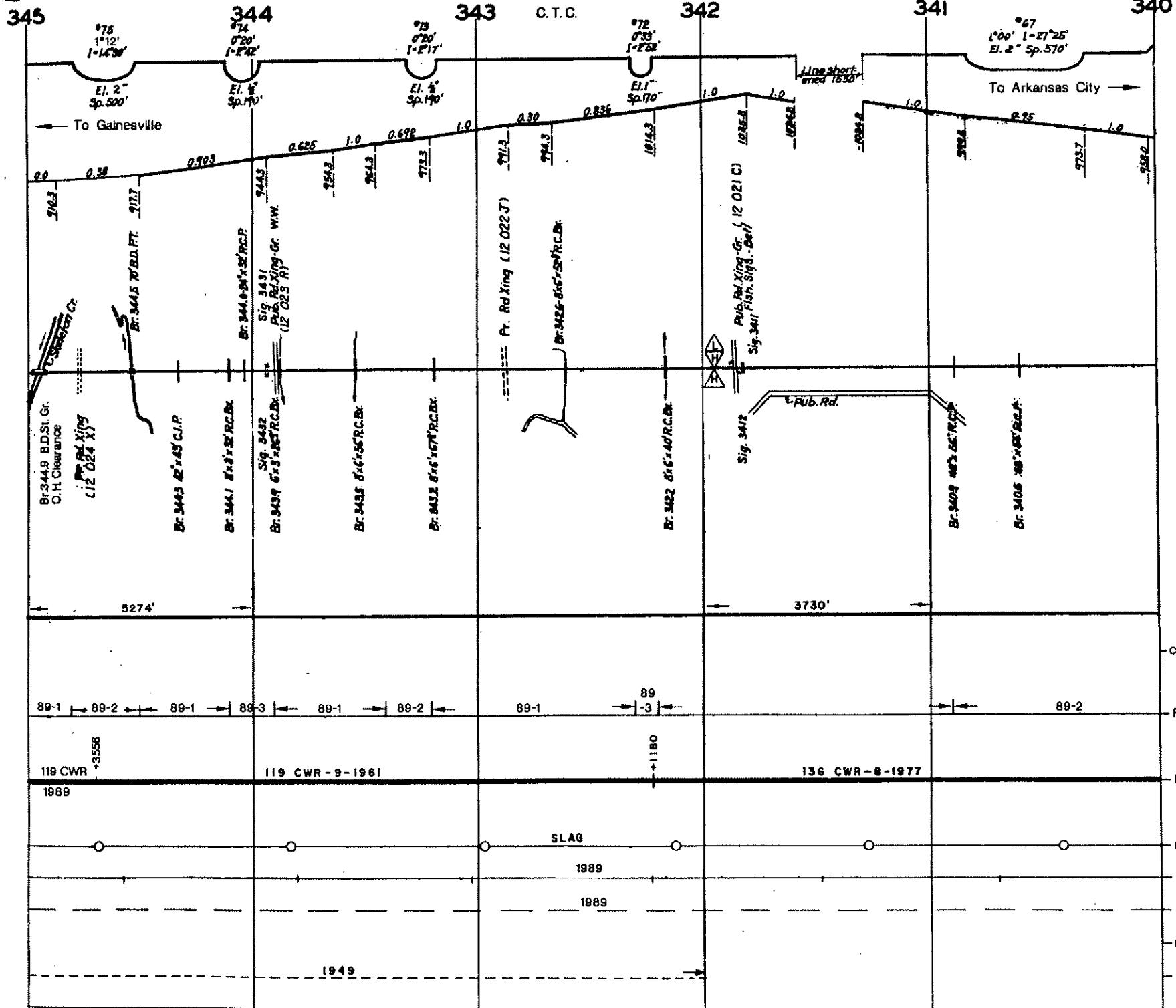
TIES

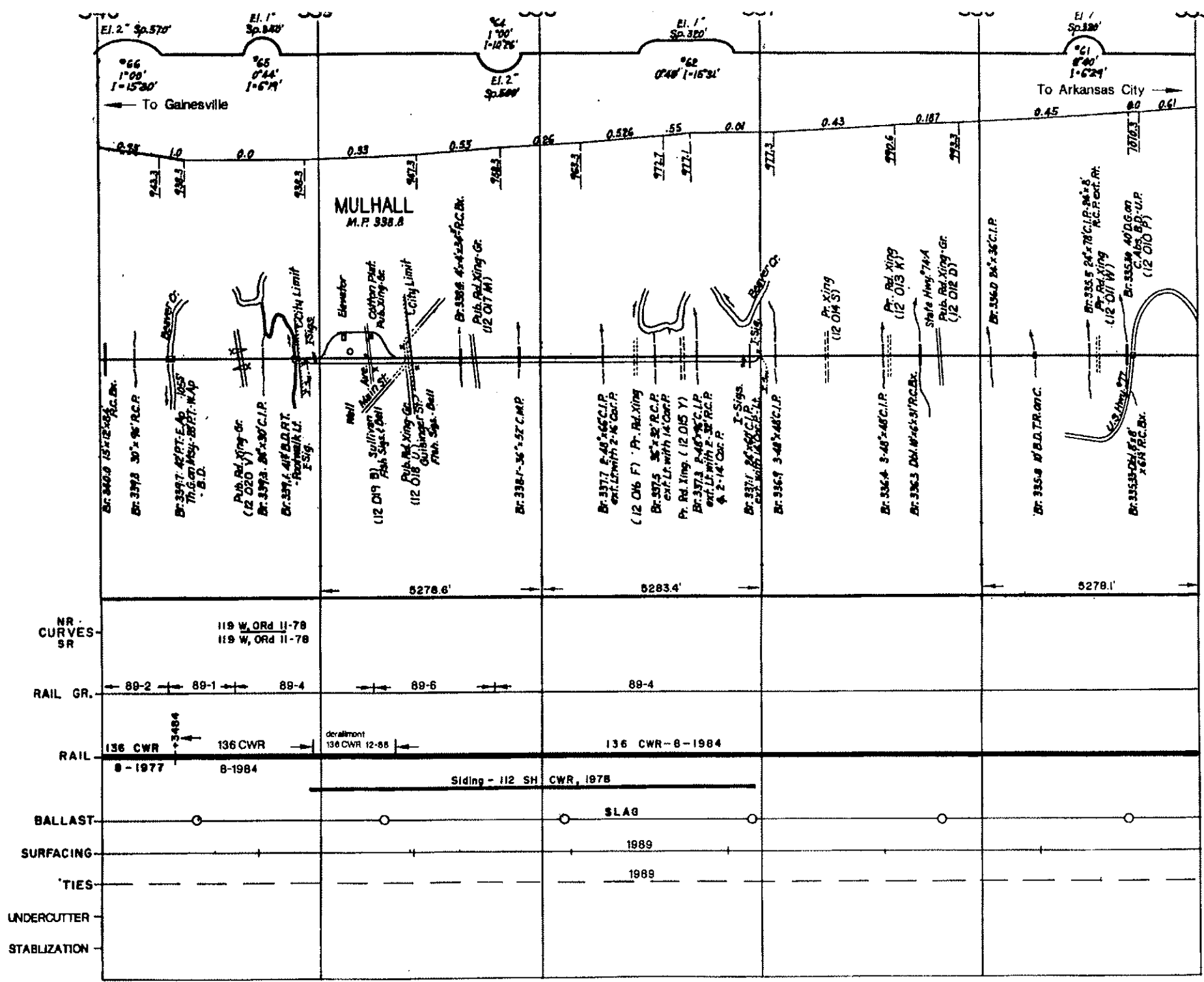
UNDERCUTTER

STABILIZATION

1949







335

334

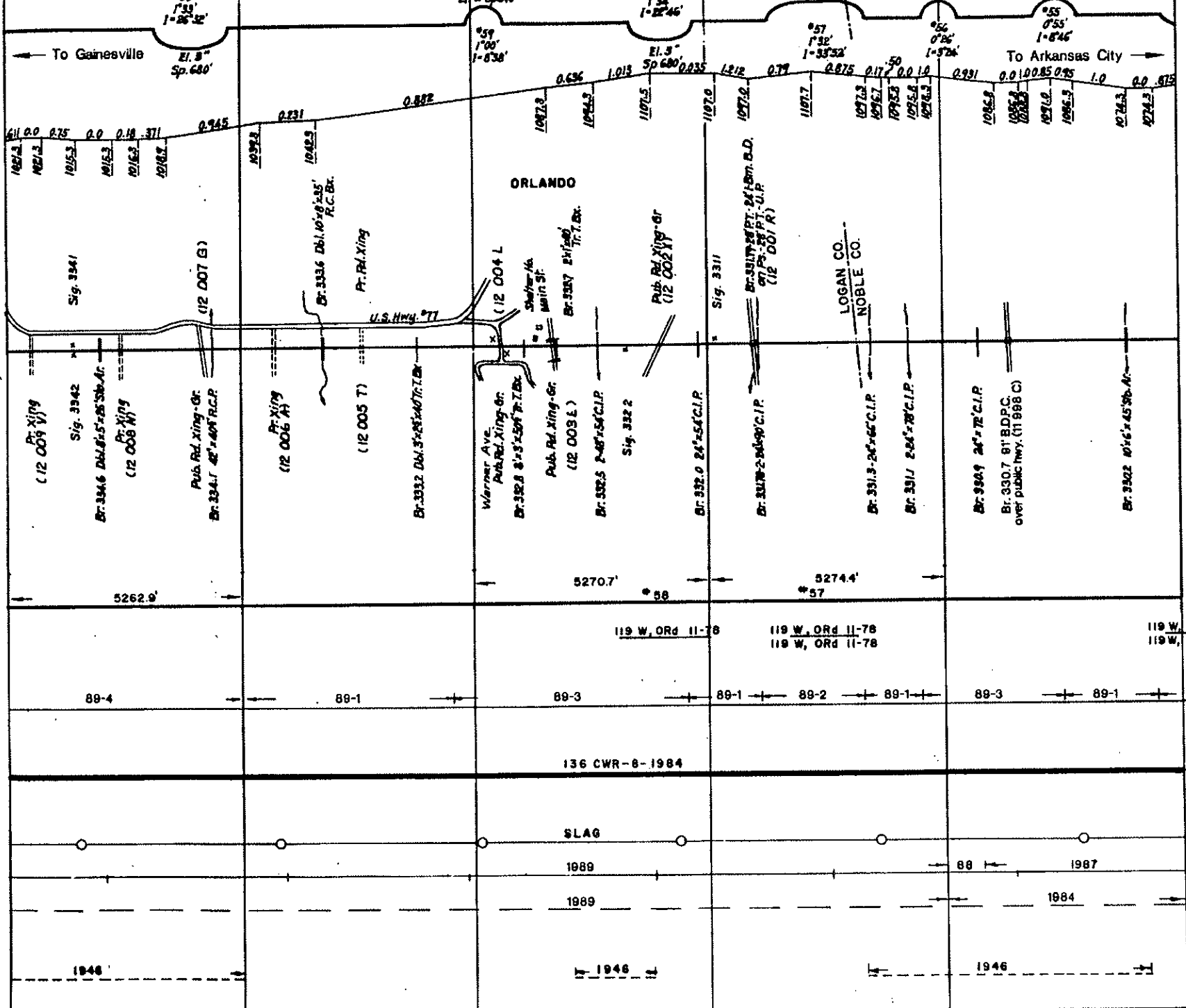
333

C.T.C.

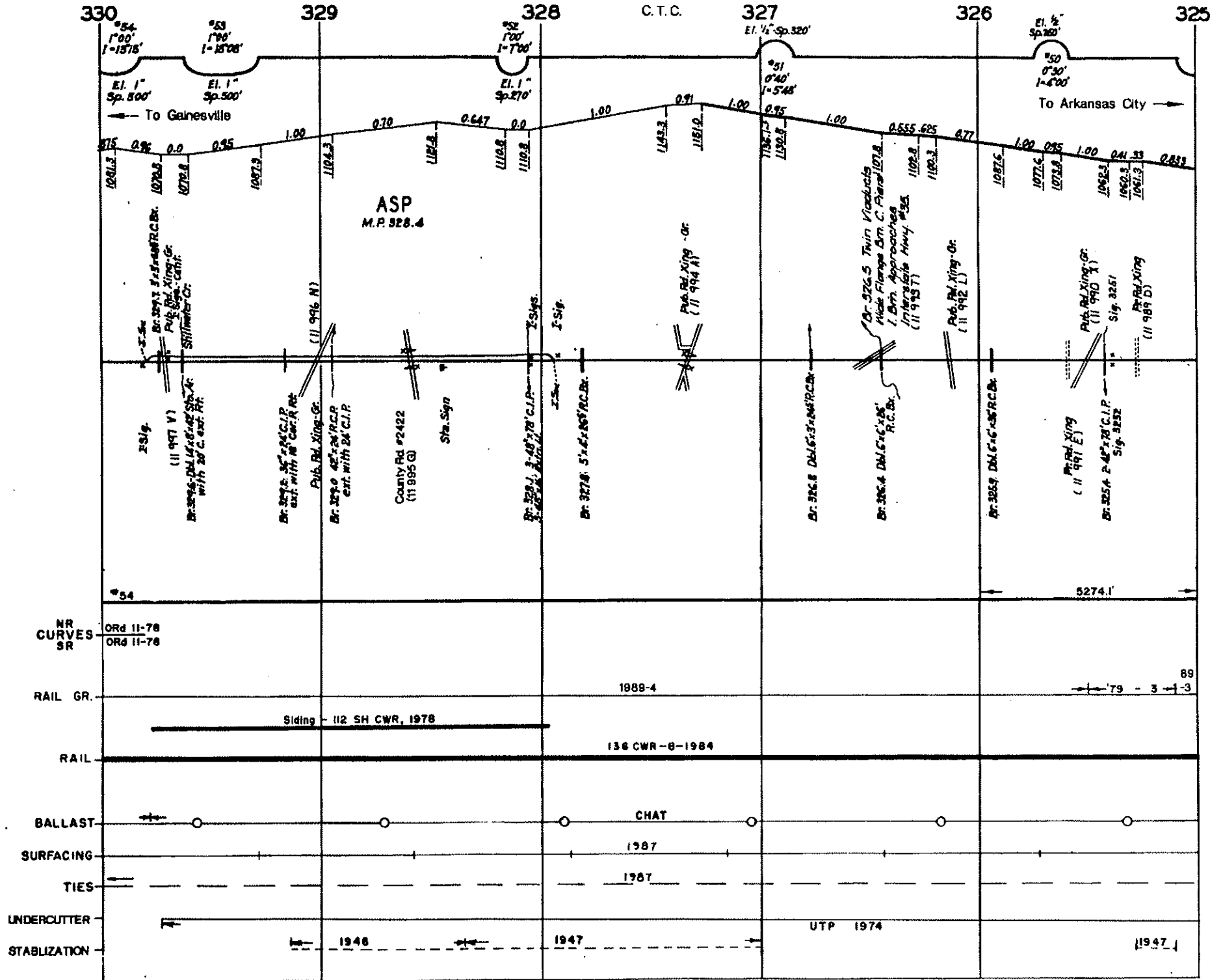
332

331

330







ASP  
M.P. 328.4

NR CURVES  
SR

RAIL GR.

RAIL

BALLAST

SURFACING

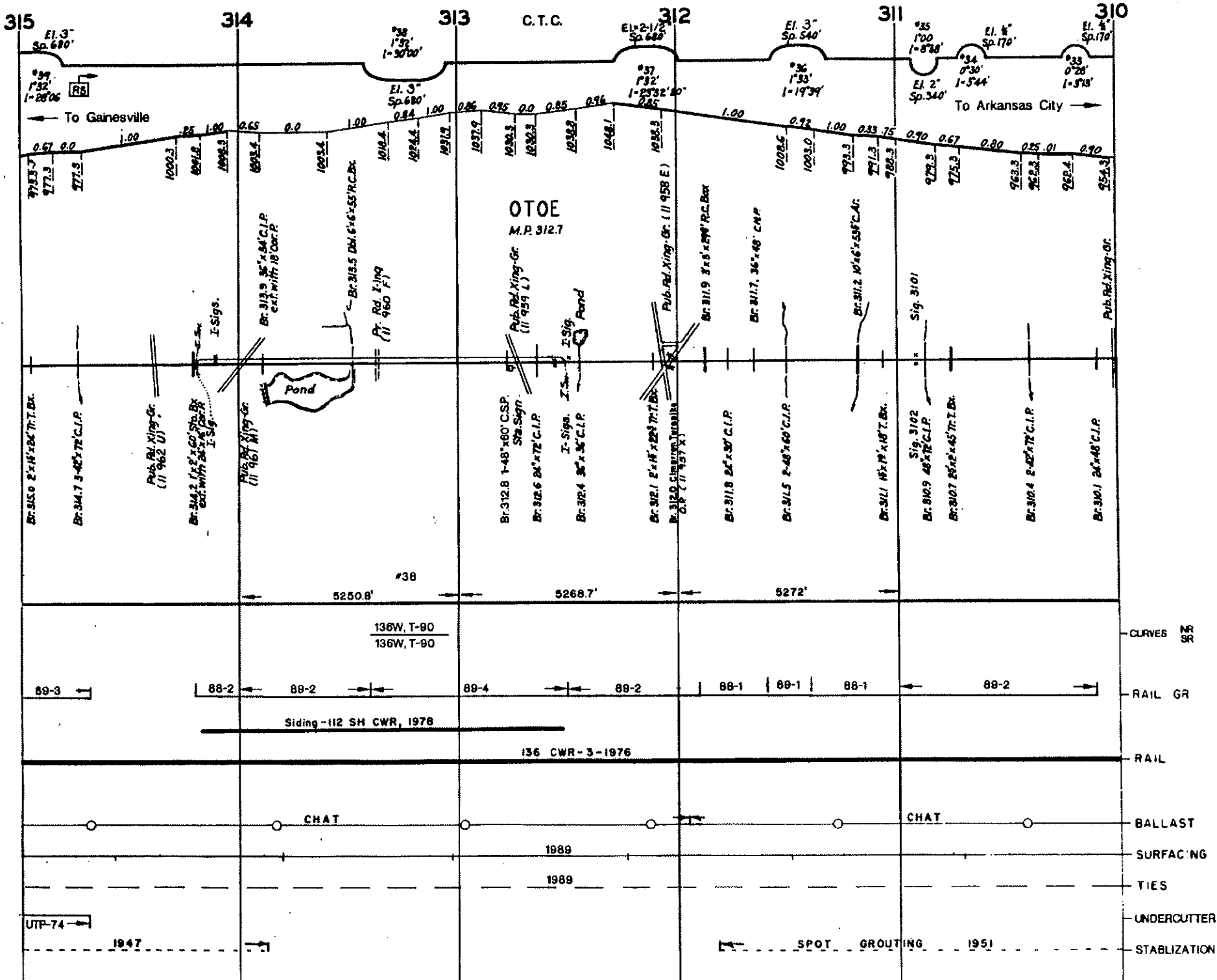
TIES

UNDERCUTTER

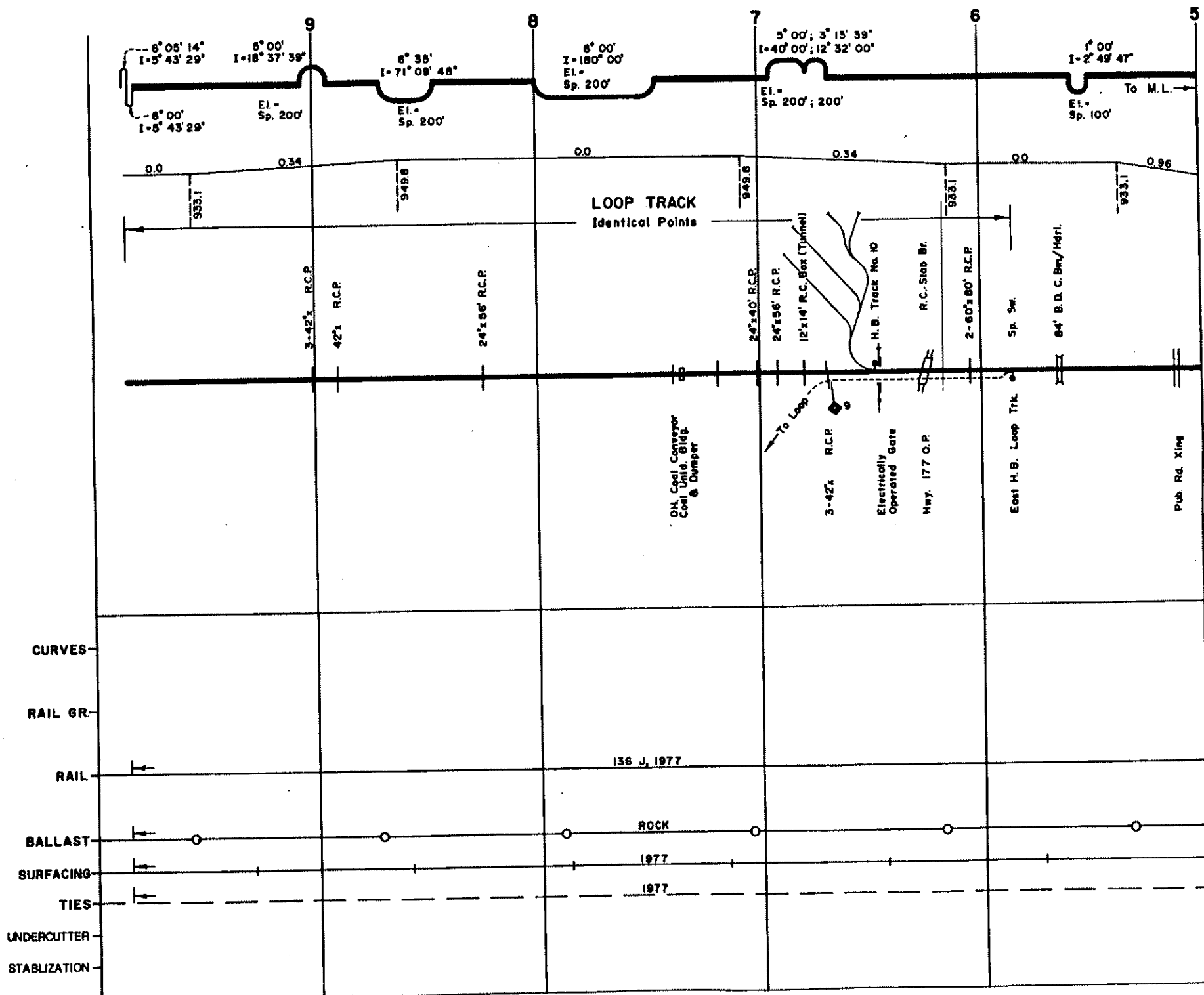
STABILIZATION







- CURVES NR SR
- RAIL GR
- RAIL
- BALLAST
- SURFACING
- TIES
- UNDERCUTTER
- STABLIZATION



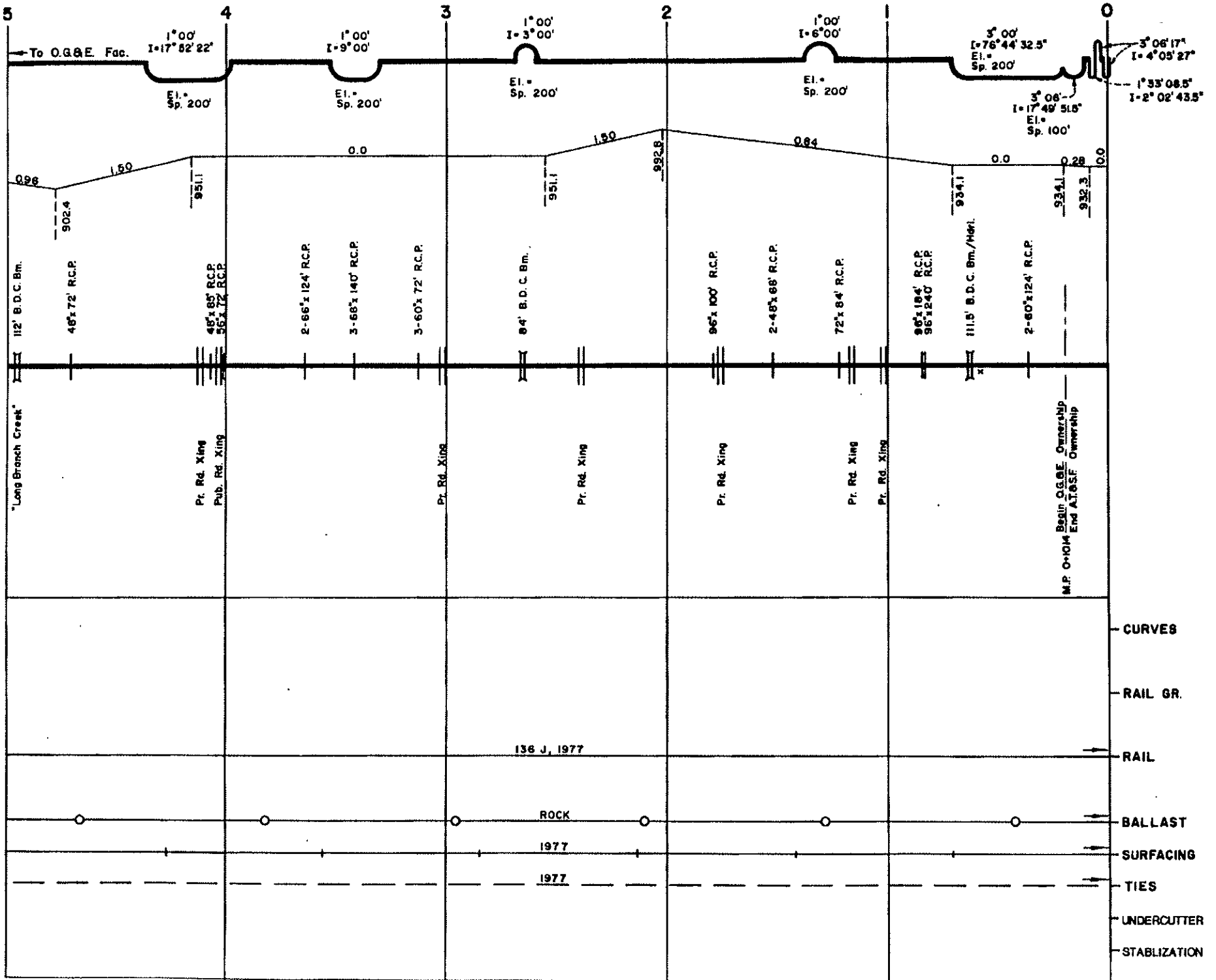
136 J, 1977

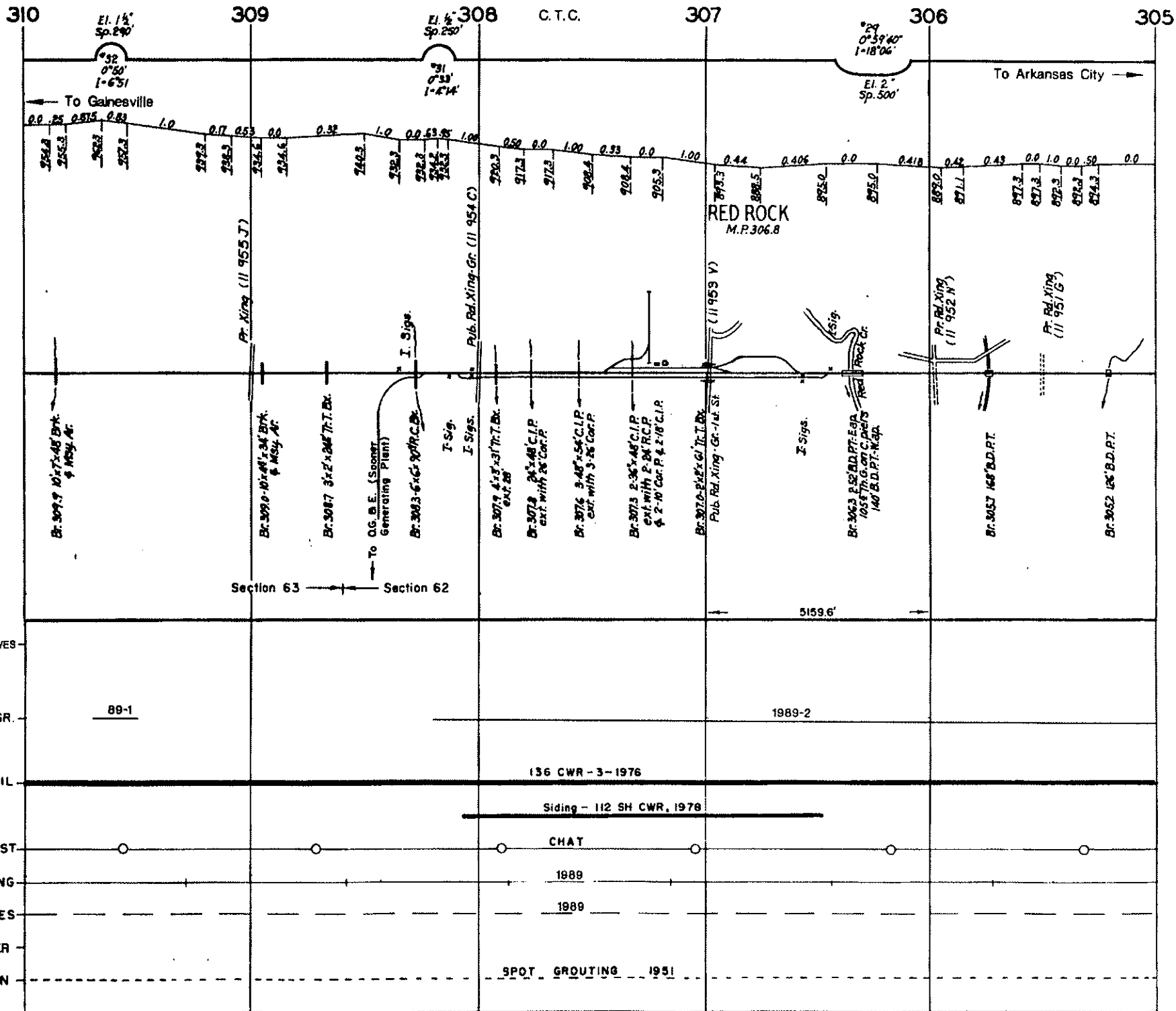
ROCK

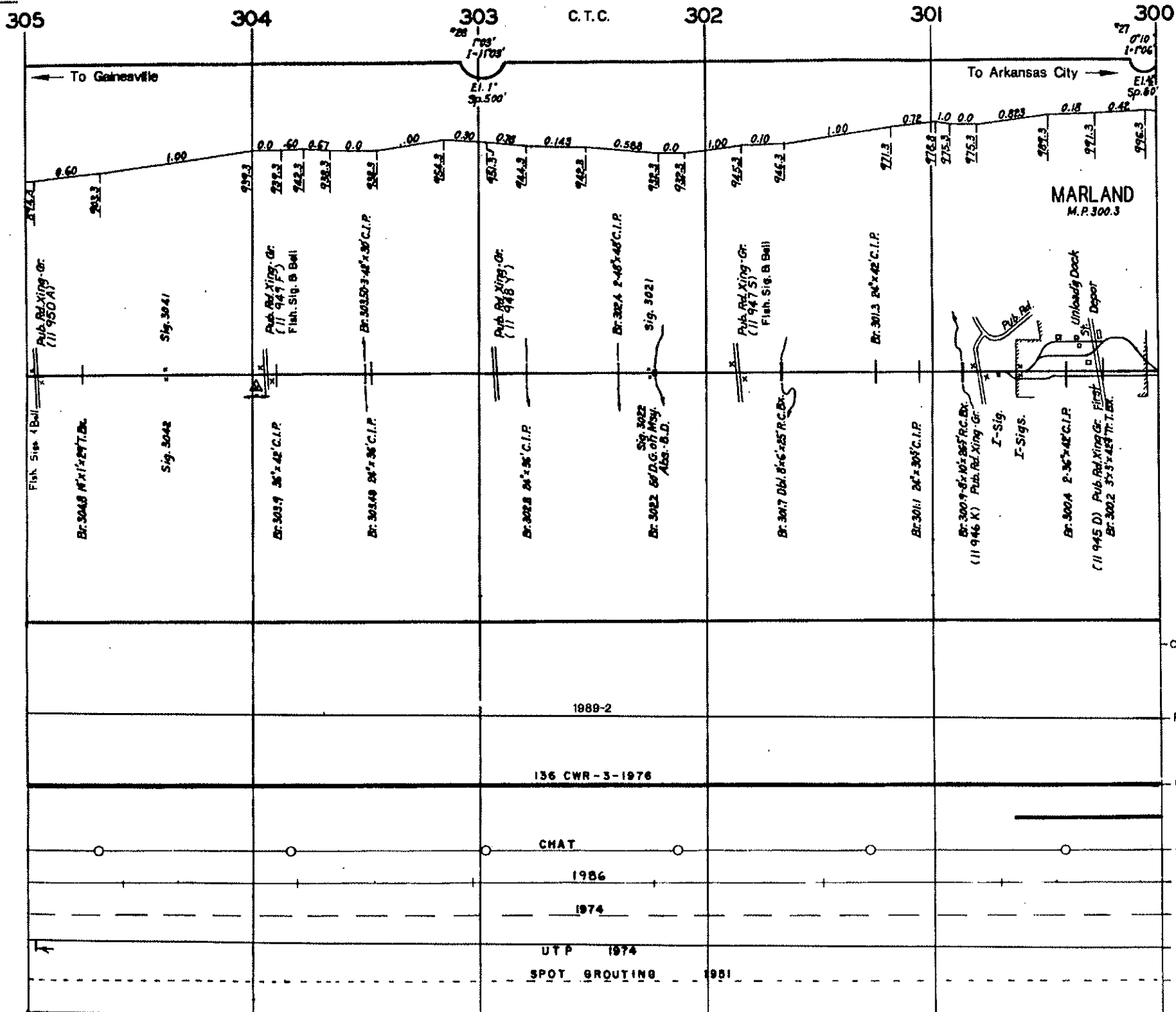
1977

1977

O.G.&E. (Sooner Generating Plant) Lead Track







CURVES 15 SR

RAIL GR.

RAIL

BALLAST

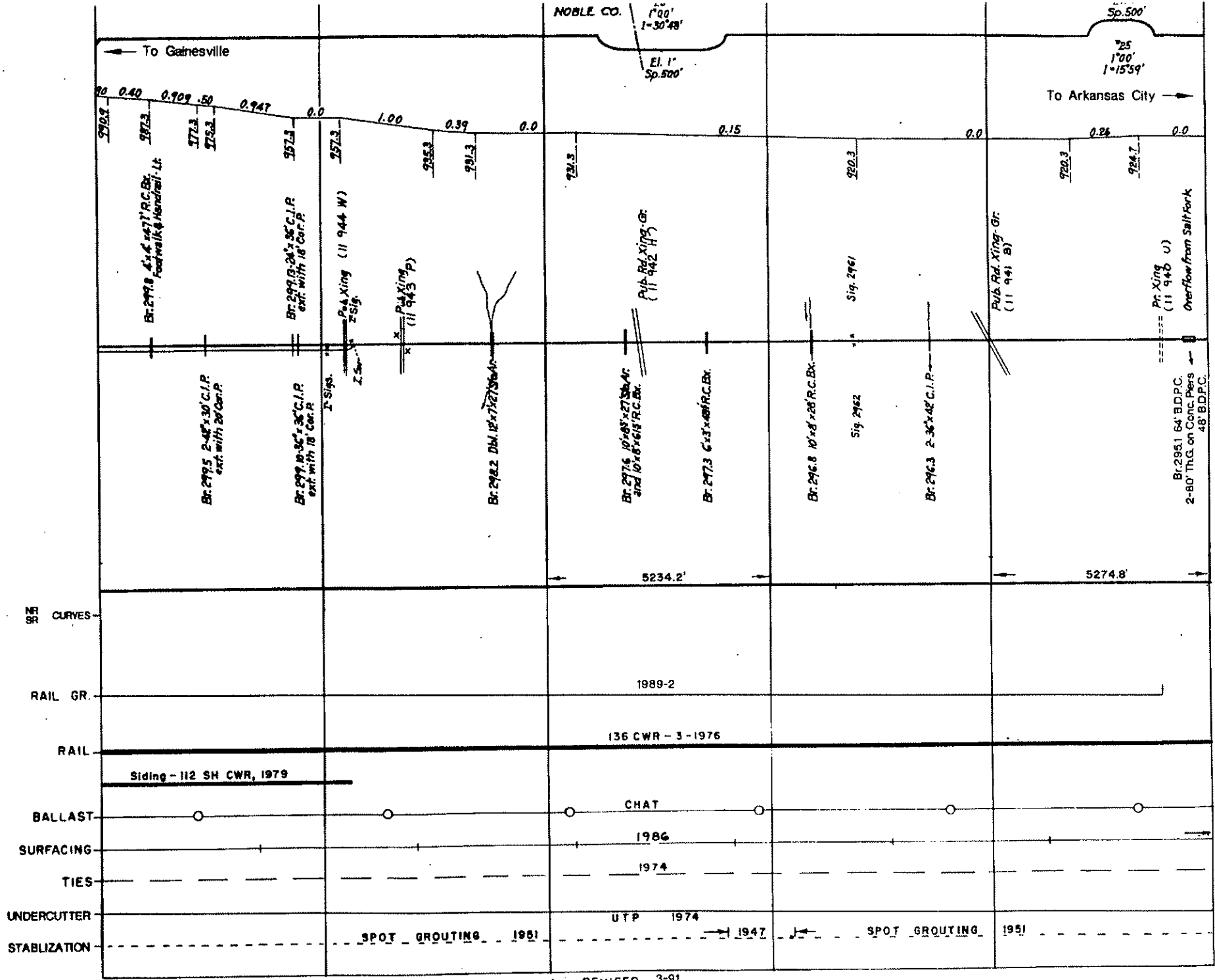
SURFACING

TIES

UNDERCUTTER

STABILIZATION





295

294

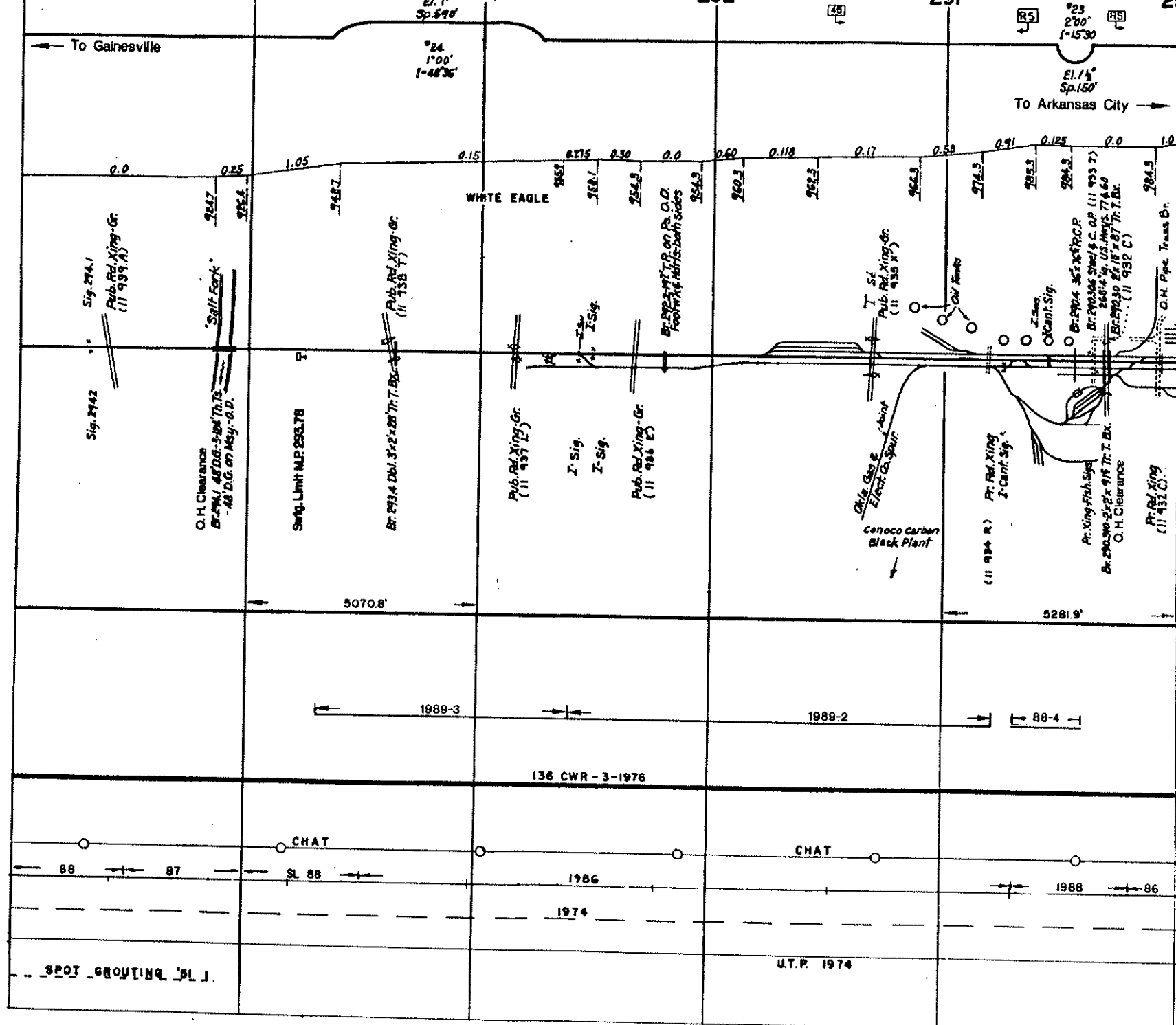
C.T.C.

293

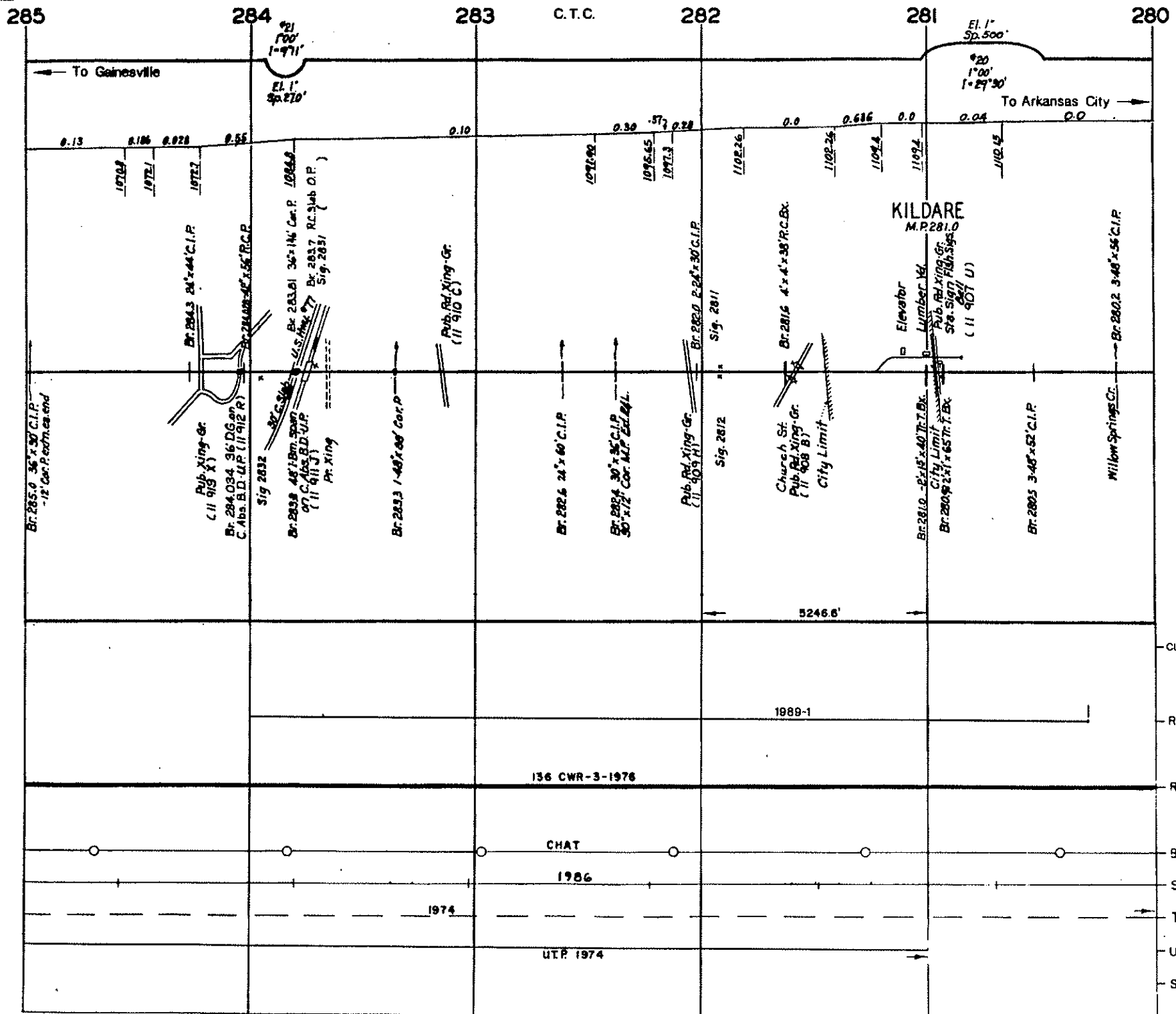
292

291

290







CURVES NR SR

RAIL GR.

RAIL

BALLAST

SURFACING

TIES

UNDERCUTTER

STABILIZATION

136 CWR-3-1976

CHAT

1986

1974

UTP 1974

1989-1

5246.6'

KILDARE  
M.P. 281.0

Church St  
Pub. Rd. Xing. Gr.  
(11 908 B)  
City Limit

Elevator  
Lumber Yd.  
Pub. Rd. Xing. Gr.  
Sta. Sign High Sign  
(11 907 U)

Willow Springs Cr. --- Br. 280.2 3-48' x 56' C.I.P.

Br. 280.5 3-48' x 52' C.I.P.

Br. 281.0 2-15' x 40' T.C. Box  
City Limit  
Br. 280.9 2-11' x 45' T.C. Box

Br. 281.6 4' x 4' x 38' R.C. Box

Pub. Rd. Xing. Gr.  
(11 909 H) --- Br. 282.0 2-24' x 30' C.I.P.

Br. 282.6 30' x 30' C.I.P.  
30' x 12' Cor. M.P. 282.4 L

Br. 282.6 24' x 60' C.I.P.

Pub. Rd. Xing. Gr.  
(11 910 C)

Br. 283.3 1-48' x 60' Cor.P

Br. 283.8 45' Lbm. Span  
C.I.P. 403 B.D. U.P.  
(11 911 J)  
Pt. Xing

Br. 284.3 24' x 44' C.I.P.  
C. Abs. S.D. U.P. (11 912 R)  
Sig. 283.2

Pub. Xing. Gr.  
(11 913 X)  
Br. 284.0 34' DG. 40'  
C. Abs. S.D. U.P. (11 912 R)

1972.9  
1972.1  
1972.7

0.13 0.106 0.028 0.55

0.30 0.57 0.28

0.0 0.636 0.0

0.04 0.0

285 284 283 C.T.C. 282 281 280

EL. 1' Sp. 500'  
920  
1'00'  
1-29'30"

921  
1'00'  
1-47'1"

To Gainesville

To Arkansas City

280

279

278

C.T.C.

277

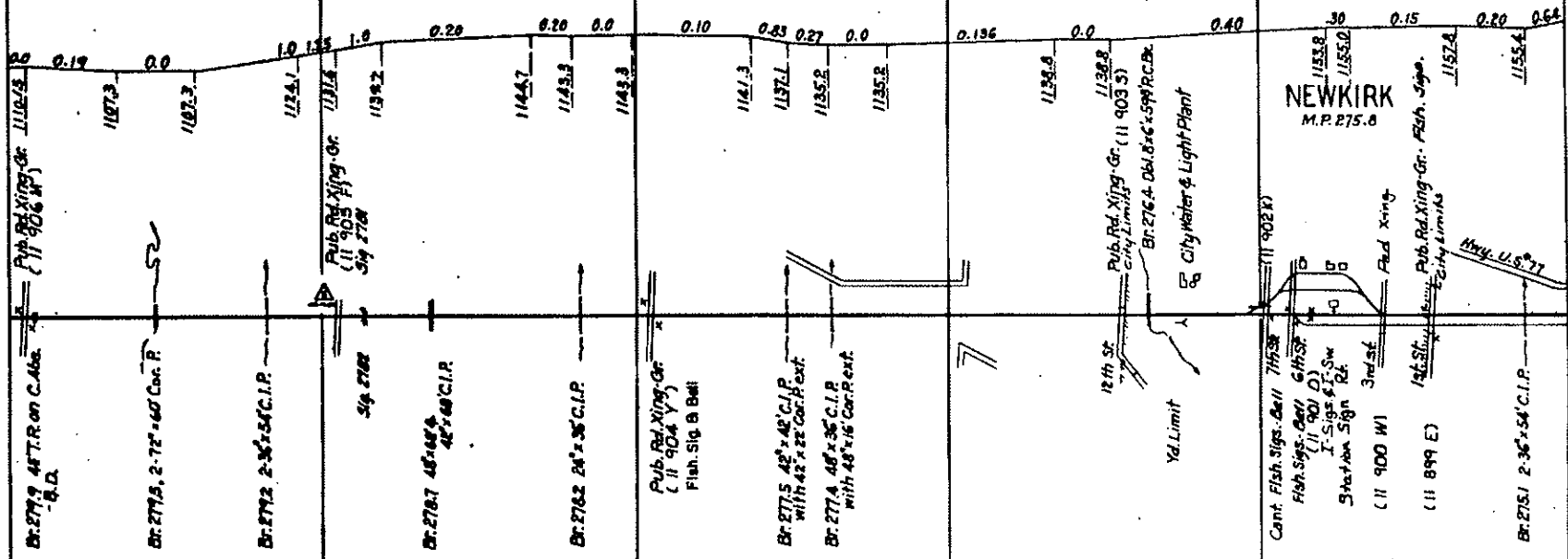
276

275

To Gainesville ←

→ To Arkansas City

Speed 45 MP 275.4 to MP 276.4



35 CURVES

RAIL GR.

RAIL

BALLAST

SURFACING

TIES

UNDERCUTTER

STABILIZATION

16-21

136 CWR - 3-1976

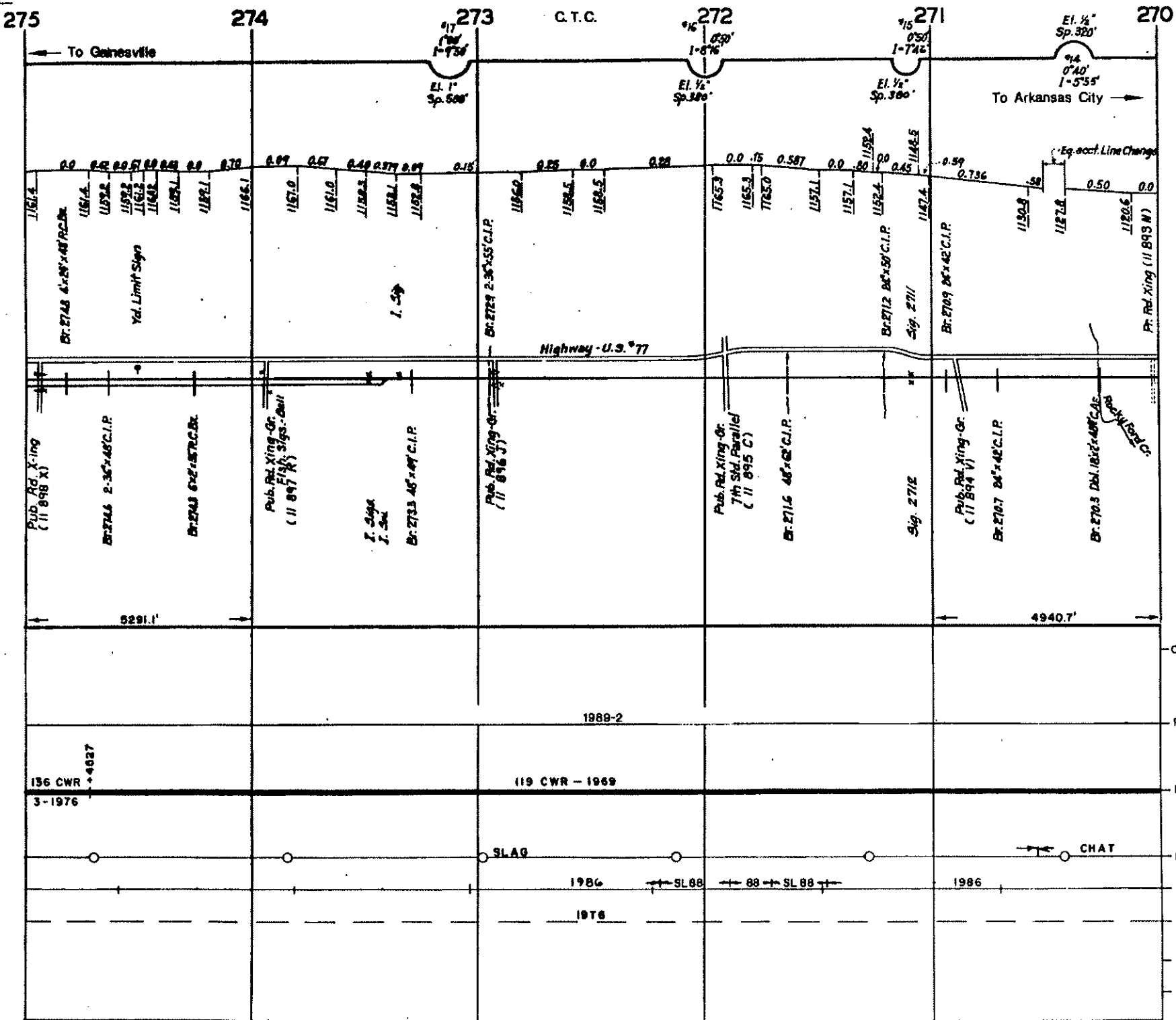
1989-2

136" TRK. NO. 23

CHAT

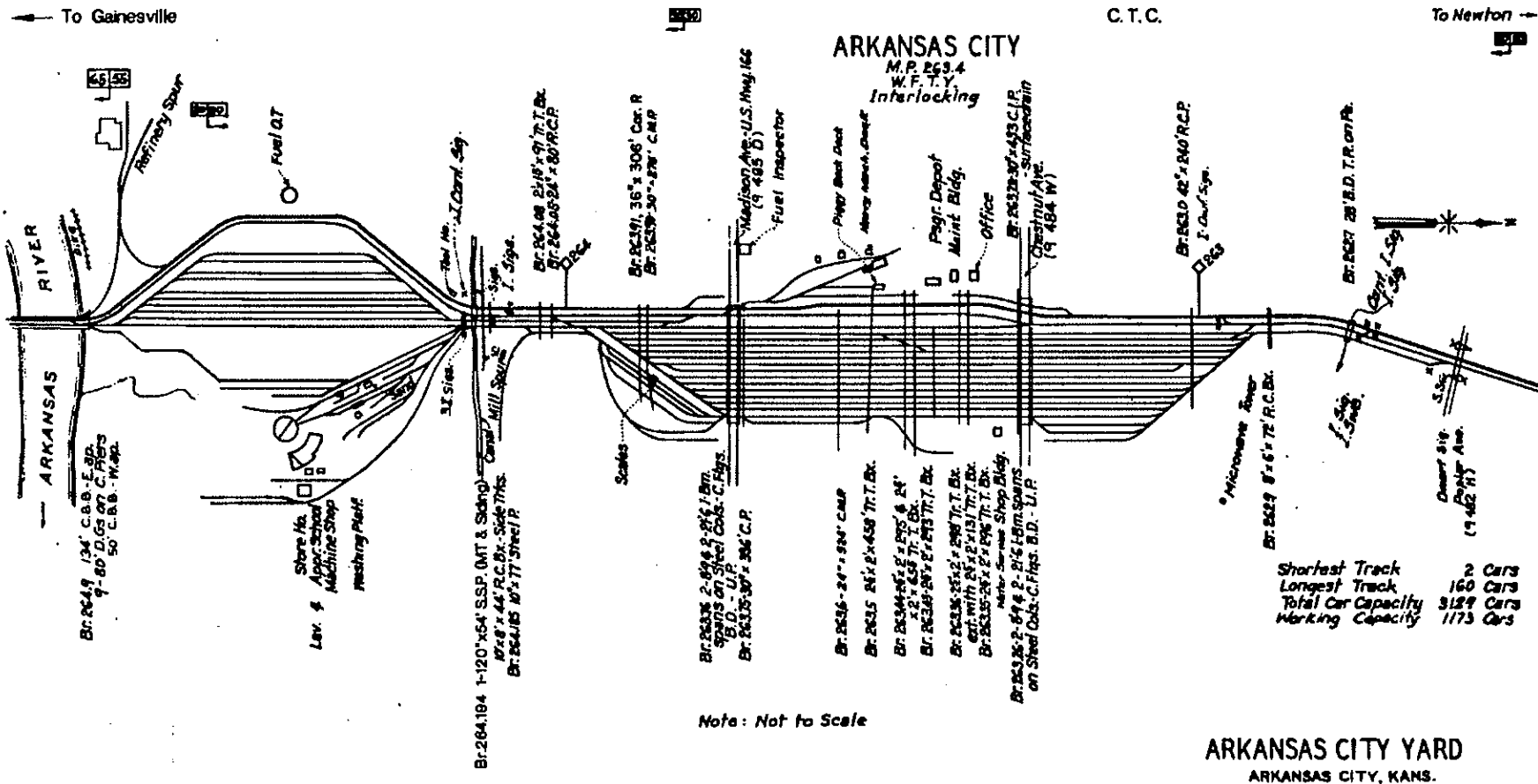
1986

1978

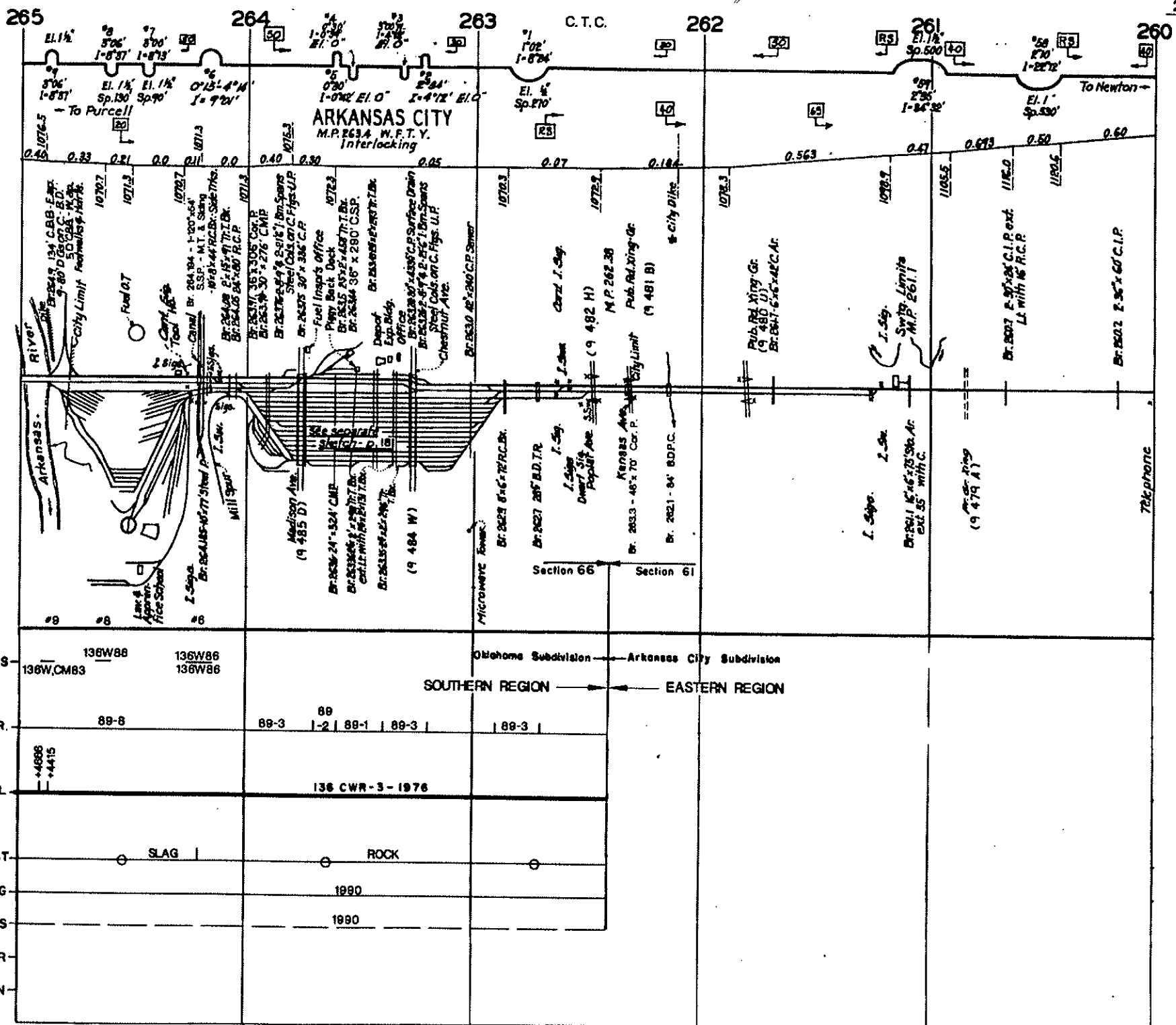


CURVES 98  
 RAIL GR.  
 RAIL  
 BALLAST  
 SURFACING  
 TIES  
 UNDERCUTTER  
 STABILIZATION









NR  
BR

CURVES

RAIL GR.

RAIL

BALLAST

SURFACING

TIES

UNDERCUTTER

STABILIZATION

136W88	136W86	Oklahoma Subdivision		Arkansas City Subdivision
136W,CM83	136W86	SOUTHERN REGION		EASTERN REGION
89-8	89-3	89	89-1	89-3
+4688 +4415	136 CWR-3-1976			
SLAG	ROCK			
	1990			
	1990			

# Oklahoma Subdivision

Arkansas City (M.P. 262.4) to Gainesville (M.P. 409.1)