

— ILLINOIS DIVISION —

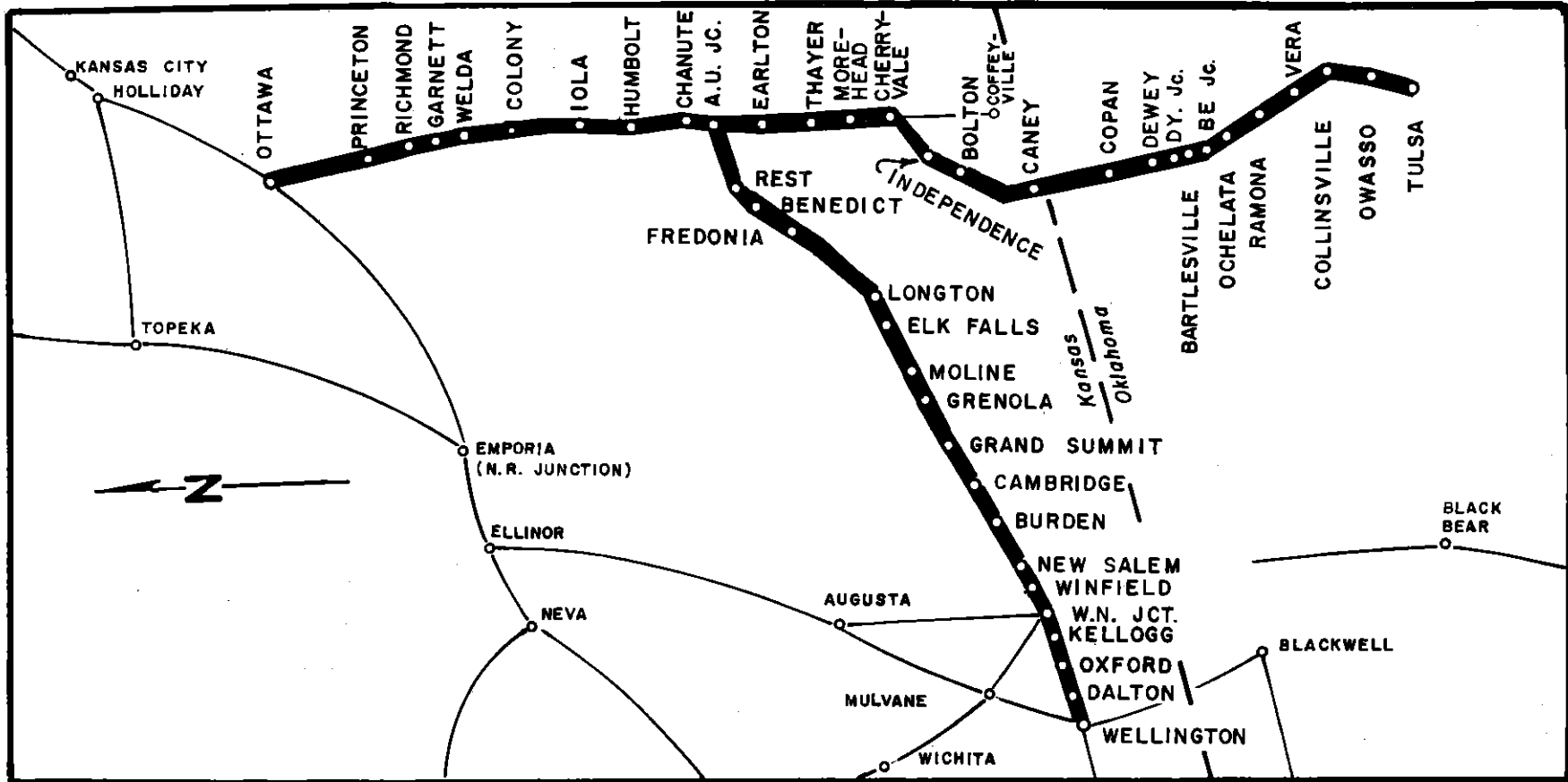
RUN THROUGH TRACK CHART BOOK

FOR

OTTAWA to CHANUTE to TULSA (PAGES 1 to 42)

and

CHANUTE to WELLINGTON (PAGES 43 to 68)



# — ABBREVIATIONS —

## BRIDGES

|                             |                                     |
|-----------------------------|-------------------------------------|
| Ab. ——— Abutment            | Msy. ——— Masonry                    |
| Ap. ——— Approach            | O. D. ——— Open Deck                 |
| Ar. ——— Arch                | Ped. ——— Pedestal                   |
| B. D. ——— Ballast Deck      | P. T. ——— Pile Trestle              |
| Bx. ——— Box                 | P. ——— Pipe                         |
| Brk. ——— Brick              | P. Ar. ——— Pipe Arch                |
| Br. ——— Bridge              | P. C. ——— Pre-stressed Concrete     |
| C. I. P. ——— Cast Iron Pipe | R. C. ——— Reinforced Concrete       |
| C. ——— Concrete             | S. S. C. ——— Steel Stringers & Caps |
| C. P. ——— Concrete Pipe     | Sta. ——— Stone                      |
| Cor. P. ——— Corrugated Pipe | S. S. P. ——— Structural Steel Pipe  |
| D. G. ——— Deck Girder       | T. R. ——— T-Rail                    |
| D. T. ——— Deck Truss        | Th. G. ——— Thru-Girder              |
| Dbl. ——— Double             | Th. T. ——— Thru-Truss               |
| Dr. Sp. ——— Draw Span       | T. ——— Timber                       |
| Ext. ——— Extension          | Tr. ——— Treated                     |
| Ftg. ——— Footing            | Trip. ——— Triple                    |
| Hdrl. ——— Handrail          | Vit. P. ——— Vitrified Pipe          |
| I-Bm. ——— I-Beam            | Wr. I. P. ——— Wrought Iron Pipe     |
| Lg. ——— Long                |                                     |

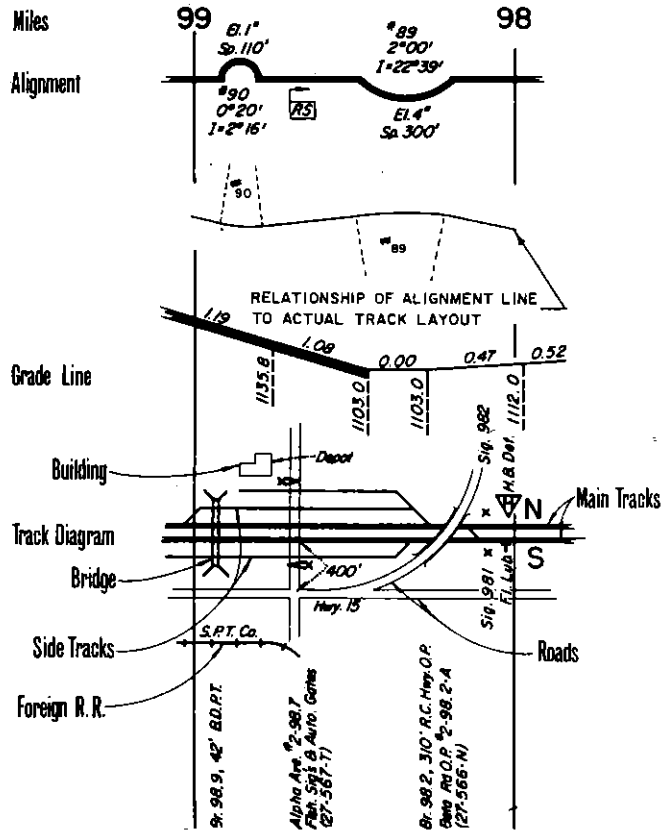
## FACILITIES

|                            |                          |
|----------------------------|--------------------------|
| B. ——— Booth Telephone     | T. ——— Turntable         |
| C. ——— Communication       | W. ——— Water             |
| F. ——— Fuel                | W. Col. ——— Water Column |
| O. Col. ——— Oil Column     | W. T. ——— Water Tank     |
| O. T. ——— Oil Tank         | Y. ——— Wye Track         |
| R. ——— Radio Communication |                          |

## MISCELLANEOUS

|   |                                      |
|---|--------------------------------------|
| A. B. S. ——— Automatic Block System     | L. Sw. ——— Lock Switch               |
| A. T. S. ——— Automatic Train Stop       | M. ——— Middle Main Track             |
| Auto. Gate. ——— Automatic Gate          | N. ——— North Main Track              |
| Ave. ——— Avenue                         | O. P. ——— Overpass                   |
| Blk. ——— Block                          | Pass. ——— Passenger                  |
| Blvd. ——— Boulevard                     | Po. ——— Power                        |
| Bch. ——— Branch                         | Pr. ——— Private                      |
| Br. Sig. ——— Bridge Signal              | Pub. ——— Public                      |
| B. P. ——— Bumping Post                  | R. R. ——— Railroad                   |
| Cant. Sig. ——— Cantilever Signal        | Ry. ——— Railway                      |
| Co. ——— County                          | Reserv. ——— Reservoir                |
| Cr. ——— Creek                           | R. S. ——— Resume Speed               |
| Div. ——— Division                       | Rt. ——— Right                        |
| D. E. Det. ——— Dragging Equip. Detector | R. W. ——— Right of Way               |
| El. ——— Elevation of Curves             | Riv. ——— River                       |
| Fl. Lub. ——— Flange Lubricators         | Rd. X-ing ——— Road Crossing          |
| Fish. Sig. ——— Flashing Signal          | Sec. ——— Section                     |
| Fl. Lt. P. ——— Flood Light Pole         | S. L. Det. ——— Shifted Load Detector |
| Fl. Lt. T. ——— Flood Light Tower        | Sig. ——— Signal                      |
| Frt. ——— Freight                        | S. ——— South Main Track              |
| Gr. ——— Grade                           | Sp. ——— Sprial Length                |
| Hwy. ——— Highway                        | S. Sw. ——— Spring Switch             |
| H. B. Det. ——— Hot Box Detector         | Sta. ——— Station                     |
| Ho. ——— House                           | St. ——— Street (also State)          |
| I. ——— Total Curve Angle                | T. C. S. ——— Traffic Control System  |
| I. Sig. ——— Interlocking Signal         | Trk. ——— Track                       |
| I. Sw. ——— Interlocking Switch          | U. P. ——— Underpass                  |
| Jct. ——— Junction                       | U. T. P. ——— Under Track Plow        |
| Lav. ——— Lavatory                       | W. W. ——— Wig Wag                    |
| Lt. ——— Left                            | Yd. ——— Yard                         |
| C. Sig. ——— Control Signal              |                                      |
| DC-Sw. ——— Dual Control Switch          |                                      |

# EXPLANATIONS



**Scales**

1" = 3000' Horizontal scale for profile, alignment and tracks. (Exceptions noted)

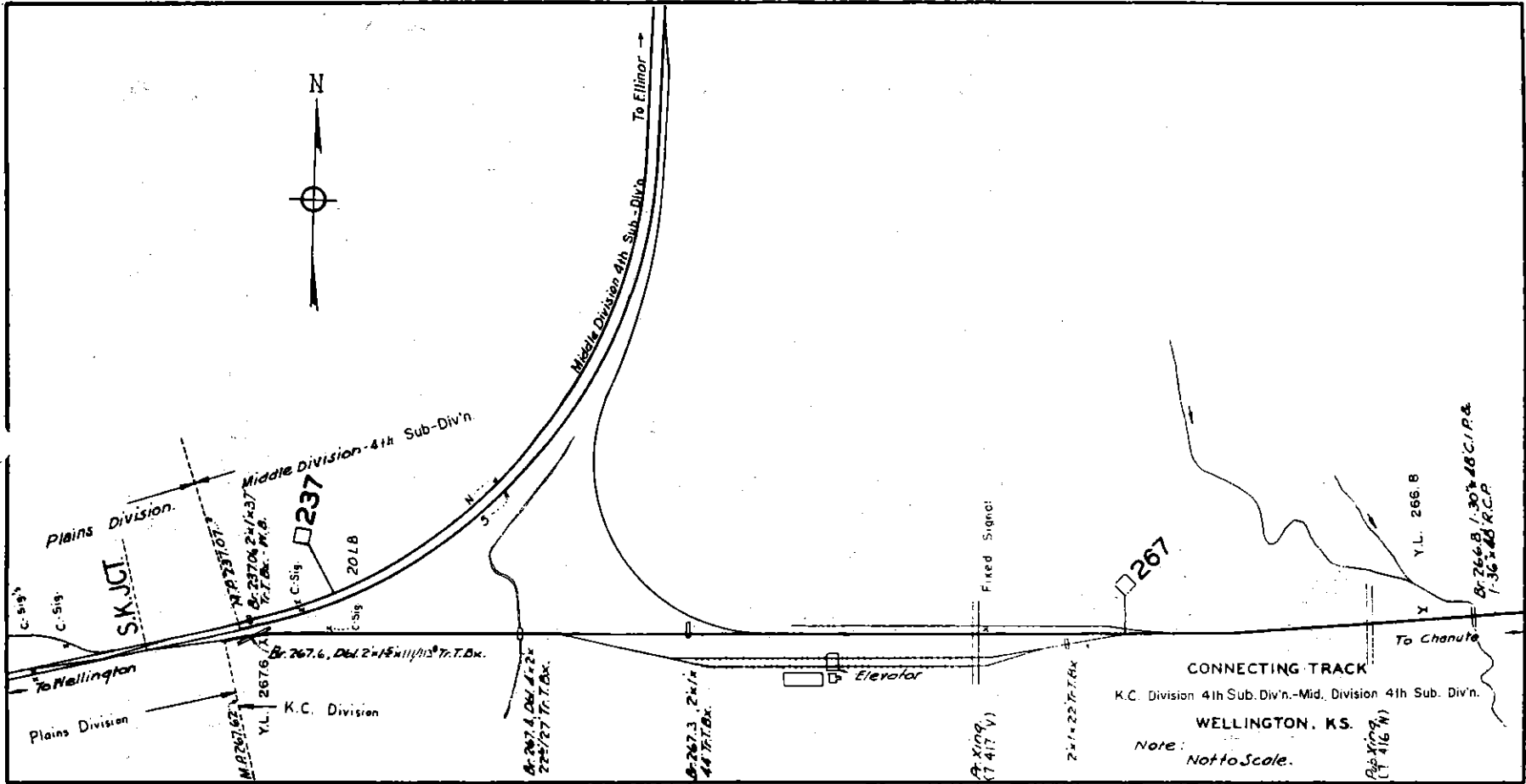
1" = 200' Vertical scale for profile.

Ascending ruling grades for Districts, shown bold.

Figures on grade lines are % gradients.

Figures below grade lines are profile subgrade elevations in feet.

- ▲ Flange Lubricator
  - × All types of signals, except bridge signals.
  - Columns, water and oil.
  - ▼ Permanent slow speed sign with appropriate speeds shown.
  - Permanent resume speed sign (green board).
  - ▽ Crossing warning devices, Flashing signals with automatic gates.
  - ▽ Shifted load detector.
  - ▲ Hot box detector.
  - Dragging equipment detector, arrow or arrows denote direction protected.
  - Y Yard limit sign.
  - Switching limit sign.
- 2-69.3-A R.U.C. number, as shown on public road crossings.
- (26-007-F) D.Q.T. number as shown on public and private crossings.
- Car capacities shown on all station detail sheets are based on 50 ft. overall allowance per car.



CONNECTING TRACK  
 K.C. Division 4th Sub. Div'n.-Mid. Division 4th Sub. Div'n.

WELLINGTON, KS.

Note:  
 Not to Scale.

Br. 267.6  
 1-30 1/2 in

To Chanute

Y.L. 266.8

Br. 266.8, 1-30 1/2 in Tr. Dk. R.C.P.

267



Middle Division 4th Sub-Div'n  
 To Filmore

Middle Division-4th Sub-Div'n

Plains Division

SKJCT

Plains Division

To Wellington

K.C. Division

Br. 267.6, Del. 2-15 1/2 in Tr. Dk.

Br. 267.4, Del. 2-15 1/2 in Tr. Dk.

Br. 267.3, 2 in Tr. Dk.

Br. 267.2, 2 in Tr. Dk.

2 in Tr. Dk.

Fixed Signal

Elevator

237

20 L.B.

Br. 237.04, 2 1/4 in Tr. Dk. M.B.

Br. 267.67, Y.L. 267.6

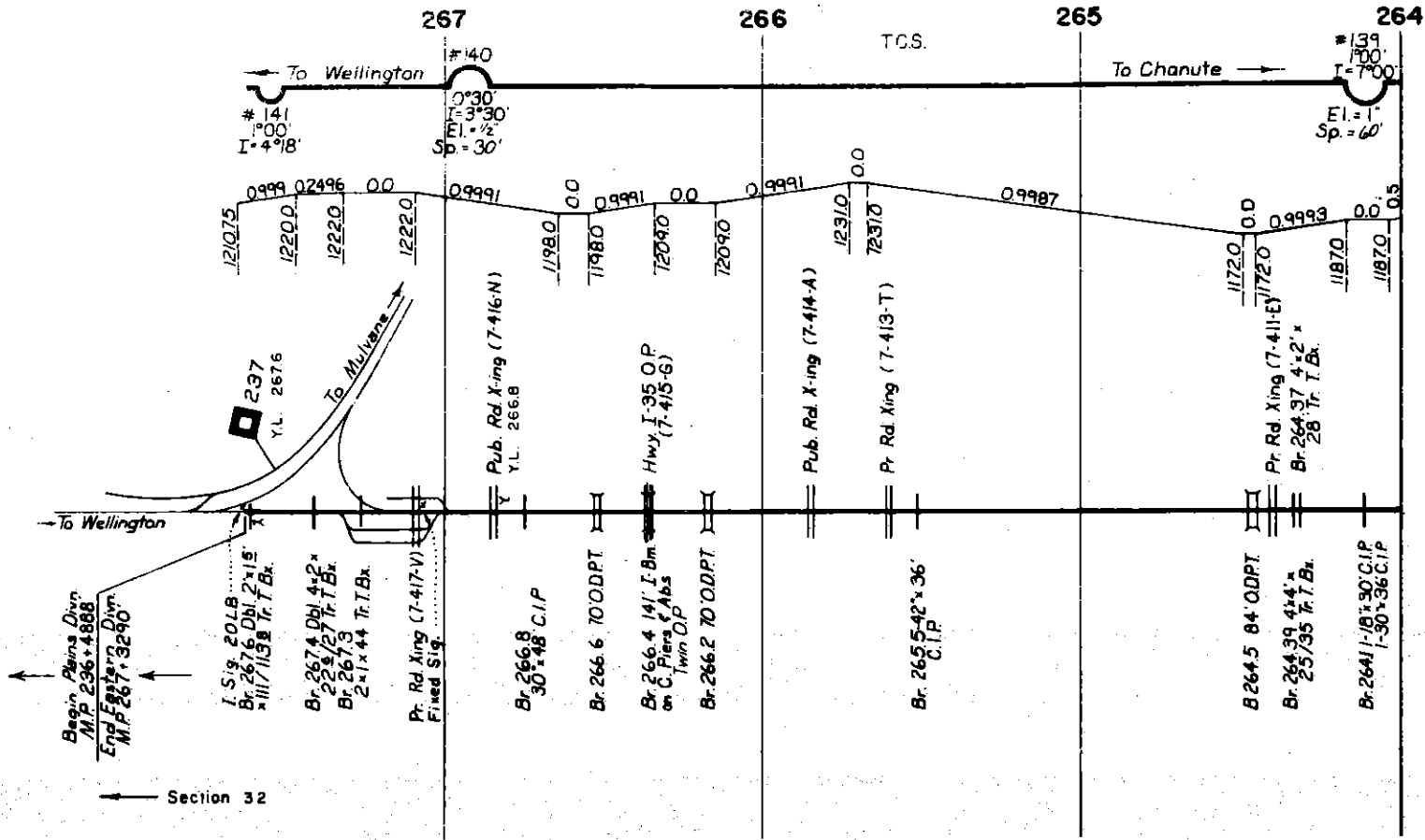
C. Sig.

C. Sig.

C. Sig.

C. Sig.

Y



Begin Plains Div.  
M.P. 236 + 4888  
End Eastern Div.  
M.P. 267 + 3290

Section 32

I.Sig. 2018  
Br. 2676 2x15 Tr. T.Bx.  
x111/113 Tr. T.Bx.

Br. 2674 2x15 Tr. T.Bx.  
22x27 Tr. T.Bx.  
Br. 2673  
2x1x44 Tr. T.Bx.

Pr. Rd. Xing (7-417-V)  
Fixed Sig.

Br. 266.8  
30' x 48' C.I.P.

Br. 266.6 70' O.D.P.T.

Br. 266.4 141' I.Bm.  
on C. Plans & Abs.  
14m O.P.

Br. 266.2 70' O.D.P.T.

Br. 265.5 42' x 36'  
C.I.P.

Br. 264.5 84' O.D.P.T.

Br. 264.39 44' x  
25' x 35' Tr. T.Bx.

Br. 264.1 118' x 30' C.I.P.  
1-30' x 36' C.I.P.

Pub. Rd. Xing (7-416-N)  
Y.L. 266.8

Pub. Rd. Xing (7-414-A)

Pr. Rd. Xing (7-413-T)

Pr. Rd. Xing (7-411-E)  
Br. 264.37 4' x 2' x  
28' Tr. T.Bx.

237  
Y.L. 2676

To Mulvane

To Wellington

To Chanute

267

266

265

264

T.C.S.

#141  
0°00'  
I = 4°18'

#140  
0°30'  
I = 3°30'  
E.I. = 1/2'  
Sp = 30'

#139  
1°00'  
I = 7°00'  
E.I. = 1'  
Sp = 60'

264

263

262

261

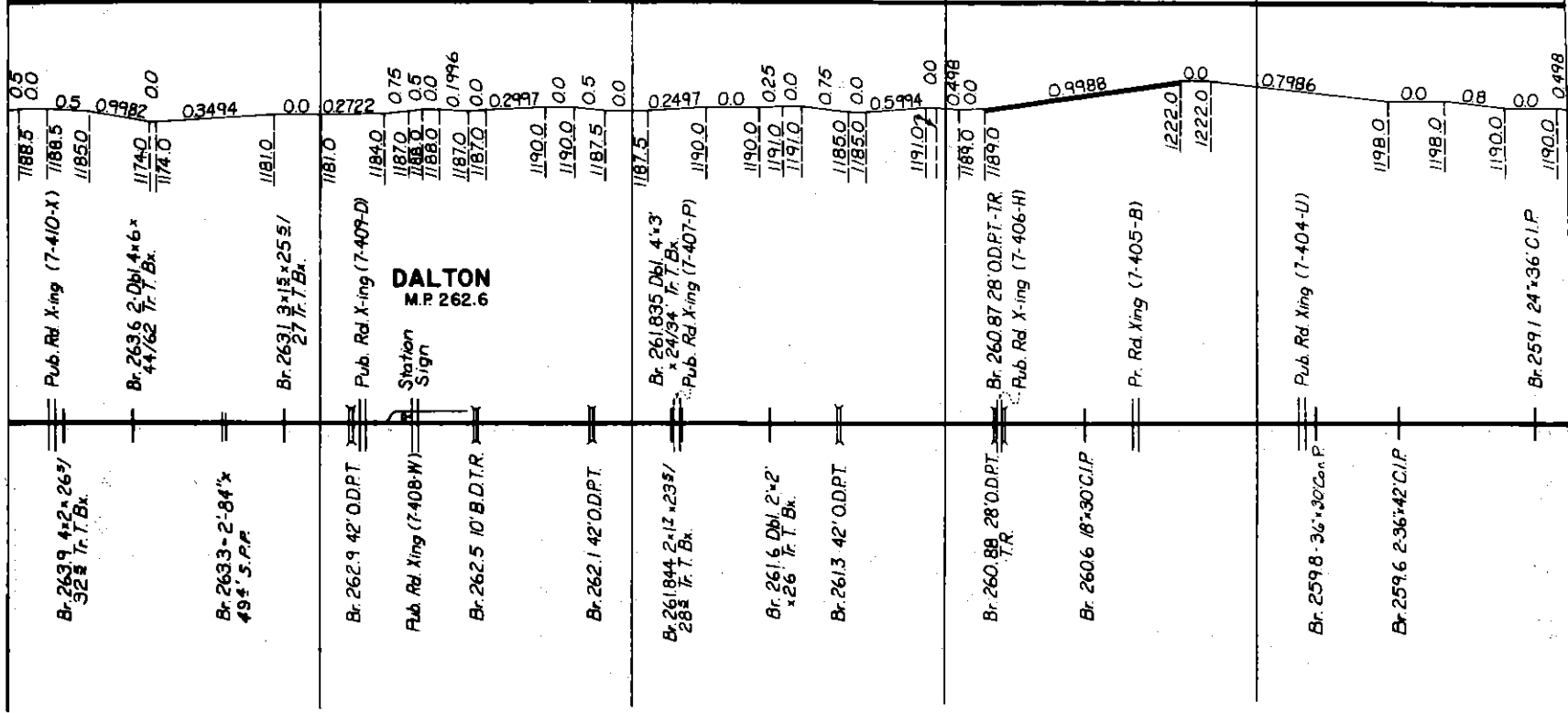
260

259

To Wellington ←

NO CURVES

→ To Chanute



Begin T.C.S. ↓

0.5  
0.0  
0.5  
0.9982  
0.0  
0.3494  
0.0  
0.2722  
0.75  
0.5  
0.0  
0.1996  
0.0  
0.2997  
0.0  
0.5  
0.0  
0.0  
0.2497  
0.0  
0.25  
0.0  
0.75  
0.0  
0.5994  
0.0  
0.398  
0.0  
0.9988  
1.222.0  
1.222.0  
0.7986  
0.0  
0.8  
0.0  
0.498

1188.5  
1188.5  
1185.0  
1174.0  
1174.0  
1181.0  
1181.0  
1184.0  
1187.0  
1188.0  
1185.0  
1187.0  
1187.0  
1190.0  
1190.0  
1187.5  
1187.5  
1190.0  
1190.0  
1191.0  
1191.0  
1185.0  
1185.0  
1191.0  
1189.0  
1189.0  
1222.0  
1222.0  
1198.0  
1198.0  
1190.0  
1190.0

Pub. Rd. X-ing (7-410-X)  
Br. 2636 2-Dbl. 4x6 x 44/62 Tr. T. Bx.  
Br. 2633-2'-84" x 49 1/2 S.P.P.  
Pub. Rd. X-ing (7-405-D)  
Station Sign  
Br. 2625 10' B.D.T.R.  
Br. 261844 2x12x23 1/2 x 28 1/2 Tr. T. Bx.  
Pub. Rd. X-ing (7-407-P)  
Br. 2616 Dbl. 2x2 x 26 Tr. T. Bx.  
Br. 2613 42' O.D.P.T.  
Br. 26088 28' O.D.P.T. - TR  
Pub. Rd. X-ing (7-406-H)  
Pr. Rd. X-ing (7-405-B)  
Pub. Rd. X-ing (7-404-U)  
Br. 2596 2.36x42 C.I.P.  
Br. 2591 24x36 C.I.P.

259

258

257

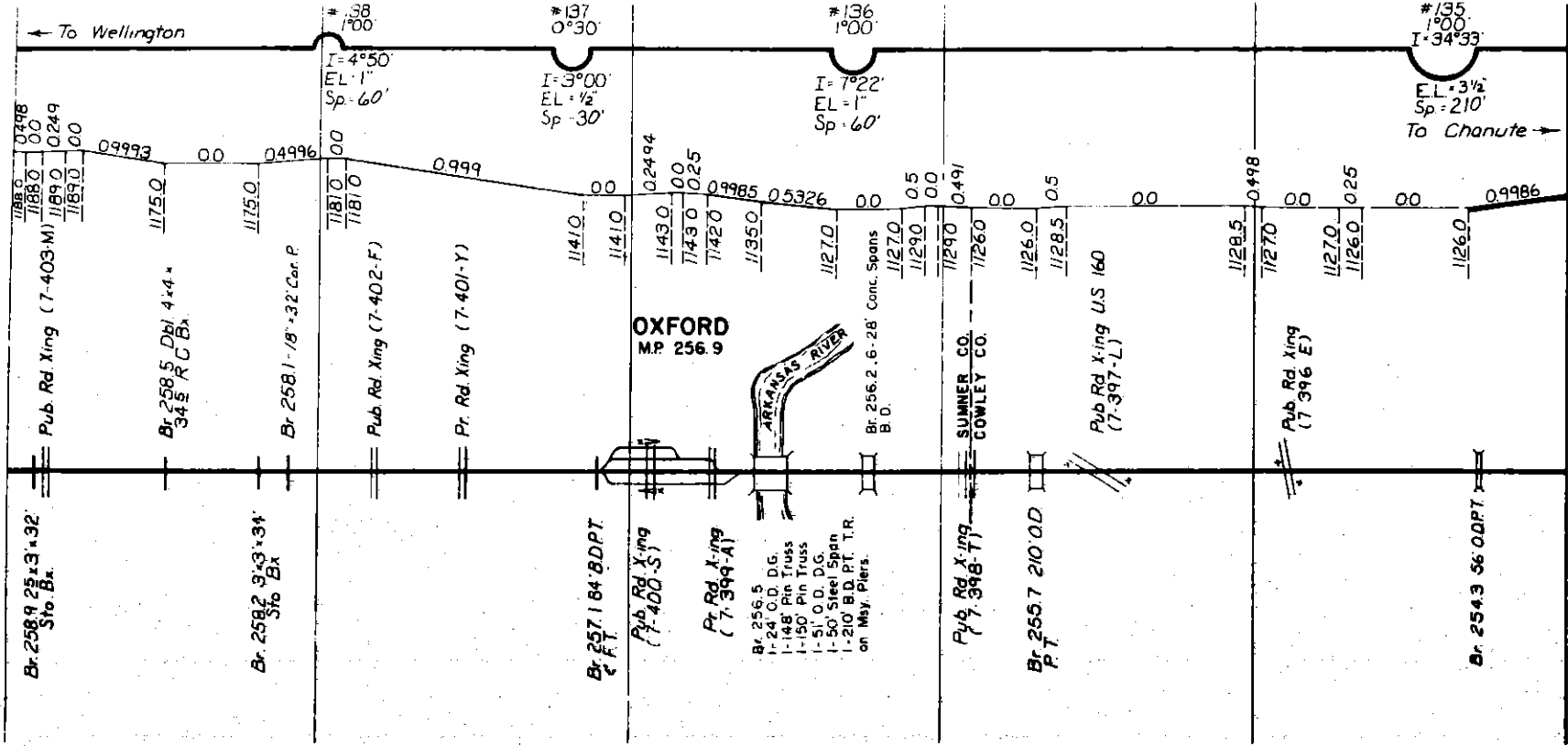
256

255

254

← To Wellington

To Chanute →



Br. 258.9 29x31.32' Sto. Bx.

Br. 258.5 Dbl. 4x4' 34 1/2' R.C. Bx.

Br. 258.2 31.3x31' Sto. Bx.

Br. 258.1-18'x32' Cor. P.

Br. 257.184 8 D.P.T.

Pub. Rd. X'ing (7-403-M)

Pr. Rd. X'ing (7-399-A)

Br. 256.5 D.G. 1-24' O.D. D.G. 1-148' Pin Truss 1-150' Pin Truss 1-51' O.D. D.G. 1-50' Steel Span 1-210' B.D. PT. T.R. on Msh. Piers.

ARKANSAS RIVER

Br. 256.2 6-28' Conc. Spans B.D.

Pub. Rd. X'ing (7-397-L)

SUMMER CO. COWLEY CO.

Br. 255.7 210' O.D. P.T.

Pub. Rd. X'ing (7-396-E)

Br. 254.3 56' O.D.P.T.

OXFORD M.P. 256.9

1186.0  
1188.0  
1189.0  
1189.0

1175.0

1175.0

1181.0  
1181.0

0.999

1141.0  
1141.0

0.2494

1143.0  
1143.0  
1142.0

1135.0

0.9985

0.5326

1127.0

0.0

0.5

1129.0  
1129.0

0.491

1129.0

1126.0

1126.0

1128.5

1127.0

1127.0

1126.0

1126.0

0.0

1126.0

0.9986

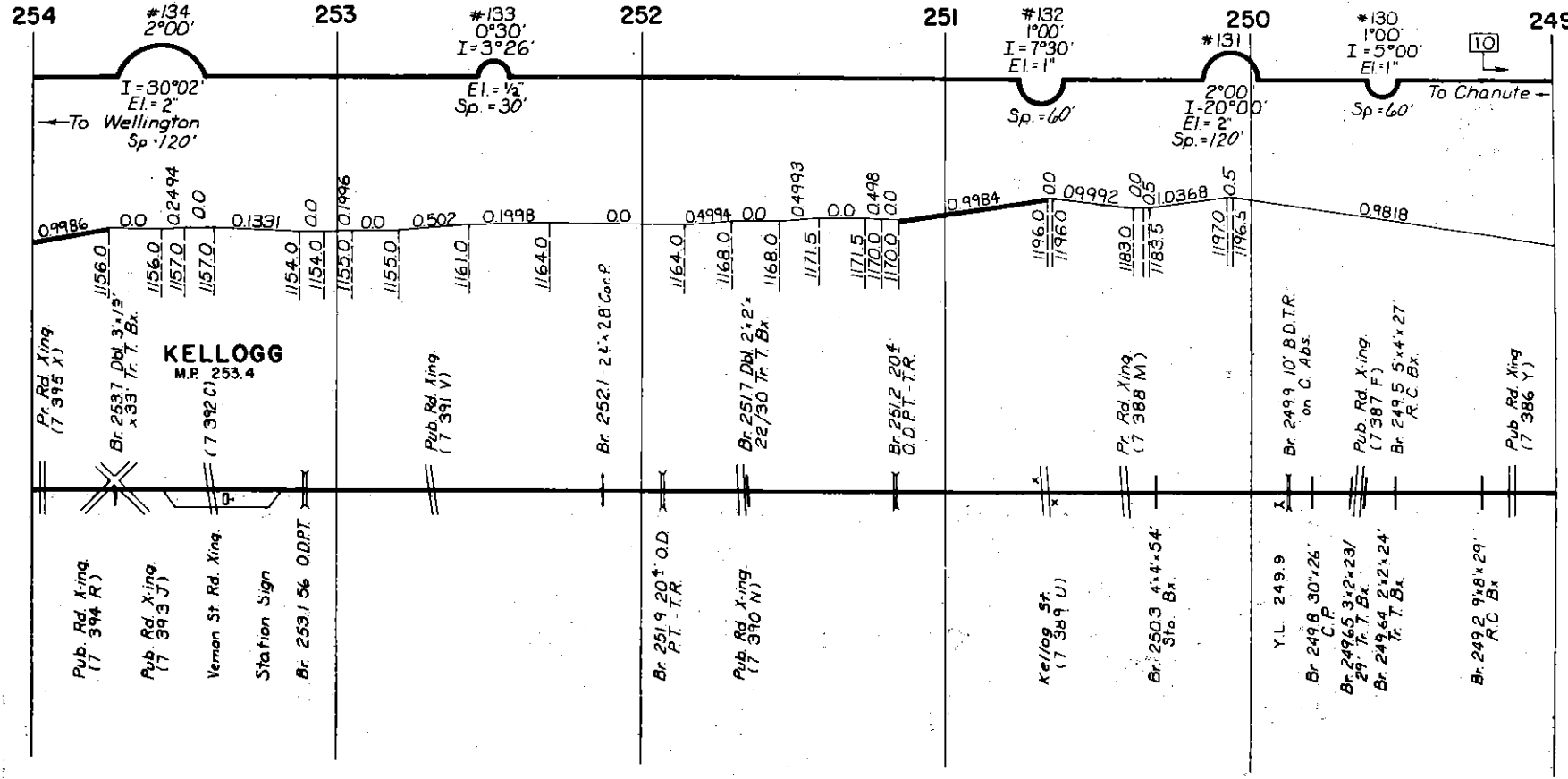
\*138  
1°00'  
I=4°50'  
EL=1'  
Sp=60'

\*137  
0°30'  
I=3°00'  
EL=1/2'  
Sp=30'

\*136  
1°00'  
I=7°22'  
EL=1'  
Sp=60'

\*135  
1°00'  
I=34°33'  
EL=3 1/2'  
Sp=210'







244

To Wellington

40

243 #119  
3°00' 1°53'  
I = 43°50' 8°07'  
El. = 3', 1'  
Sp. = 248' 155'

#118  
1°41' 24"  
I = 14° 16'  
El. = 1'  
Sp. = 100'

242 #117  
1°40' 24"  
I = 8°58'  
El. = 1'  
Sp. = 100'

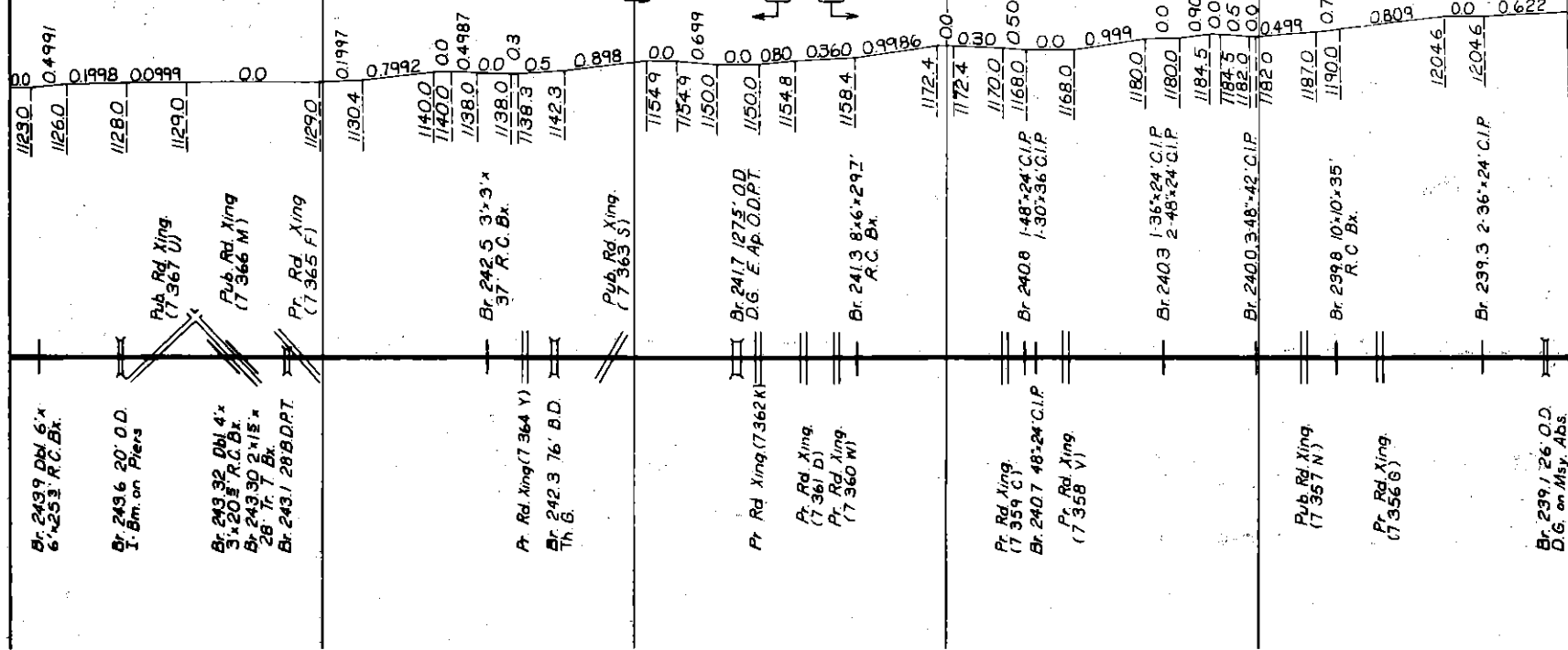
#116  
3°24'  
I = 18°08'  
El. = 3'  
Sp. = 360'

241 #115  
1°00'  
I = 21°13'  
El. = 1/2'  
Sp. = 200'

\*114 240  
1°30' I = 21°00'  
El. = 1/2' Sp. = 618'

239

To Chanute



Br. 243.9 Dbl 6'x  
6'x25' R.C. Bx.

Br. 243.6 20' O.D.  
1.5m on Piers

Br. 243.32 Dbl 4'x  
3'x20' R.C. Bx.  
Br. 243.30 2'x15'x  
28' Tr. T. Bx.  
Br. 243.1 20' B.D.P.T.

Pr. Rd. Xing (7.364 Y)

Br. 242.3 76' B.D.  
Th. B.

Pub. Rd. Xing  
(7.363 S)

Pr. Rd. Xing (7.362 K)

Pr. Rd. Xing  
(7.361 D)  
Pr. Rd. Xing  
(7.360 W)

Br. 241.7 127.5' O.D.  
D.G. E. Ap. O.D.P.T.

Br. 241.3 8'x6'x2.97'  
R.C. Bx.

Pr. Rd. Xing  
(7.359 C)  
Br. 240.7 48'x24' C.I.P.

Br. 240.8 1'48'x24' C.I.P.  
1'30'x36' C.I.P.

Pr. Rd. Xing  
(7.358 V)

Br. 240.3 1'36'x24' C.I.P.  
2'48'x24' C.I.P.

Pub. Rd. Xing  
(7.357 N)

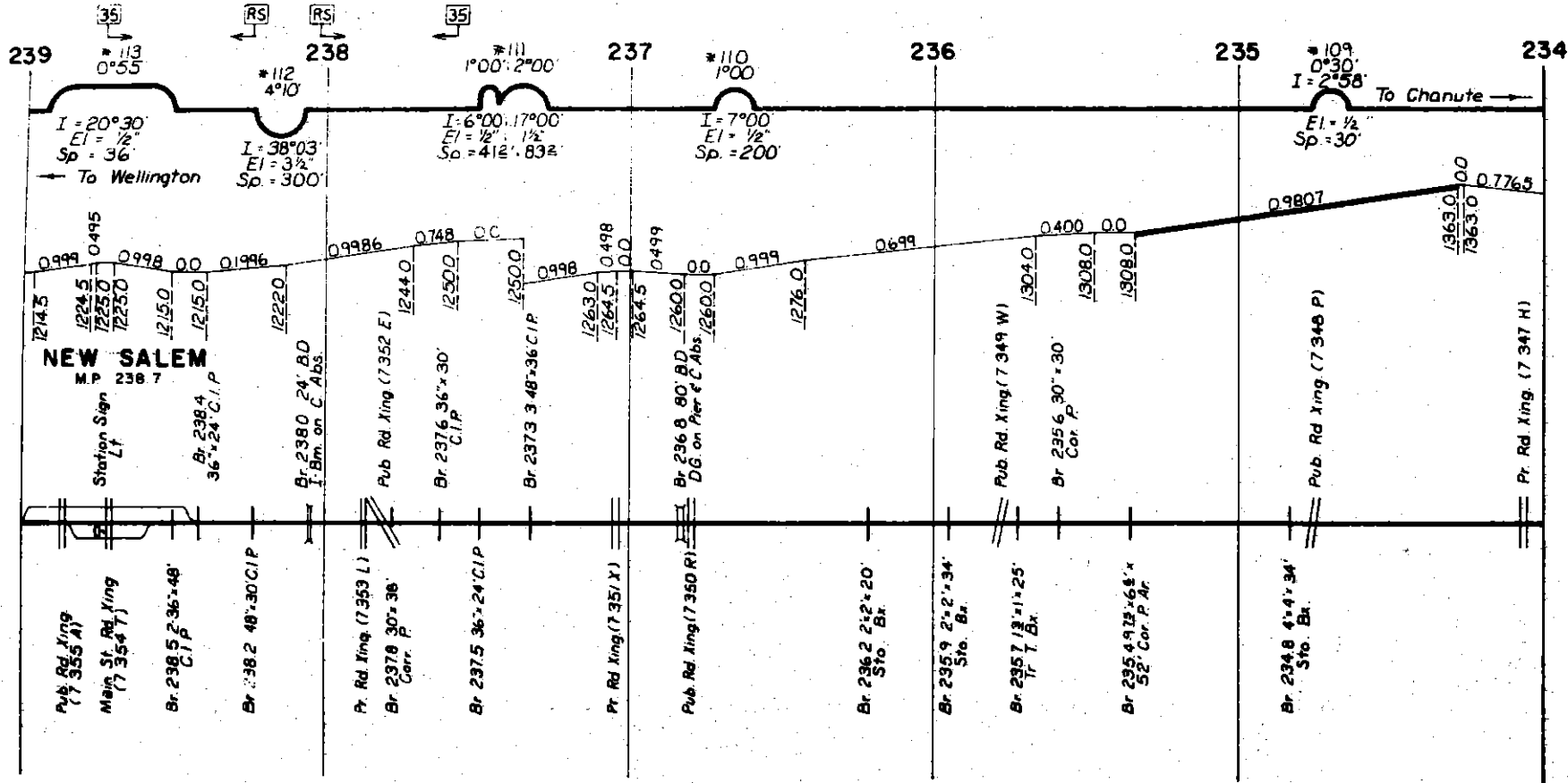
Br. 240.0 3'48'x42' C.I.P.

Pr. Rd. Xing  
(7.356 S)

Br. 239.8 10'x10'x35'  
R.C. Bx.

Br. 239.3 2'36'x24' C.I.P.

Br. 239.1 26' O.D.  
D.G. on Misy. Abs.



**NEW SALEM**  
M.P. 238.7

Pub. Rd. Xing  
(7355 A)

Main St. R.R. Xing  
(7354 T)

Br. 238.5 2'36" x 48'  
C.I.P.

Br. 238.2 48' x 30' C.I.P.

Pr. Rd. Xing (7353 L)

Br. 237.8 30' x 36'  
Carr. P.

Br. 237.5 36' x 24' C.I.P.

Pr. Rd. Xing (7351 X)

Pub. Rd. Xing (7350 R)

Br. 236.2 2' x 2' x 20'  
Sto. Br.

Br. 235.9 2' x 2' x 34'  
Sto. Br.

Br. 235.7 13' x 11' x 25'  
I.T. Br.

Br. 235.4 9'13' x 6'8" x  
52' Cor. P. Ar.

Br. 234.8 4' x 4' x 34'  
Sto. Br.

Pub. Rd. Xing (7348 P)

Pr. Rd. Xing (7347 H)

To Chanute →

Station Sign  
LT

Br. 238.4  
36' x 24' C.I.P.

Br. 238.0 24' BD  
I. Bm. on C. Abs.

Pub. Rd. Xing (7352 E)

Br. 237.6 36' x 30'  
C.I.P.

Br. 237.3 3'48" x 36' C.I.P.

Br. 236.8 80' BD  
DG. on Per. & C. Abs.

Pub. Rd. Xing (7349 W)

Br. 235.6 30' x 30'  
Cor. P.

0.9807

1365.0  
7353.0

0.7765

234

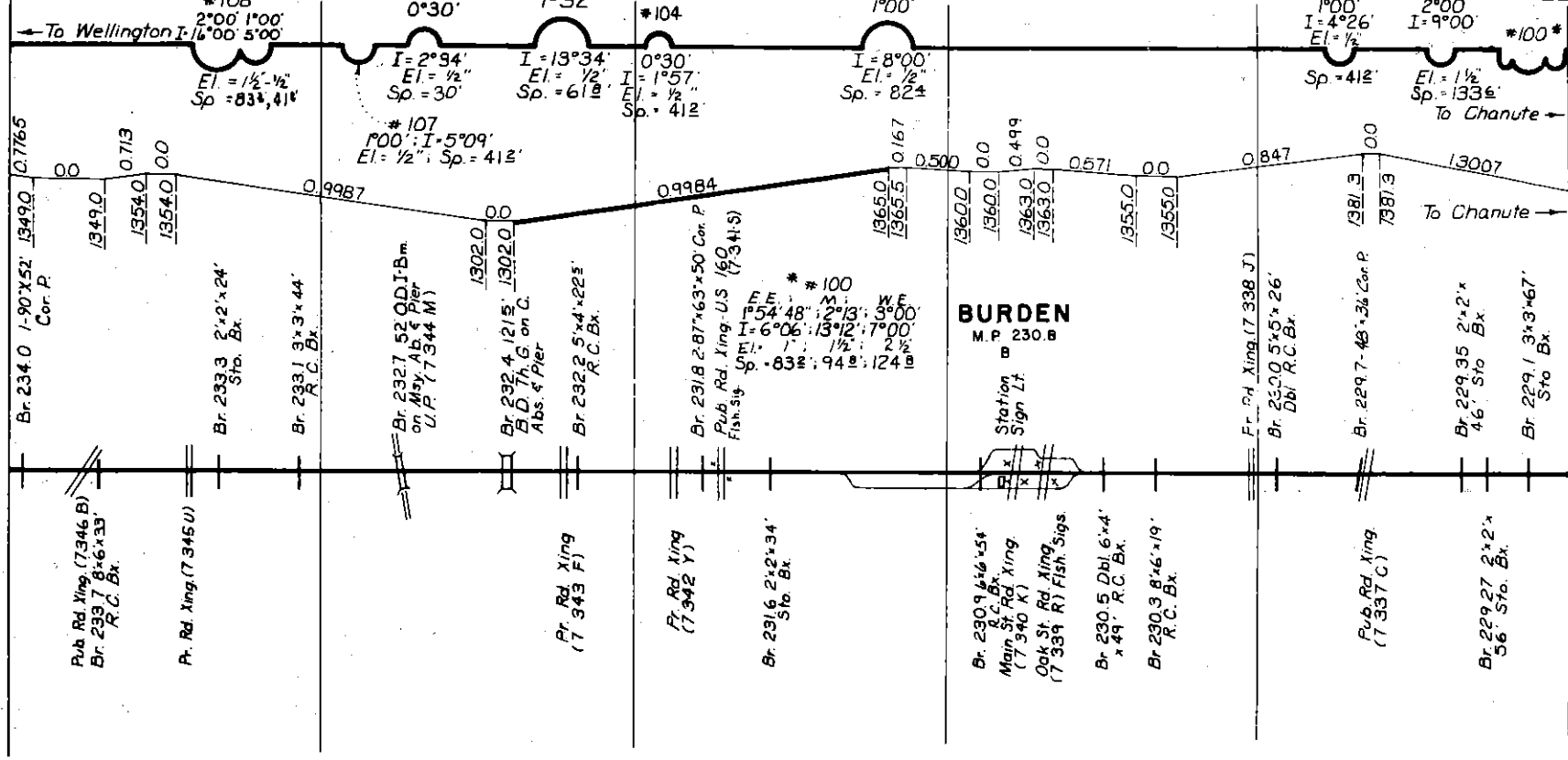
233

232

231

230

229



← To Wellington I = 2°00' 1°00' 1°00' 5'00'  
 #108  
 EI = 1/2" - 1/2"  
 Sp = 83 1/2, 41 1/2

#106  
 I = 2°34'  
 EI = 1/2"  
 Sp = 30"

#105  
 I = 1°32'  
 EI = 1/2"  
 Sp = 61 1/2"

#104  
 I = 0°30'  
 EI = 1°57"  
 Sp = 412"

#103  
 I = 8°00'  
 EI = 1/2"  
 Sp = 82 1/2"

#102  
 I = 4°26'  
 EI = 1/2"  
 Sp = 41 1/2"

#101  
 I = 2°00'  
 EI = 9°00'  
 Sp = 133 1/2"

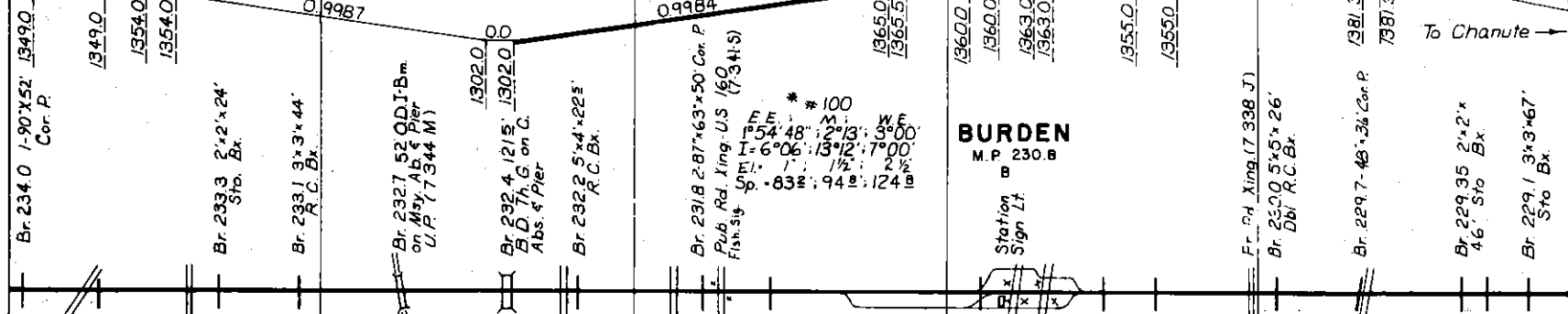
#100

#107  
 P.O.D. I = 5°09'  
 EI = 1/2", Sp = 41 1/2"

0.167  
 0.500  
 0.0  
 0.499  
 0.0

0.571  
 0.0  
 0.847

0.0  
 1.3007



0.7165  
 0.0  
 0.713  
 0.0  
 0.0

0.9987  
 0.0  
 0.0

0.9984  
 0.167  
 0.500  
 0.0  
 0.499  
 0.0

0.571  
 0.0  
 0.847

0.0  
 1.3007

Br. 231 8 2.87'x63'x50' Cor. P.  
 Pub. Rd. Xing - US 160  
 Fish. Sig. (7341 S)

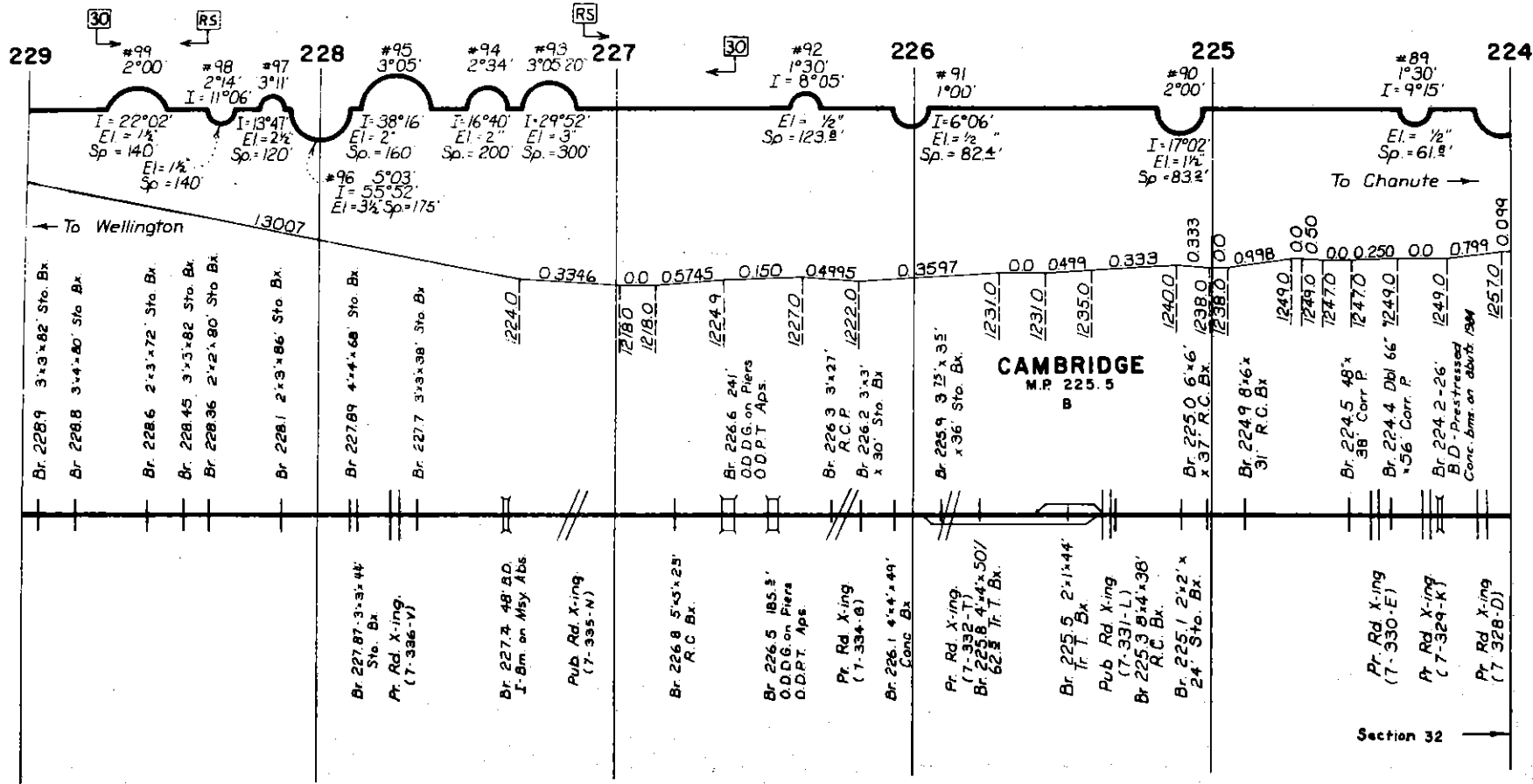
#100  
 I = 5°48'  
 EI = 1/2"  
 Sp = 83 1/2"

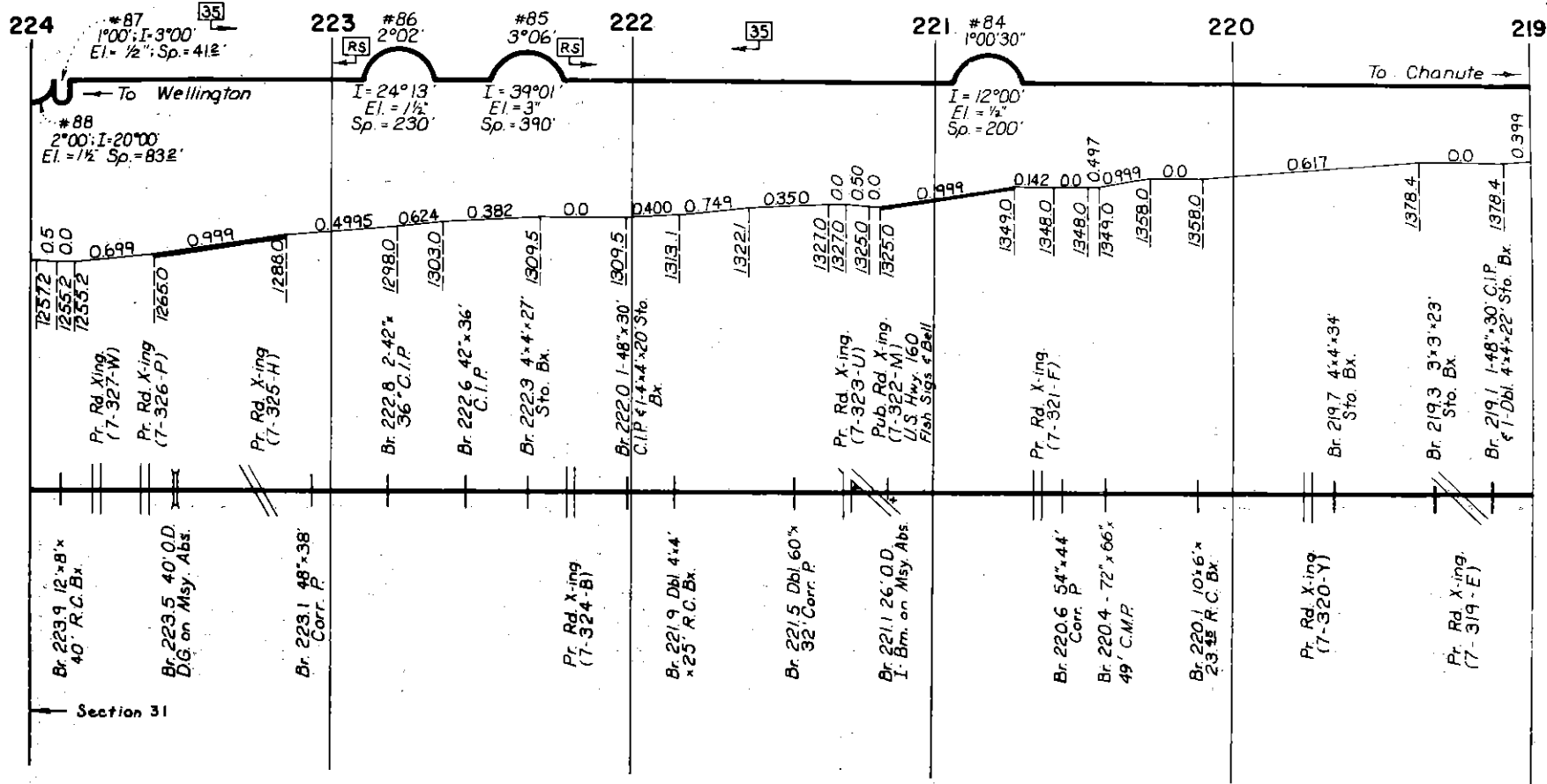
**BURDEN**  
 M.P. 230 B

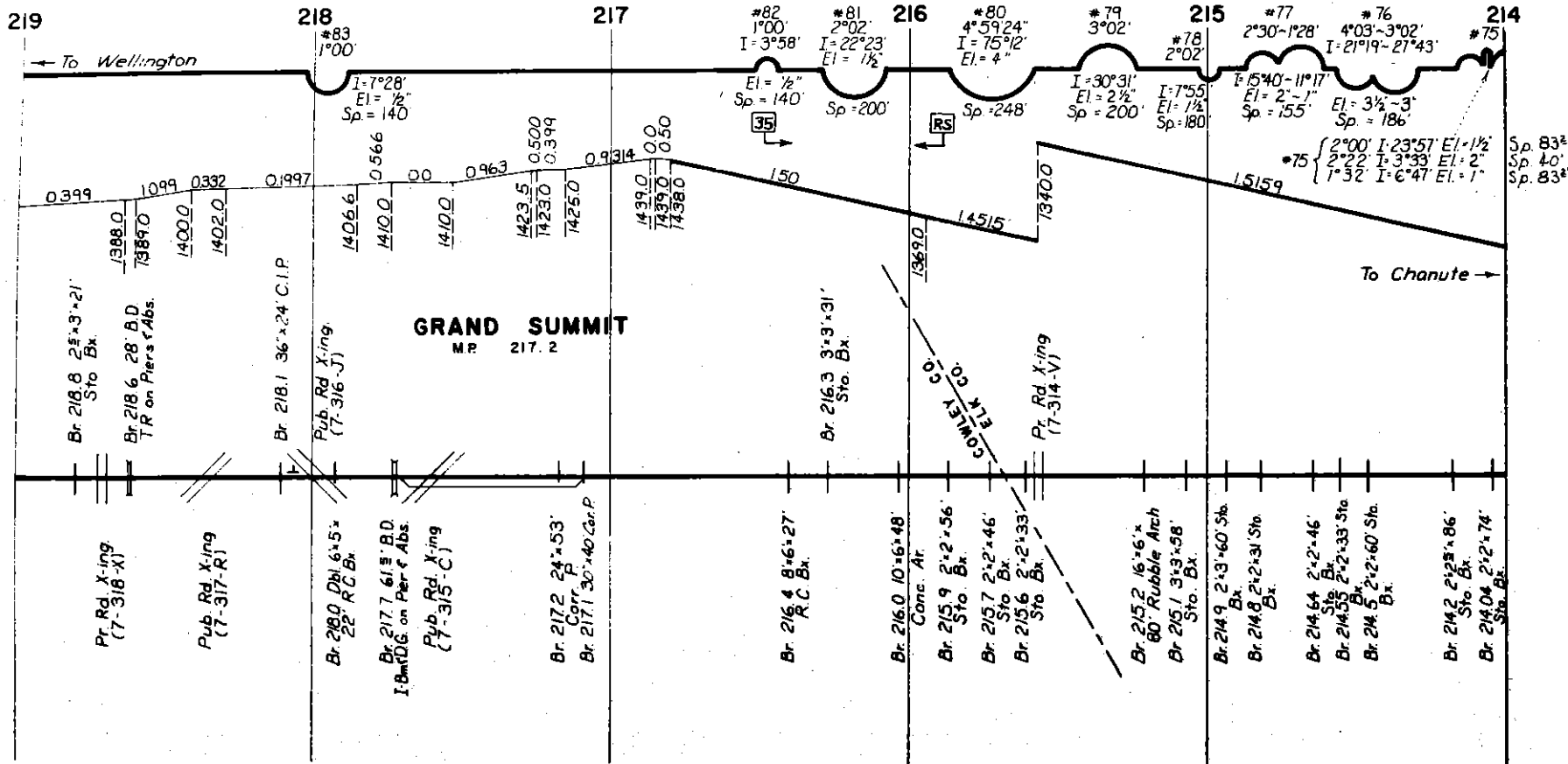
Station Sign Lt.

To Chanute →

← To Wellington

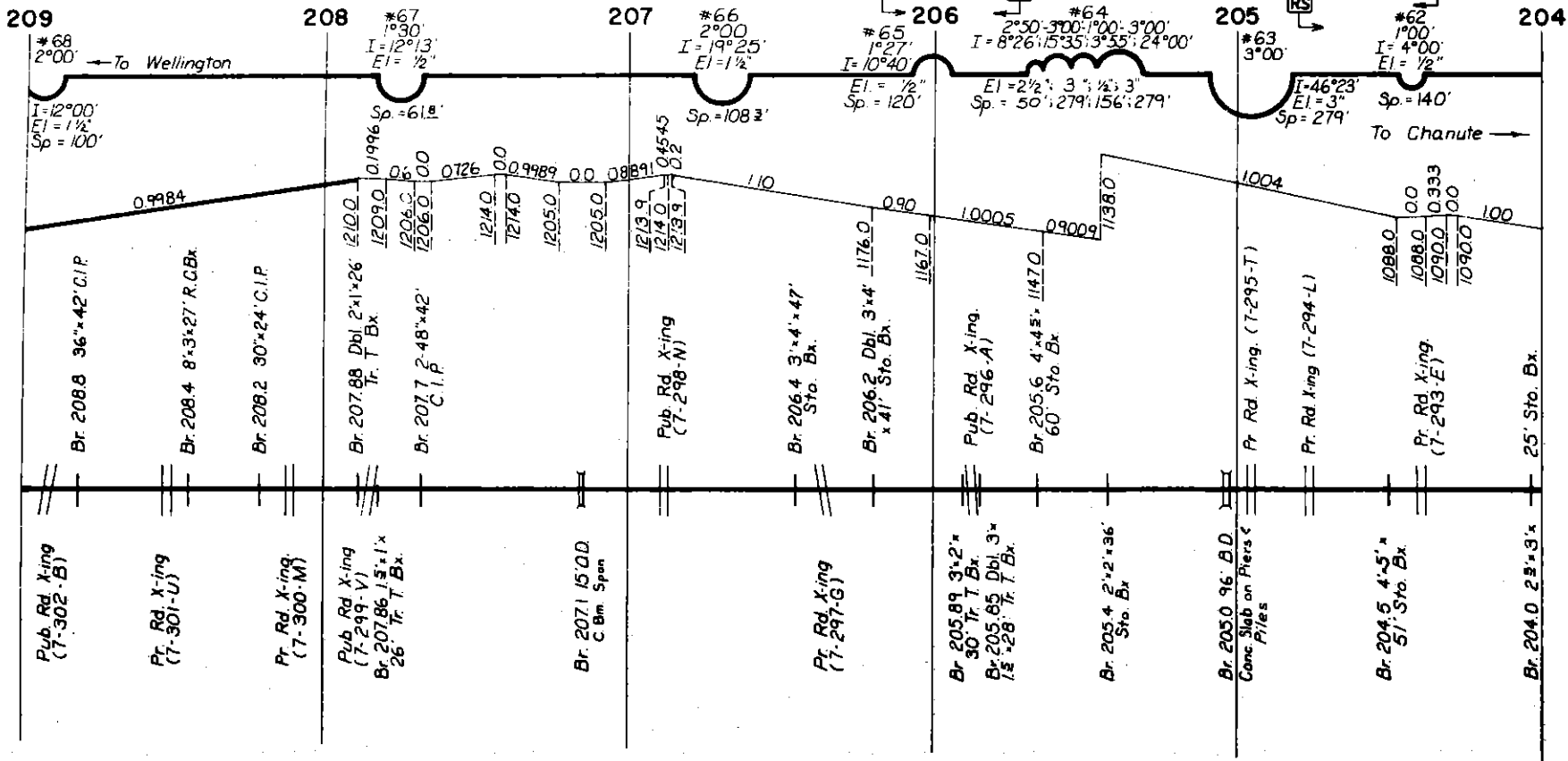












\*68  
2°00'  
← To Wellington

I=12°00'  
EI=1 1/2"  
Sp=100'

\*67  
1°30'  
I=12°13'  
EI= 1/2"

Sp=61.8'

\*66  
2°00'  
I=19°25'  
EI=1 1/2"

Sp=108.3'

\*65  
1°27'  
I=10°40'  
EI= 1/2"  
Sp=120'

\*64  
2°50' 3°00' 1°00' 3°00'  
I=8°26' 15°35' 3°55' 24°00'  
EI=2 1/2" 3" 1/2" 3"  
Sp=50' 279' 156' 279'

\*63  
3°00'  
I=46°23'  
EI=3"  
Sp=279'

\*62  
1°00'  
I=4°00'  
EI= 1/2"  
Sp=140'

→ To Chanute

Br. 208.8 36'x42' C.I.P.

Br. 208.4 8'x3'x27' R.C.Bx.

Br. 208.2 30'x24' C.I.P.

Br. 207.88 Dbl. 2x1x26'  
Tr. T. Bx.

Br. 207.7 2-48'x42'  
C.I.P.

Pub. Rd. X-ing  
(7-298-N)

Br. 206.4 3'x4'x47'  
Sto. Bx.

Br. 206.2 Dbl. 3'x4'  
x41' Sto. Bx.

Pub. Rd. X-ing.  
(7-296-A)

Br. 205.6 4'x4.5'x1147.0  
60' Sto. Bx.

Br. 205.4 2'x2'x36'  
Sto. Bx.

Br. 205.0 96' B.D.  
Conc. Slab on Piers &  
Piles

Pr. Rd. X-ing (7-294-L)

Pr. Rd. X-ing.  
(7-293-E)

Br. 204.0 2.5'x3'x25'  
Sto. Bx.

0.9984

1210.0  
1209.0  
1208.0  
1206.0

1214.0  
1214.0

1205.0

1205.0

1213.9  
1214.0  
1213.9

1176.0

1167.0

1000.5

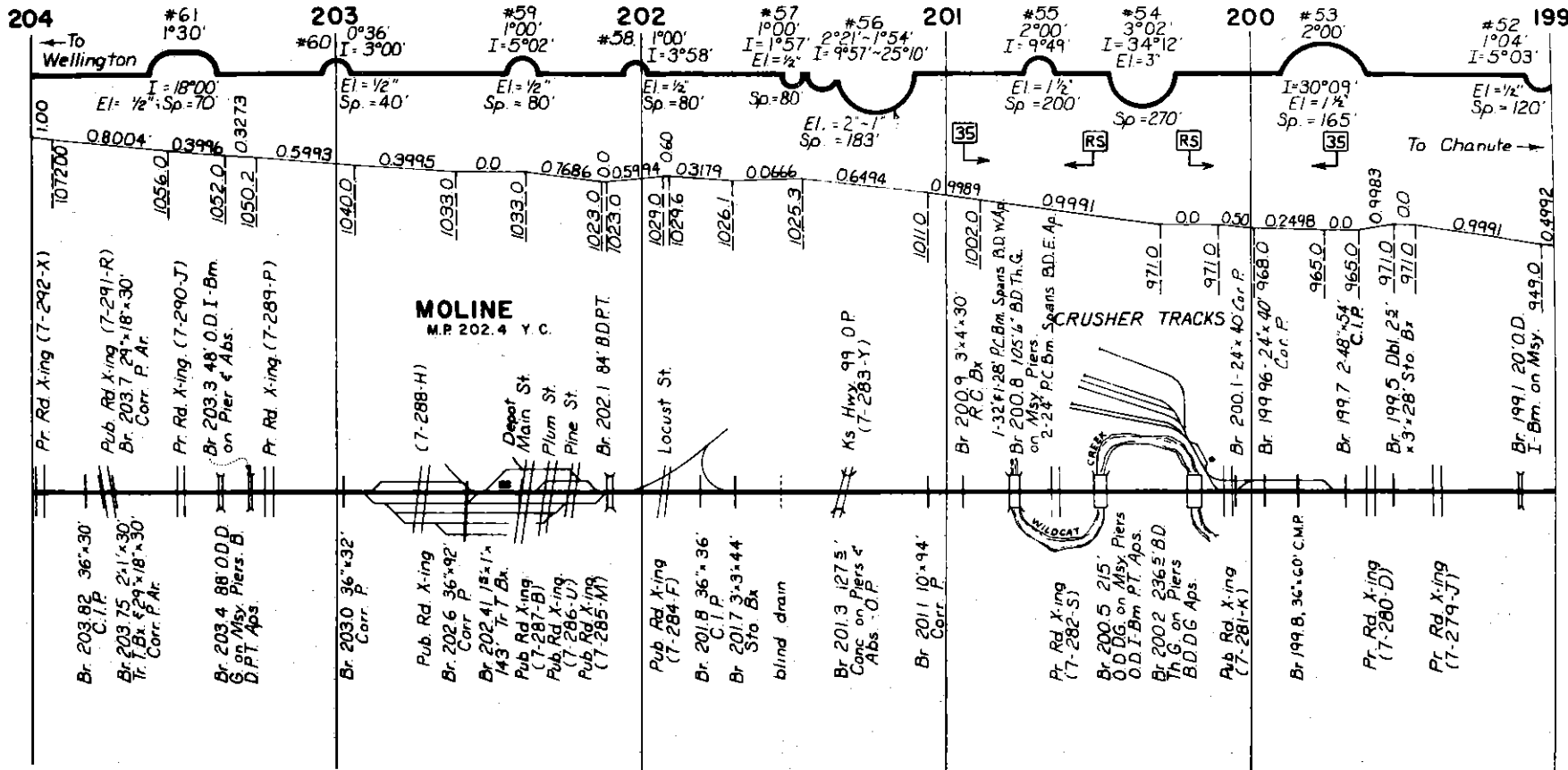
0.9009

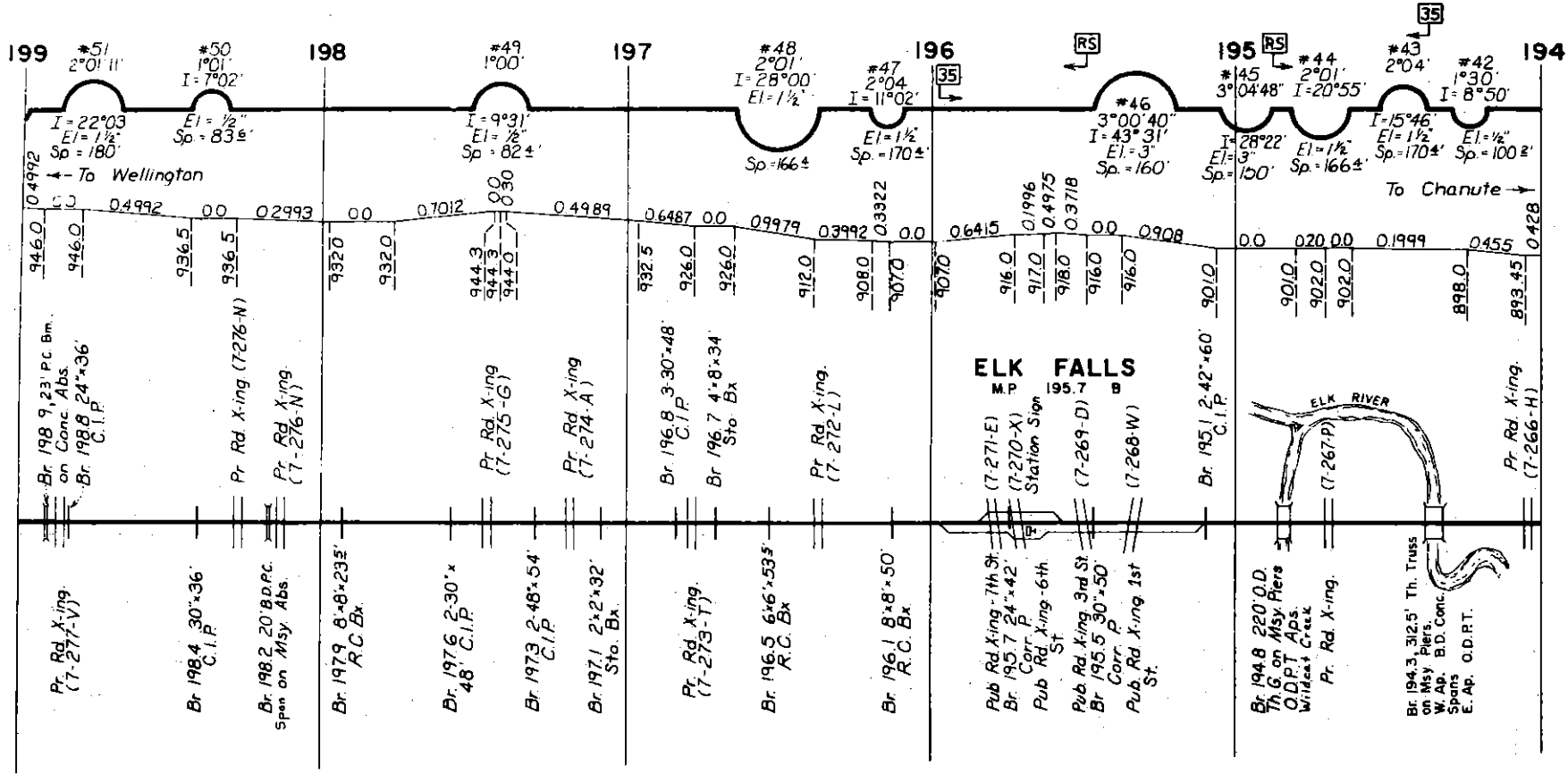
1.380

100.4

1088.0  
1088.0  
1090.0  
1090.0

100





199    \*51 2°01'11"    \*50 1°01' I=7°02'    198    \*49 1°00' I=9°31' EI=7/8' Sp=82.4    197    \*48 2°01' I=28°00' EI=1 1/2' Sp=166.4    196    \*47 2°04' I=11°02' EI=1 1/2' Sp=170.4    195    \*45 3°04'48" I=20°55' EI=1 1/2' Sp=160    \*44 2°01' I=20°55' EI=1 1/2' Sp=166.4    \*43 2°04' I=15°46' EI=1 1/2' Sp=170.4    \*42 1°30' I=8°50' EI=1/2' Sp=100.2    194

0.4992    0.4992    0.00    0.2993    0.00    0.7012    0.00    0.4989    0.6487    0.00    0.9979    0.3992    0.3322    0.00    0.6415    0.1996    0.4975    0.3718    0.00    0.9908    0.00    0.2000    0.1999    0.4555    0.428

946.0    946.0    936.5    936.5    932.0    932.0    944.3    944.3    944.0    932.5    926.0    926.0    912.0    908.0    907.0    907.0    916.0    917.0    918.0    916.0    916.0    901.0    901.0    902.0    902.0    898.0    893.45

← To Wellington    To Chanute →

Br. 198 9, 23' P.C. Bm. on Conc. Abs. (7-27-V) C.I.P.  
 Pr. Rd. X-ing (7-276-N)  
 Pr. Rd. X-ing (7-276-N)  
 Pr. Rd. X-ing (7-275-G)  
 Pr. Rd. X-ing (7-274-A)  
 Br. 196.8 3-30'x48' C.I.P.  
 Br. 196.7 4'x8'x34' Sto. Bx.  
 Pr. Rd. X-ing (7-272-L)  
 Pub. Rd. X-ing - 7th St. Br. 195.7 24'x42' Corr. P.  
 Pub. Rd. X-ing - 6th St. Br. 195.5 30'x50' Corr. P.  
 Pub. Rd. X-ing 1st St. Br. 195.5 30'x50' Corr. P.  
 Pub. Rd. X-ing 1st St. Br. 195.5 30'x50' Corr. P.  
 Pub. Rd. X-ing 1st St. Br. 195.5 30'x50' Corr. P.  
 Br. 194.8 220' O.D. Th. G. on Misy. Piers. O.D.P.T. Aps. Wildcat Creek  
 Pr. Rd. X-ing.  
 Br. 194.3 312.5' Th. Truss on Misy. Piers. W. Ap. B.D. Conc. Spans E. Ap. O.D.R.T.

**ELK FALLS**



194

\*41  
2°00'  
I = 10°00'  
El = 1 1/2"  
Sp = 165'

\*40  
2°00'  
I = 8°24'  
El = 2"  
Sp = 123'

193

\*39  
2°00'  
I = 17°15'  
El = 2"  
Sp = 180'

\*38  
3°00'24"  
I = 43°32'  
El = 3"  
Sp = 220'

192

\*37  
1°56'  
I = 10°15'  
El = 1"  
Sp = 1592'

\*36  
2°00'  
I = 11°09'  
El = 1 1/2"  
I = 165'

191

\*35  
1°31'  
I = 15°00'  
El = 1/2"  
Sp = 123'

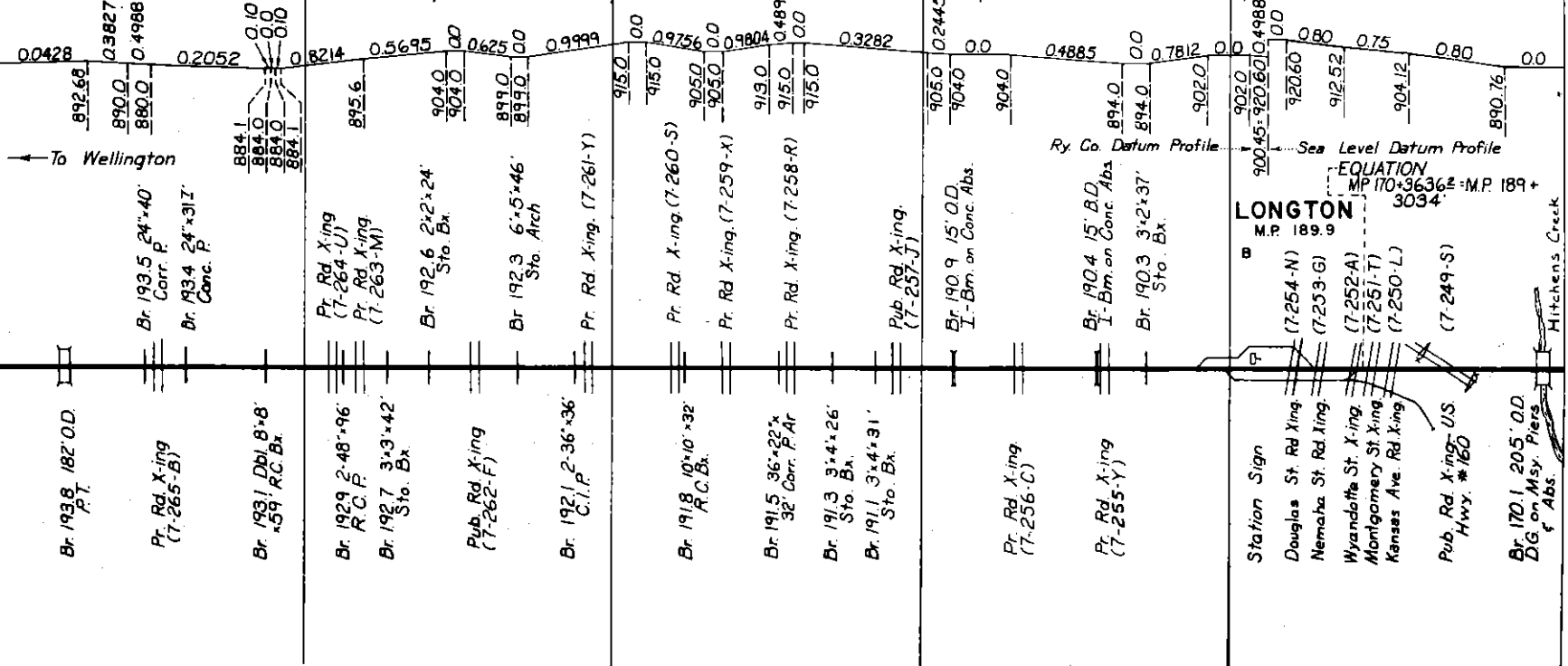
\*34  
2°00'  
I = 10°14'  
El = 1 1/2"  
Sp = 165'

190

\*33  
I = 1°00'  
I = 4°00'  
Sp = 66'

\*29  
2°00'  
I = 1°12'  
El = 2"  
Sp = 120'

170



← To Wellington

Ry. Co. Datum Profile

Sea Level Datum Profile

LONGTON

M.P. 189.9  
EQUATION  
M.P. 170.3636 = M.P. 189 + 3034

Hitchens Creek

Br. 193.6 182' O.D.  
P.T.

Pr. Rd. X-ing  
(7-265-B)

Br. 193.1 Dbl. 8'x8'  
59' R.C. Bx.

Br. 192.9 2'x48'x96'  
R.C. F.

Br. 192.7 3'x3'x42'  
Sto. Bx.

Pub. Rd. X-ing  
(7-262-F)

Br. 192.1 2'x36'x36'  
C.I.P.

Br. 191.8 10'x10'x32'  
R.C. Bx.

Br. 191.5 36'x22'x  
32' Corr. P. Ar.

Br. 191.3 3'x4'x26'  
Sto. Bx.

Br. 191.1 3'x4'x31'  
Sto. Bx.

Pr. Rd. X-ing  
(7-256-C)

Pr. Rd. X-ing  
(7-255-Y)

Station Sign

Douglas St. Rd. X-ing  
(7-254-N)

Nemaha St. Rd. X-ing  
(7-253-G)

Wyandette St. X-ing  
(7-252-A)

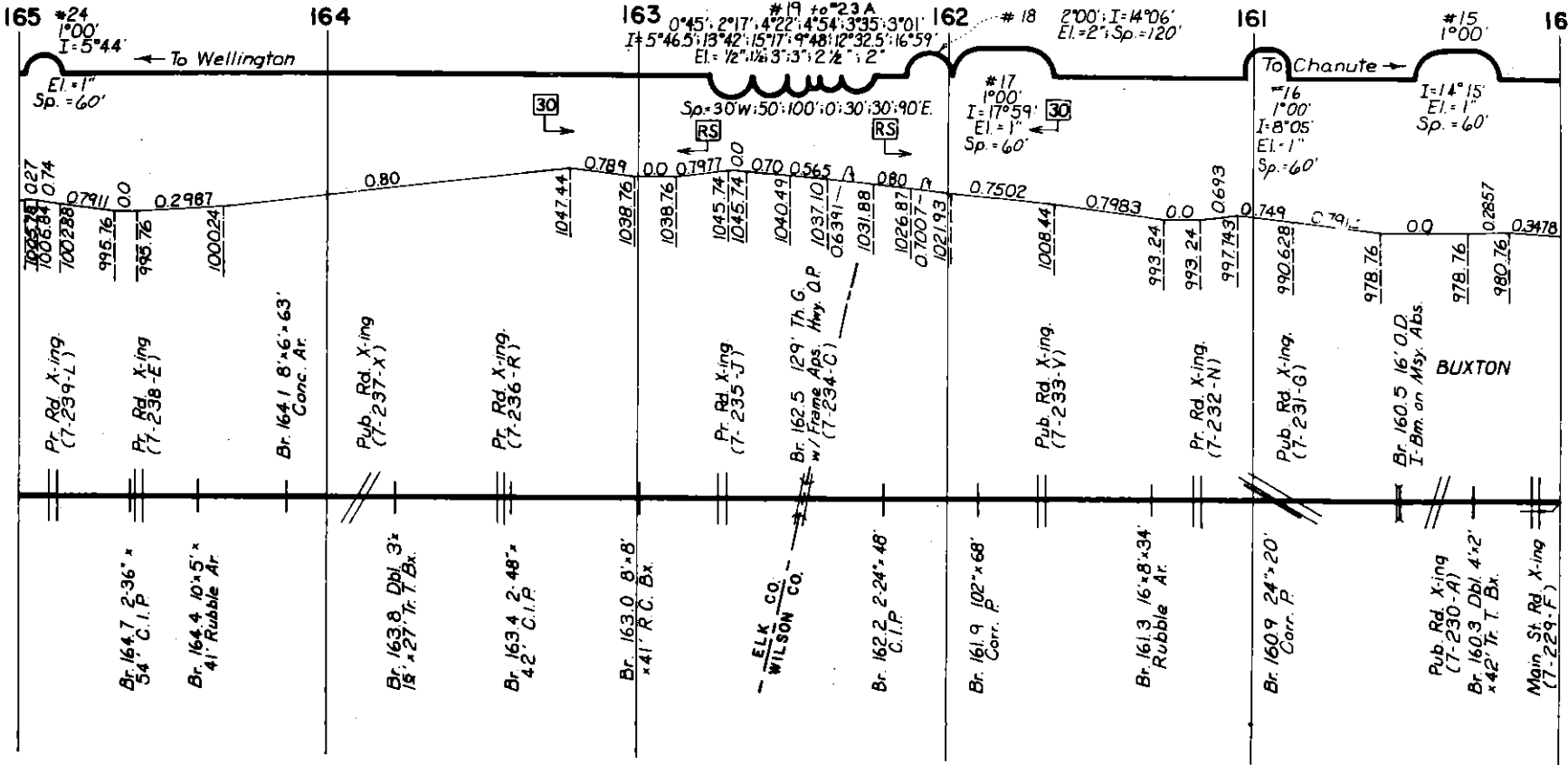
Montgomery St. X-ing  
(7-251-T)

Kansas Ave. Rd. X-ing  
(7-250-L)

Pub. Rd. X-ing- US  
Hwy. #160  
(7-249-S)

Br. 170.1 205' O.D.  
DG. on Msy. Piers  
f' Abs.





165 \*24 I=1°00' Sp.=60'

164

163 #19 +o=23A I=5°46.5'13'42'15'77'9'48'12'32.5'16'59' El.=1/2'1/2'3'3'2 1/2'2'

162 #18 2'00'; I=14°06' El.=2'; Sp.=120'

161

#15 I=14°15' El.=1' Sp.=60'

160

1000.24  
995.76  
995.76  
700.288  
1606.84  
1027  
0.74

Pr. Rd. X-ing  
(7-234-L)  
Pr. Rd. X-ing  
(7-238-E)

Br. 164.7 2-36' x  
54' C.I.P.  
Br. 164.4 10x5' x  
41' Rubble Ar.

Br. 164.1 8'x6'x63'  
Conc. Ar.

Pub. Rd. X-ing  
(7-237-X)

Br. 163.8 Dbl. 3x  
15' x 27' Tr. T. Bx.

Pr. Rd. X-ing  
(7-236-R)

Br. 163.4 2-48' x  
4.2' C.I.P.

Br. 163.0 8'x8'  
x 41' R.C. Bx.

ELK CO.  
WILSON CO.  
Br. 162.5 129' Th. G.  
w/ Frame Abs. Hwy. O.P.  
(7-234-C)

Br. 162.2 2-24'x48'  
C.I.P.

Br. 161.9 102'x68'  
Corr. P.

Br. 161.3 16'x8'x34'  
Rubble Ar.

Br. 160.9 24'x20'  
Corr. P.

Pub. Rd. X-ing  
(7-230-A)  
Br. 160.3 Dbl. 4x2'  
x 42' Tr. T. Bx.

Main St. Rd. X-ing  
(7-229-F)

Br. 160.5 16' O.D.  
I-Bm. on Msy. Abs.

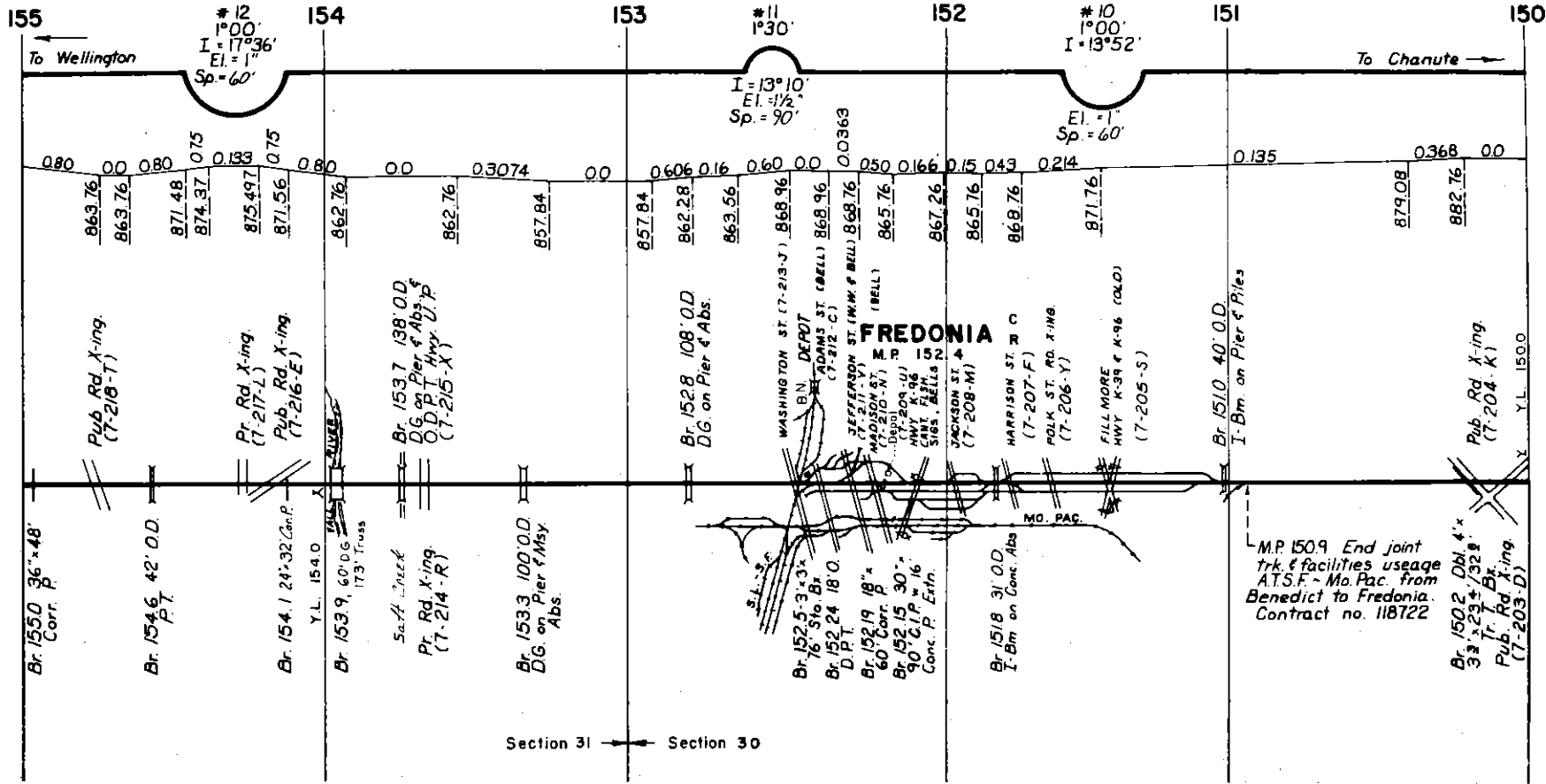
BUXTON

To Wellington

To Chanute







155

154

153

152

151

150

To Wellington

To Chanute

\*12  
 I = 17°36'  
 El. = 1'  
 Sp. = 60'

\*11  
 I = 13°10'  
 El. = 1/2'  
 Sp. = 90'

\*10  
 I = 13°52'  
 El. = 1'  
 Sp. = 60'

0.80 0.00 0.80 0.75 0.133 0.75 0.80 0.00 0.3074 0.00 0.606 0.16 0.60 0.00 0.0363 0.50 0.166 0.15 0.49 0.214 0.135 0.368 0.0

863.76 863.76 871.48 874.37 875.497 871.56 862.76 862.76 857.84 862.28 863.56 868.96 868.96 868.76 865.76 867.26 865.76 869.76 871.76 879.08 882.76

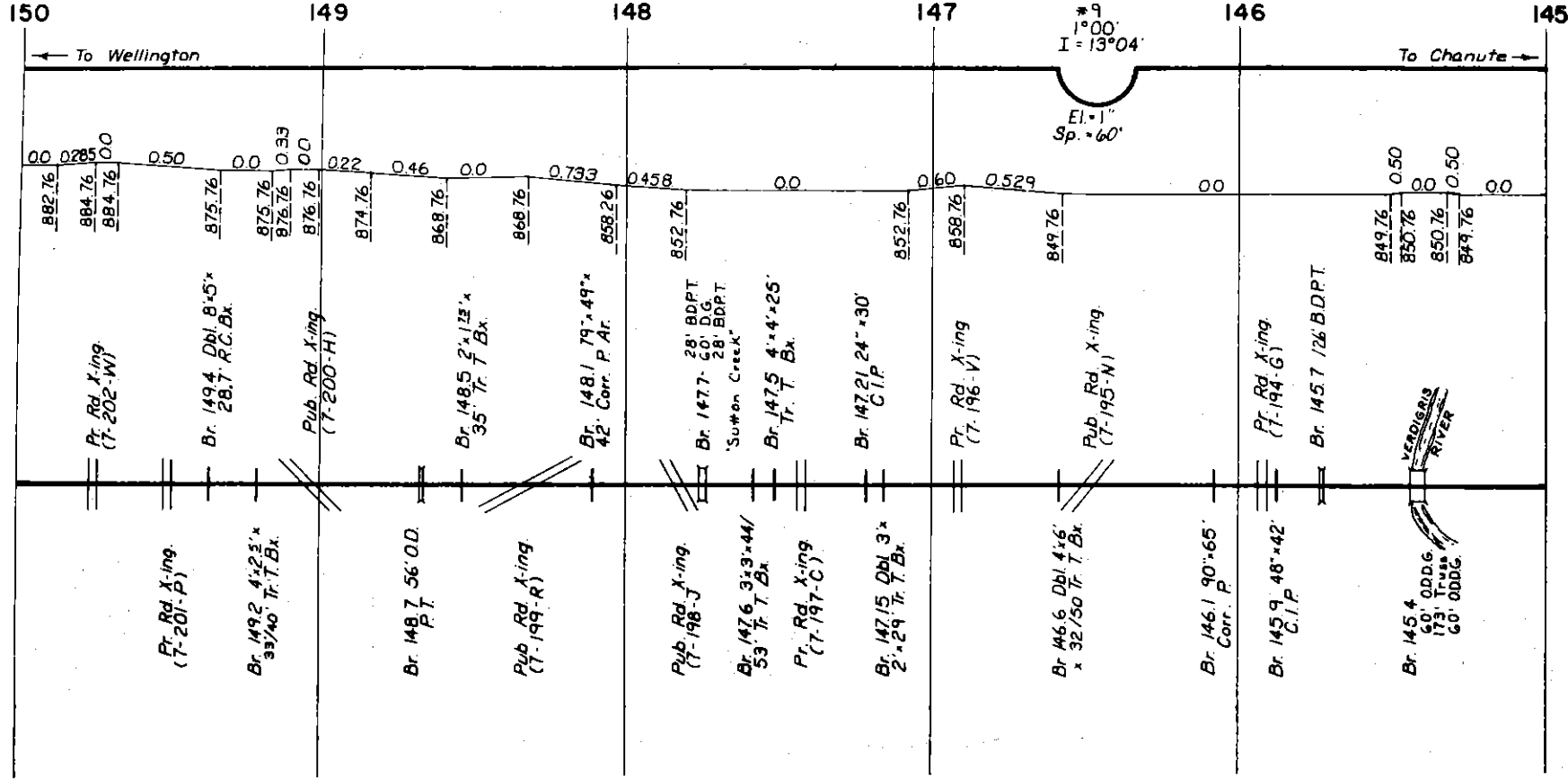
Pub. Rd. X-ing. (7-218-T)  
 Pr. Rd. X-ing. (7-217-L)  
 Pub. Rd. X-ing. (7-216-E)  
 Y.L. 154.0  
 Br. 153.9 60' O.G. 173' Truss  
 Sash Deck  
 Br. 153.7 138' O.D. D.G. on Pier f/ Abs. f/ O.D. f/ Hwy. U.P. (7-215-X)  
 Br. 153.3 100' O.D. D.G. on Pier f/ Msy Abs.  
 Br. 152.8 108' O.D. D.G. on Pier f/ Abs.  
 WASHINGTON ST. (7-213-J)  
 B.N. DEPOT  
 ADAMS ST. (BELL) (7-212-C)  
 JEFFERSON ST. (W.M. & BELL) (BELL) (7-211-V)  
 MADISON ST. (Legal) (7-209-U)  
 JACKSON ST. (7-208-M)  
 HARRISON ST. (7-207-F)  
 POLK ST. RD. X-ING (7-206-Y)  
 FILLMORE HWY K-39 & K-96 (OLD) (7-205-S)  
 Br. 151.0 40' O.D. I-Brn. on Pier f/ Piles  
 Pub. Rd. X-ing. (7-204-K)  
 Y.L. 150.0

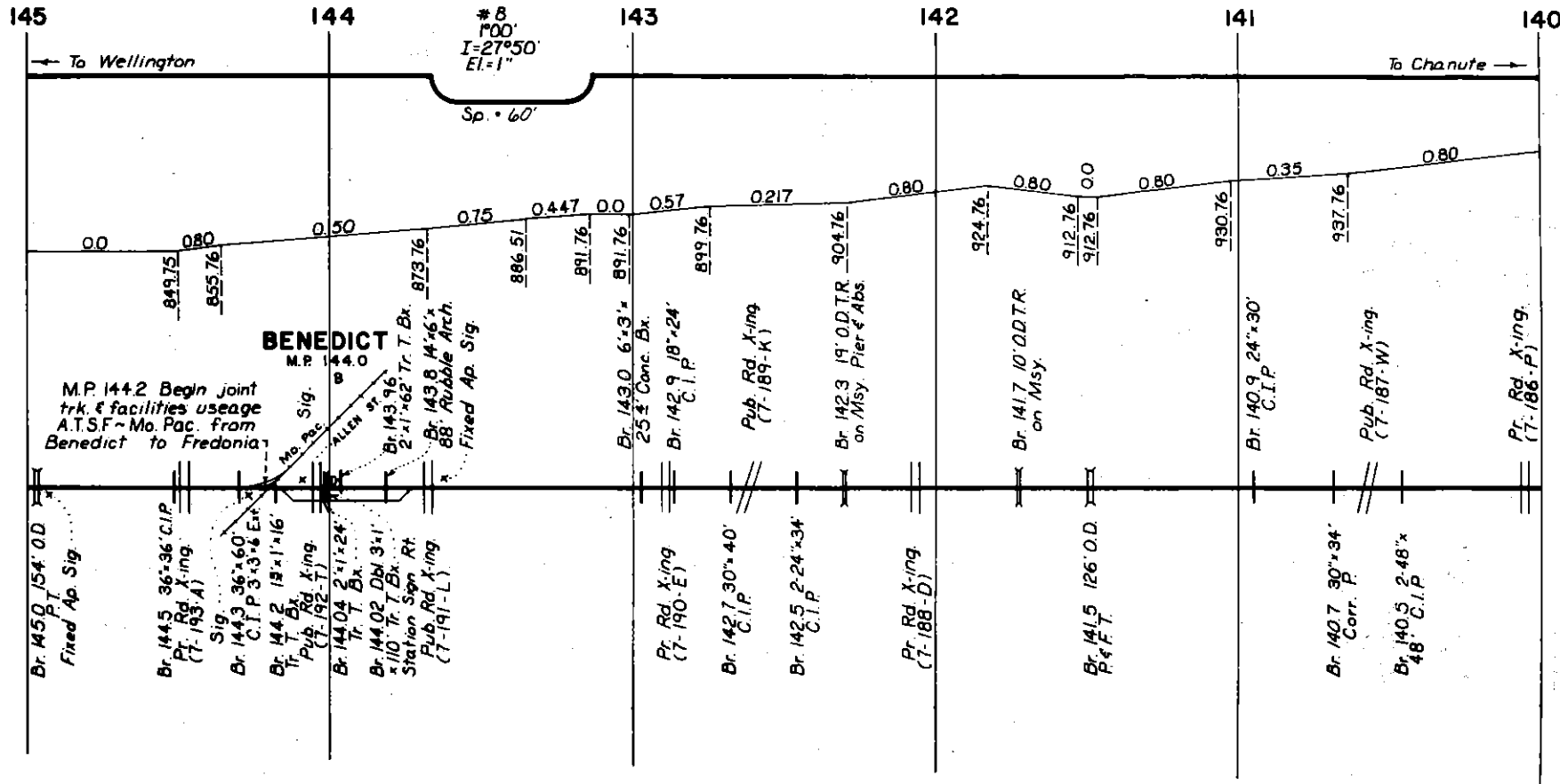
FREDONIA

MO. PAC.

Br. 155.0 36'x48' Cor.  
 Br. 154.6 42' O.D. P.T.  
 Br. 154.1 24'x32' Con.P.  
 Br. 153.9 60' O.G. 173' Truss  
 Br. 153.7 138' O.D. D.G. on Pier f/ Abs. f/ O.D. f/ Hwy. U.P. (7-215-X)  
 Br. 153.3 100' O.D. D.G. on Pier f/ Msy Abs.  
 Br. 152.8 108' O.D. D.G. on Pier f/ Abs.  
 Br. 152.5 3'x3'x 76' 3/4' Br.  
 Br. 152.4 18' O.D.P.T.  
 Br. 152.19 18'x 60' Cor. P.  
 Br. 152.15 30'x 40' C.P. = 16' Conc. P. Extn.  
 Br. 151.8 31' O.D. I-Brn. on Conc. Abs.  
 Br. 150.9 End joint trk. & facilities usage A.T.S.F. - Mo. Pac. from Benedict to Fredonia. Contract no. 118722  
 Br. 150.2 Dbl. 4'x 33'x29'x1328' Tr. Br. X-ing. Pub. Rd. X-ing. (7-203-D)

Section 31 Section 30





M.P. 144.2 Begin joint trk. & facilities usage A.T.S.F. - Mo. Pac. from Benedict to Fredonia

**BENEDICT**

\* 8  
100'  
I=27°50'  
El.=1'  
Sp.=60'

To Wellington ←

→ To Chanute

Br. 1450, 154' O.D. Pt. Fixed Ap. Sig.

Br. 1445, 36'x36' C.I.P. Pt. Rd. X-ing (7-193-A) Sig.

Br. 1443, 36'x60' C.I.P. 3'x3'x6' Ex.

Br. 1442, 12'x1'x16' Tr. Pub. Rd. X-ing (7-192-T)

Br. 14404, 2'x1'x24' Tr. T. Bx.

Br. 14402, Dbl. 3'x1'x10' Tr. T. Bx. Station Sign Rt.

Br. 14396, 2'x1'x62' Tr. T. Bx.

Br. 14388, 14'x6'x' Rubble Arch. Fixed Ap. Sig.

Br. 1430, 6'x3'x25' Conc. Bx. C.I.P.

Br. 1429, 18'x24' Pub. Rd. X-ing (7-189-K)

Br. 1423, 19' O.D. TR on Msy. Pier & Abs.

Br. 1415, 126' O.D. P & F T.

Br. 1417, 10' O.D. TR on Msy.

Br. 1409, 24'x30' C.I.P.

Br. 1407, 30'x34' Corr. P.

Br. 1405, 2'x48'x48' C.I.P.

Pr. Rd. X-ing (7-186-P)

0.0

0.80

0.50

0.75

0.447

0.0

0.57

0.217

0.80

0.80

0.0

0.80

0.35

0.80

924.76

912.76

912.76

990.76

937.76

0.80

849.75

855.76

886.51

891.76

891.76

899.76

924.76

912.76

912.76

990.76

937.76

0.80

849.75

855.76

886.51

891.76

891.76

899.76

924.76

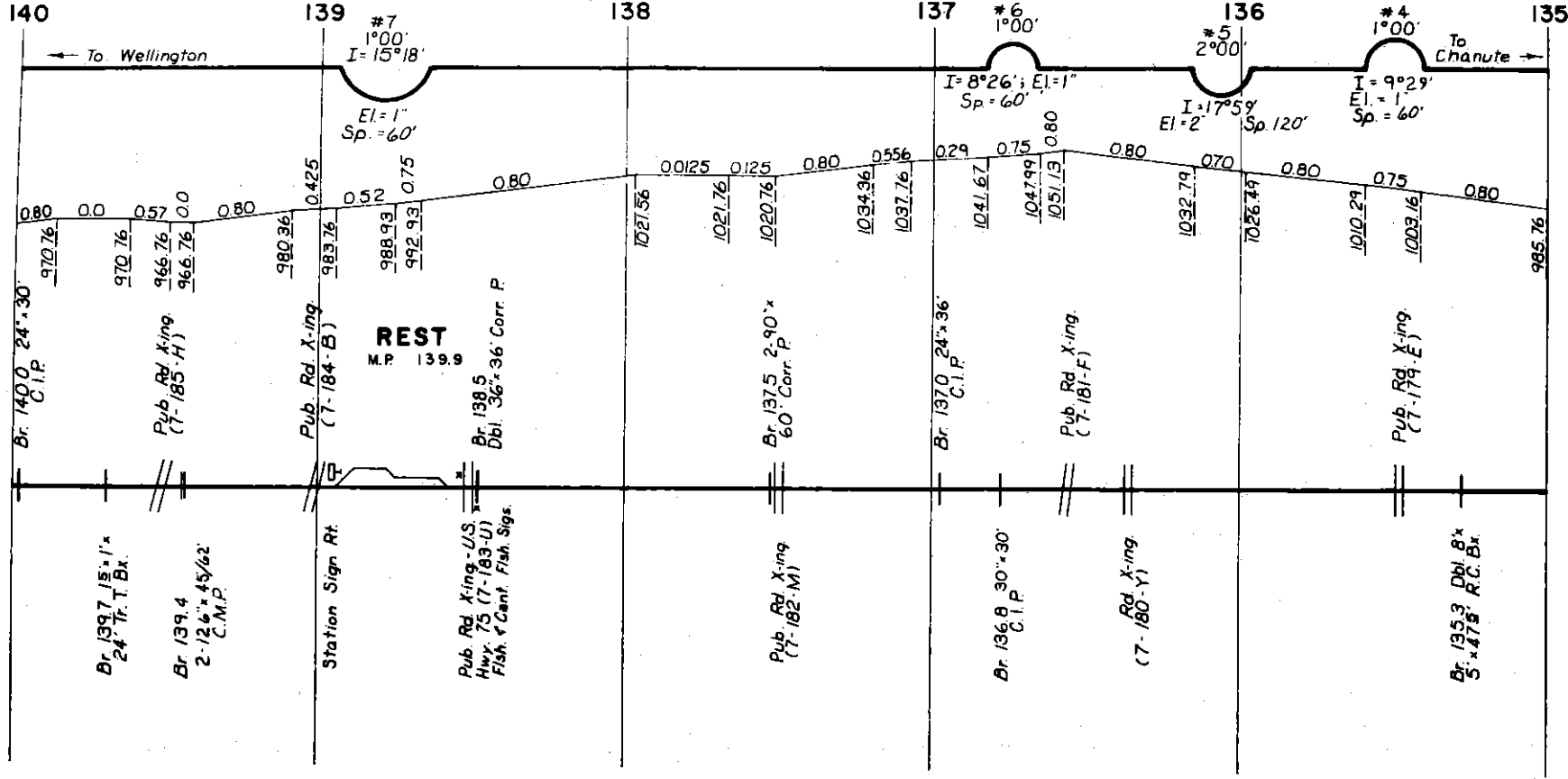
912.76

912.76

990.76

937.76

0.80



135

134

133

132

131

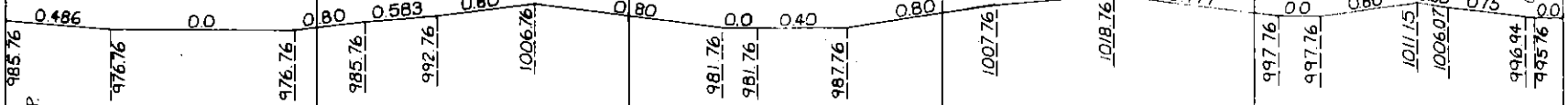
130

→ To Wellington

To Chanute →

#3  
1°59'  
I = 20°25'  
El = 2" Sp = 120'

#2  
1°00'  
I = 12°10'  
El = 1" Sp = 60'



985.76  
Br. 134.9 - 48" x 4' Con.P.

Main St.  
Pub. Rd. X-ing  
(7-177-R)  
Pub. Rd. X-ing  
(7-176-J)  
Br. 134.6 84' O.D.  
P&F T. Village Creek

Br. 134.1 74' O.D.  
I. Bm. on Conc.

Pub. Rd. X-ing.  
(7-176-J)

Br. 132.7 Dbl. 10' x 46' RC Bx.

Br. 132.4 3' 36" x 30' C.I.P.

Pub. Rd. X-ing  
(7-175-C)

Pub. Rd. X-ing  
(7-174-V)  
WILSON CO.  
NEOSHO CO.

Pr. Rd. X-ing  
(7-173-N)

Br. 1310 8' 8" x 50' RC Bx.

Y.L. 130.61  
Pub. Rd. X-ing  
(7-172-G)

Br. 130.2 24" x 46' C.I.P.

Pub. Rd. X-ing  
(7-170-T)

995.76

130

129

128

#1  
7°04' 7°30'

6°05'  
I=5°43'30'

I=63°06' 6°22'

← To Wellington

00 04375 06562 00 02576  
945.76 1002.76 960.76 960.76 965.14

Pub. Rd. X-ing.  
(7-169-Y)

Br. 128.99 2-54'x49' CMP  
Br. 128.91 1-36'x40' CMP  
Br. 128.95 4 wid.  
steel plate girder  
spans 1-50' 2-64.3'  
& 1-50' (U.S. 169 U.P.)

Br. 128.2 2-90'x  
48' Corr. P.

Washington St. X-ing  
(7-167-K)  
Pr. Rd. X-ing (7-166-D)  
Jct. Sign At.

To Ottawa  
Y.L. 127.65  
To Tulsa  
Br. 127.7-18"-20' Corr.P.

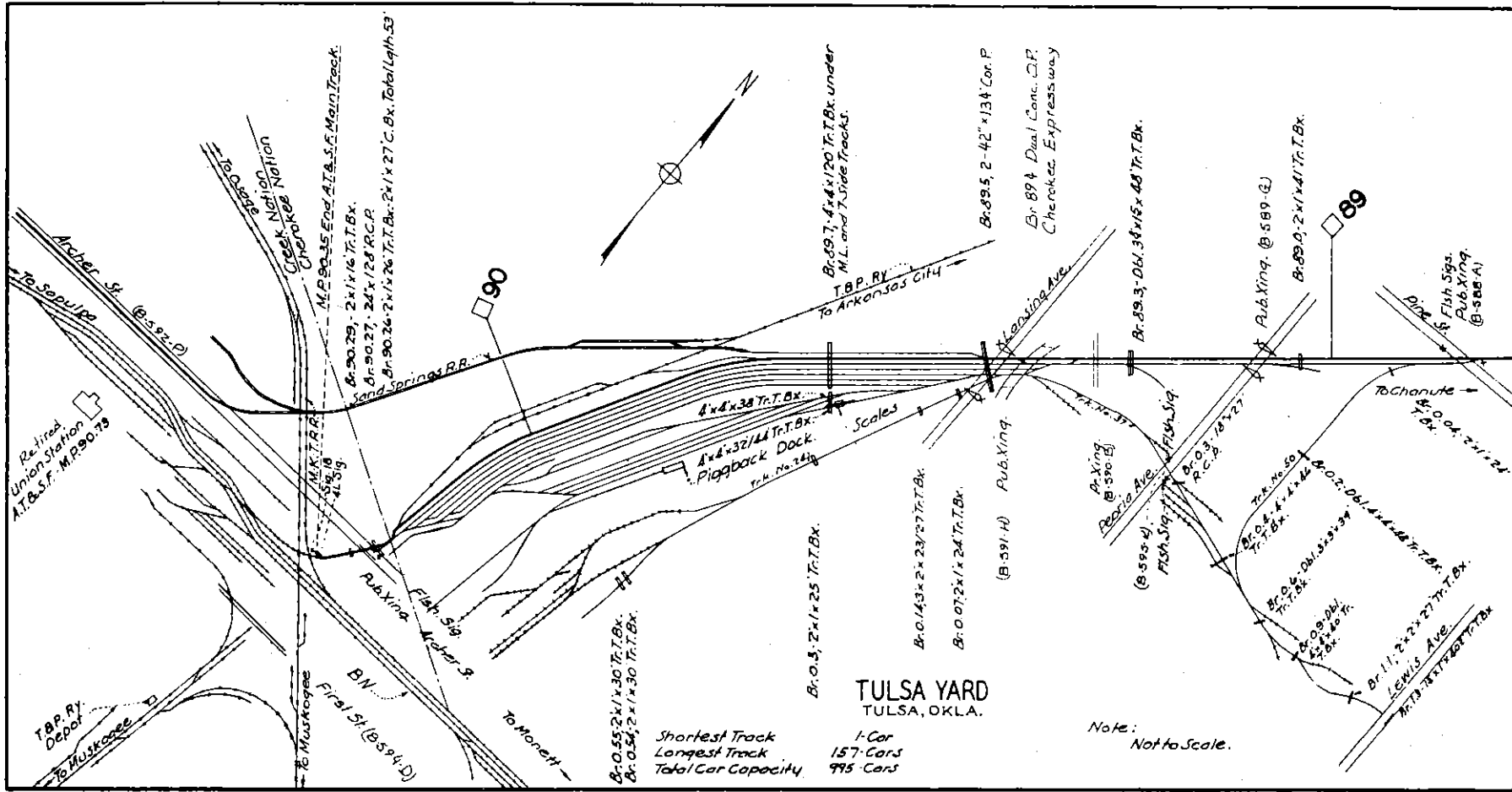
CHANUTE  
A. U. JCT.  
M.P. 127.7  
T. Y

Pub. Rd. X-ing.  
(7-168-S)

Section 30  
Section 34

Br. 130.0 24'x52'  
Conc. P.

FOR DETAILS OF CHANUTE, SEE PAGE 78.



Retired Union Station  
AT & S.F. M.P. 90.75

T.O.P. Ry. Depot

Shortest Track  
Longest Track  
Total Car Capacity

**TULSA YARD**  
TULSA, OKLA.

1-Car  
157-Cars  
995-Cars

Note:  
Not to Scale.





86

85

84

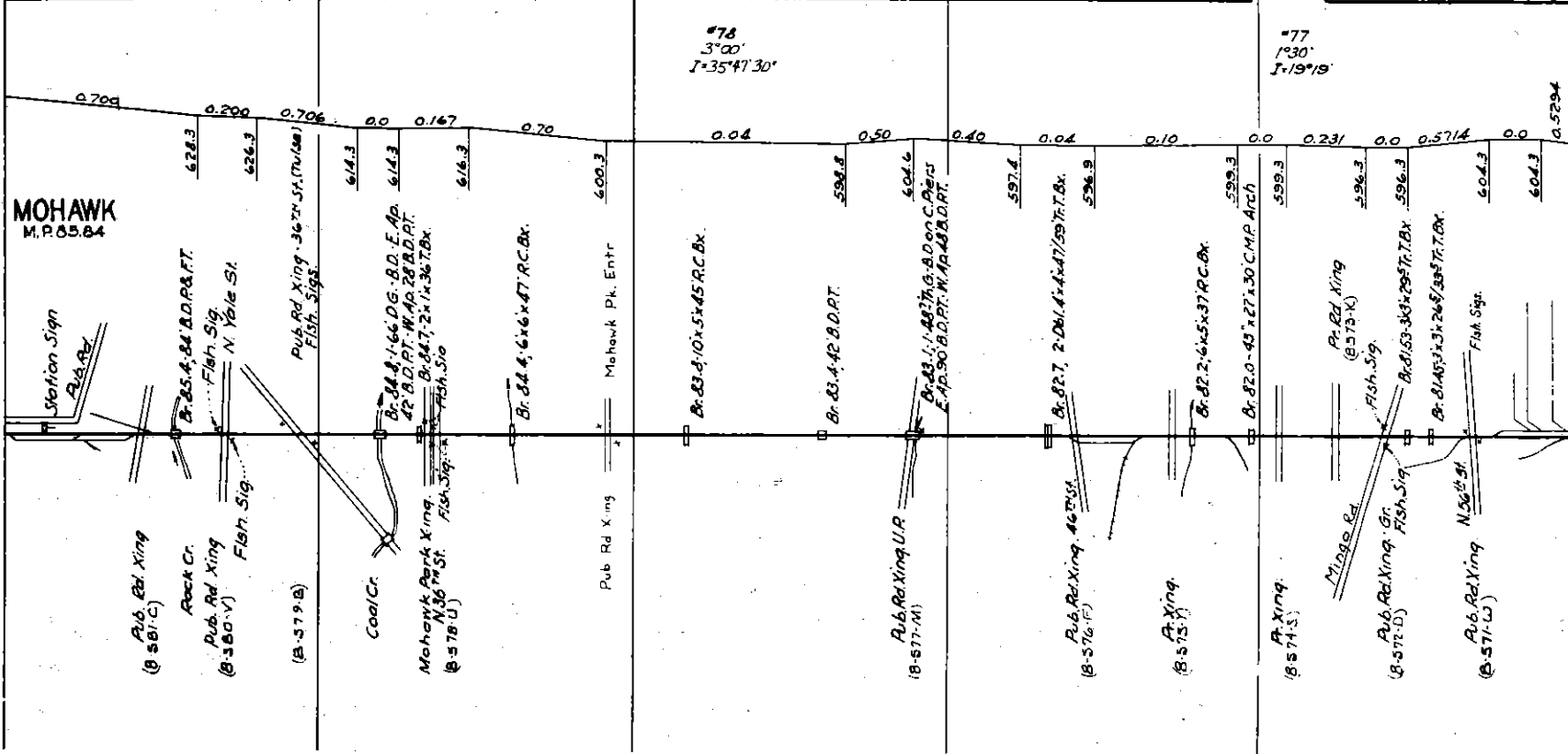
83

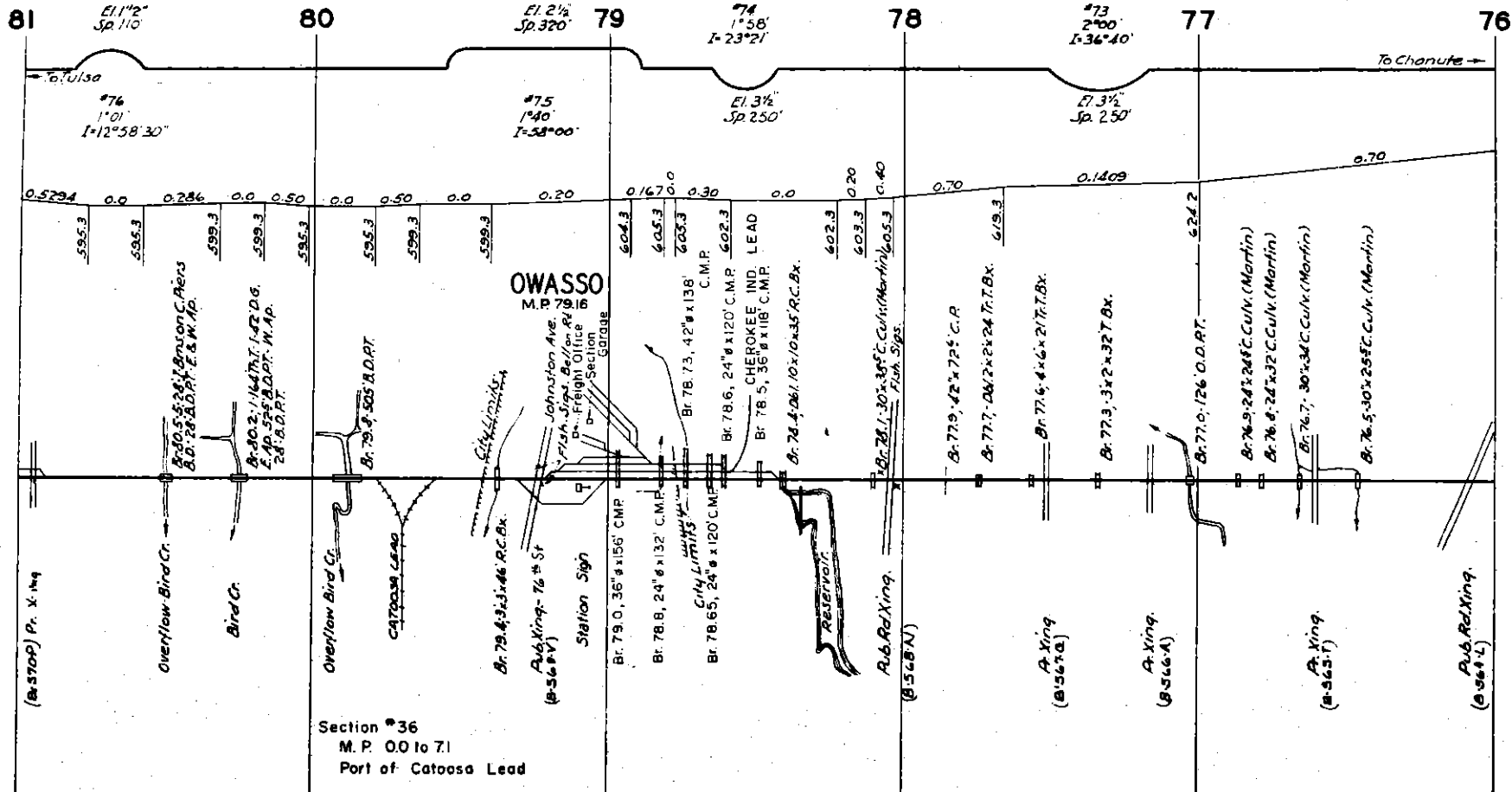
82

81

← To Tulsa

To Chanute →





Section #36  
 M. P. 00 to 71  
 Port of Catoosa Lead

**OWASSO**

M. P. 79.0 to 79.6

- Johnston Ave.
- Fish Sign, Bulletin Box
- City Limits
- Field Office
- Sec. Office
- Garage

RESERVOIR

To Tulsa

To Chanute

0.5794 0.0 0.286 0.0 0.50 0.0 0.50 0.0 0.20 0.167 0.0 0.30 0.0 0.20 0.40 0.70 0.1409 0.70 0.247

595.3 595.3 599.3 599.3 595.3 595.3 599.3 599.3 604.3 605.3 603.3 602.3 602.3 603.3 605.3 619.3 619.3 624.2

Overflow Bird Cr.  
 Br. 80.5, 24" x 24" Benson C. Piers  
 Br. 79.2, 116" Int. 1-42 D.G.  
 F.A.D. 525' B.D.P.T. W.A.P.  
 28' B.D.P.T.

Bird Cr.

Overflow Bird Cr.  
 Br. 79.8, 505' B.D.P.T.

CATDOOSA LEAD

Br. 79.4, 33' x 46' R.C. Box

Pub. King- 76' St  
 (B-366V)

Station Sign

Br. 79.0, 36" x 156' C.M.P.

Br. 78.8, 24" x 132' C.M.P.

City Limits

Br. 78.65, 24" x 120' C.M.P.

CHEROKEE IND. LEAD

Br. 78.5, 36" x 118' C.M.P.

Br. 78.4, 06' 10" x 10' x 35' R.C. Box

Pub. Rd. Xing.  
 (B-368N)

A Xing  
 (B-368A)

A Xing  
 (B-366A)

A Xing  
 (B-365F)

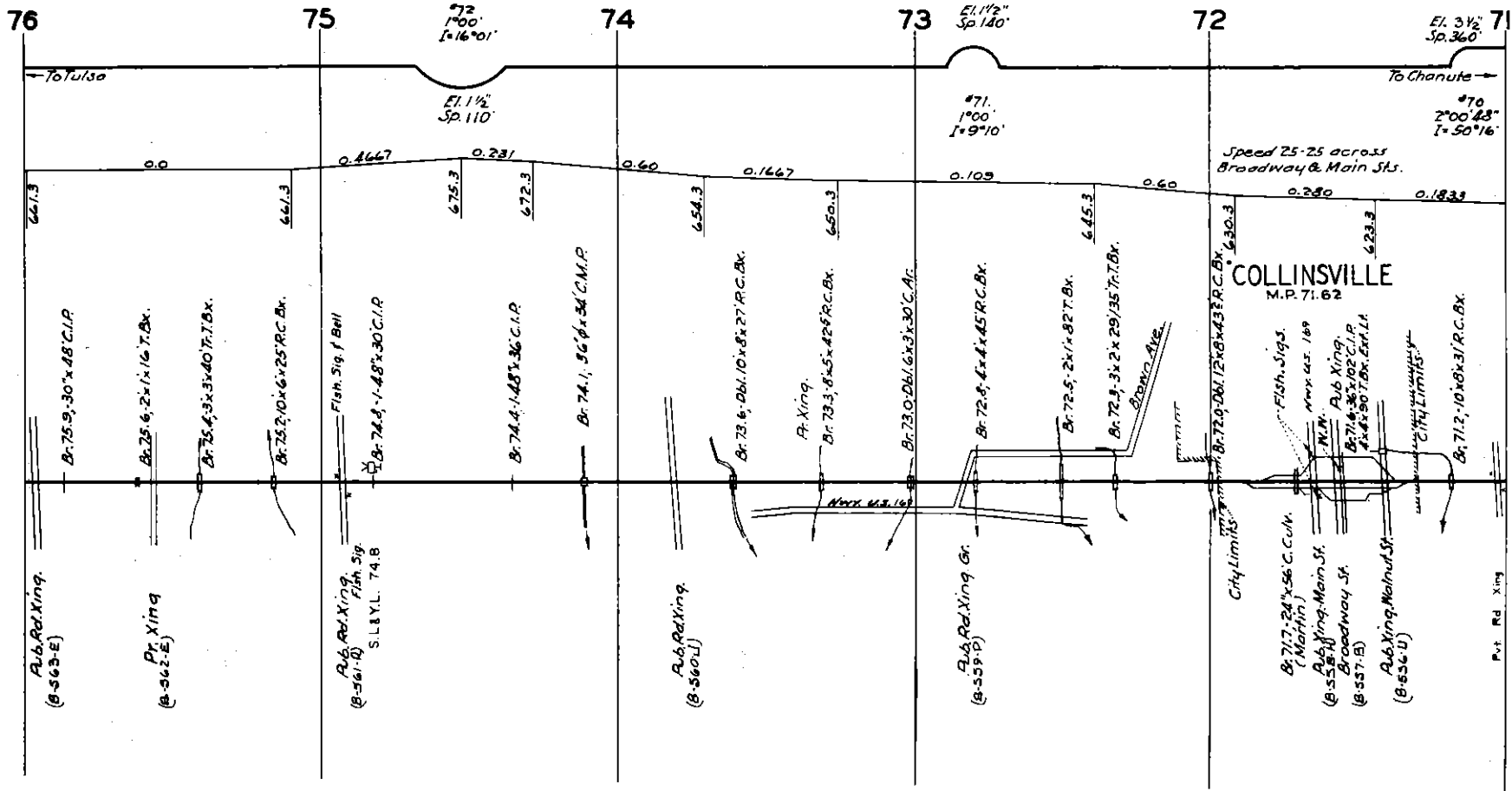
Pub. Rd. Xing.  
 (B-365L)

Br. 76.9, 24" x 24" C. Culv. (Martin)

Br. 76.8, 24" x 32" C. Culv. (Martin)

Br. 76.7, 30" x 34" C. Culv. (Martin)

Br. 76.5, 30" x 25" C. Culv. (Martin)



76

75

74

73

72

71

To Tulse

To Chanute

72  
1°00'  
Sp. 110'  
El. 1 1/2'  
Sp. 110'

71  
1°00'  
Sp. 140'  
El. 1 1/2'  
Sp. 140'

70  
2°00' 48"  
1°50' 16"  
El. 3 1/2'  
Sp. 360'

Speed 25-25 across  
Broadway & Main Sts.

COLLINSVILLE  
M.P. 71.62

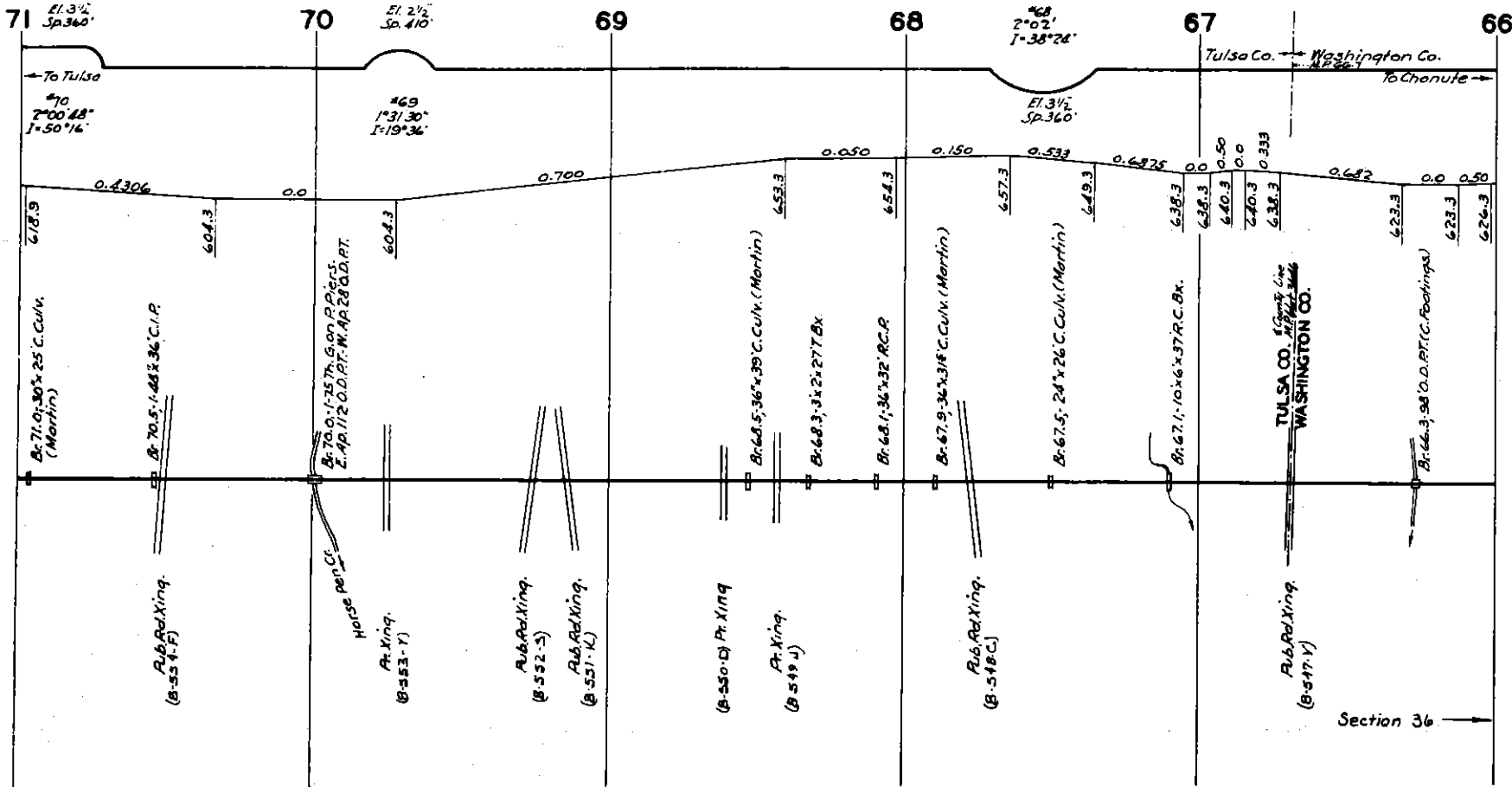
0.0  
0.4667  
0.281  
0.60  
0.1667  
0.109  
0.60  
0.280  
0.1833

661.3  
661.3  
675.3  
672.3  
654.3  
650.3  
645.3  
630.3  
623.3

Br. 75.9, 30' x 48' C.I.P.  
Br. 75.6, 2' x 16' T.Bx.  
Br. 75.4, 3' x 3' x 40' Tr. T.Bx.  
Br. 75.2, 10' x 6' x 25' R.C.Bx.  
Fish. Sig. f Bell  
Br. 74.4, 1-48' x 30' C.I.P.  
Br. 74.4, 1-48' x 36' C.I.P.  
Br. 74.1, 56' x 34' C.M.P.  
Br. 73.6, Dbl. 10' x 8' x 27' R.C.Bx.  
Pr. Xing.  
Br. 73.3, 8' x 5' x 42' R.C.Bx.  
Br. 73.0, Dbl. 6' x 3' x 30' C. Ar.  
Br. 72.8, 6' x 4' x 45' R.C.Bx.  
Br. 72.5, 2' x 1' x 82' T.Bx.  
Br. 72.3, 3' x 2' x 29' / 35' Tr. T.Bx.  
Dip 1/2"  
Br. 72.0, Dbl. 12' x 6' x 43' R.C.Bx.  
Fish. Sig. 3  
Pub. Rd. Xing. (B-563-E)  
Pub. Rd. Xing. (B-562-E)  
Pub. Rd. Xing. (B-561-U)  
Pub. Rd. Xing. (B-560-U)  
Pub. Rd. Xing. Gr. (B-559-P)  
Pub. Rd. Xing. (B-558-U)  
Pub. Rd. Xing. (B-557-B)  
Pub. Rd. Xing. (B-556-U)

Pub. Rd. Xing. (B-563-E)  
Pr. Xing (B-562-E)  
Pub. Rd. Xing. Fish Sig. S.L. Y.L. 74.8  
Pub. Rd. Xing. (B-561-U)  
Pub. Rd. Xing. (B-560-U)  
Pub. Rd. Xing. Gr. (B-559-P)  
Pub. Rd. Xing. (B-558-U)  
Pub. Rd. Xing. (B-557-B)  
Pub. Rd. Xing. (B-556-U)

City Limits  
Br. 71.7, 24' x 56' C. C. W.  
(Main St.)  
Pub. Rd. Xing. Main St.  
(B-558-U)  
Broadway St.  
(B-557-B)  
Pub. Rd. Xing. Walnut St.  
(B-556-U)





61

60

59

58

57

56

→ To Tulsa

← To Chanute

El. 2' Sp. 360'

El. 2' Sp. 290'

El. 1' Sp. 210'

#64  
2°02'36"  
I=27°04'

#63  
2°06'54"  
I=20°14'

El. 1' Sp. 210'

683.3  
0.4286  
677.3  
677.3  
681.3  
677.3  
681.3  
685.3

0.700

660.3  
659.3

0.0

659.3  
658.3  
658.3  
658.3

0.6675

0.700

0.3158

0.182

0.294

0.0

B-60.8-42.0 D.P.&F.T.  
C. Poolings

B-60.8-24.1 x 26.7 B.X.

A. Xing.  
(B-554-U)

Pub. Rd. Xing.  
(B-555-M)

B-59.7-84.8 D.P.T.

(B-552-F) A. Xing.  
B-59.5-30.1 x 26. C. Culv.  
(Martin)

B-59.2-2-30 x 36 C.I.P.

(B-551-V) Pub. Rd. Xing.  
B-59.0-64 x 59 R.C.B.X.

B-58.8-22-58 C.I.P. 42' x  
16' R.C. EXT.  
(B-530-S)

B-58.51-11 x 37 T. B.X.  
(B-529-K)-1

B-58.4924 x 100 C. Culv.  
(Martin)

A. Rd. Xing.  
(B-527-N) Double Cr.

RAMONA

M. 58.58

Delaware Ave.  
Sta. Sign  
Posterior Xing  
Osage Ave.  
Quadey Ave.  
City Limits

Dom

B-58.1-80 D.O.G. on R  
Peri. O.D. E. 10.70 O.D.  
P.L. W. 11.24 D.P.T.  
15' x 12' 13' O.P.T.  
2-30' x 150' Spacing  
C. Piers & Abutts.  
(B-526-C)

(B-553-V) A. Rd. Xing  
J. Fork Double Cr.

Pub. Rd. Xing.  
(B-524-N)

B-57.7-8 x 8 x 42 R.C.B.X.

B-57.2-1-24 x 24 C.I.P.

B-57.1-1-24 x 12 C.I.P. 2 x 3 x 3 No. B.X.

E. End & 1-24 x 13 Vit. R

B-56.8-70.0 D.P.&F.T.

B-56.4-1-30 x 26 C. Culv. (Martin)

B-56.5-2-42 x 36 C.I.P.

Pub. Rd. Xing.  
(B-525-G)

B-56.1-15 x 1 x 26 T. T. B.X.

56

← To Tulsa.

#61  
2°03'  
I-29°10'

El. 2'  
Sp. 360'

55

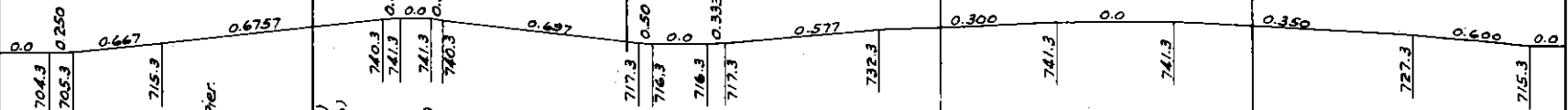
54

53

52

51

To Chanute →



Pub. Rd. Xing.  
(B-522-A)  
Br. 53-5, 42' 0.0' P&F.T.  
C. Footings.

Br. 53-3, 19' TR. B. Dan C. Abus & Pier.

(B-521-T) Fr. Xing.

Br. 54-0, 24' x 28' C. Culv. (Martin)  
Br. 54-9, 26' x 25' C. Culv. (Martin)

Pub. Rd. Xing.  
(B-520-L)

740.3  
741.3  
0.0  
741.3  
740.3  
0.50

Br. 54-6, 30' x 22' C. Culv. (Martin)  
B. 4.4' x 35' T. D.K.

Fr. Xing.  
(B-519-S)

Br. 53-9, 70' B.D. P&F.T.  
C. Footings.

Fr. Xing.  
(B-518-K)  
(B-517-D) Pub. Rd. Xing.  
J. Fork Double Cr.

717.3  
716.3  
0.0  
716.3  
717.3  
0.333

Br. 53-6, 70' B.D. P&F.T.  
C. Footings.

Pub. Rd. Xing.  
(B-516-W)

Br. 53-1, 28' x 27' C. Culv. (Martin)

732.3

Br. 53-0, 8' x 6' x 30' R.C. Box.

Br. 52-7, 24' x 45' T. D.K.  
Pub. Xing - South Ave.  
(B-515-P)

0.300

City Limits

Pub. Sign

City Limits

Sta. Sign  
Pub. Xing. Main St. Run. Sign  
(B-514-T)

OCHELATA  
M.P. 52.46

741.3

Br. 52-3, 50' x 30' x 36'  
Culv. Arch

741.3

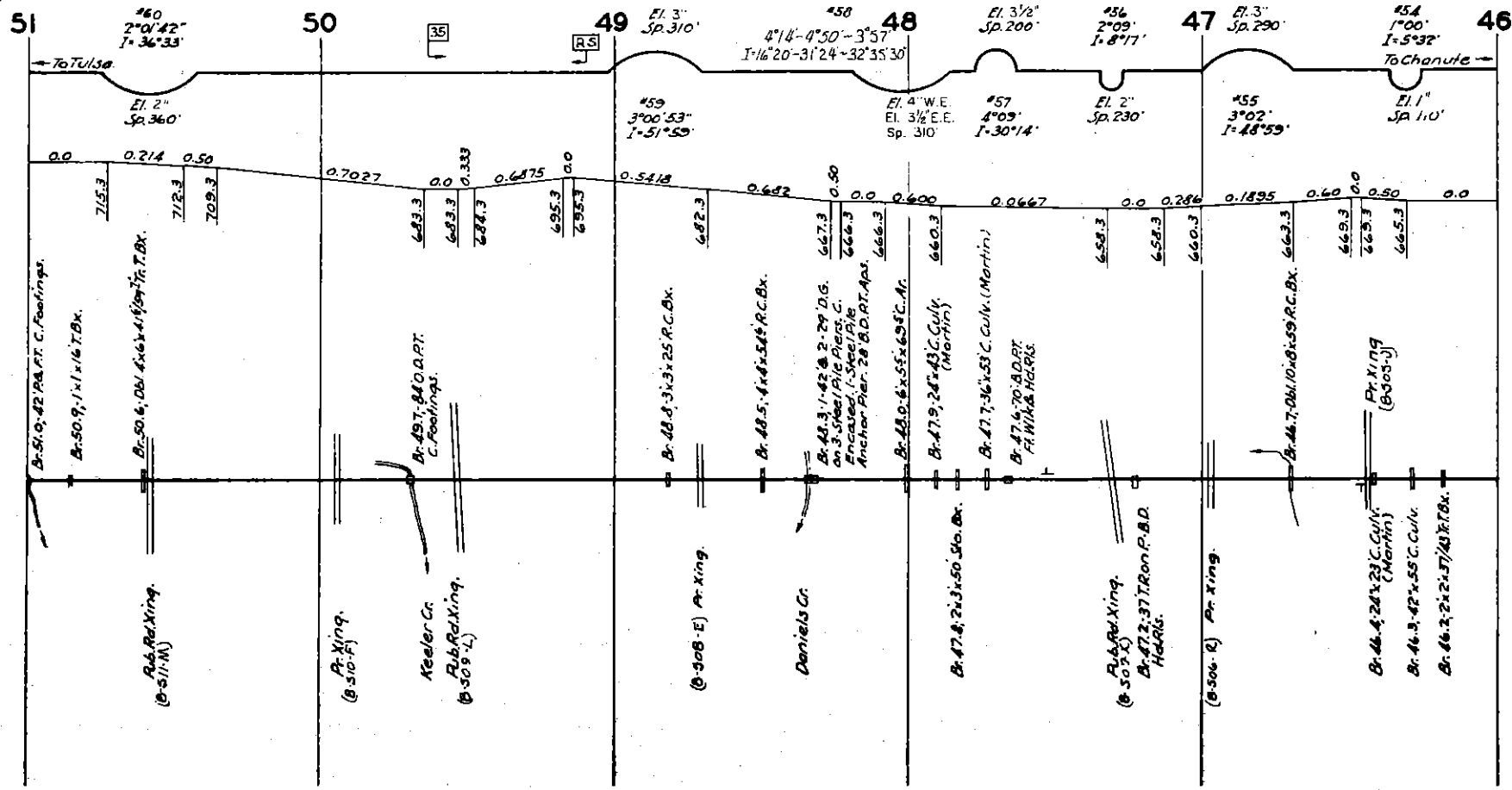
Pub. Rd. Xing.  
(B-513-B)

727.3

Br. 51-2, 06.12' x 10' x 27' C. Box.

Fr. Xing  
(B-512-U)

0.600  
715.3  
0.0



51

50

49

48

47

46

460  
2°0'42"  
I=36°33'

El. 3"  
Sp. 310'

458  
4°14'-4°50'-3°57'  
I=16°20'-31°24'+32°35'30"

El. 3 1/2"  
Sp. 200'

El. 3"  
Sp. 290'

454  
1°00'  
I=5°32'

El. 2"  
Sp. 360'

459  
3°00'53"  
I=51°50'

El. 4" W.E.  
El. 3 1/2" E.E.  
Sp. 310'

457  
4°09"  
I=30°14'

El. 2"  
Sp. 230'

455  
3°02"  
I=48°59'

El. 1"  
Sp. 110'

0.0 0.214 0.50 0.7027 0.0 0.333 0.6875 0.0 0.5418 0.682 0.50 0.0 0.600 0.0 0.667 0.0 0.286 0.1895 0.60 0.0 0.50 0.0

Br. 51.0: 42' PA. FT. C. Footings.

Br. 50.9: 1x1x16 T.Bx.

Rub. Red. Xing.  
(P-511-N)

P. Xing.  
(P-510-F)

Keeler Cr.  
Br. 49.7: 64' O.D.P.T.  
C. Footings.

Rub. Red. Xing.  
(P-509-L)

Br. 48.3: 3x3x25 R.C. Bx.

(P-508-E) P. Xing.

Br. 48.5: 4x4x54 R.C. Bx.

Deniels Cr.

Br. 48.3: 1-42" & 2-24" D.G.  
on 3 Steel Pile Piers. C.  
Encased in Steel Pile  
Anchor Pier. 28' O.D. AT. Apps.

Br. 48.0: 6x55x65 C. Ar.

Br. 47.8: 2x3x50 S40. Bx.

Br. 47.9: 24x43 C. Culv.  
(Martin)

Br. 47.7: 36x53 C. Culv. (Martin)

Br. 47.4: 70' O.D.P.T.  
FL. Wlk. & Hd. LRS.

Rub. Red. Xing.  
(P-509-X)  
Br. 47.2: 37' Iron P.B.D.  
Hd. LRS.

658.3

658.3

650.3

(P-506-E) P. Xing.

663.3

669.3

669.3

665.3

P. Xing  
(P-505-J)

Br. 46.4: 24x23 C. Culv.  
(Martin)

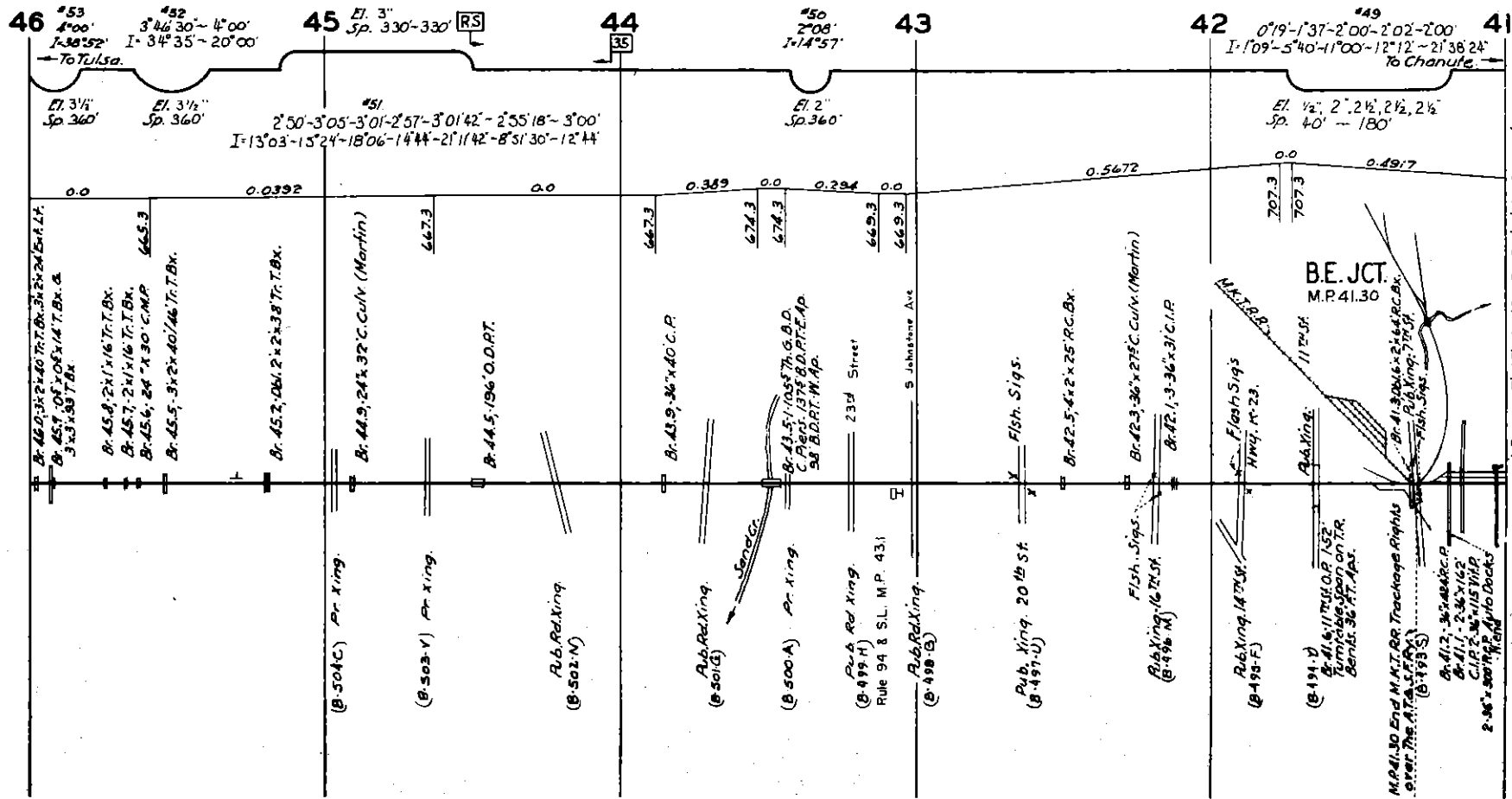
Br. 46.3: 42x55 C. Culv.

Br. 46.2: 2x2x37/43 T.Bx.

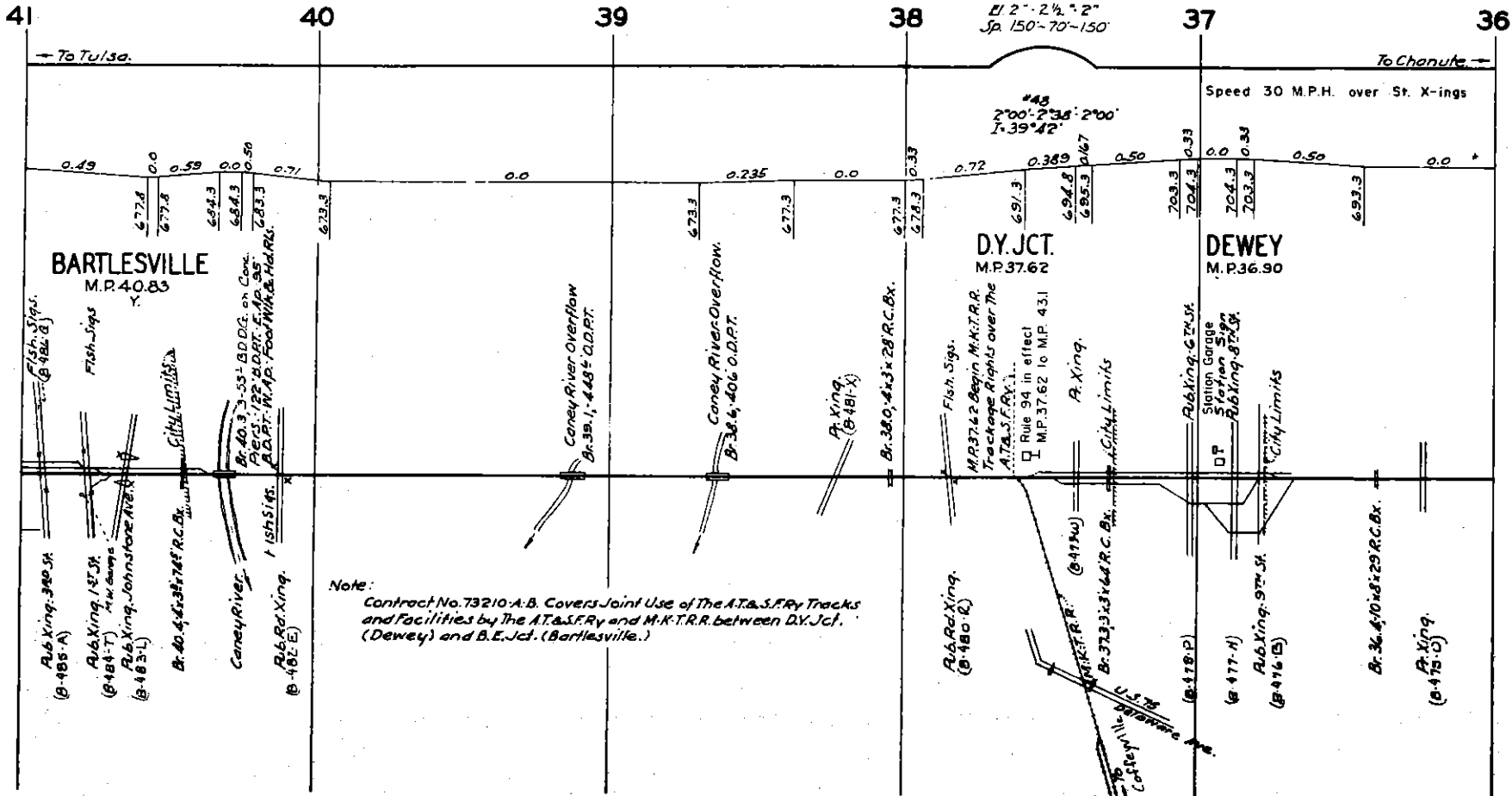
← To Tulse

To Charute →





Br. 41.0:  $7 \times 75 \times 287$  R.C. Bx. Redestrian Tunnel



36

35

34

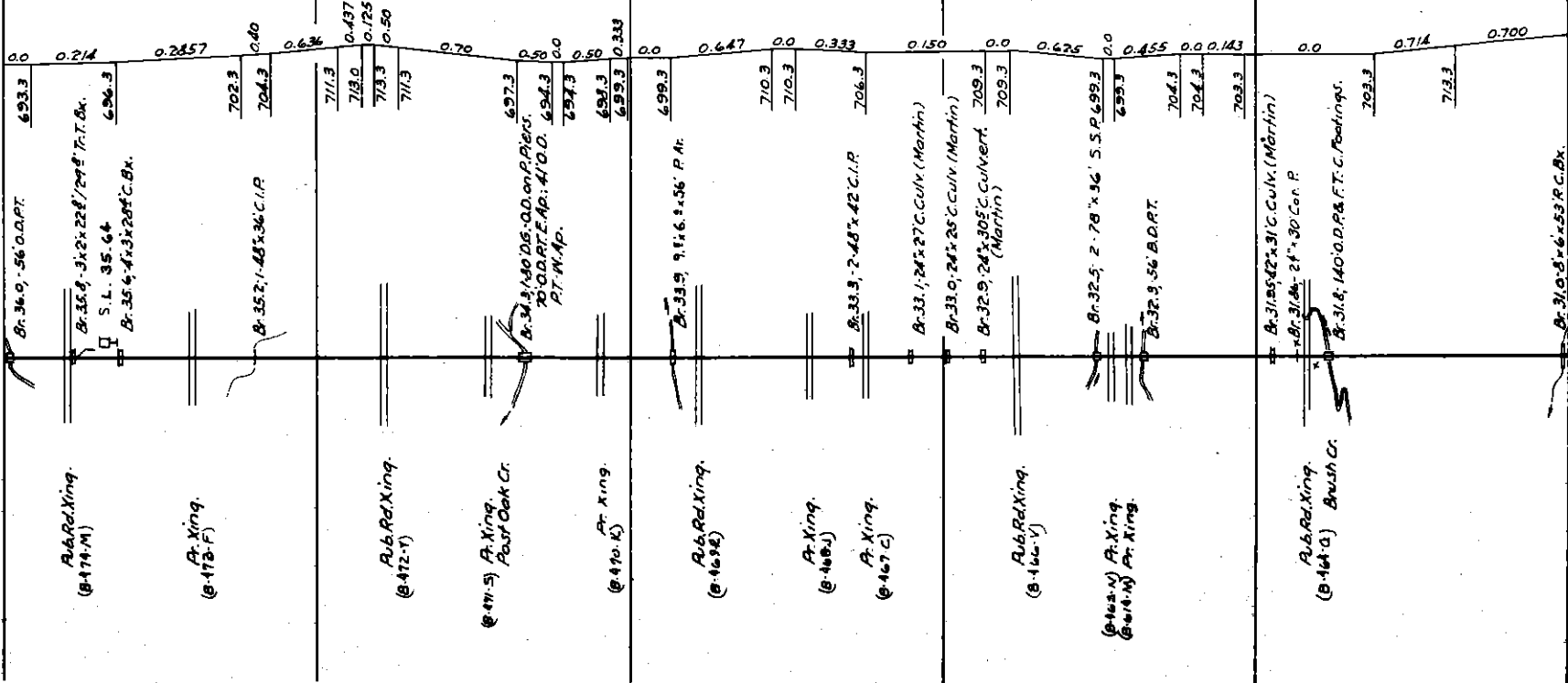
33

32

31

To Tulsa.

To Chanute.



447  
1°01'  
1-10°30'

El. 1 1/2"  
Sp. 270'  
446  
1°02'  
1-8°21'

445  
2°08'  
1-17°11'

To Tulsa.

To Chanute.

El. 3 1/2'  
Sp. 320'

El. 1 1/2'  
Sp. 180'

El. 1 1/2'  
Sp. 180'

#44  
2°03'  
I=17°24'

#43-A  
1°30'  
I=40°38'03"

#43  
1°30'  
I=42°48'12"

0.70

755.3  
758.3  
759.3

0.10

752.3  
746.7  
746.7

COPAN  
M.P. 29.99

0.70

742.3  
746.3

0.70

749.2  
740.1

0.0

Ribbed Xing. (B-423-A)  
15x15' x 18' x 30' Con. P.  
15x15' x 30' T.B.X.

Br. 30.5, 18' x 30' Con. P.  
Br. 30.3, 15' x 30' T.B.X.

City Limits

Br. 30.08, 12' x 48' C.M.P.  
STATION SIGN  
Br. 30.03, 24' x 24' C.M.P.  
Br. 29.99, 18' x 40' Con. P.  
Br. 29.96, 2' x 37' R.C.B.X.

City Limits

Ribbed Xing. (B-401-L)  
Golden Ave.  
Br. 29.6, 70' O.D.P.T.

Pt. Xing.  
(B-460-E)

Ribbed Xing.  
(B-459-K)

+ 3611.5 = END LINE CHANGE \*

+ 2760.2  
Br. 28.5, 3-5' x 4' x 74' R.C.B.X.

Br. 28.1, 3-5' x 4' x 86' R.C.B.X.

Pooler Creek

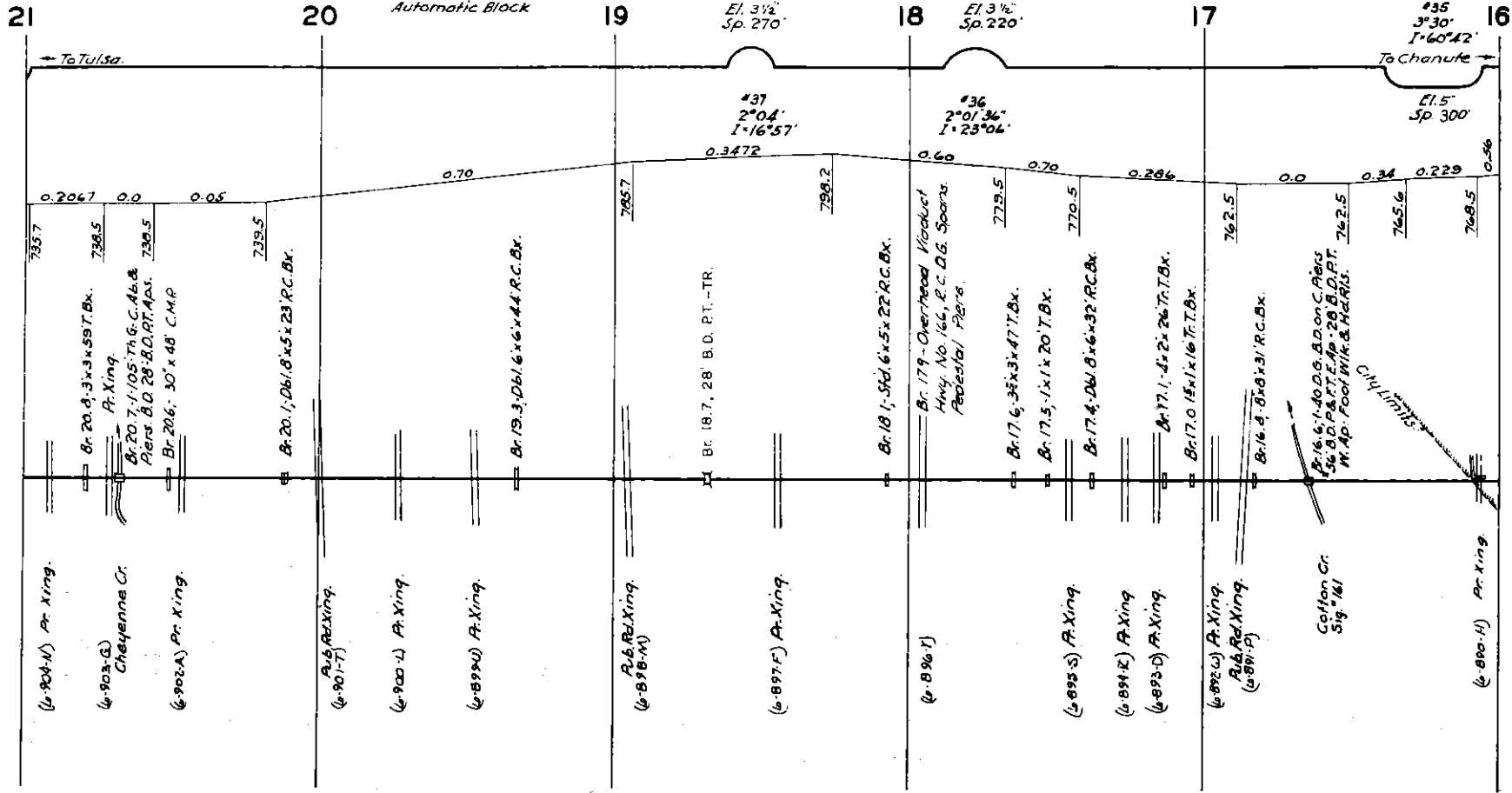
Br. 27.9, 158' P.C. Bm.-B.D./Hdri.  
3 span on steel. Lt. & Rt.

Cotton Creek

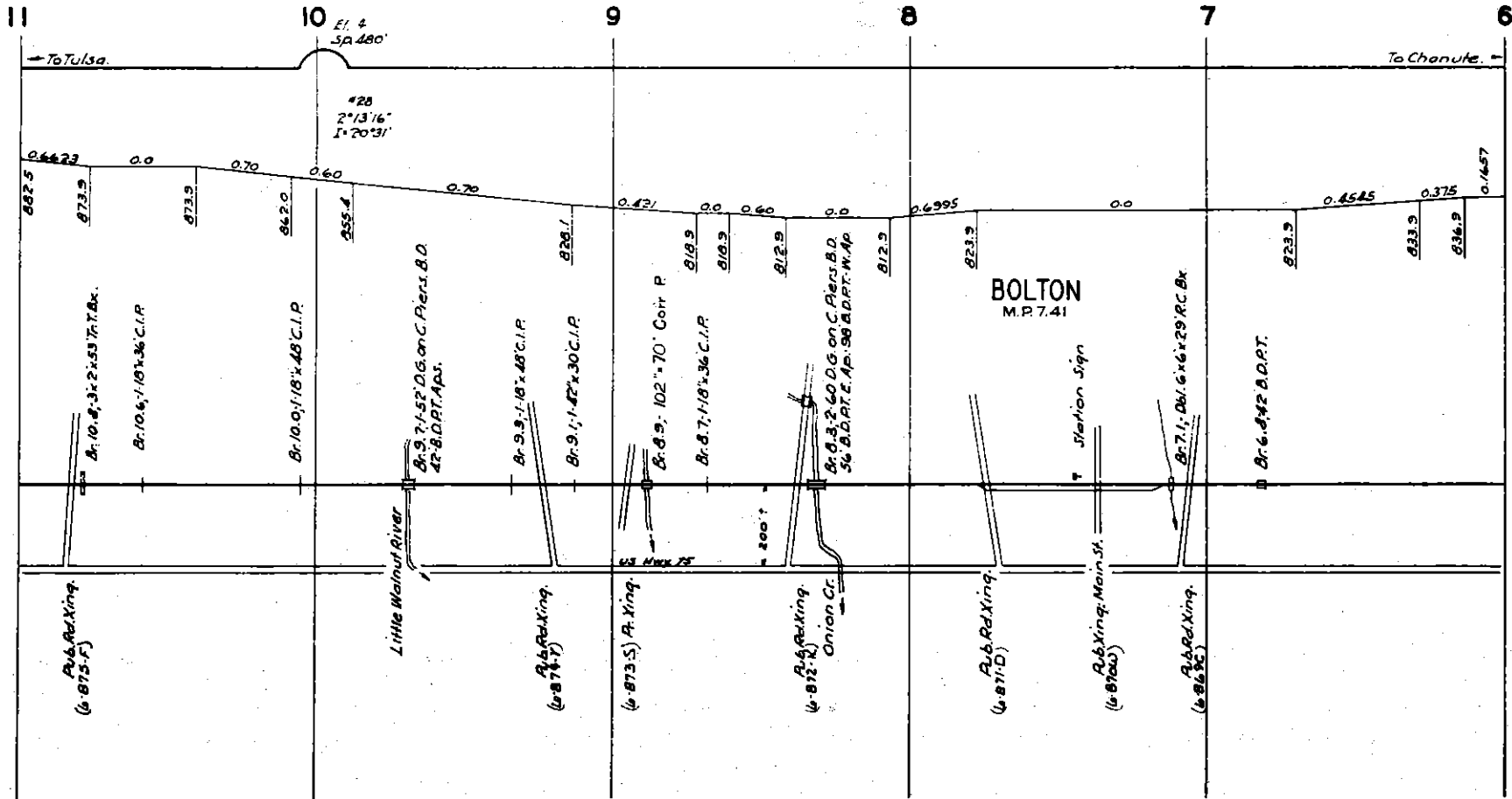
Br. 27.7, 3' x 3' x 105' R.C.B.X.  
Br. 27.4, 265' P.C. Bm.-B.D./Hdri.  
5 span on steel. Lt. & Rt.

Br. 26.6, 8' x 5' x 138' R.C.B.X.

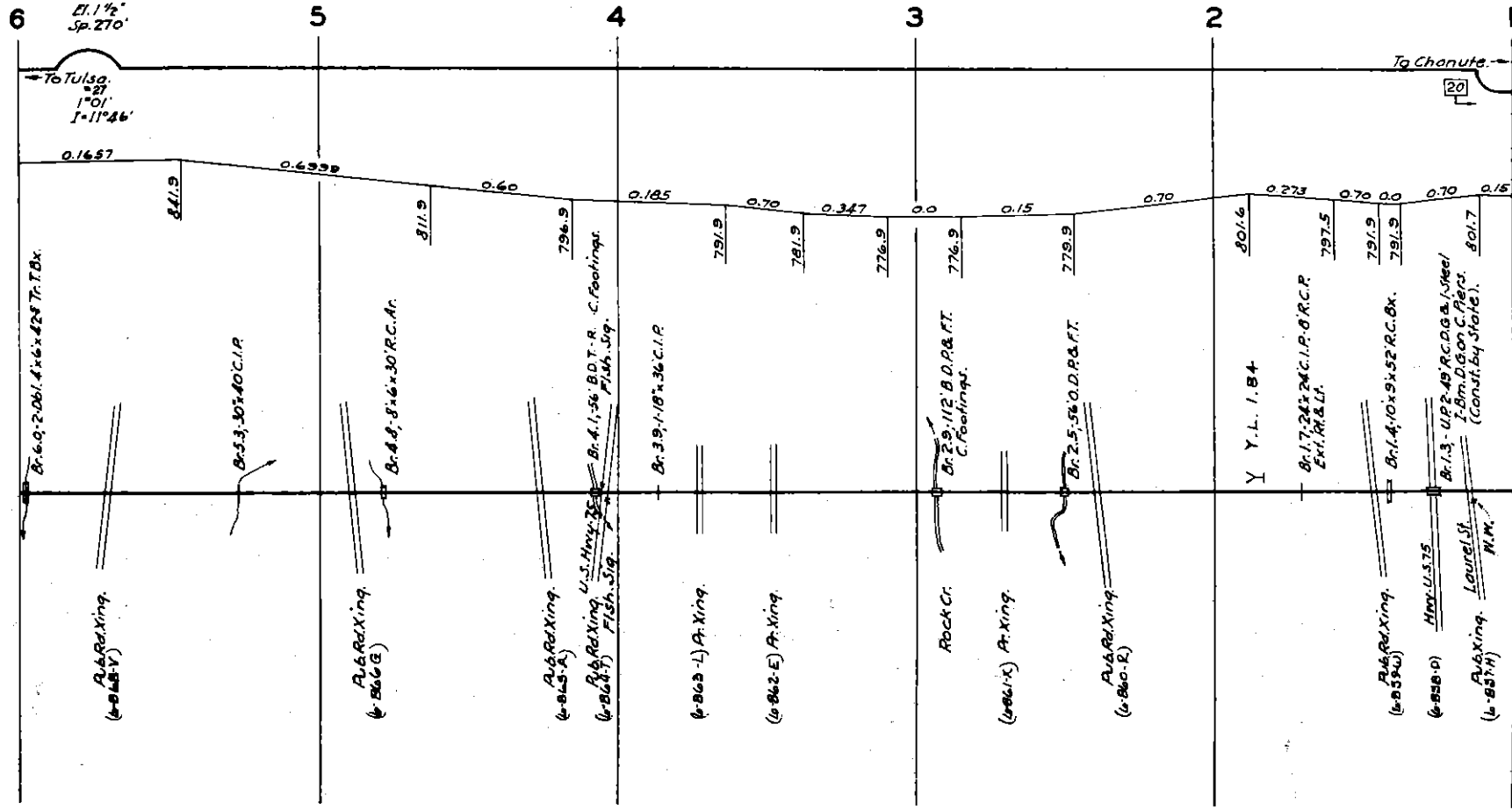














162

161

160

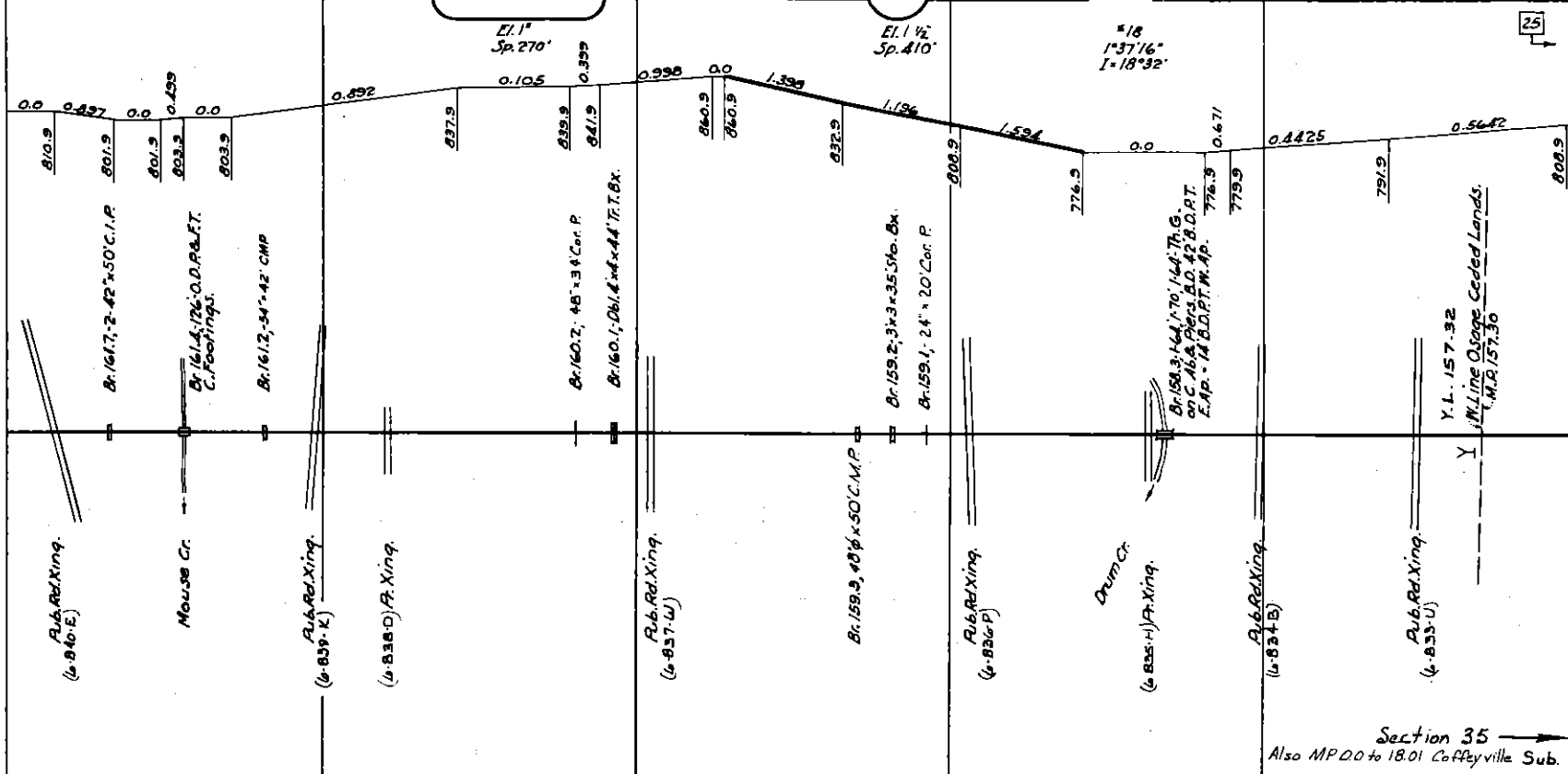
159

158

157

To Tulsa

To Chanute



\*20  
 1°00'18"  
 I=29°00'

1°32'24"  
 I=15°40'  
 \*19

El. 1 1/2  
 Sp. 410

El. 1 1/2  
 Sp. 410

\*18  
 1°37'16"  
 I=18°32'

Pub. Rd. Xing.  
 (L-840-E)

Mouse Cr.

Pub. Rd. Xing.  
 (L-839-K)

(L-838-D) P. Xing.

Pub. Rd. Xing.  
 (L-837-W)

Br. 159.9, 40' x 50' C.M.P.

Pub. Rd. Xing.  
 (L-836-P)

Dump Cr.

(L-835-H) P. Xing.

Pub. Rd. Xing.  
 (L-834-B)

Pub. Rd. Xing.  
 (L-833-U)

Br. 158.3, 164.1, 70.1, 64.7, 6.0,  
 on C. 16 & Pers. D.O. 42 B.D.P.T.  
 E.A.P. - 14 B.D.P.T. M.A.P. 776.9

Y.L. 157.92  
 (M. Line O. Stage Ceded Lands.  
 M.A.P. 157.36)

25

Section 35  
 Also MP 0.0 to 18.0 Coffeyville Sub.

157

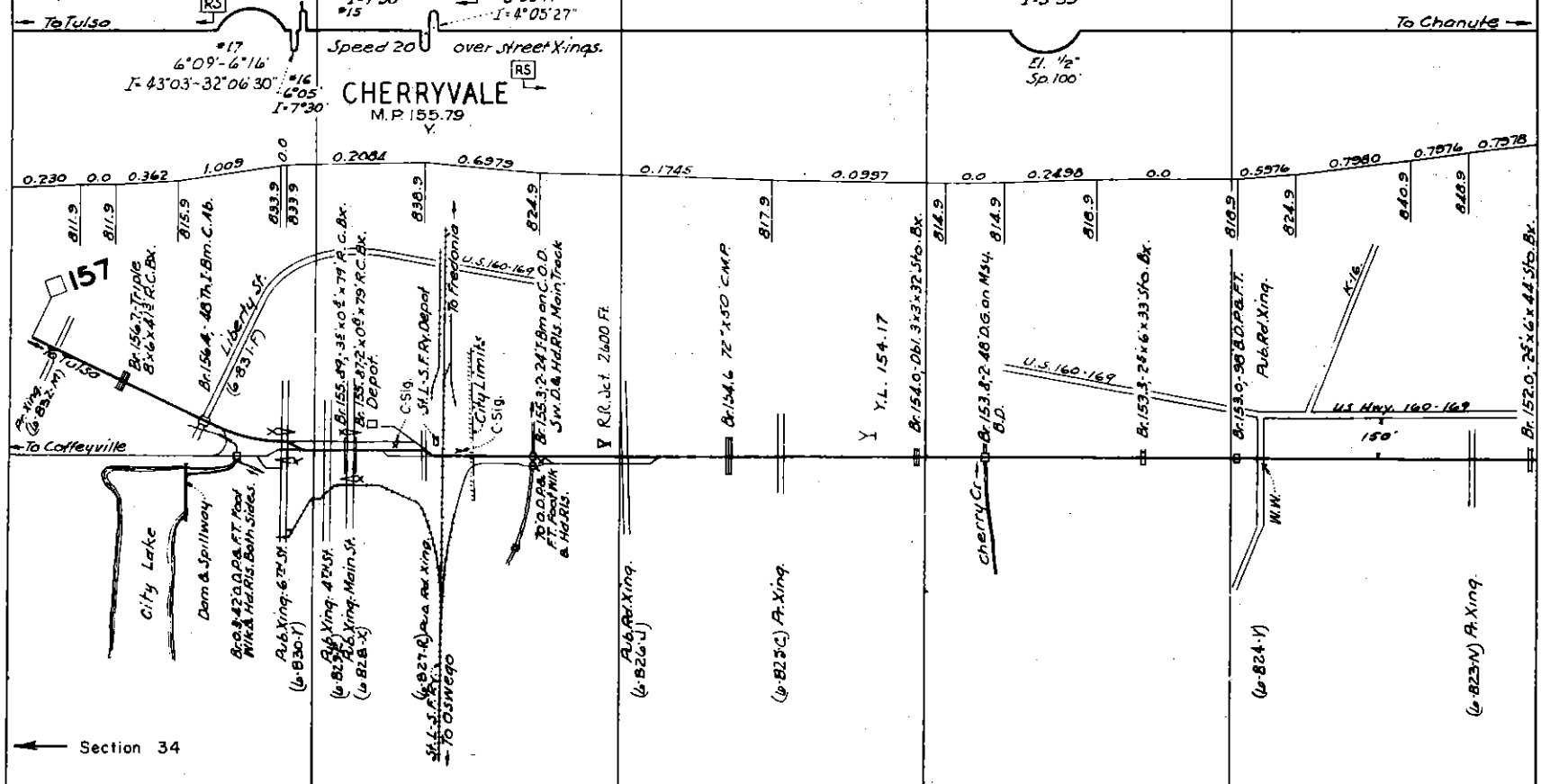
156

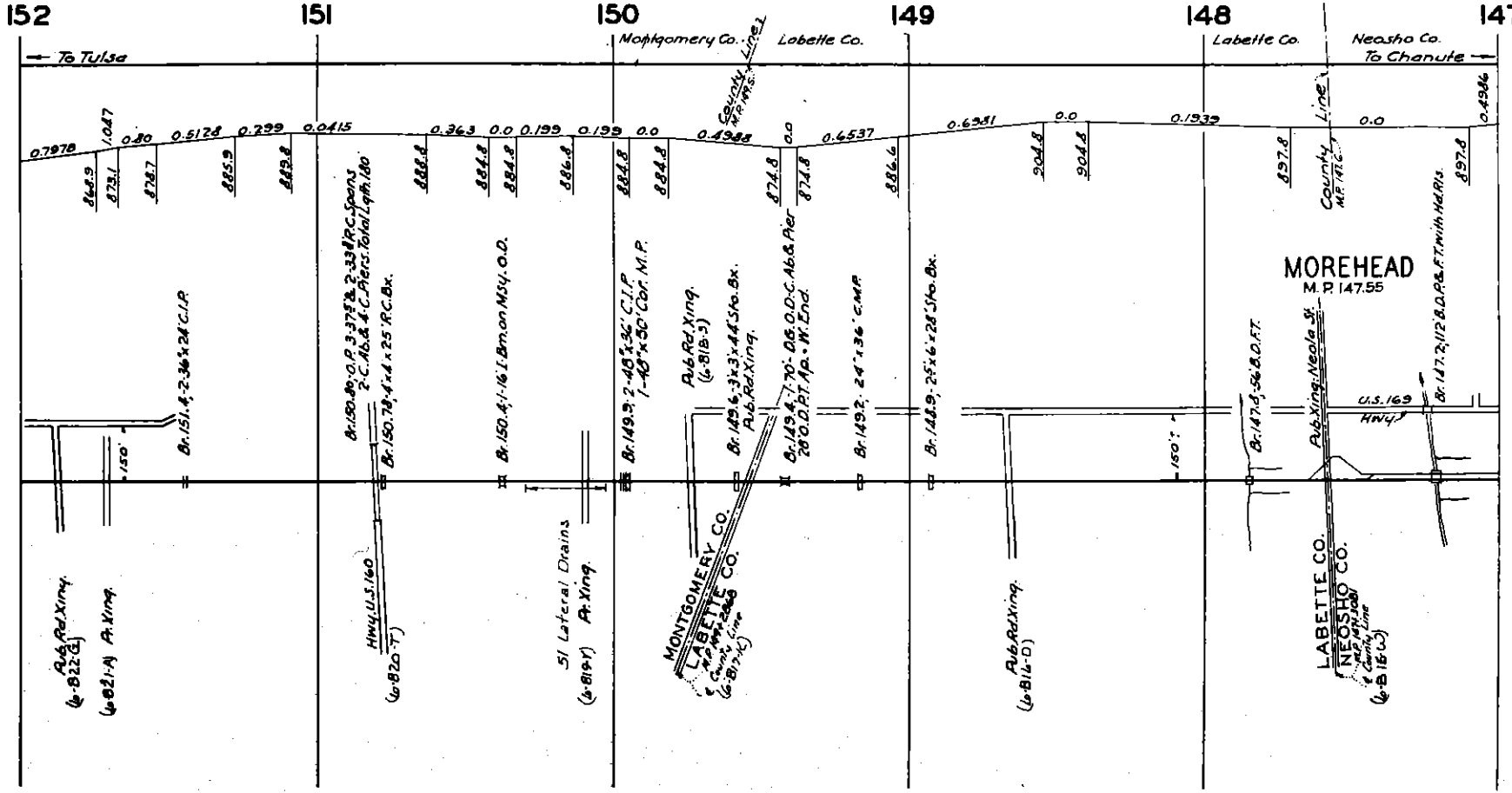
155

154

153

152





152

151

150

149

148

147

To Tulsa

Montgomery Co. Labette Co.

Labette Co. Neosho Co. To Chanute

0.7978 1.047 0.80 0.5128 0.239 0.0415 0.363 0.0 0.199 0.199 0.0 0.4388 0.0 0.4537 0.6281 0.0 0.1339 0.97.8 0.0 0.4986

868.9 873.1 878.7 885.9 888.8 888.8 888.8 888.8 888.8 888.8 888.8 871.8 871.8 886.6 904.8 904.8 897.8 897.8

Br. 151.4-2-36 x 24 C.I.P.  
 Br. 150.4-1-16 I. Br. on Msy. O.O.  
 Br. 149.9-2-40 x 36 C.I.P. 1-40' x 50' Cor. M.P.  
 Br. 149.6-3 x 3 x 44.560 Bx.  
 Br. 149.4-1-70' DG O.O.C. Ab. & Pier 200.0 D.T. Ap. - W. End.  
 Br. 149.2-24' x 36' C.M.P.  
 Br. 148.9-25' x 6' x 28' 5/8" Bx.  
 Br. 147.5-56.5 D.F.T.  
 Br. 147.2-112' B.D.R. & F.T. WITH HD. RIG.

Pub. Rd. Xing. (6-822-B)  
 Pub. Rd. Xing. (6-821-A)  
 Hwy. U.S. 160 (6-820-T)  
 51 Lateral Drains (6-819-Y) Pub. Xing.  
 MONTGOMERY CO. LABETTE CO. LABETTE CO. NEOSHO CO.  
 Pub. Rd. Xing. (6-814-D)  
 Pub. Rd. Xing. Neola St. (6-815-W)  
 MOREHEAD M.P. 147.55  
 U.S. 169 HWY.  
 County M.P. Line

County M.P. Line

County M.P. Line

147

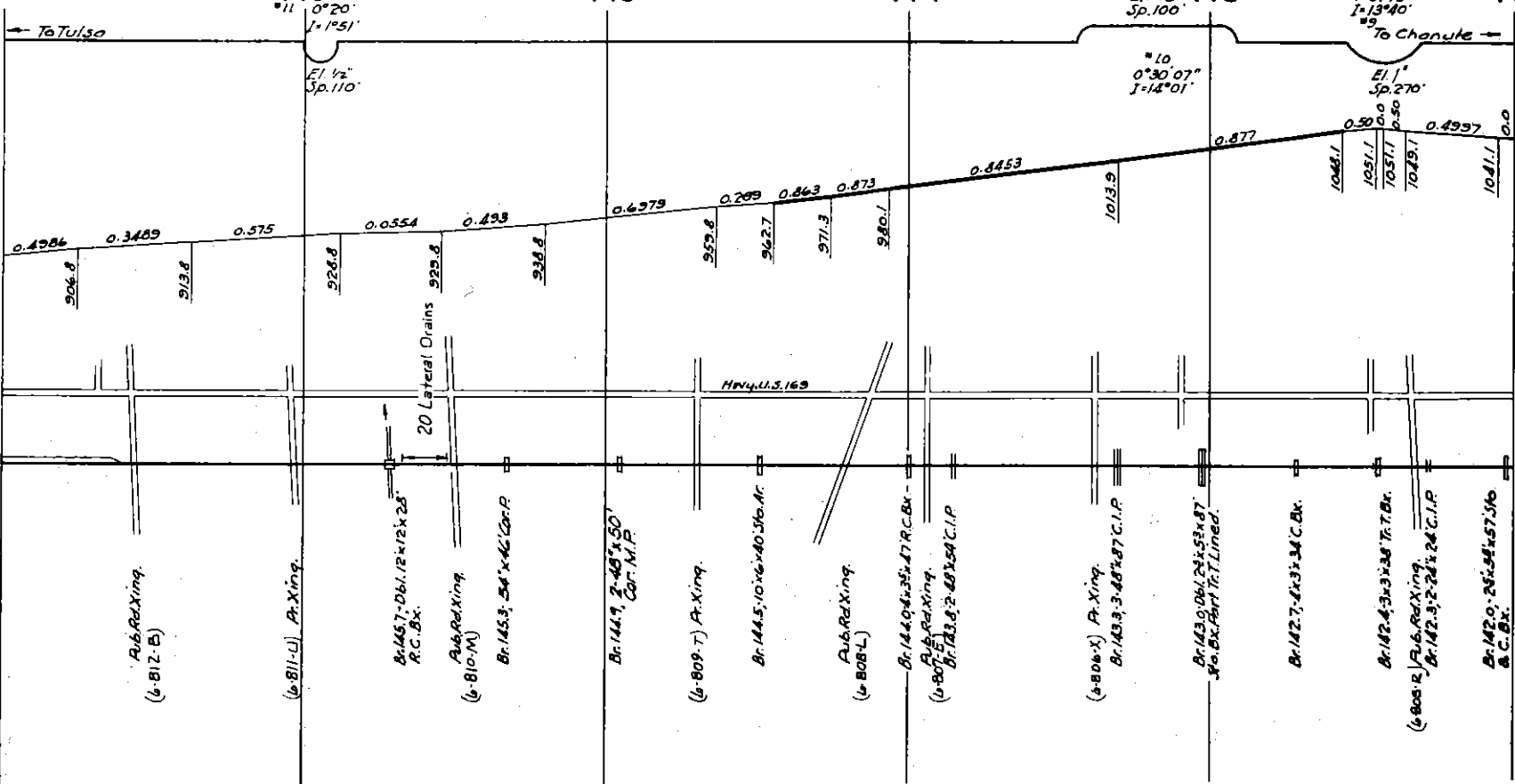
146

145

144

143

142



R.C. Box  
(6-812-E)

(6-811-U) R.C. Box

6-145, 7-06, 1/2" x 12 x 25"  
R.C. Box

R.C. Box  
(6-810-M)

6-145, 3-54" x 42" C.I.P.

6-144, 2-48" x 50"  
C.I.P.

(6-809-T) R.C. Box

6-144, 5-10" x 40" Sp. Ar.

R.C. Box  
(6-808-L)

6-144, 4-36" x 47" R.C. Box  
R.C. Box  
(6-807-E)  
6-143, 3-48" x 50" C.I.P.

(6-806-X) R.C. Box

6-143, 3-48" x 57" C.I.P.

6-143, 0-06, 2-45" x 87"  
R.C. Box Part T.C. Lined.

6-142, 2-48" x 34" C. Box

6-142, 2-36" x 38" T.C. Box  
R.C. Box  
(6-805-R)  
6-142, 3-24" x 24" C.I.P.

6-142, 0-25, 3-58" x 57"  
R.C. Box

142

141

140

139

138

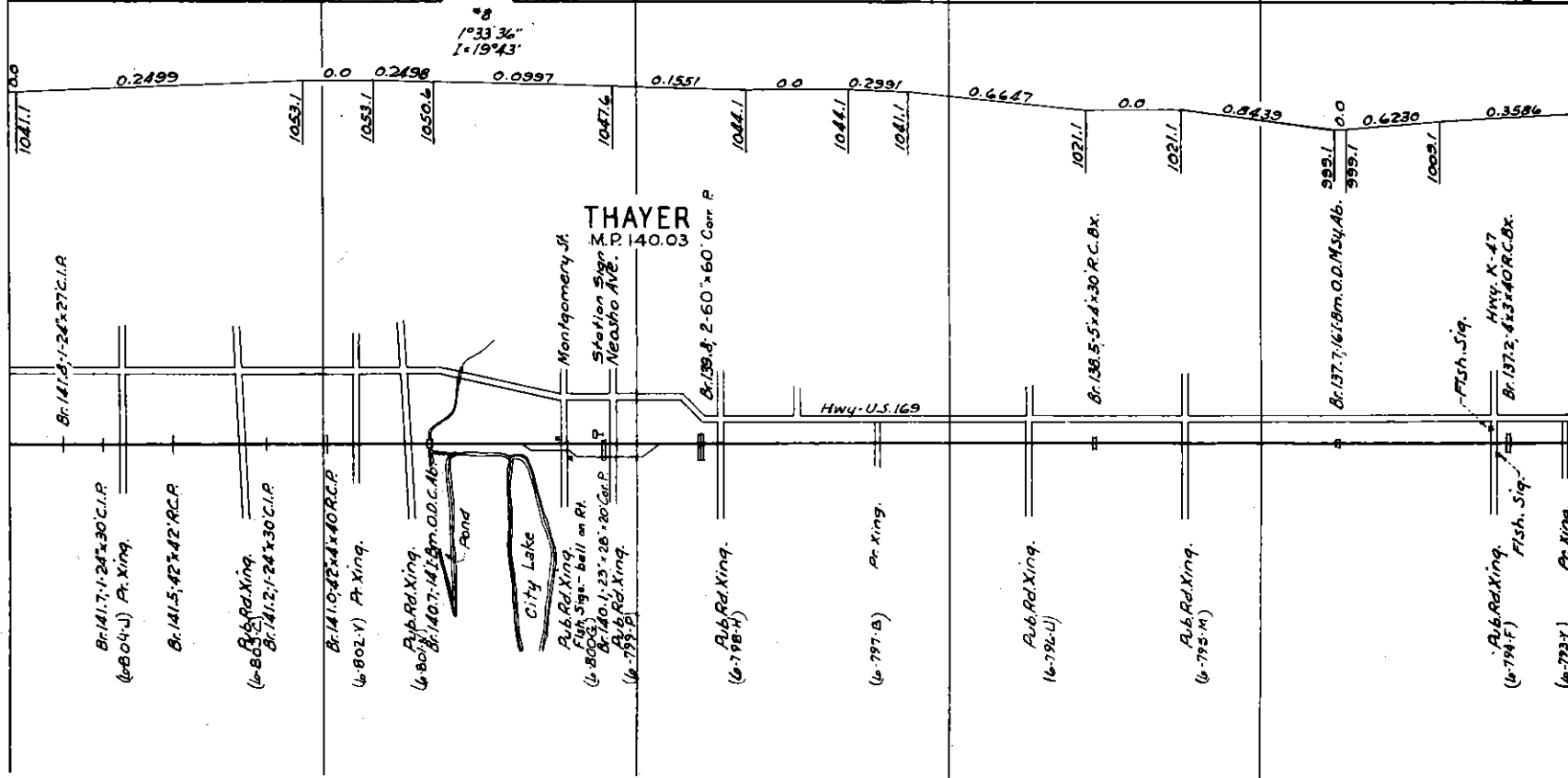
137

← To Tulsa

To Chanula →

El. 1 1/2  
Sp. 410

\*8  
1°33'36"  
1°19'43"



0.0  
1041.1

0.2499

0.0  
1053.1

0.2498  
1053.1

0.0997  
1050.6

0.1551  
1047.6

0.0  
1044.1

0.2991  
1044.1

0.6647  
1021.1

0.0  
1021.1

0.4439  
999.1

0.0  
999.1

0.6230  
1009.1

0.3586  
1009.1

THAYER  
D. 140.03

Hwy. U.S. 169

Hwy. X-47

Br. 141.1, 1-24 x 27 C.I.P.

Br. 141.7, 1-24 x 30 C.I.P.  
(6-804-U) P. Xing.

Br. 141.5, 42 x 42 R.C.P.

Pub. Rd. Xing.  
Br. 141.2, 1-24 x 30 C.I.P.

Br. 141.0, 42 x 40 R.C.P.  
(6-802-Y) P. Xing.

Pub. Rd. Xing.  
Br. 140.7, 14' Sign. O.D. C. Ab.

Pond  
City Lake

Pub. Rd. Xing.  
Fish. Sig. - Ball on Rt.  
(6-803-10), 23' x 26' x 20' C.I.P.  
Pub. Rd. Xing.  
(6-799-Z)

Montgomery St.

Station Sign X

Neatho Ave.

Br. 139.8, 2-60 x 60' Corr. P.  
Pub. Rd. Xing.  
(6-798-H)

(6-797-B) P. Xing.

Pub. Rd. Xing.  
(6-792-L)

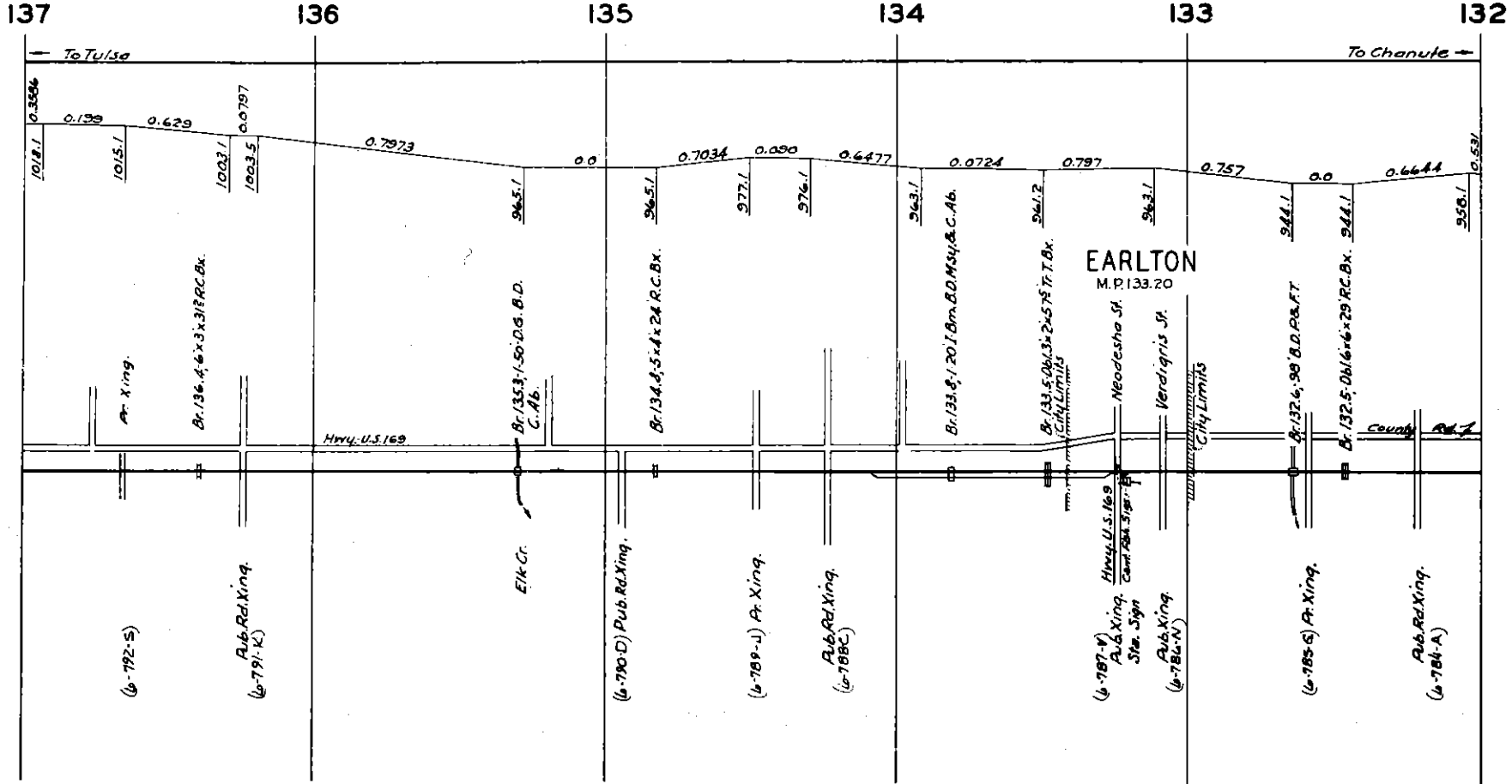
Br. 138.5, 5' x 4' x 30' R.C. Bx.

Pub. Rd. Xing.  
(6-795-M)

Br. 137.1, 6x8m. O.D. M.S. 4 Ab.

Pub. Rd. Xing.  
(6-794-F) Fish. Sig.  
Fish. Sig.  
Hwy. X-47  
Br. 137.2, 4' x 3' x 40' R.C. Bx.

(6-793-T) P. Xing.



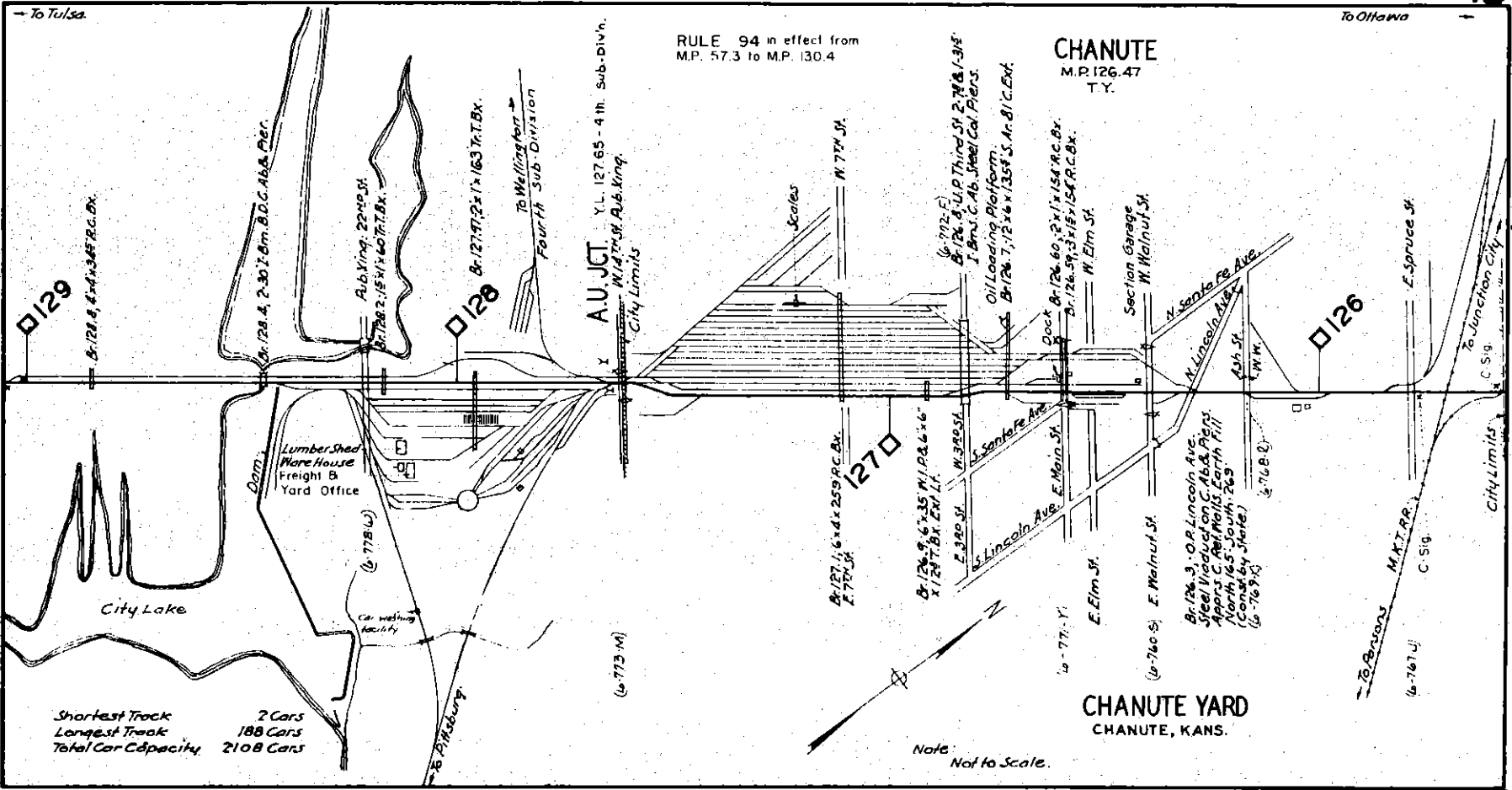


- To Tulsa

To Ottawa

RULE 94 in effect from  
M.P. 57.3 to M.P. 130.4

**CHANUTE**  
M.P. 126.47  
T.Y.



Shortest Track 2 Cars  
 Longest Track 188 Cars  
 Total Car Capacity 2108 Cars

**CHANUTE YARD**  
 CHANUTE, KANS.

Note  
 Not to Scale.

D129

D128

D127

D126

Br. 128.8, 4' x 4' 3/4" R.C. Bx.

Br. 128.4, 2' 30" x 8m. B.O.C. Ab&B. P.C.

Pub. Yrd. 22' x 9' St.  
 Br. 128.2, 15' x 40' Tr. Bx.

Br. 127.97, 2' x 163' Tr. Bx.

Y. A.U. JCT. Y.L. 127.65 - 4 m. Sub-Div'n.  
 W. 147' St. Pub. Yrd.  
 City Limits

Scales

N. 77th St.

Br. 126.8, U.P. Third St. 2' 7 1/2" x 1 3/8"  
 I. Br. 126.8, Ab. Steel Cal. Piers  
 Oil Loading Platform  
 Br. 126.7, 12' x 6' x 135' S. Ar. 8' C. Bx.

Doc. Br. 126.80, 2' x 1' x 154' R.C. Bx.  
 Br. 126.59, 3' x 15' x 154' R.C. Bx.

W. Elm St.

Section Garage  
 W. Walnut St.

N. Santa Fe Ave.  
 X. Lincoln Ave.

Br. 126.3, O.P. Lincoln Ave.  
 Steel Vard on C. Ab&B. Piers  
 Apprs. C. Rail. Malls. Earth Fill  
 North 165' South 269'  
 (Const. by State)  
 (6-769-3)

E. Elm St.

(6-760-6) E. Walnut St.

E. Spruce St.

To Parsons M.K.T.R.R.  
 (6-761-4) C. Sig.

To Junction City  
 C. Sig.

City Limits

City Lake

Dam  
 Lumber Shed  
 Ware House  
 Freight B.  
 Yard Office

Car washing  
 facility

To Pittsburg

(6-773-14)

(6-771-1)

(6-761-4)

C. Sig.

132

131

130

129

128

127

To Tulsa

To Chanute

#7  
1°00'  
I-12°32'

#4  
3°06'17"  
I-4°05'27"

#5  
3°06'17"  
I-4°05'27"

0.53 0.0 0.27 0.0 0.485 0.249 0.022 0.0 0.7977 0.0 0.54 0.0 0.598 0.0 0.818 0.0739 0.697 0.176 0.73 0.32 0.28 0.538 0.477

950.1

950.1

953.1

953.1

942.1

941.1

941.3

941.3

946.1

946.1

940.1

940.1

940.5

940.5

922.1

950.1

943.1

944.1

946.1

943.1

942.7

947.7

Br. 131.7, DL, 3'x5'x30' S.W. Bx.

Br. 131.2, 3'x5'x40' S.W. Bx.

Br. 130.9, 154' B.D. P. & F.T. C. Footings & Piers.

S.L. 130.42

Br. 130.2, 2' x 30' Cor. P.

Br. 129.9, 6'x6'x36' R.C. Bx.

Br. 128.1, 20' L. Bm. on Hwy. O.D. Pub. R. Xing.

Br. 128.8, 4'x4'x34' R.C. Bx.

Br. 128.4, 2'x0'10m. B.D. C. Ab. P. I.C.

Br. 128.2, 15'x1'60' Tr. Bx.

Br. 127.97, 2'x1'163' Tr. Bx.

A.U.J.C.T. Y.L. 127.65 Pub. Xing. (4th Sub-Div'n)

Br. 127.1, 6'x4'x25' R.C. Bx.

46 Lateral Drains

Pub. Rd. Xing. (6-782-1)

Turkey Cr.

(6-782-1) A. Xing.

Pub. Rd. Xing. (6-781-E)

(6-780-X) P. Xing.

(6-779-D)

City Lake

Dem.

Pub. Rd. Xing (6-778-W)

Freight. Yard Office

75th Highway

6-775-W) N. 147th St.

City Limits

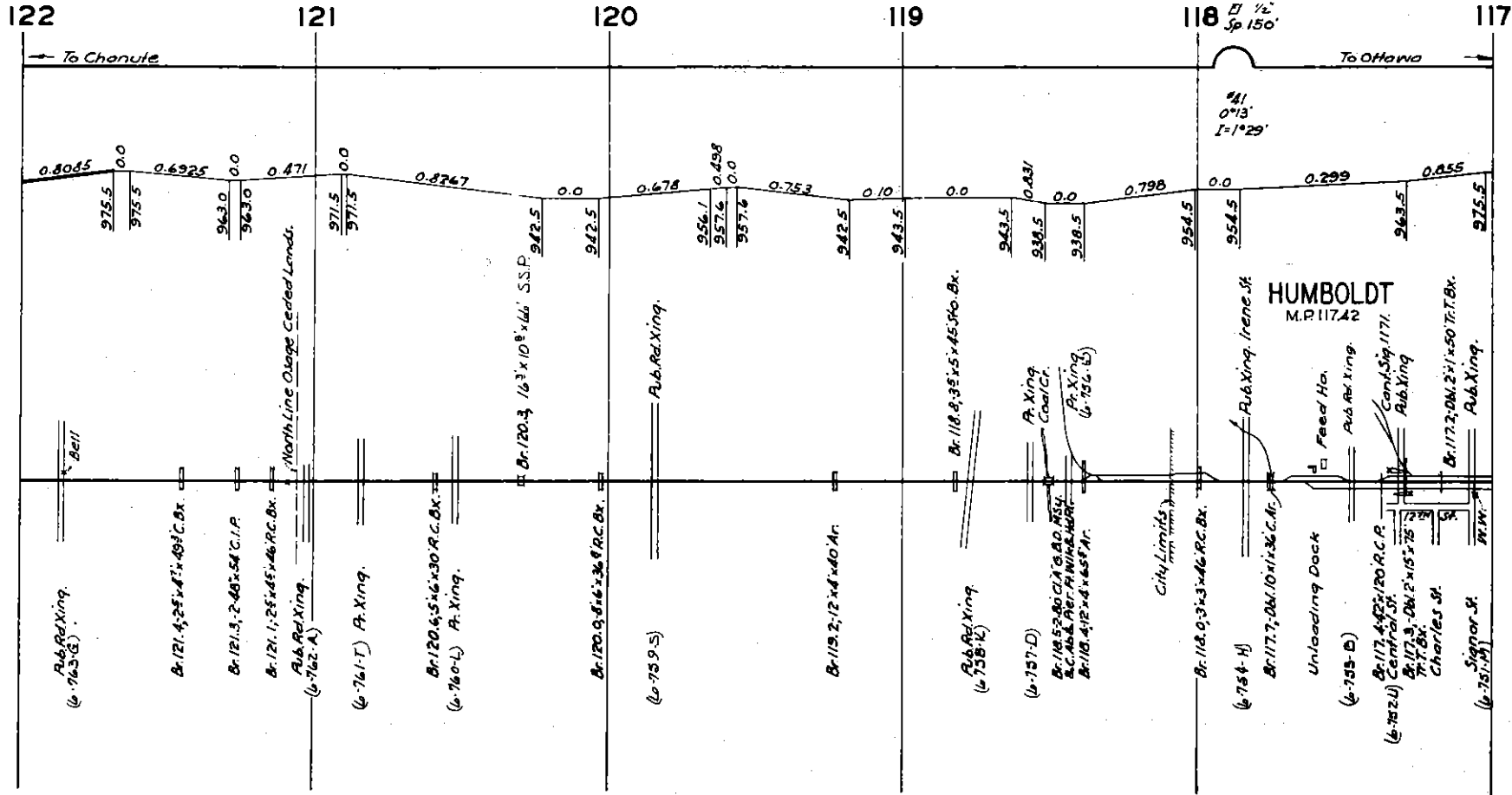
To Wellington Fourth Sub-Div'n

See Separate Sketch Page - 76

28

28





122

121

120

119

118

117

El 1/2  
Sp 150'

→ To Chanute

To Ottawa →

0.8045  
975.5  
975.5  
0.6925  
0.0  
963.0  
963.0  
0.471  
0.0  
971.5  
971.5  
0.8747  
0.0  
942.5  
942.5  
0.678  
0.0  
943.5  
943.5  
0.10  
0.0  
943.5  
943.5  
0.831  
0.0  
936.5  
936.5  
0.798  
0.0  
954.5  
954.5  
0.299  
0.0  
963.5  
963.5  
0.855

#1  
0°13'  
I=1°29'

Pub. Red Xing.  
(6-763-G)

Br. 121.4, 25' x 41' x 49' C. Bx.

Br. 121.3, 2' 40" x 54' C.I.P.

Br. 121.1, 25' x 45' x 46' R.C. Bx.

Pub. Red Xing.  
(6-762-A)

(6-761-T) P. Xing.

Br. 120.6, 5' 6" x 30' R.C. Bx.

(6-760-L) P. Xing.

Br. 120.3, 16' 3" x 10' 6" x 46' S.S.P.

Br. 120.9, 6' 6" x 36' R.C. Bx.

(6-759-S) Pub. Red Xing.

Br. 119.2, 12' x 4' x 40' Ar.

Pub. Red Xing.  
(6-758-K)

Br. 118.8, 35' x 5' x 45' 7/8" Bx.

(6-757-D) P. Xing.  
Coal Cr.

Br. 118.5, 2' 80' C.I. 10' 80' MSY.  
C.I. 10' 80' R.C. Bx. 10' 80' MSY.  
Br. 118.4, 12' x 4' x 65' Ar.

P. Xing.  
(6-756-L)

City Limits

Br. 118.0, 3' x 3' x 46' R.C. Bx.

(6-754-H)

Br. 117.7, 20' 10" x 12' 3/4" Ar.

Pub. Xing. Irene St.

Unloading Dock

Feed Ho.

(6-755-B)

Pub. Red Xing.

Br. 117.4, 4' 25" x 20' R.C.P.

(6-752-U) Central St.

Br. 117.3, 20' 2" x 15' 7/8" T.F. Bx.

Charles St.

Br. 117.2, 20' 2" x 15' 7/8" T.F. Bx.

Pub. Xing.

(6-751-W) Signor St.

HUMBOLDT  
M.P. 117.42

Conf. Sig. 1171.  
Pub. Xing.

Pub. Xing.

W.W.

117

40  
106  
116  
I-11'53'

115

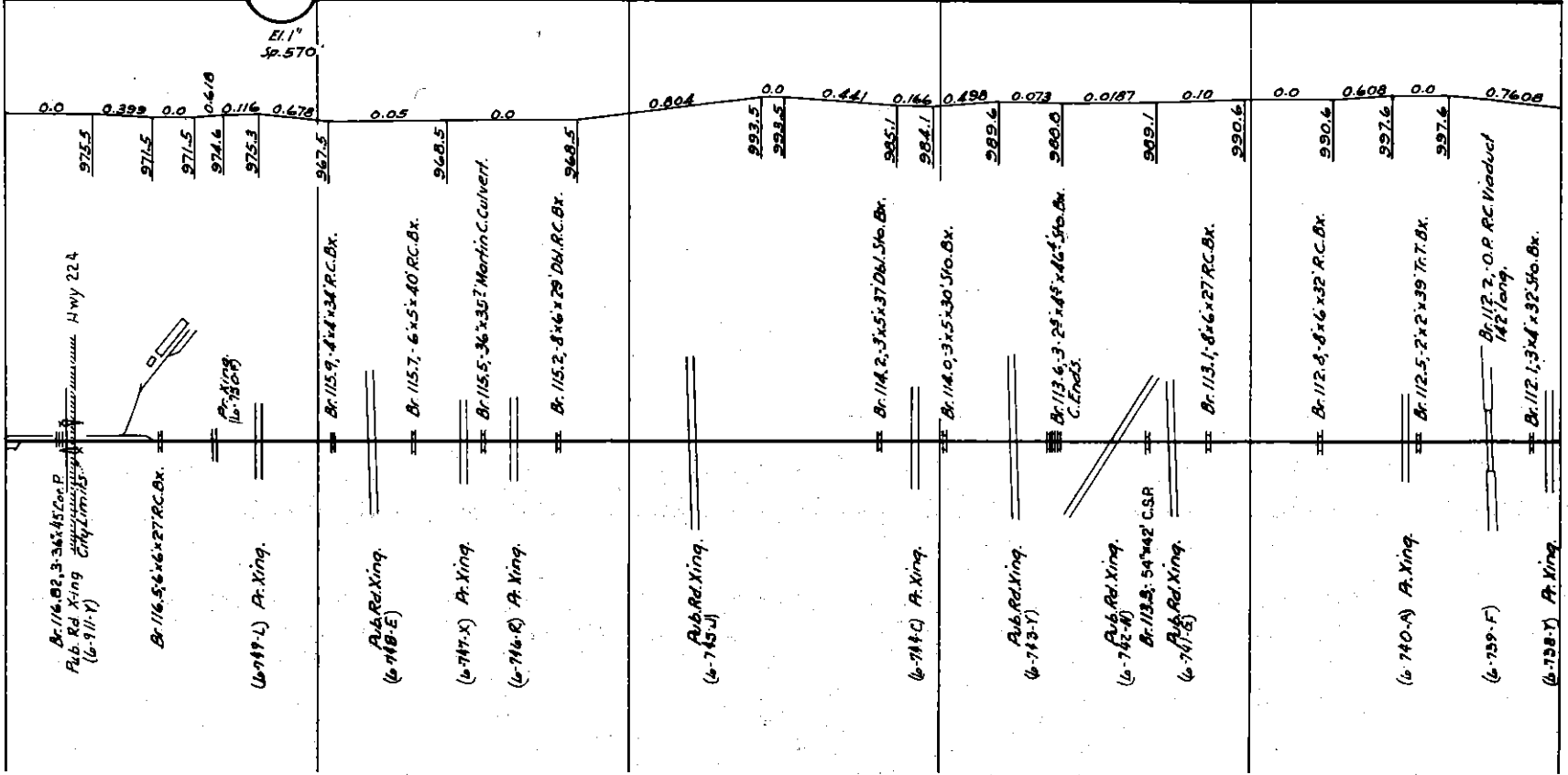
114

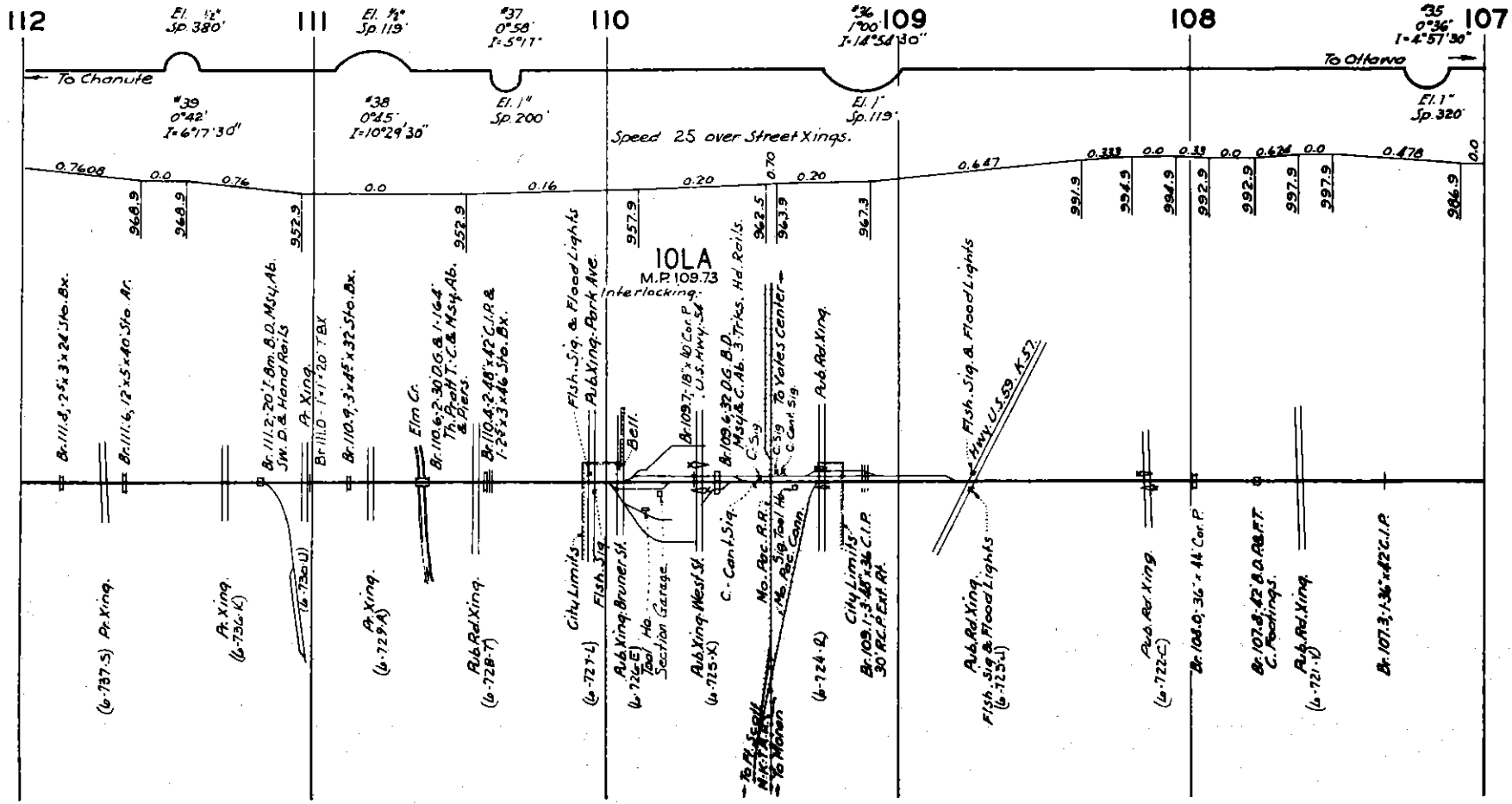
113

112

To Chanute

To Ottawa





112 El. 1/2" Sp. 330'

111 El. 1/4" Sp. 119'

110 #37 0°58' I-5°17'

#36 1°00' I-14°54'30"

108

#35 0°36' I-4°57'30"

#39 0°42' I-6°17'30"

#38 0°45' I-10°29'30"

El. 1" Sp. 200'

El. 1" Sp. 119'

El. 1" Sp. 320'

Speed 25 over Street Xings.

To Ottawa

To Chanute

Br. 111.8, 75x3x24 S.W. Bx.

Br. 111.6, 12x5x40 Sto. Ar.

Br. 111.20, 18m. B.D. M.Sy. Ab. S.W. D. & Hand Rails

Br. 110.9, 3x4x32 Sto. Bx.

Br. 110.6, 230 D.G. & 1-164' T.P. B.W. T.C. & M.Sy. Ab. & Pipes.

Br. 110.4, 248x42 C.I.P. & 1-28x3x46 Sto. Bx.

IOLA M.P. 109.73 Interlocking

Br. 109.7, 18x40 Cor. P. U.S. Hwy. 54

Br. 109.6, 32 D.G. B.D. M.Sy. & C. Ab. 3 Trks. Hd. Rails

Br. 109.4, 248x42 C.I.P. & C. Cent. Sig.

Br. 108.0, 36x44 Cor. P.

Br. 107.8, 42 B.D. RAFT C. Footings.

Br. 107.3, 136x42 C.I.P.

(6-737-S) Pub. Xing.

(6-736-X) Pub. Xing.

(6-729-A) Pub. Xing.

(6-728-T) Pub. Rd. Xing.

(6-727-L) Pub. Xing.

(6-726-E) Pub. Xing.

(6-725-X) Pub. Xing.

(6-724-Q) Pub. Xing.

(6-722-C) Pub. Rd. Xing.

(6-721-V) Pub. Rd. Xing.

City Limits

City Limits

Pub. Rd. Xing. Fish. Sig. & Flood Lights

Fish. Sig. & Flood Lights Pub. Xing. Park Ave.

Fish. Sig. & Flood Lights Pub. Xing. U.S. 59 A-52

To Chanute  
To Ottawa  
To Moberly

107

106

105

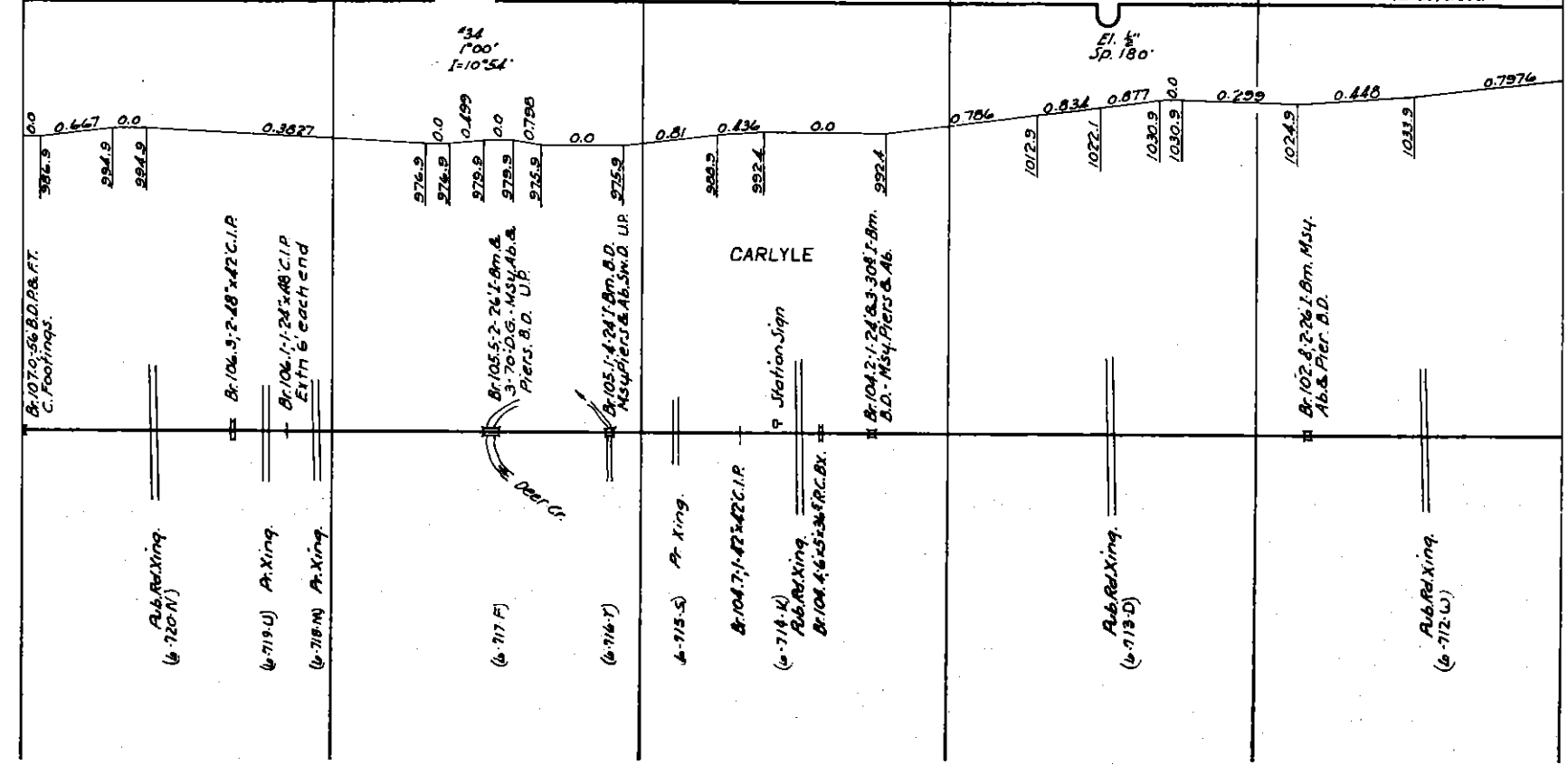
104

103

102 10

← To Charute

To Offawa →



El. 142'  
Sp. 570'

34  
100'  
I=10°54'

33  
0°20'  
I=2°11'

El. 1/2"  
Sp. 180'

CARLYLE

DEER CR.

Station Sign

B: 107.0-56 B.D. P.B. & FT.  
C. Footings.

B: 106.3-2-18-42 C.I.P.

B: 106.1-1-24-48 C.I.P.  
Extrn. each end

B: 105.5-2-26-1.8m. &  
3-70-0.6-MS4, Ab. &  
Piers. B.D. U.P.

B: 105.1-4-24-1.8m. B.D.  
MS4 Piers & Ab. S.W.D. U.P.

B: 104.2-1-24-8.3-308' 1.8m.  
B.D. MS4 Piers & Ab.

B: 102.8-2-26-1.8m. MS4.  
Ab. & Pier. B.D.

Ab. & Pier. B.D.  
(6-720-N)

Ab. & Pier. B.D.  
(6-719-U) A-Xing

Ab. & Pier. B.D.  
(6-718-W) A-Xing

(6-717-F)

(6-716-Y)

(6-715-E) A-Xing

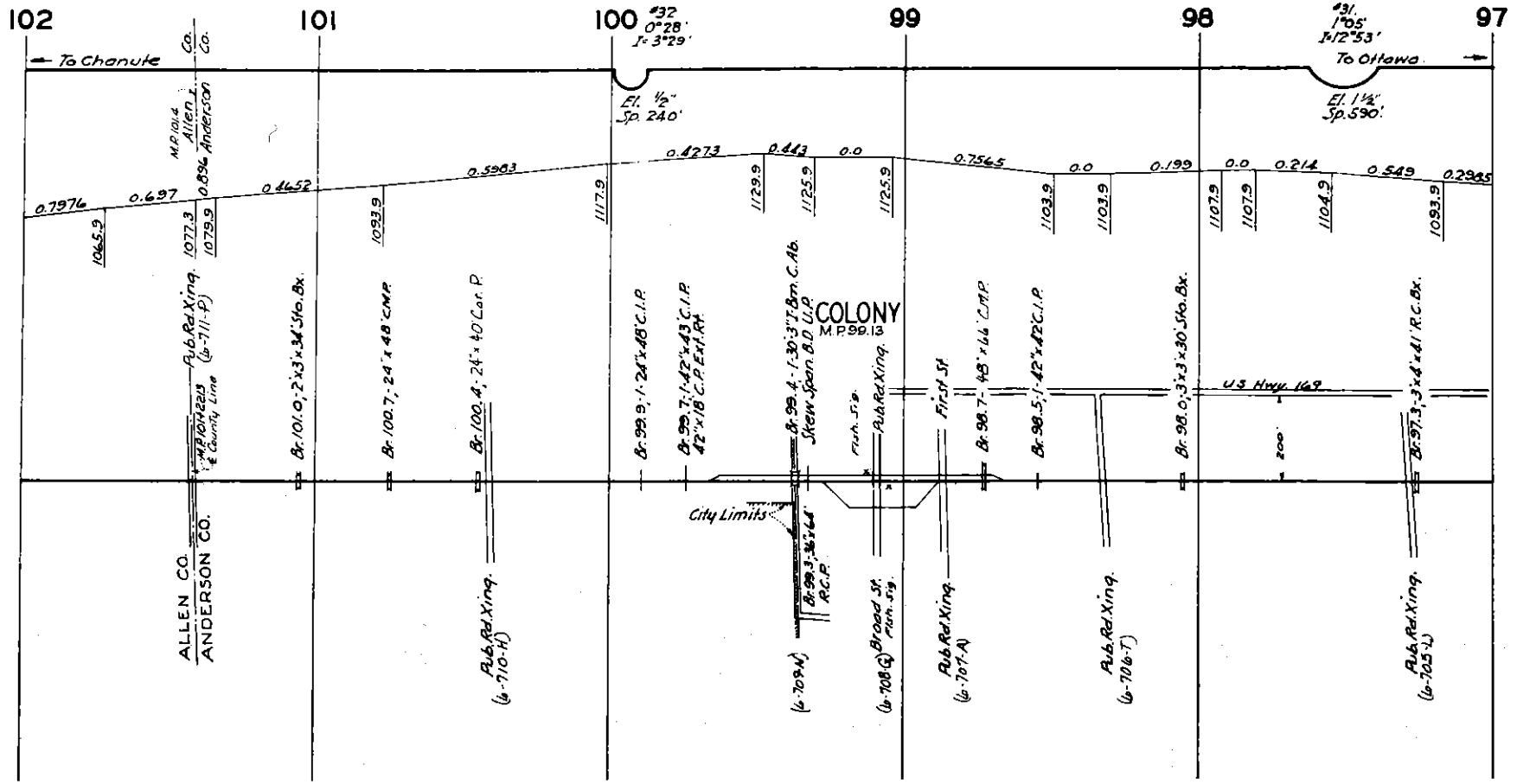
B: 104.7-1-17-42 C.I.P.

Ab. & Pier. B.D.  
(6-714-U)

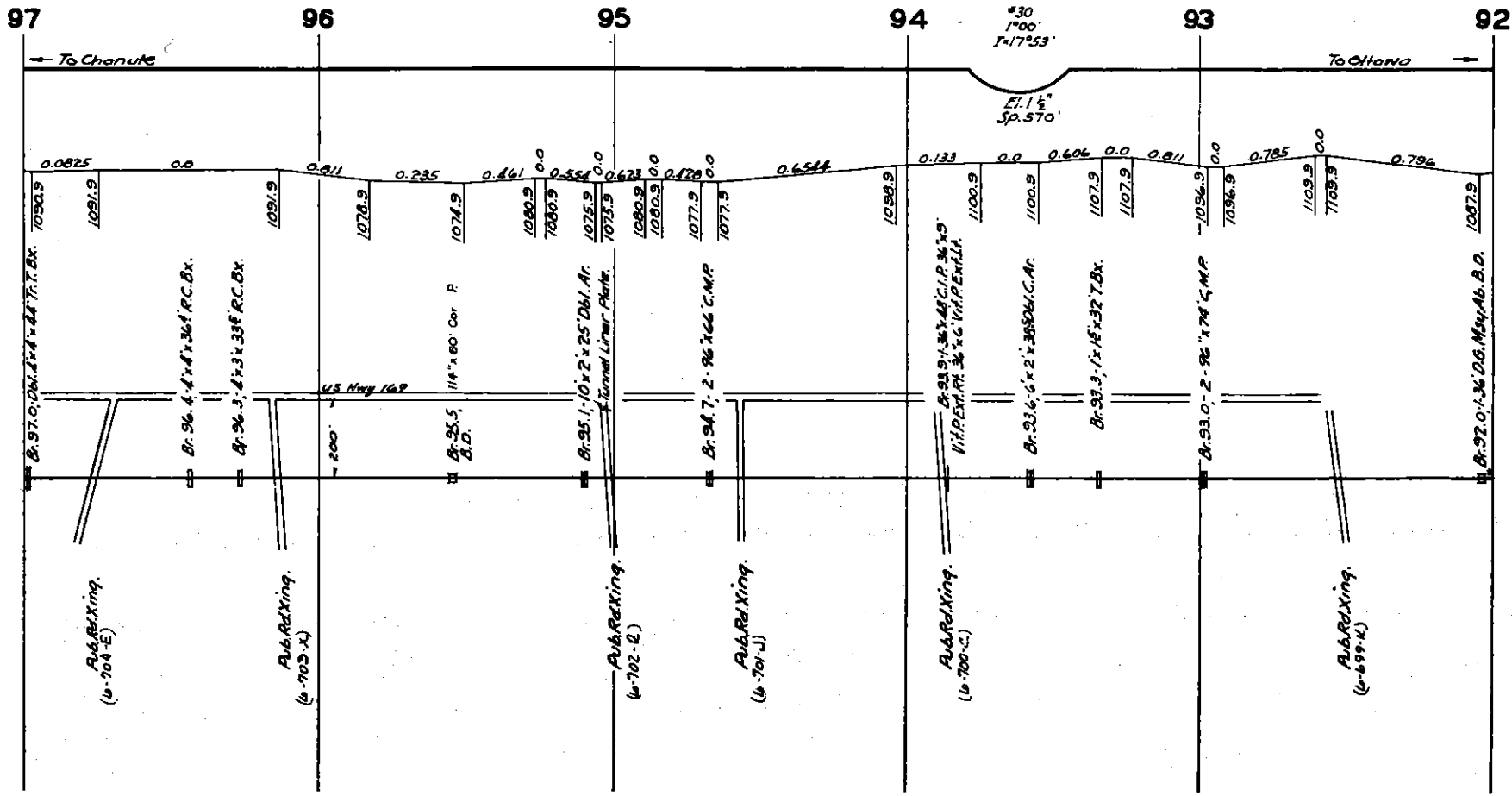
B: 104.4-5-5-36-1-8m. B.D.

Ab. & Pier. B.D.  
(6-713-D)

Ab. & Pier. B.D.  
(6-712-W)







92

#1 1/2  
Sp. 570'-430'

90

#28  
El. 1 1/2  
Sp. 570'

88

#27  
2°04'  
I=20'51"

87

#26  
2°07'  
I=26'78"

To Chonute

To Ottawa

#29  
1°00'36"-0°59'  
I=23'55"30"-13'53"

#28  
1°03'  
I=18'16'30"

El. 3 1/2  
Sp. 480'

El. 4'  
Sp. 480'

0.786

0.0

0.756

0.0

0.756

0.0

0.829

0.803

0.0

0.348

1102.9

1102.9

1082.9

1082.9

1114.9

1114.9

1087.9

1031.9

1031.9

Dr. 91.7, R.C. Viaduct, 343' long.

Bc. 91.5, 3'x2'x32' Tr. T. Bx.

Bc. 91.3, 5'x4'x43' R.C. Bx.

Fish Sign

Fish Sign & Bell

Main St.

**WELDA**  
M.P. 91.02

Bc. 90.3, 1'x8' D.G. Msy. Ab.

Pr. Xing.

Bc. 89.9, 4'x3'x52' R.C. Bx.

Bc. 89.7, 1'x36'x36' C.I.P.

Bc. 89.1, 4'x3'x34' R.C. Bx.

Bc. 88.9, 1'x36'x32' R.C. R.

Bc. 88.6, 3'x4'x41' Sfo. Bx.

Bc. 88.5, 3'x3'x46' Dbl. Sfo. Bx.

Bc. 88.3, 34' B.D.C. Bm. Msy. Ab.

Bc. 87.8, 1'x36' D.G. Msy. Ab.

B.D.

Bc. 87.6, 10'x4'x24' Ac.

Bc. 87.5, 5'x4'x48' R.C. Bx.

Bc. 87.4, 6'x5'x32' R.C. Bx.

Bc. 87.1, 8'x5'x48' R.C. Bx.

(U-695-D)

(U-697-W)

Ab. Rd. Xing.  
(U-696-P)

Ab. Rd. Xing.  
(U-695-H)

Sta. Sign

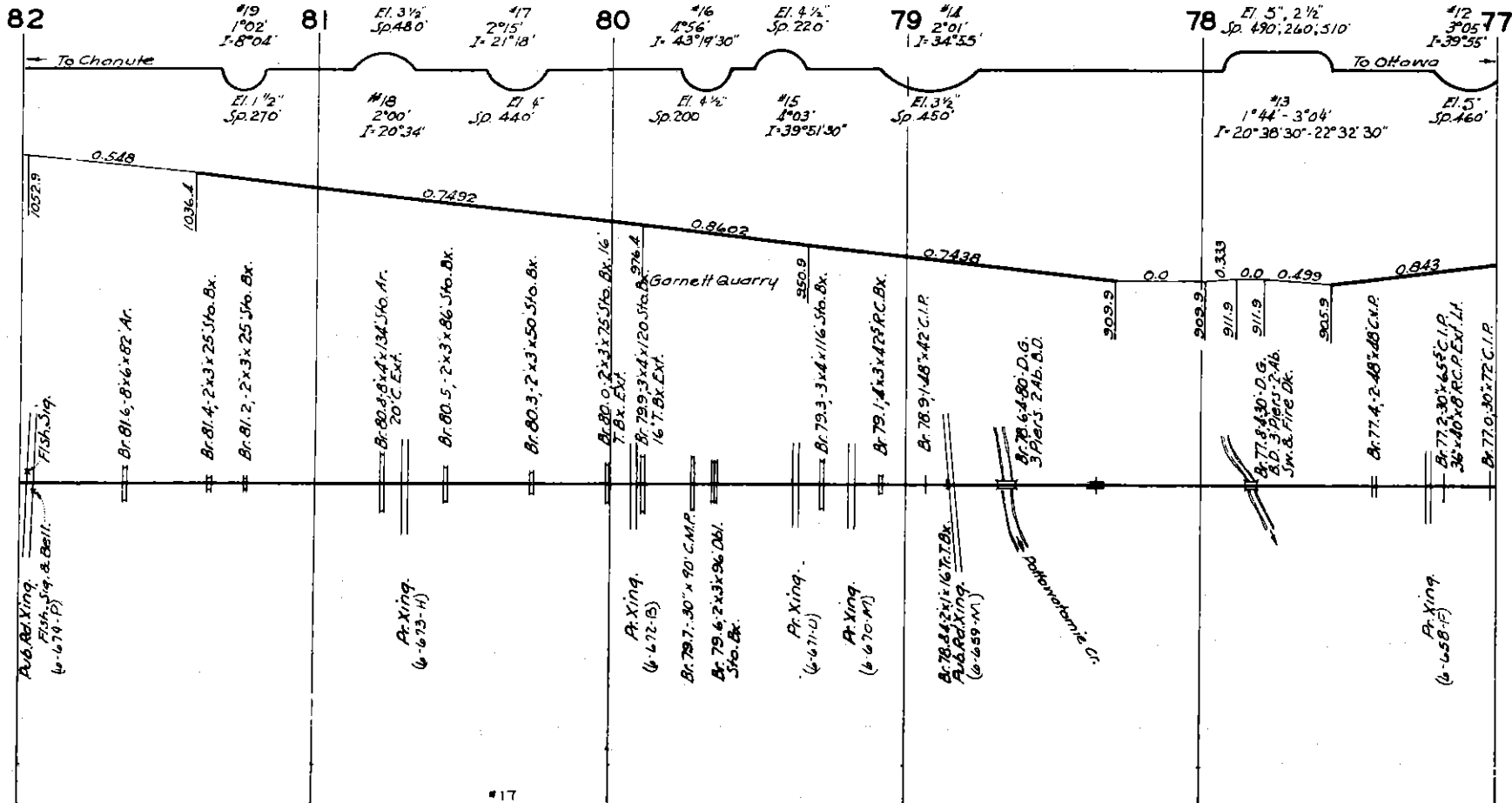
Ab. Rd. Xing.  
(U-694-B)

(U-693-L)

Ab. Rd. Xing.  
(U-692-M)

Pr. Xing.  
(U-691-F)





77

El. 3 1/2' Sp. 360'

El. 5' Sp. 320'

76

El. 4' Sp. 670'

75

74

El. 1 1/2' Sp. 570' I-16°32'

73

El. 1" 1 1/2' Sp. 250', 420', 570' I-7°12' I-11°39' To Ottawa

72

4

To Chanute

#11 2°10' I-17°25'

#10 2°10' I-13°14'

#9 2°10' I-30°02'

0.843

0.7475

0.6884

0.00

1023.4

1023.4

0.449

0.00

0.20

0.014

0.126

0.125

0.00

0.139

0.469

0.472

0.00

Br. 76-9-8x5x94 Ar

Br. 76-7-1-30x48 C.I.P. 28x28x17 1/2 In. Ext. Each End

Br. 76-6-24x30 Cor. P

Br. 76-5-16x7x45 Ar

Br. 76-4-36x36 Cor. P

Br. 76-3-60x64 Cor. P

Br. 76-1-181 Br. A.D. Msy. Ab. Fr. Mir. & Hd. R.I.

Br. 76-0-21 I. Br. B.D. Msy. Ab. Fr. Mir. & Hd. R.I.

Br. 75-7-1-30x36 C.I.P.

Br. 75-3-3-24x20 Cor. P

Br. 75-3-24x20 Cor. P

Br. 75-0-1-18x34 C.I.P.

Br. 74-4-1-18x24 C.I.P.

Pub. Rd. Anderson (6-655-X)

FRANKLIN CO. M.P. 73.55

FRANKLIN CO. M.P. 73.55

City Limits

U.S. HWY 59 (6-654-D)

Br. 74-3-0-18x24 C.I.P. Pub. Rd. Xing. City Limits

Br. 74-3-0-18x24 C.I.P. Pub. Rd. Xing. City Limits

Br. 74-2-10-0-D.T.R. on Piles. City Limits

Br. 73-6-7-24x36 C.I.P.

Br. 73-5-24x20 Cor. P

Br. 73-5-24x20 Cor. P

Br. 73-5-24x20 Cor. P

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Br. 73-5-24x20 Cor. P

Br. 73-5-24x20 Cor. P

Br. 73-5-24x20 Cor. P

944.8

1000.6

1018.5

1018.5

1022.6

1022.6

1022.2

1022.2

1019.3

1011.3

1003.8

944.8

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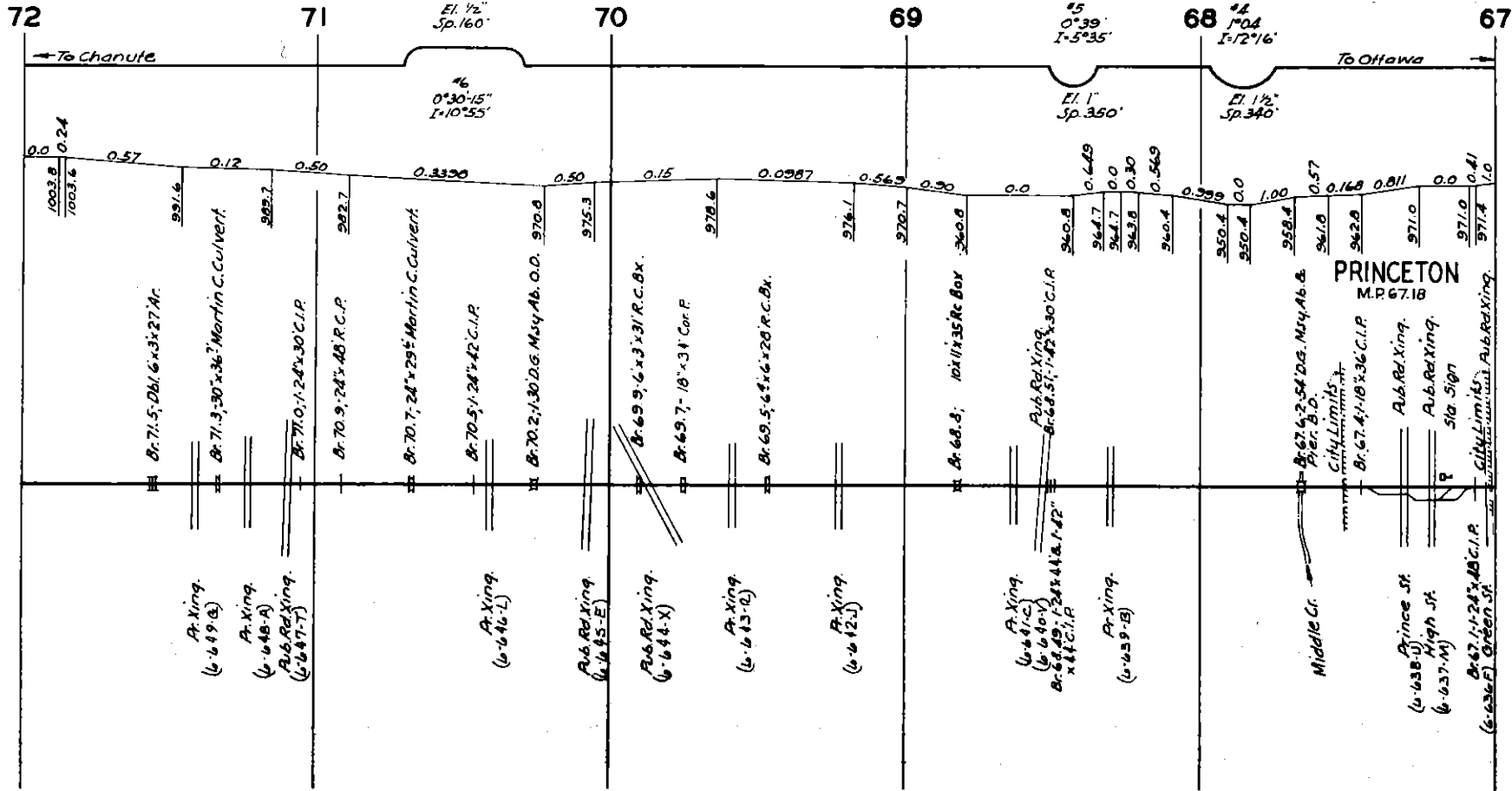
944.8

944.8

944.8

944.8

944.8



72

71

70

69

68

67

← To Chanute

To Ottawa →

El. 1/2  
Sp. 160

El. 1  
Sp. 350

El. 1/2  
Sp. 340

16  
0°30'15"  
1:10°55'

45  
0°39'  
1:5°35'

44  
1°04'  
1:12°16'

1003.8  
1003.6

0.0 0.24

0.57

0.12

0.50

0.3390

0.50

0.15

0.0987

0.569

0.90

0.0

0.449

0.0

0.30

0.569

0.399

0.0

1.00

0.57

0.168

0.811

0.0

0.41

1.0

Br. 71.5: DBL. 6x3x27" A.C.

Br. 71.3: 30x36" Martin C. Culvert

Br. 71.0: 1-24x30 C.I.P.

Br. 70.9: 24x48 R.C.P.

Br. 70.7: 24x29" Martin C. Culvert

Br. 70.5: 24x42 C.I.P.

Br. 70.2: 1-30 D.G. Msy. Ab. O.D. 970.8

Br. 69.9: 6x3x31 R.C. Box

Br. 69.7: 18x31 Cor. F

Br. 69.5: 6x6x28 R.C. Box

Br. 68.8: 10x11x35 Ac. Box

Br. 68.5: 1-42x44x1-12" C.I.P.

Br. 67.5: 54x54 D.G. Msy. Ab. & Pier. B.D.

Br. 67.4: 18x36 C.I.P.

Br. 67.1: 24x48 C.I.P.

A. Xing.  
(6-649-A)

A. Xing.  
(6-648-A)  
A.B. Rd. Xing.  
(6-647-T)

A. Xing.  
(6-646-L)

A.B. Rd. Xing.  
(6-645-E)

A.B. Rd. Xing.  
(6-644-X)

A. Xing.  
(6-643-B)

A. Xing.  
(6-642-S)

A. Xing.  
(6-641-C)  
(6-640-Y)  
Br. 68.9: 1-24x44x1-12" C.I.P.

A. Xing.  
(6-639-B)

Middle Cr.

Prince St.  
(6-638-J)  
High St.  
(6-637-M)

City Limits  
Green St.  
(6-636-F)

PRINCETON  
M.P. 67.18

City Limits  
Green St.

City Limits  
Green St.

67

66

65

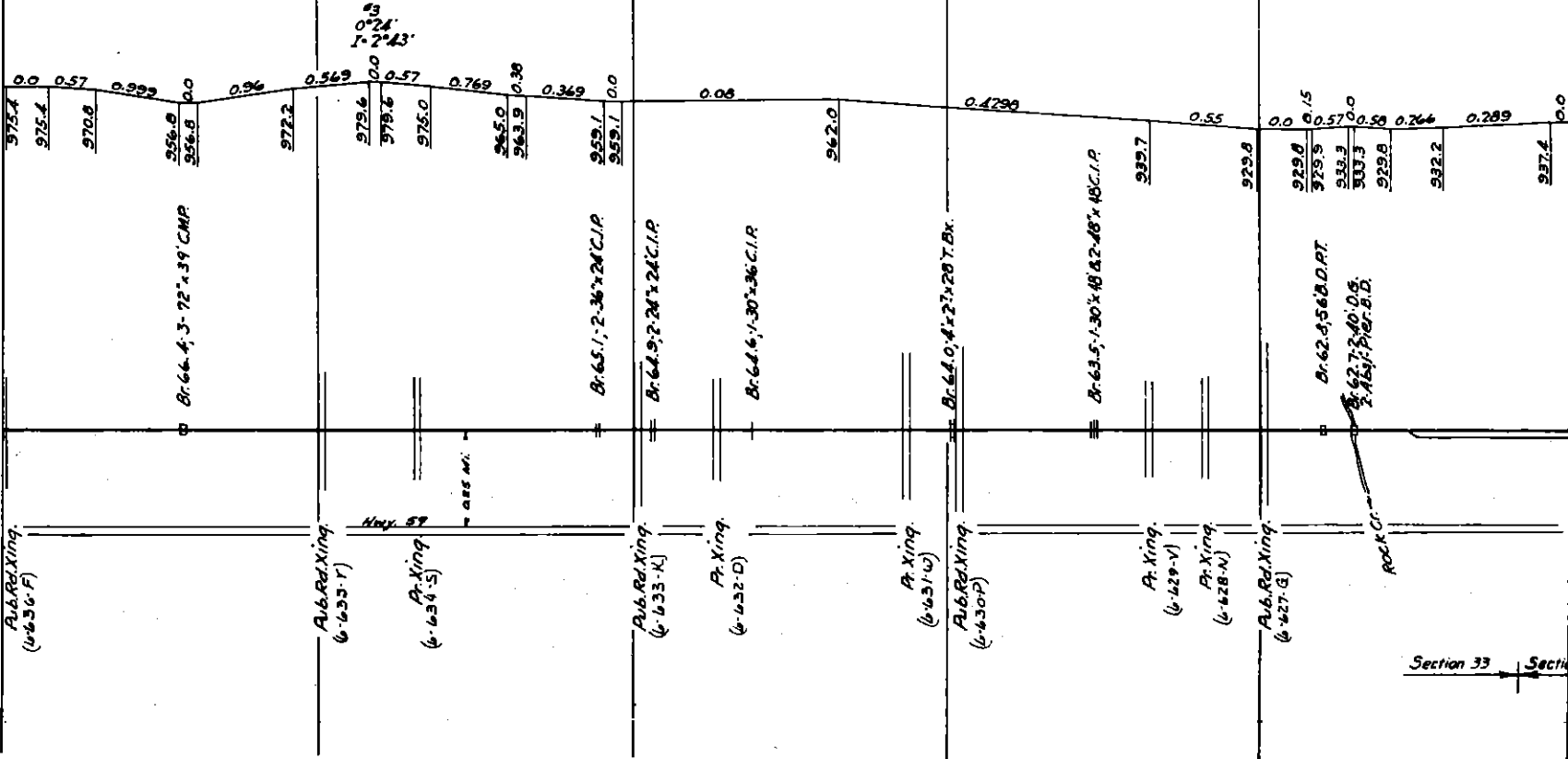
64

63

62

To Chanute

To Ottawa



← To Chanute

Speed - 20-20  
to M.P. 59

0.06 0.249 0.0 0.50 0.30 0.0 0.739 0.57 0.50 0.56 0.803 0.31 0.559 0.977 0.0 0.80 0.06 0.249 0.0 0.70 0.179 0.508 0.085 0.13 0.70 1.00 0.76 0.0

